



City of Sunnyvale

Notice and Agenda

Planning Commission

Monday, February 26, 2024

7:00 PM

Online and Bay Conference Room
(Room 145), City Hall,
456 W. Olive Ave.,
Sunnyvale, CA 94086

No Study Session | Public Hearing - 7:00 PM

Meeting Online Link: <https://sunnyvale-ca-gov.zoom.us/j/91827390357>

Public Participation

- *In-person participation: You may provide public comment by filling out a speaker card (optional) and giving it to the Recording Officer.*

As a courtesy, and technology permitting, members of the public may also attend online. However, the City cannot guarantee that the public's access to online technology will be uninterrupted, and technical difficulties may occur from time to time. Unless required by the Ralph M. Brown Act, the meeting will continue despite technical difficulties for participants using the online option.

The Chair may determine it would be impractical to include remote public comment during Oral Communications.

- *Online participation: You may provide audio public comment by connecting to the meeting online or by telephone. Use the Raise Hand feature to request to speak (*9 on a telephone):*

Meeting Online Link: <https://sunnyvale-ca-gov.zoom.us/j/91827390357>

*Meeting call-in telephone number: 833-548-0276 | Meeting ID: 918 2739 0357
(*9 to request to speak | *6 to unmute/mute)*

- *Watch the Planning Commission meeting at <http://youtube.com/SunnyvaleMeetings> or on television over Comcast Channel 15, AT&T Channel 99*
- *Submit written comments to the Planning Commission no later than 4 hours prior to the meeting start to planningcommission@sunnyvale.ca.gov or by mail to: Sunnyvale Planning Division, 456 W. Olive Avenue, Sunnyvale, CA 94086-3707*

- Review recordings of this meeting and past meetings at <https://sunnyvaleca.legistar.com/calendar.aspx> or <http://youtube.com/SunnyvaleMeetings>

Accessibility/Americans with Disability Act (ADA) Notice

Pursuant to the Americans with Disabilities Act (ADA), if you need special assistance to provide public comment, or for other special assistance; please contact the City at least 48 hours prior to enable the City to make reasonable arrangements to ensure accessibility to this meeting. The Planning Division may be reached at 408-730-7440 or at planning@sunnyvale.ca.gov (28 CFR 35.160 (b) (1)).

NO STUDY SESSION

7 P.M. PLANNING COMMISSION MEETING

CALL TO ORDER

Call to Order via teleconference and in the Bay Conference Room.

SALUTE TO THE FLAG

ROLL CALL

ORAL COMMUNICATIONS

The Chair may determine it would be impractical to include remote public comment during Oral Communications for the purpose of timeliness of the meeting or conducting an orderly meeting. Such a determination shall be made prior to opening public comment on Oral Communications.

This category provides an opportunity for members of the public to address the Planning Commission on items not listed on the agenda and is limited to 15 minutes (may be extended or continued after the public hearings/general business section of the agenda at the discretion of the Chair) with a maximum of up to three minutes per speaker. Please note the Brown Act (Open Meeting Law) does not allow the Planning Commission to take action on an item not listed on the agenda. If you wish to address the Planning Commission, please refer to the notice at the beginning of this agenda. Individuals are limited to one appearance during this section.

CONSENT CALENDAR

All matters listed on the consent calendar are considered to be routine and will be

acted upon by one motion. There will be no separate discussion of these items. If a member of the public would like a consent calendar item pulled and discussed separately, please refer to the notice at the beginning of this agenda.

- 1.A [24-0388](#) Approve Planning Commission Meeting Minutes of November 30, 2023

Recommendation: Approve Planning Commission Meeting Minutes of November 30, 2023 as submitted.

- 1.B [24-0389](#) Approve Planning Commission Meeting Minutes of February 12, 2024

Recommendation: Approve Planning Commission Meeting Minutes of February 12, 2024 as submitted.

- 1.C [24-0265](#)

Proposed Project:

DESIGN REVIEW: Demolish an existing home and construct a new two-story single-family home resulting in 4,838 square feet (3,864 square feet living area, 512 square feet garage, and 462 rear covered patio) and 40% floor area ratio (FAR), and a six-foot fence in the front yard.

Location: 541 South Bayview Avenue (APN: 209-31-036)

File #: PLNG-2023-0366

Zoning: R-0 (Low Density Residential)

Applicant / Owner: Michelle Miner / Christopher & Margaret Allan

Environmental Review: A Class 1 Categorical Exemption relieves this project from California Environmental Quality Act (CEQA) provisions.

Project Planner: Robby Miller, (408) 730-7429,
rmiller@sunnyvale.ca.gov

Recommendation: Alternative 1: Approve the Design Review based on the Findings in Attachment 3 and subject to the Recommended Conditions of Approval in Attachment 4.

PUBLIC HEARINGS/GENERAL BUSINESS

If you wish to speak to a public hearing/general business item, please refer to notice at the beginning of this agenda. Each speaker is limited to a maximum of three minutes. For land-use items, applicants are limited to a maximum of 10 minutes for opening comments and 5 minutes for closing comments.

2. [24-0385](#) **Proposed Project:** Consider actions related to implementation of the 2023-2031 Housing Element to amend the By-Right Approval process for certain parcels within the City:
- A. Introduce an Ordinance** to Amend By-Right Approvals (Chapter 19.73 of the Sunnyvale Municipal Code) to clarify ministerial review of projects as it relates to proposed subdivisions.
- File #:** PLNG-2024-0105

Location: Citywide

Applicant: City of Sunnyvale

Environmental Review: Non-discretionary project (Public Resources Code Section 21080(b)(1), CEQA Guidelines Sections 15268, 15357)

Project Planner: Julia Klein, (408) 730-7463,
jklein@sunnyvale.ca.gov

3. [24-0141](#)

Forward a Recommendation to the City Council to Evaluate the Minimum Automobile Off-Street Parking Requirements for Residential Uses (Study Issue CDD 19-07)

File #: 2022-7404

Environmental Review: The action is exempt from the California Environmental Quality Act (CEQA) pursuant to CEQA Guidelines Section 15378(a).

Project Planner: George Schroeder, (408) 730-7443,
gschroeder@sunnyvale.ca.gov

- Recommendation:** Recommend Alternative 1 to the City Council to direct staff to conduct community outreach and prepare amendments to the Sunnyvale Municipal Code (SMC) and applicable Specific Plans parking regulations, to be considered at future hearings. The amendments to be studied include:
- a. Clarifying parking standards to be objective and without discretionary review.
 - b. Single/Two-Family Residential:
 - i. Lowering minimum off-street parking spaces per property, with flexibility in type and arrangement.
 - ii. Exploring maximum number of parking spaces and allowing tandem parking
 - c. Multi-Family Residential:
 - i. Introducing unbundled parking.
 - ii. Adjusting/simplifying the unassigned space requirement (currently dependent on the type of covered assigned space provided).
 - 1. For example, require the minimum parking standards to be a flat rate of 1 space per zero to one-bedroom unit; 1.5 spaces per two to three-bedroom unit; and 2 spaces for units with four or more bedrooms.
 - iii. Examining maximum amount of parking required, except for areas within a half-mile of major transit stops.
 - iv. Allowing flexibility in type (covered, uncovered) and arrangement, including a tandem allowance for up to 100% of units.
 - v. Evaluating continued use of compact parking spaces.
 - vi. Updating the Limited Street Parking Provisions to specify objective parking requirements in certain conditions.

STANDING ITEM: CONSIDERATION OF POTENTIAL STUDY ISSUES

NON-AGENDA ITEMS AND COMMENTS

-Commissioner Comments

-Staff Comments

ADJOURNMENT

Notice to the Public:

Any agenda related writings or documents on this agenda distributed to members of the Planning Commission are available by contacting the Planning Division at 408-730-7440 or planning@sunnyvale.ca.gov. Agendas and associated reports are also available 72 hours before the meeting on the City's website at sunnyvale.ca.gov and during normal business hours at the NOVA Workforce Services reception desk located on the first floor of City Hall at 456 W. Olive Avenue.

*Planning a presentation for a Planning Commission meeting?
To help you prepare and deliver your public comments, please review the "Making Public Comments During City Council or Planning Commission Meetings" document available on the City website.*

PLEASE TAKE NOTICE that if you file a lawsuit challenging any final decision on any public hearing item listed in this agenda, the issues in the lawsuit may be limited to the issues which were raised at the public hearing or presented in writing to the City at or before the public hearing.

PLEASE TAKE FURTHER NOTICE that Code of Civil Procedure section 1094.6 imposes a 90-day deadline for the filing of any lawsuit challenging final action on an agenda item which is subject to Code of Civil Procedure section 1094.5.



City of Sunnyvale

Agenda Item 1.A

24-0388

Agenda Date: 2/26/2024

SUBJECT

Approve Planning Commission Meeting Minutes of November 30, 2023

RECOMMENDATION

Approve Planning Commission Meeting Minutes of November 30, 2023 as submitted.



City of Sunnyvale

Meeting Minutes - Draft Planning Commission

Thursday, November 30, 2023

6:30 PM

Online and Bay Conference Room
(Room 145), City Hall,
456 W. Olive Ave.,
Sunnyvale, CA 94086

Special Meeting: Joint Study Session of the Bicycle and Pedestrian Advisory Commission, Planning Commission and Sustainability Commission

CALL TO ORDER

Chair Kunz called the joint meeting to order at 6:36 p.m. in the Bay Conference Room (Room 145) at City Hall.

SALUTE TO THE FLAG

Chair Kunz led the salute to the flag.

ROLL CALL

Present: 17 - Vice Chair Nathan Iglesias
Commissioner Galen Kim Davis
Commissioner Daniel Howard
Commissioner Michael Serrone
Commissioner Neela Shukla
Douglas Kunz
Tonya Veitch
Kathryn Besser
Bobbykin Makwana
Jeffery Nabhan
Kristina Pistone
Kristel Wickham
Bryce Beagle
Dan Hafeman
Sharlene Liu
Leia Mehlman
Timothy Oey

Absent: 4 - Chair Martin Pyne
Commissioner John Howe
Alex Bonne
Arwen Davé

Planning Commission Vice Chair Iglesias arrived at 7:15 p.m.

Planning Commission Chair Pyne and Planning Commissioner Howe were absent.

BPAC Commissioner Dave and BPAC Commissioner Bonne were absent.

Council Liaison Melton and Council Liaison Din were absent.

STUDY SESSION

23-1053

Review and discuss Draft Game Plan 2028. The complete draft can be downloaded at bit.ly/GamePlan28

Madeline Khair, Environmental Programs Manager, provided an overview of the Climate Action Playbook (CAP) carbon neutrality amendment, updated metrics and Draft Game Plan 2028. Ms. Khair concluded her presentation requesting that Commissioners first provide input on the new CAP carbon neutrality target, then metric updates and finally, discuss Draft Game Plan 2028 Strategies, Plays and Moves. Ms. Khair; Ramana Chinnakotla, Director of Environmental Services; and Melody Tovar, Regulatory Programs Division Manager provided answers to the Commissioner's questions.

Planning Commissioner Howard asked why emissions went up in 2019 and what feedback the City Council had to the proposed decrease in the vehicle miles traveled (VMT) CAP metric.

BPAC Chair Oey thanked staff for their progress on the CAP to-date. Chair Oey asked for a breakdown of the greenhouse gas (GHG) emissions for on-road transportation. He then asked staff to provide more detail on commercial gas and what uses make up the emissions in that sector. Chair Oey inquired if off-road equipment is solely lawn equipment or also construction equipment.

Sustainability Commissioner Pistone asked staff to clarify what CO2 equivalent means, which greenhouse gases are tracked and what defines carbon neutrality.

Sustainability Commissioner Makwana asked what the process is for adding new Moves or updating current Moves in Game Plan 2028 during its five-year time frame. Staff clarified there is no process for updating the Game Plan once its adopted. Commissioner Makwana asked for the potential to amend Game Plan

2028 as new innovations and practices are emerging.

Planning Commissioner Davis thanked staff and supports the City adopting more aggressive GHG targets than the State. Commissioner Davis noted not being supportive of current methods used to calculate VMT. Commissioner Davis stated concern that the City's 2045 carbon neutrality target would require a significant lifestyle change in the community. Commissioner Davis advised that the City start talking about the upcoming lifestyle and land use impacts to bring awareness to the community.

BPAC Commissioner Mehlman thanked staff for the presentation. Commissioner Mehlman suggested the City focus on micro-mobility infrastructure in Strategy 3: Decarbonizing Transportation & Sustainable Land Use and not solely on EV adoption. Commissioner Mehlman recommended adding a Move on micro-mobility. Commissioner Mehlman voiced concern on the focus of Strategy 4: Managing Resources Sustainably, being on reducing emissions from organics and not on reducing plastic or from other municipal solid waste streams. Commissioner Mehlman voiced concern that there is not enough in Draft Game Plan 2028 about transit infrastructure and recommended prioritizing transit infrastructure to make public transit options more accessible. Commissioner Mehlman recommended staff work with mobile home parks to help them electrify their homes.

BPAC Commissioner Hafeman asked if delivery vehicles are accounted for and affect commercial VMT data. Commissioner Hafeman asked if the CAP metric to increase on-road zero emission vehicle adoption to 42% by 2030 is for new vehicles only or all vehicles. Commissioner Hafeman voiced concern that car manufacturers may not be able to transition fast enough to all-electric fleet or acquire enough lithium to produce electric vehicles (EV) to meet our 2030 targets. Commissioner Hafeman recommended that staff focus more on reducing reliance on cars instead of replacing them with EV's.

Sustainability Commissioner Wickham commended staff for getting three Commissions together to discuss Draft Game Plan 2028. Commissioner Wickham stated support of the new carbon neutrality target in Draft Game Plan 2028 to align with the State's 2045 target. Commissioner Wickham recommended several changes to the carbon neutrality language that could make the target stronger and more achievable. Sustainability Chair Kunz and Vice Chair Veitch supported these suggestions.

Planning Vice Chair Iglesias recommended staff include a metric to understand the costs on the City, residents or a combination to implement the targets stated in the

Draft Game Plan 2028.

BPAC Chair Oey requested staff invest in real data to track VMT's in Sunnyvale and suggested using methods such as stop light data to track transportation instead of using estimates in the current methodology.

Sustainability Commissioner Wickham asked if the current CAP qualified for California Environmental Quality Act (CEQA) streamlining purposes.

Sustainability Chair Kunz recommended staff continue to pursue qualifying the CAP for CEQA streamlining purposes.

Sustainability Commissioner Pistone emphasized having better and stronger goals in Game Plan 2028 to help Sunnyvale achieve its targets. Commissioner Pistone recommended expanding the scope of Game Plan 2028 goals to include lifecycle impacts to bolster the City's efforts to reduce emissions.

Sustainability Chair Kunz opened discussion on the Strategy Plays and Ms. Khair responded to questions.

BPAC Chair Oey, BPAC Vice Chair Beagle, Planning Vice Chair Iglesias, Sustainability Commissioner Besser, and BPAC Commissioner Liu voiced concern about reducing the VMT per person target in Play 3.1.

BPAC Chair Oey asked whether tracking GHG emissions in transportation includes emissions from asphalt in parking lots and roadways.

Planning Commissioner Davis agreed with the landfill organics targets in Strategy 4: Managing Resources Sustainably.

Sustainability Commissioner Besser suggested increasing the Play 3.1 target slightly could encourage more people to strive for the higher goal and help the City achieve its true target of 10%. Commissioner Besser echoed statements previously made that EV adoption should not be the sole solution due to the cost barrier to purchase an EV as well as the worldwide impacts of lithium production.

BPAC Commissioner Liu suggested that staff account for emissions from EV production and usage, and asphalt creation and repaving when measuring GHG emissions in transportation. Commissioner Liu was concerned that the City is asking residents to switch from gas to electric vehicles, but not advocating for expanded public transit, bicycle routes and walkable streets. Commissioner Liu

recommended staff work with the Transportation Division to require that all high-density housing projects include safe routes for residents that will live in the complexes to get around without a vehicle. Commissioner Liu asked the City to consider making this recommendation a policy.

Sustainability Commissioner Wickham voiced support for the new metrics in Strategy 2, 3 and 4. Commissioner Wickham emphasized liking that Strategy 2's metrics focus on natural gas use. Commissioner Wickham praised the percent reduction in landfill organics metric in Strategy 4 and asked if there could be further improvements to reduce landfill organics between 2030 and 2045.

Sustainability Commissioner Makwana asked to add the City's current status in meeting the metric targets in Game Plan 2028.

Sustainability Commissioner Pistone noted support for increasing the percent of off-road zero emission equipment in Play 3.4. Commissioner Pistone suggested the metric come with a behavioral change framework to help move people away from unsustainable landscaping practices, such as gas-powered leaf blowers and lawn mowers.

BPAC Commissioner Hafeman asked if emissions from tires of cars or EVs are counted. Commissioner Hafeman asked if there is any discussion on measuring the percentage of people taking transit in Sunnyvale versus using single occupancy vehicles. Commissioner Hafeman suggested seeing the data on percentage of miles traveled on public transit in comparison to miles traveled in cars could be a helpful metric.

Sustainability Commissioner Makwana suggested an increase in the local battery storage metric target for Play 1.3.

Sustainability Commissioner Nabhan was concerned to see the Strategy 4: Managing Resources Sustainably metric to reduce landfill garbage to one pound per person when it is currently at 3.6 pounds per person. Commissioner Nabhan asked if the data is trending that way and whether it would be feasible to meet.

BPAC Commissioner Mehlman inquired how waste per person or per household is measured. Commissioner Mehlman asked if that calculation includes residential and commercial sources. Commissioner Mehlman questioned how construction waste is counted. Commissioner Mehlman asked the status of a commercial organics recycling program. Commissioner Mehlman inquired about compliance measures the City will be taking to ensure businesses are separating their

organics. Commissioner Mehlman asked if hotel and motel residents' waste are captured in the calculation, if these residents are considered part of Sunnyvale's population and if they are required to separate organics during their stay.

BPAC Chair Oey expressed worry that Sunnyvale is unable to calculate the true total of GHGs and asked what percentage of total GHGs are outside of the City's scope. Chair Oey recommended including clarification in the CAP about GHG inventories being focused on Scope 1 emissions. Chair Oey emphasized that focusing on reducing VMTs in general could help the City reduce emissions in Scopes 1, 2 and 3 due to lower car sales, repaving and tire manufacturing.

Sustainability Chair Kunz voiced support on the new metrics that are being added and shared the same concerns about decreasing the VMT target.

Sustainability Chair Kunz opened discussion on the Strategy Moves and Ms. Khair guided the Commissioners on the structure staff would like to take feedback.

Sustainability Commissioner Pistone asked for clarity on where the source of organics is coming from in Move 1.C.

BPAC Commissioner Mehlman recommended for Strategy 1 or Strategy 2 that there be an added Move to implement solar panels in parking lots to reduce heat island effects and promote clean electricity generation and battery storage.

Sustainability Commissioner Pistone asked for more clarity on why people would want to opt out of Silicon Valley Clean Energy's (SVCE) clean energy programs.

Sustainability Commissioner Wickham asked the status of the City's GreenPrime enrollment. Commissioner Wickham asked staff to get the City re-enrolled in GreenPrime.

Sustainability Chair Kunz opened the discussion on Strategy 2: Decarbonizing Buildings.

BPAC Vice Chair Beagle asked for an explanation on the difference between the terms decarbonization and zero emission.

BPAC Commissioner Hafeman expressed excitement for Move 2.H.

Planning Commissioner Davis supported the list of Moves in Strategy 2 and recommended staff work to switch residents with wood burning stoves to

all-electric.

BPAC Chair Oey was pleased with the Moves listed in Strategy 2.

Sustainability Commissioner Pistone and Vice Chair Veitch stated concern for renters as they do not have authority over electrifying their appliances. Commissioner Pistone called out that Moves 2.D, 2.F, 2.M and 2.I all encourage accelerated electrification adoption, but it is more difficult for renters to take advantage of these incentives without property manager support.

Sustainability Vice Chair Veitch recommended the municipal GHG inventory for Move 2.C be conducted every two or three years. Vice Chair Veitch suggested language changes to Move 2.M.

BPAC Commissioner Mehlman inquired about the timeline to implement an ordinance for 2.G and requested there be no exemptions for manufactured home parks. Commissioner Mehlman additionally recommended staff design an assistance program for low-income households to bring them up to code.

Planning Vice Chair Iglesias recommended implementation costs be identified for each Move throughout Game Plan 2028 to prioritize the Moves that are cost-effective on residents and the City. BPAC Chair Oey supported this suggestion.

Sustainability Commissioner Wickham suggested the City prioritize designing an awareness campaign to assist residents and businesses in electrifying their homes and buildings, specifically to help support the Bay Area Air Management District's (BAAQMD) ruling to ban natural gas water heater sales by 2027.

Sustainability Commissioner Pistone asked why the local environmental quality icon in the community benefits section of Strategy 2 is identified for Move 2.J but not for Moves 2.D, 2.E and 2.G. Commissioner Pistone recommended staff monitor new enhancements to measuring methane leaks from existing buildings.

Sustainability Chair Kunz stated excitement for all new Moves in Strategy 2 and was particularly excited about Move 2.H. Chair Kunz stated uncertainty about the feasibility of reaching the 44% percent reduction goal in existing residential natural gas use by 2030. Chair Kunz asked how the set of tools in Moves 2.F, 2.L and 2.N fit together and suggested imbedding a diagram to show the course of action to achieve the 2030 target in Strategy 2.

Sustainability Chair Kunz opened the discussion on Strategy 3: Decarbonizing Transportation & Sustainable Land Use.

BPAC Commissioner Mehlman asked what progress has been made on Move 3.A. Commissioner Mehlman recommended connecting and expanding the shuttle program identified in Move 3.F to other areas of the city to connect people to the VTA light rail and Caltrain.

BPAC Commissioner Liu requested that the supporting text in Moves 3.D and 3.I be more specific and quantify the high priority projects in the Active Transportation Plan (ATP).

Planning Commissioner Serrone articulated the barriers renters have to purchase an EV when their building does not have EV charging infrastructure and asked what triggers more EV chargers in multi-family buildings. Commissioner Serrone asked if the City can implement an ordinance to require EV chargers in new multi-family building projects. Commissioner Serrone mentioned that the State plans to ban the sale of natural gas-powered off-road equipment and asked staff if this will also ban the operation of them.

BPAC Commissioner Hafeman asked how the City plans to reduce street parking in Move 3.A to put more ATP infrastructure in. Commissioner Hafeman recommended modifying one of the Moves to include that existing street parking will be removed after any new construction project is complete. Commissioner Hafeman also requested adding a new goal to Strategy 3 that seeks more funding for the City's ATP.

BPAC Chair Oey stated support for Move 3.I. Chair Oey requested the City install safer, more convenient bicycle lockers in existing parking lots. Chair Oey also suggested there be more emphasis on bicycle education and outreach in the Safe Routes to Schools program and in driver training programs. Chair Oey urged for more focus on funding in Game Plan 2028 and asked to better define "robust first and last mile" in Move 3.C.

Planning Vice Chair Iglesias expressed safety concerns to bike on the roads in Sunnyvale and suggested a more reliable and accessible public transit network around the city. Vice Chair Iglesias suggested imbedding a measurable goal in Strategy 3 to call out how long it should take people to get to and from a destination in Sunnyvale using alternatives modes of transportation.

BPAC Vice Chair Beagle strongly agreed with Vice Chair Iglesias, and emphasized

that mode shift should be the focus of Strategy 3. Vice Chair Beagle suggested rephrasing Play 3.3 to increase the percentage of electric vehicles clarifying that the overall number of vehicles should decrease. Vice Chair Beagle asked for clarity on Move 3.J and how the largest employers in the city is defined. He recommended the City encourage companies to support transit through incentives. Vice Chair Beagle recommended Move 3.E include car share as an option.

BPAC Commissioner Mehlman asked why the threshold for Move 3.E is 1,000 employees and not 500.

Planning Commissioner Howard recommended a shift in focus to promoting smaller EV equipment such as bicycles and scooters and utilizing retail spaces in Sunnyvale to promote the sale and use of micro mobility devices.

Planning Commissioner Davis supported Move 3.I and recommends creating more space on roadways for safe walking and biking routes.

Sustainability Commissioner Wickham questioned if the City's Specific Plans and Area Plans are enough to achieve the land use goals in Strategy 3 or if the City needed more zoning policies or higher density policies to implement.

Sustainability Chair Kunz opened the discussion on Strategy 4: Managing Resources Sustainably

Sustainability Commissioner Pistone asked if there is a plan to expand infrastructure to accept paper products in organics collection.

BPAC Commissioner Hafeman supported Move 4.I but suggests the Move also include residential construction projects. Commissioner Hafeman questioned why composting was left out of Strategy 4.

BPAC Commissioner Mehlman requested that 4.K be seen as a high priority Move. Commissioner Mehlman suggested merging Moves 4.K and 4.J.

Sustainability Commissioner Wickham commented that Move 4.L is in need of funding. Commissioner Wickham went on to state the entire CAP needs funding and encouraged staff to think of creative ways to find the funding necessary to implement all of the Moves in Game Plan 2028. Chair Kunz supported these comments.

Planning Vice Chair Iglesias asked what a campaign looks like for Move 4.A.

Sustainability Commissioner Pistone questioned how to encourage businesses like grocery stores to reduce plastic packaging and provide more sustainable options for consumers.

Sustainability Chair Veitch suggested including the potential to explore climate bonds or climate taxes to help fund the CAP. Vice Chair Veitch also suggested establishing more private-public partnerships for funding.

Sustainability Chair Kunz asked what Sunnyvale's Scope 3 emissions are and suggested establishing a consumption-based inventory to help the City identify target areas to drive policy changes.

Sustainability Chair Kunz opened the discussion on Strategy 5: Empowering Our Community and Strategy 6: Adapting to a Changing Climate.

BPAC Chair Oey stated support for Strategy 5 overall and encouraged more City recognition of residential demonstration projects. Chair Oey commented his support of the Sustainability Speaker Series.

BPAC Commissioner Mehlman inquired about the work currently happening on Strategy 5 and suggested expanding the current environmental education programs to include more sustainability topics.

BPAC Commissioner Liu suggested adding a Move about incorporating Safe Routes to School in the elementary school curriculum.

Sustainability Commissioner Pistone commented that Governor Newsom signed Assembly Bill 285 to mandate climate change education in California in K-12 grade classrooms. Commissioner Pistone suggested rewording Move 6.C to make it clearer to understand what the intent is.

Sustainability Vice Chair Veitch voiced appreciation for Strategy 5. Vice Chair Veitch inquired about a way to integrate climate change information into the Community Emergency Response Team (CERT) Program. Vice Chair Veitch voiced excitement for Move 6.F and emphasized the importance of partnering with Non-Profit Organizations and Community Based Organizations to operate as resilience hubs.

BPAC Commissioner Mehlman inquired about strategies to achieve the Game Plan 2028 Moves, where the City's accountability is and if the City's budget can be

altered to support the completion of the Moves in Game Plan 2028.

Sustainability Chair Kunz opened public comment.

Sustainability Chair Kunz closed public comment.

Sustainability Chair Kunz and Ms. Khair provided closing comments and thanked the Commissioners for attending the Joint Study Session on Draft Game Plan 2028.

ADJOURNMENT

The meeting was adjourned at 9:37 p.m.



City of Sunnyvale

Agenda Item 1.B

24-0389

Agenda Date: 2/26/2024

SUBJECT

Approve Planning Commission Meeting Minutes of February 12, 2024

RECOMMENDATION

Approve Planning Commission Meeting Minutes of February 12, 2024 as submitted.



City of Sunnyvale

Meeting Minutes - Draft Planning Commission

Monday, February 12, 2024

6:00 PM

Online and Bay Conference Room
(Room 145), City Hall,
456 W. Olive Ave.,
Sunnyvale, CA 94086

Special Meeting: Study Session - 6:00 PM | Public Hearing - 7:00 PM

6 P.M. STUDY SESSION

Call to Order

Commissioner Howard called the meeting to order at 6:00 PM.

Roll Call

Present: 4 - Commissioner Galen Kim Davis
Commissioner Daniel Howard
Commissioner Michael Serrone
Commissioner Neela Shukla

Absent: 3 - Chair Martin Pyne
Vice Chair Nathan Iglesias
Commissioner John Howe

Chair Pyne arrived at 6:10 PM.

The absences of Commissioner Howe and Vice Chair Iglesias are excused.

Study Session

A. [24-0271](#)

Proposed Project:

SPECIAL DEVELOPMENT PERMIT: Redevelop a 0.62-acre commercial property. Demolish the existing gas station canopy and auto service building and construct a new 4,400 square foot gas station canopy and a 3,614 square foot one-story commercial building comprising a 2,396 square foot convenience store, and a 1,218 square foot takeout restaurant and resulting in 34% floor area ratio (FAR).

Location: 898 East Fremont Avenue (APN: 309-10-015)

File #: 2016-7978

Zoning: C-1/PD (Neighborhood Business/ Planned Development)

combining district)

Applicant / Owner: Petroleum Investments, LLC (applicant) / MI Architects, Inc. (owner)

Environmental Review: No additional review required pursuant to California Environmental Quality Act (CEQA) Guidelines 15162 and 15168(c)(2) and (4) - environmental impacts of the project are addressed in the 2016 Land Use and Transportation Element (LUTE) Environmental Impact Report (EIR - State Clearinghouse No. 2012032003).

Project Planner: Aastha Vashist, (408) 730-7458, avashist@sunnyvale.ca.gov

Adjourn Study Session

7 P.M. PLANNING COMMISSION MEETING

CALL TO ORDER

Chair Pyne called the meeting to order at 7:00 PM.

SALUTE TO THE FLAG

Chair Pyne led the salute to the flag.

ROLL CALL

Present: 5 - Chair Martin Pyne
Commissioner Galen Kim Davis
Commissioner Daniel Howard
Commissioner Michael Serrone
Commissioner Neela Shukla
Absent: 2 - Vice Chair Nathan Iglesias
Commissioner John Howe

The absences of Commissioner Howe and Vice Chair Iglesias are excused.

ORAL COMMUNICATIONS

Stephen Meier provided comments on the lawsuit between San Francisco Baykeeper and the City of Sunnyvale.

CONSENT CALENDAR

There were no public speakers for this agenda item.

MOTION: Commissioner Howard moved and Commissioner Davis seconded the motion to approve the Consent Calendar.

The motion carried by the following vote:

Yes: 5 - Chair Pyne
Commissioner Davis
Commissioner Howard
Commissioner Serrone
Commissioner Shukla

No: 0

Absent: 2 - Vice Chair Iglesias
Commissioner Howe

This decision, as it applies to Agenda Item 1.B, is final unless appealed or called up for review by the City Council by 5:00 PM on Tuesday, February 27, 2024.

1.A [24-0279](#) Approve Planning Commission Meeting Minutes of January 22, 2024

Approve Planning Commission Meeting Minutes of January 22, 2024 as submitted.

1.B [24-0207](#) **Proposed Project:**

DESIGN REVIEW: Construct a first-story addition of 222 square feet to an existing one-story duplex, resulting in 4,104 square feet (2,440 square feet living area, 595 square feet garage, and 847 covered patio) and 42% Floor Area Ratio (FAR).

Location: 560-562 Crawford Drive (APN:201-330-48)

File #: PLNG-2023-0699

Zoning: R-0 (Low Density Residential)

Applicant / Owner: Zijun (Jackey) Yan

Environmental Review: A Class 1 Categorical Exemption relieves this project from California Environmental Quality Act (CEQA) provisions.

Project Planner: Shila Bagley, (408) 730-7418,
sbagley@sunnyvale.ca.gov

Alternative 1: Approve the Design Review with the Conditions of Approval in Attachment 4.

PUBLIC HEARINGS/GENERAL BUSINESS

None.

STANDING ITEM: CONSIDERATION OF POTENTIAL STUDY ISSUES

None.

NON-AGENDA ITEMS AND COMMENTS

-Commissioner Comments

None.

-Staff Comments

None.

ADJOURNMENT

Chair Pyne adjourned the meeting at 7:10 PM.



City of Sunnyvale

Agenda Item 1.C

24-0265

Agenda Date: 2/26/2024

REPORT TO PLANNING COMMISSION

SUBJECT

Proposed Project:

DESIGN REVIEW: Demolish an existing home and construct a new two-story single-family home resulting in 4,838 square feet (3,864 square feet living area, 512 square feet garage, and 462 rear covered patio) and 40% floor area ratio (FAR), and a six-foot fence in the front yard.

Location: 541 South Bayview Avenue (APN: 209-31-036)

File #: PLNG-2023-0366

Zoning: R-0 (Low Density Residential)

Applicant / Owner: Michelle Miner / Christopher & Margaret Allan

Environmental Review: A Class 1 Categorical Exemption relieves this project from California Environmental Quality Act (CEQA) provisions.

Project Planner: Robby Miller, (408) 730-7429, rmiller@sunnyvale.ca.gov

REPORT IN BRIEF

General Plan: Low Density Residential, Office

Existing Site Conditions: One-story single-family home

Surrounding Land Uses

North: One-story single-family home

South: Three-story office building

East: One-story single-family home

West: One-story single-family home

Issues: Exceeding 3,600 square feet floor area in Zoning District.

Staff Recommendation: Approve the Design Review and exemption from CEQA based on the Findings in Attachment 3 and subject to the Recommended Conditions of Approval in Attachment 4.

BACKGROUND

Description of Proposed Project: The proposed project is located on a 12,120 square foot residential lot with an existing one-story single-family home. The applicant proposes to demolish the existing home and construct a two-story home resulting in 4,838 square feet and 40% Floor Area Ratio (FAR).

A Design Review application is required for a new home to evaluate compliance with development standards and with the Single-Family Home Design Techniques. Planning Commission review is required for homes with a gross floor area exceeding 3,600 square feet or floor area ratios greater than 45%. The project results in a floor area of 4,838 square feet, which requires Planning Commission review and approval.

See Attachment 1 for a map of the vicinity and mailing area for notices and Attachment 2 for the Data Table of the proposed project.

Previous Actions on the Site: There are no previous Planning approvals for the site.

Applicable Design Guidelines: The City's Design Guidelines provide recommendations for site layout, architecture, and design. These guidelines are referenced in the discussion and analysis below.

ENVIRONMENTAL REVIEW

A Class 1 and Class 3 Exemption relieves this proposed project from the California Environmental Quality Act (CEQA) provisions. The demolition of one single family house is exempt under Class 1. The new construction of one single family home is exempt under Class 3.

DISCUSSION

Present Site Conditions: The project site contains an existing one-story single-family ranch style home with an attached garage. The site is located in a residential neighborhood north of Old San Francisco Road which is generally bounded by Bishop Avenue to the north, Old San Francisco Road to the south, Carroll Street to the west, and South Bayview Avenue to the east. The site is surrounded to the north, west, and east by predominately one-story single-family homes with a two-story single-family home within immediate neighborhood. The 2 to 3-story Palo Alto Medical Foundation medical clinic is directly south of the project site. The single-family homes in the immediate neighborhood are generally comprised of ranch style homes with hipped and gable roof forms, attached garages, and recessed front porch.

Architecture and Site Layout: The new two-story single-family home includes common areas, 5 bedrooms, two covered rear patios, and a front courtyard area. The plate height is 9 feet on the first floor and 8 feet on the second floor. The architectural style is Transitional Ranch with white stucco walls, black windows with white trim, hardi plank siding on the roofed gable ends, gray composite roof shingles, and hipped and gabled roofs with a 5:12 roof slope. The first-floor front façade has a two-car garage with wood tone garage door, recessed entry, and a picture window accented by a metal roof. The front entry is accessed through a courtyard area with a 6-foot-tall wall. Hipped roofs are used along the façade to blend in with the neighborhood and reduce bulk. As proposed, the design is compatible with the existing ranch style character found in the neighborhood.

Floor Area and Floor Area Ratio (FAR): A single-family home proposing a gross floor area greater than 3,600 square feet and a FAR greater than 45% requires Planning Commission review and approval. The proposed project has a gross floor area of 4,838 square feet on a 12,120-square foot lot, which results in 40% FAR.

Homes in the neighborhood range in size from 1,164 square feet to 3,615 square feet with an average of 2,174 square feet. The existing FARs in the vicinity range from 19% to 59%, with an average of 32%. In the neighborhood, the proposed project will be the largest individual home in square feet. The detailed information of neighboring homes in the immediate neighborhood can be found in Attachment 6.

Staff finds that the floor area is appropriate for the neighborhood. The project site is one of the larger lots in the neighborhood at 12,120 square feet. Much of the floor area is located on the first floor. The

floor area on the first floor is 3,392 square feet whereas the second floor is 934 square feet. The new second story is set back 80 feet from the front property line where 25 feet is required. The projects two-story design serves as a transition between the office building and the single-family neighborhood.

Courtyard Wall: Fences located in the front yard greater than 4 feet and up to 6 feet in height require a Miscellaneous Plan Permit (MPP) pursuant to Sunnyvale Municipal Code (SMC) Table 19.48.025. The project is proposing a courtyard in the front yard with a stucco wall measuring 6 feet in height from the top-of-curb. The wall has a front setback of 13 feet and 8 inches as measured from the front property line.

There are several front yard fences with heights of 4 feet or less in the neighborhood. A front yard stucco wall around 6 feet tall is found at two houses located to the north of the project site. The neighbor's wall is in a similar location to the one being proposed. The proposed stucco wall has a 13 foot and 8-inch front setback which creates a buffer between the wall and the public right-of-way. The stucco wall compliments the proposed stucco siding of the main house and the existing 10-foot-tall medical clinic wall along the southern property line. Based on these reasons, a 6-foot fence would be acceptable in the neighborhood.

Landscaping and Trees: The existing landscaping will be modified in the front yard to accommodate the project. A new walkway and plant material will replace the existing walkway and grass. No tree removal is proposed.

Development Standards: The proposed project complies with the applicable development standards as set forth in the SMC. The Project Data Table for the proposed project can be found in Attachment 2. No deviations are proposed.

FISCAL IMPACT

No fiscal impacts other than normal fees and taxes are expected.

PUBLIC CONTACT

As of the date of staff report preparation, staff has received no comments from the neighbors.

Notice of Public Hearing

- Published in the *Sunnyvale Sun* newspaper
- Posted on the site
- 34 notices mailed to property owners and residents within 300 feet of the project site

Staff Report

- Posted on the City's website

Agenda

- Posted on the City's official notice bulletin board
- Posted on the City's website

ALTERNATIVES

1. Approve the Design Review based on the Findings in Attachment 3 and subject to the

Recommended Conditions of Approval in Attachment 4.

2. Approve the Design Review with modified conditions
3. Deny the Design Review and provide direction to staff and the applicant where changes should be made.

STAFF RECOMMENDATION

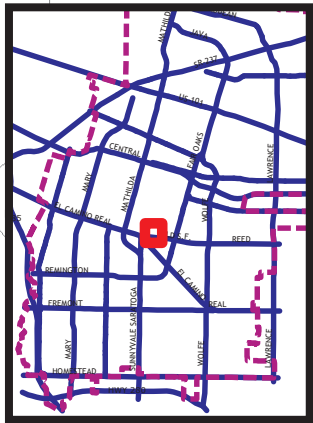
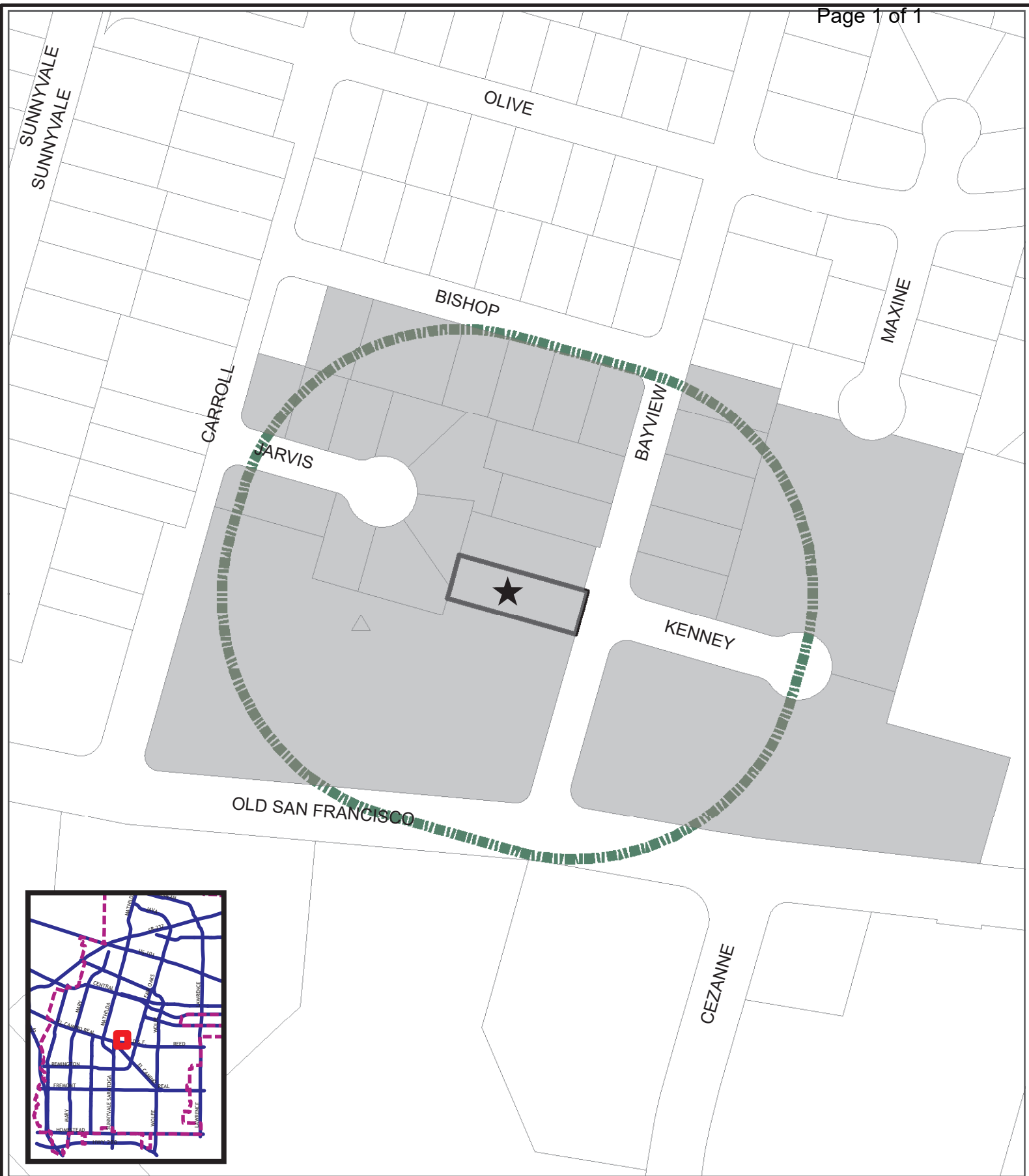
Alternative 1: Approve the Design Review based on the Findings in Attachment 3 and subject to the Recommended Conditions of Approval in Attachment 4.

Prepared by: Robby Miller, Associate Planner

Approved by: Julia Klein, Principal Planner

ATTACHMENTS

1. Vicinity and Noticing Map
2. Project Data
3. Recommended Findings
4. Recommended Conditions of Approval
5. Site and Architectural Plans
6. Neighborhood Analysis
7. Color Material Board



PLNG-2023-0366
 541 S. Bayview Ave (APN: 209-31-036)
 DESIGN REVIEW
 300-ft Area

0 55 110 220 Feet



PROJECT DATA TABLE

	EXISTING	PROPOSED	REQUIRED/ AS PERMITTED
General Plan	Low Density Residential, Office	Same	-
Zoning District	R-0	Same	-
Lot Size	12,120 s.f.	Same	-
Gross Floor Area	2,150 s.f.	4,838 s.f.	45% (4,790 s.f.) ¹
Lot Coverage	20%	33%	40% max.
Floor Area Ratio (FAR)	20%	40%	45% ¹
Building Height	12'	21'-7"	30' max.
No. of Stories	One	Two	Two max.
Setbacks			
Front			
1st Floor	26'-6"	20'-4"	20' min.
2nd Floor	n/a	80'	25' min.
Left Side			
1st Floor	7'-11"	5"	4' min.
2nd Floor	n/a	8'	7' min.
Right Side			
1st Floor	10'-1"	7'	4' min.
2nd Floor	n/a	10'	7' min.
Combined Side			
1st Floor	18'	12'	12' min.
2nd Floor	n/a	18'	18' min.
Rear			
1st Floor	65'	52'-3"	20' min. ²
2nd Floor	n/a	58'-3"	20' min.
Parking			
Total Spaces	4	Same	4 min.
Covered Spaces	2	Same	2 min.

¹ Threshold for Planning Commission Review

² Per SMC 19.48.050, any one-story structure in residential zoning districts may extend 10 feet into the required yard providing the area of the extension does not exceed 25% of the required rear yard.

RECOMMENDED FINDINGS

Design Review

The proposed project is desirable in that the project's design and architecture conforms with the policies and principles of the Single Family Home Design Techniques.

Basic Design Principle	Comments
<i>2.2.1 Reinforce prevailing neighborhood home orientation and entry patterns</i>	The prominent front entry pattern of the immediate neighborhood is recessed front entry. The new front entry for the proposed project will face the street and is recessed under a covered porch. A large picture window and a pronounced two car garage will be added to the front which are features prominent in the neighborhood and common for Ranch homes. Finding met.
<i>2.2.2 Respect the scale, bulk and character of homes in the adjacent neighborhood.</i>	The proposed project will add to the limited existing two-story single-family homes in the neighborhood, and will be the largest home in the neighborhood in terms of floor area when it is built. The floor area is appropriate for the neighborhood. The project site is one of the larger lots in the neighborhood at 12,120 square feet. The majority of the floor area is located on the first floor. The proposed first floor area is 3,392 whereas the second floor is 934 square feet. The new second story is set back 80 feet from the front property line where 25 feet is required. The first-floor façade utilizes hipped roofs and a recessed entry typical of Ranch style homes. The new second story is under 35% of the first floor, further reducing mass and bulk impacts on the neighborhood. Therefore, the proposed mass and bulk of the addition is minimized and will not negatively impact the streetscape or privacy of neighbors. Finding met.
<i>2.2.3 Design homes to respect their immediate neighbors</i>	The proposed project complies with zoning code requirements related to height and setback and is respectful of surrounding neighborhood. There are minimal privacy impacts from windows: the two second-story façade windows facing north toward the adjacent neighbor are high sill windows; the windows facing south on the second-story façade are overlooking the medical clinic parking lot; and the windows facing west on the second-story

Basic Design Principle	Comments
	façade are setback over 58 feet from the rear property line. Finding met.
<i>2.2.4 Minimize the visual impacts of parking.</i>	The proposed garage meets the minimum setback, but it utilizes a hip roof to reduce scale. A prominent front yard garage is common in the neighborhood and for this ranch style architecture. A courtyard entry wall extends 6 feet 8 inches in front of the garage which further deemphasizes the scale and prominence of the garage. Finding met.
<i>2.2.5 Respect the predominant materials and character of front yard landscaping.</i>	Landscaping for the project consists of plant material, rock, and tanbark in the front yard, and a new street tree in the parkstrip. There is no discernible neighborhood landscape character for front yard. Finding met.
<i>2.2.6 Use high quality materials and craftsmanship</i>	The applicant proposes to utilize high-quality materials for the proposed project including stucco siding, hardi plank cladding and fascia for the roof gable ends, window and door trim, and a stucco belly band along the façade. Finding met.
<i>2.2.7 Preserve mature landscaping</i>	The project proposes removal of grass and some plant material, but no trees are being removed. Less than 500 square feet of landscaping is being modified. Finding met.

**RECOMMENDED
CONDITIONS OF APPROVAL AND
STANDARD DEVELOPMENT REQUIREMENTS
FEBRUARY 26, 2024**

Planning Application **PLNG-2023-0366**
541 South Bayview Avenue

DESIGN REVIEW: Demolish an existing home and construct a new two-story single-family home resulting in 4,838 square feet (3,864 square feet living area, 512 square feet garage, and 462 rear covered patio) and 45.4% floor area ratio (FAR), and a six-foot fence in the front yard.

The following Conditions of Approval [COA] and Standard Development Requirements [SDR] apply to the project referenced above. The COAs are specific conditions applicable to the proposed project. The SDRs are items which are codified or adopted by resolution and have been included for ease of reference, they may not be appealed or changed. The COAs and SDRs are grouped under specific headings that relate to the timing of required compliance. Additional language within a condition may further define the timing of required compliance. Applicable mitigation measures are noted with “Mitigation Measure” and placed in the applicable phase of the project.

In addition to complying with all applicable City, County, State and Federal Statutes, Codes, Ordinances, Resolutions and Regulations, Permittee expressly accepts and agrees to comply with the following Conditions of Approval and Standard Development Requirements of this Permit:

GC: THE FOLLOWING GENERAL CONDITIONS AND STANDARD DEVELOPMENT REQUIREMENTS SHALL APPLY TO THE APPROVED PROJECT.

GC-1. CONFORMANCE WITH APPROVED PLANNING APPLICATION:

All building permit drawings and subsequent construction and operation shall substantially conform with the approved planning application, including: drawings/plans, materials samples, building colors, and other items submitted as part of the approved application. Any proposed amendments to the approved plans or Conditions of Approval are subject to review and approval by the City. The Director of Community Development shall determine whether revisions are considered major or minor. Minor changes are subject to review and approval by the Director of Community Development. Major changes are subject to review at a public hearing. [COA] [PLANNING]

GC-2. ENTITLEMENTS—EXERCISE AND EXPIRATION:

- The approved entitlements shall be null and void two years from the date of approval by the final review authority if the approval is not exercised, unless a written request for an extension is received prior to the expiration date and is approved by the Director of Community Development. [SDR] (PLANNING)
- GC-3. ENTITLEMENTS—DISCONTINUANCE AND EXPIRATION:
The entitlements shall expire if discontinued for a period of one year or more. [SDR] (PLANNING)
- GC-4. INDEMNITY:
The applicant/developer shall defend, indemnify, and hold harmless the City, or any of its boards, commissions, agents, officers, and employees (collectively, "City") from any claim, action, or proceeding against the City to attack, set aside, void, or annul, the approval of the project when such claim, action, or proceeding is brought within the time period provided for in applicable state and/or local statutes. The City shall promptly notify the developer of any such claim, action or proceeding. The City shall have the option of coordinating the defense. Nothing contained in this condition shall prohibit the City from participating in a defense of any claim, action, or proceeding if the City bears its own attorney's fees and costs, and the City defends the action in good faith. [COA] [OFFICE OF THE CITY ATTORNEY]
- GC-5. NOTICE OF FEES PROTEST:
As required by California Government Code Section 66020, the project applicant is hereby notified that the 90-day period has begun as of the date of the approval of this application, in which the applicant may protest any fees, dedications, reservations, or other exactions imposed by the city as part of the approval or as a condition of approval of this development. The fees, dedications, reservations, or other exactions are described in the approved plans, conditions of approval, and/or adopted city impact fee schedule. [SDR] [PLANNING / OCA]
- GC-6. NOISE:
Any outdoor mechanical equipment, including heat pumps, shall comply with Sunnyvale Municipal Code Noise Ordinance 19.42.030-Noise or Sound Level. [SDR] [PLANNING / OCA]

PS: THE FOLLOWING CONDITIONS SHALL BE MET PRIOR TO SUBMITTAL OF BUILDING PERMIT, AND/OR GRADING PERMIT.
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- PS-1. EXTERIOR MATERIALS REVIEW:

Final exterior building materials and color scheme are subject to review and approval by the Director of Community Development prior to submittal of a building permit. [COA] [PLANNING]

BP: THE FOLLOWING CONDITIONS SHALL BE ADDRESSED ON THE CONSTRUCTION PLANS SUBMITTED FOR ANY DEMOLITION PERMIT, BUILDING PERMIT, GRADING PERMIT, AND/OR ENCROACHMENT PERMIT AND SHALL BE MET PRIOR TO THE ISSUANCE OF SAID PERMIT(S).

BP-1. CONDITIONS OF APPROVAL:

Final plans shall include all Conditions of Approval included as part of the approved application starting on sheet 2 of the plans. [COA] [PLANNING]

BP-2. STREET DEDICATION:

Along the South Bayview Avenue project frontage, it is required a 5-foot-wide street dedication in form of easement. The street easement shall be kept open and free from buildings and structures of any kind except those appurtenances associated with the defined easements. The homeowner shall execute the easement deed prior to building permit issuance. [COA] [PUBLIC WORKS]

BP-3. RESPONSE TO CONDITIONS OF APPROVAL:

A written response indicating how each condition has or will be addressed shall accompany the building permit set of plans. [COA] [PLANNING]

BP-4. NOTICE OF CONDITIONS OF APPROVAL:

A Notice of Conditions of Approval shall be filed in the official records of the County of Santa Clara and provide proof of such recordation to the City prior to issuance of any City permit, allowed use of the property, or Final Map, as applicable. The Notice of Conditions of Approval shall be prepared by the Planning Division and shall include a description of the subject property, the Planning Application number, attached conditions of approval and any accompanying subdivision or parcel map, including book and page and recorded document number, if any, and be signed and notarized by each property owner of record.

For purposes of determining the record owner of the property, the applicant shall provide the City with evidence in the form of a report from a title insurance company indicating that the record owner(s) are the person(s) who have signed the Notice of Conditions of Approval. [COA] [PLANNING]

BP-5. BLUEPRINT FOR A CLEAN BAY:

The building permit plans shall include a "Blueprint for a Clean Bay" on one full sized sheet of the plans. [SDR] [PLANNING]

BP-6. BEST MANAGEMENT PRACTICES - STORMWATER:

The project shall comply with the following source control measures as outlined in the BMP Guidance Manual and SMC 12.60.220. Best management practices shall be identified on the building permit set of plans and shall be subject to review and approval by the Director of Public Works:

- a) Storm drain stenciling. The stencil is available from the City's Environmental Division Public Outreach Program, which may be reached by calling (408) 730-7738.
- b) Landscaping that minimizes irrigation and runoff, promotes surface infiltration where possible, minimizes the use of pesticides and fertilizers, and incorporates appropriate sustainable landscaping practices and programs such as Bay-Friendly Landscaping.
- c) Appropriate covers, drains, and storage precautions for outdoor material storage areas, loading docks, repair/maintenance bays, and fueling areas.
- d) Covered trash, food waste, and compactor enclosures.
- e) Plumbing of the following discharges to the sanitary sewer, subject to the local sanitary sewer agency's authority and standards:
 - i) Discharges from indoor floor mat/equipment/hood filter wash racks or covered outdoor wash racks for restaurants.
 - ii) Dumpster drips from covered trash and food compactor enclosures.
 - iii) Discharges from outdoor covered wash areas for vehicles, equipment, and accessories.
 - iv) Swimming pool water, spa/hot tub, water feature and fountain discharges if discharge to onsite vegetated areas is not a feasible option.
 - v) Fire sprinkler test water, if discharge to onsite vegetated areas is not a feasible option. [SDR] [PLANNING]

<p>EP: THE FOLLOWING CONDITIONS SHALL BE ADDRESSED AS PART OF AN ENCROACHMENT PERMIT APPLICATION.</p>
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EP-1. UTILITY METER/VAULT:

No existing or new utility meters or vaults shall be located within the new driveway approach. All existing or new utility vaults serving the

project site shall be located on-site and not within the public utility easement, if any. The water meter box shall be located in the public right-of-way per the City of Sunnyvale Potable Water System Design Guidelines. [COA] [PUBLIC WORKS]

EP-2. CITY STREET TREES:

The developer shall install required street tree in the park-strip within the public right-of-way along the project frontage as follows: S Bayview Ave: *White Crape Myrtle*. New street trees shall be 24-inch box size or 15 gallon size. No street trees are to be planted within 10' of a sanitary sewer lateral and within any existing or proposed Public Utility Easement. [SDR] [PUBLIC WORKS]

EP-3. DAMAGE TO EXISTING PUBLIC IMPROVEMENTS:

Developer shall be responsible to rectify any damage to the existing public improvements fronting and adjacent to the project site as a result of project construction, to City's satisfaction by the Public Works Department. [COA] [PUBLIC WORKS]

DC: THE FOLLOWING CONDITIONS SHALL BE COMPLIED WITH AT ALL TIMES DURING THE CONSTRUCTION PHASE OF THE PROJECT.

DC-1. BLUEPRINT FOR A CLEAN BAY:

The project shall be in compliance with stormwater best management practices for general construction activity until the project is completed and either final occupancy has been granted. [SDR] [PLANNING]

DC-2. CLIMATE ACTION PLAN – OFF ROAD EQUIPMENT REQUIREMENT:

OR 2.1: Idling times will be minimized either by shutting equipment off when not in use or reducing the maximum idling time to 5 minutes (as required by the California airborne toxics control measure Title 13, Section 2485 of California Code of Regulations [CCR]), or less. Clear signage will be provided at all access points to remind construction workers of idling restrictions.

OR 2.2: Construction equipment must be maintained per manufacturer's specifications.

OR 2.3: Planning and Building staff will work with project applicants to limit GHG emissions from construction equipment by selecting one of the following measures, at a minimum, as appropriate to the construction project:

- a) Substitute electrified or hybrid equipment for diesel- and gasoline-powered equipment where practical.

- b) Use alternatively fueled construction equipment on-site, where feasible, such as compressed natural gas (CNG), liquefied natural gas (LNG), propane, or biodiesel.
- c) Avoid the use of on-site generators by connecting to grid electricity or utilizing solar-powered equipment.
- d) Limit heavy-duty equipment idling time to a period of 3 minutes or less, exceeding CARB regulation minimum requirements of 5 minutes. [COA] [PLANNING]

DC-3. DUST CONTROL:

At all times, the Bay Area Air Quality Management District's CEQA Guidelines and "Basic Construction Mitigation Measures Recommended for All Proposed Projects", shall be implemented. [COA] [PLANNING]

DC-4. RIGHT OF WAY:

All construction related materials and equipment need to be stored on-site and the public streets need to be kept free and clear of construction debris. [COA] [PUBLIC WORKS]

END OF CONDITIONS

A NEW HOUSE FOR

Chris & Peggy Allan

541 S. Bayview Ave. Sunnyvale, Ca 94086

NOTE TO CONTRACTOR

THE CONTRACTOR AND/OR SUPPLIER OF MATERIALS SHALL NOT SCALE ANY DIMENSIONS FOR CONSTRUCTION PURPOSES. IN THE EVENT A DIMENSION IS REQUIRED THAT DOES NOT OCCUR ON THE DRAWINGS AND/OR A DIMENSION ERROR IS FOUND ON THE DRAWINGS, THE CONTRACTOR AND/OR SUPPLIER OF MATERIALS WILL NOTIFY THE OFFICE OF MICHELLE MINER DESIGN, AND REQUEST ASSISTANCE AS SOON AS POSSIBLE. IF ANY ERROR IS FOUND ON PLAN OF ANY KIND NOTIFY MICHELLE MINER DESIGN. THE CONTRACTOR AND/OR SUPPLIER OF MATERIALS SHALL BE SOLELY RESPONSIBLE FOR THE RESULTS OF ERRORS, DISCREPANCIES AND OMISSIONS WHICH THE CONTRACTOR AND/OR MATERIAL SUPPLIER FAILED TO NOTIFY THE OFFICE OF MICHELLE MINER DESIGN. PRIOR TO CONSTRUCTION AND/OR FABRICATION OF THE WORK, NO DEVIATION FROM THE PLANS IN ANY WAY SHALL BE MADE WITHOUT THE WRITTEN CONSENT OF MICHELLE MINER DESIGN. APPROVAL BY THE CITY INSPECTOR DOES NOT CONSTITUTE AUTHORITY TO DEVIATE FROM THE PLANS OR OTHER DOCUMENTS PROVIDED BY THE OFFICE OF MICHELLE MINER DESIGN.

SPECIAL NOTES

BEFORE YOU START CONSTRUCTION REVIEW ALL SHEETS CAREFULLY. READ THE GREEN CHECKLIST SHEETS AND THE TITLE 34 SHEETS FOR REQUIREMENTS AS RULES HAVE CHANGED AND THERE MAY BE THINGS YOU ARE NOT EXPECTING

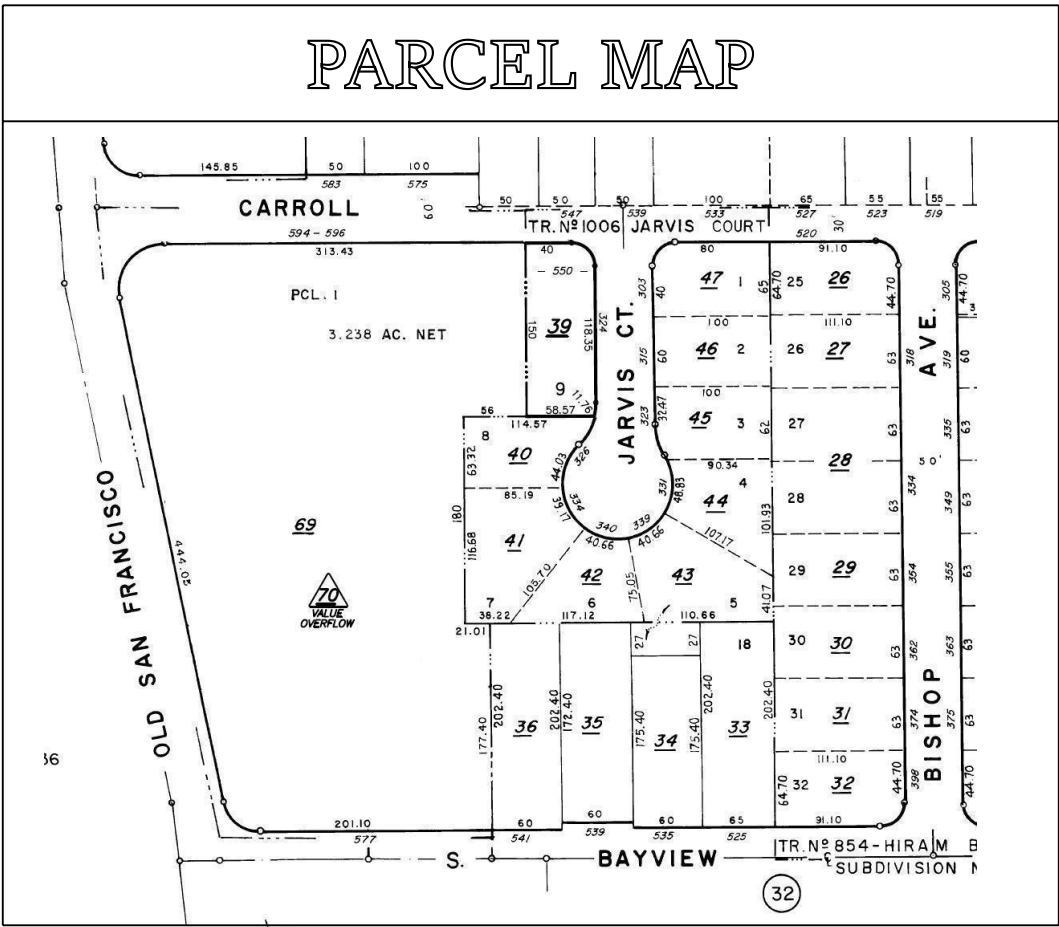
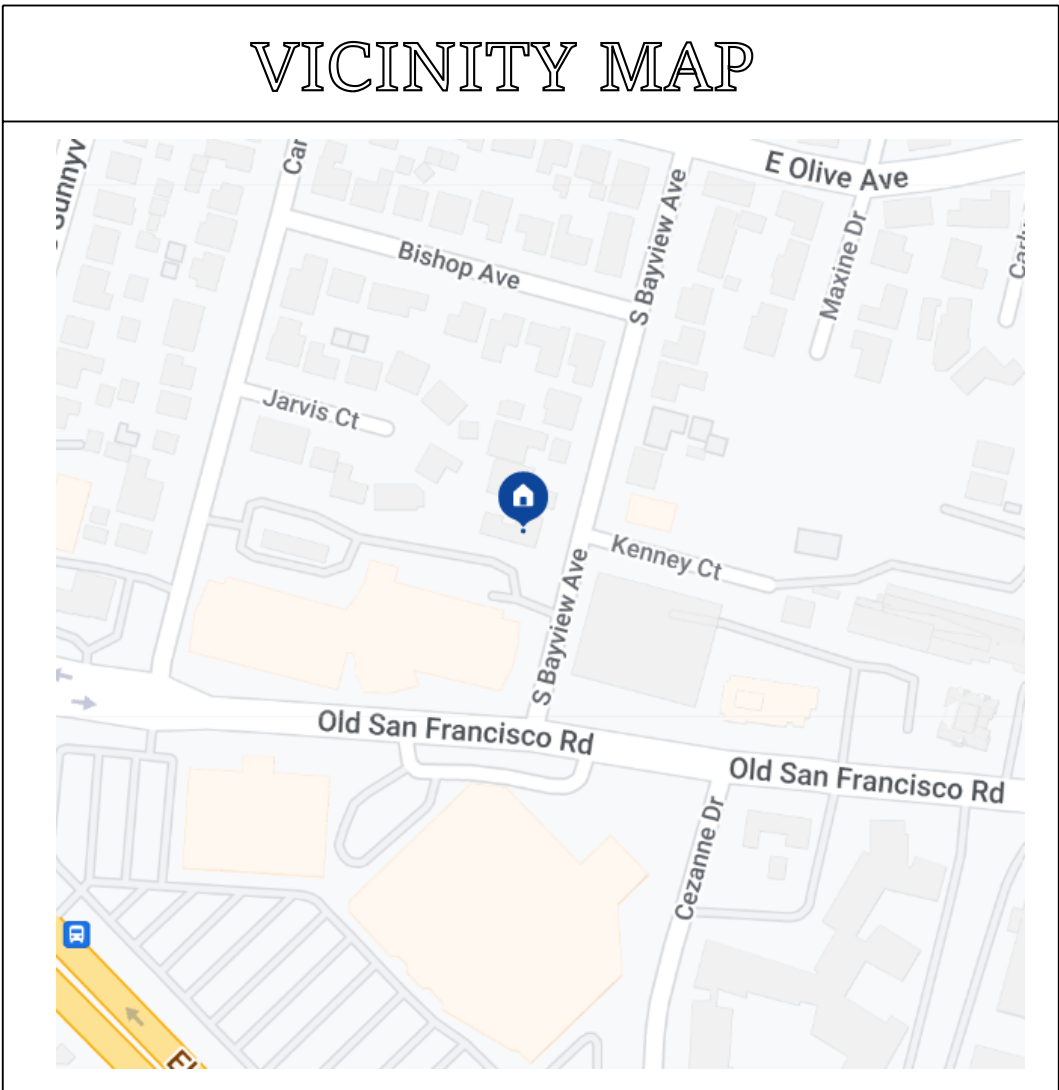
SCOPE OF WORK

DEMOLISH EXISTING HOUSE AND BUILD A NEW 2 STORY HOME WITH 2 CAR GARAGE

ANALYSIS

ASSESSOR'S PARCEL #	209-31-036
GROSS LOT AREA: (AFTER DEDICATION)	10,644 S.F.
ZONING:	Ro
TYPE OF CONSTRUCTION:	V-B
OCCUPANCY RATING:	R-3, U
EXISTING USE:	SINGLE FAMILY RES.
SLOPE OF LOT	FLAT LOT
FIRE SPRINKLERS	YES
STORIES	2
FLOOR AREA	
EXISTING LIVING:	1200 S.F.
EXISTING GARAGE:	550 S.F.
BREEZEWAY:	400 S.F.
EXISTING FLOOR AREA	2150 S.F.
NEW LOWER FLOOR	
NEW UPPER FLOOR	2930 S.F.
NEW GARAGE	512 S.F.
REAR COVERED PATIOS	462 S.F.
PROPOSED FLOOR AREA	4838 S.F.

- GENERAL NOTES
- ALL WORK SHALL CONFORM TO THE REQUIREMENTS OF APPLICABLE FEDERAL, STATE, AND LOCAL CODES AND OTHER REQUIREMENTS WHICH HAVE BEEN ADOPTED BY THE LOCAL JURISDICTION OR ARE OTHERWISE APPLICABLE TO THIS PROJECT.
 - THE GENERAL CONTRACTOR SHALL BE RESPONSIBLE FOR THE VERIFICATION OF ALL DIMENSIONS, GRADES, AND OTHER CONDITIONS, AND SHALL CORRELATE AT THE JOB SITE ALL SUCH ITEMS. GENERAL CONTRACTOR SHALL REPORT ANY DISCREPANCIES TO THE ARCHITECT FOR CLARIFICATION AND CORRECTION PRIOR TO BEGINNING ANY WORK.
 - THE GENERAL CONTRACTOR SHALL BE RESPONSIBLE FOR THE WORK AND THE COORDINATION OF ALL TRADES AND GOVERNING AGENCIES, AND SHALL PROVIDE ALL MATERIALS AND LABOR (SHOWN OR INFERRED) ON THESE PLANS TO RENDER THE WORK COMPLETE.
 - IT SHALL BE THE GENERAL CONTRACTORS RESPONSIBILITY FOR THE SUPERVISION OF THE WORK.
 - THE ARCHITECT ASSUMES NO RESPONSIBILITY FOR THE SUPERVISION OF THE WORK OR THE PROPER EXECUTION OF THE SAME.
 - WRITTEN DIMENSIONS SHALL HAVE PRECEDENCE OVER SCALED DIMENSIONS. ANY AND ALL DISCREPANCIES SHALL BE REPORTED TO THE ARCHITECT IMMEDIATELY, PRIOR TO COMMENCEMENT OF WORK.
 - THESE DRAWINGS SHALL BE CONSIDERED SUBSTANTIALLY COMPLETE. IT IS THE RESPONSIBILITY OF THE GENERAL CONTRACTOR TO PROVIDE ALL LABOR AND MATERIALS NECESSARY TO RENDER THE WORK COMPLETE, AS IS THE INTENT OF THESE DRAWINGS, EITHER SHOWN OR INFERRED HEREIN, THROUGH PROPER AND ESTABLISHED CONSTRUCTION PRACTICES.
 - EXISTING CONSTRUCTION DETAILS SHOWN HEREIN ARE ASSUMED TO BE SUBSTANTIALLY CORRECT AND MAY NOT DEPICT THE ACTUAL CONDITION. THE GENERAL CONTRACTOR SHALL VERIFY EXISTING CONDITIONS AND NOTIFY THE ARCHITECT ANY DISCREPANCIES PRIOR TO COMMENCEMENT OF WORK.
 - ANY PROPOSED SHUT DOWN OF UTILITIES SHALL BE REGISTERED IN WRITING AT LEAST SEVEN (7) WORKING DAYS IN ADVANCE. REQUESTS SHALL BE DIRECTED TO THE ARCHITECT.
 - ANY PROPOSED WORK THAT TAKES PLACE AFTER NORMAL BUSINESS HOURS SHALL BE MADE IN WRITING AT LEAST SEVEN (7) WORKING DAYS IN ADVANCE. REQUESTS SHALL BE DIRECTED TO THE ARCHITECT.
 - PROVIDE ALL REQUIRED FIRE BLOCKING IN ACCORDANCE WITH SECTION 718 OF THE CURRENT ADOPTED EDITION OF C.B.C.
 - EXITING NOTE: THIS BUILDING OR SPACE SHALL PROVIDE A READILY DISTINGUISHABLE MEANS OF EGRESS COMPLYING WITH CHAPTER 10 AND CHAPTER 11 (WHERE APPLICABLE FOR ACCESSIBILITY PURPOSES) OF THE CURRENT EDITION OF THE CALIFORNIA BUILDING CODE. THE EXIT SYSTEM SHALL MAINTAIN A CONTINUOUS, UNOBSTRUCTED AND UNDIMINISHED PATH OF EXIT TRAVEL FROM ANY OCCUPIED POINT WITHIN THE BUILDING TO A PUBLIC WAY.
 - JOB COPIES OF THE APPROVED BUILDING PLANS, REVISIONS, AND DEFERRED SUBMITTALS SHALL BE ON-SITE DURING INSPECTIONS.
 - RECYCLE AND/OR SALVAGE FOR REUSE A MINIMUM OF 65 PERCENT OF THE NONHAZARDOUS CONSTRUCTION AND DEMOLITION WASTE. THE CITY OF SUNNYVALE REQUIRES THE USE OF GREEN HALO, THE CONSTRUCTION AND DEMOLITION WASTE MANAGEMENT PLAN (CDWMP) WASTE-TRACKING PROGRAM TO DOCUMENT AND MONITOR COMPLIANCE.
 - AN OPERATION AND MAINTENANCE MANUAL SHALL BE PROVIDED TO THE BUILDING OCCUPANT OR OWNER.
 - RECYCLE AND/OR SALVAGE FOR REUSE A MINIMUM OF 65 PERCENT OF THE NONHAZARDOUS CONSTRUCTION AND DEMOLITION WASTE. THE CITY OF SUNNYVALE REQUIRES THE USE OF GREEN HALO, THE CONSTRUCTION AND DEMOLITION WASTE MANAGEMENT PLAN (CDWMP) WASTE-TRACKING PROGRAM TO DOCUMENT AND MONITOR COMPLIANCE.



APPLICABLE CODE

ALL CONSTRUCTION SHALL COMPLY WITH:

2022	CALIF. FIRE CODE
2022	CALIF. BLDG CODE
2022	CALIF. RESIDENTIAL CODE
2022	CALIF. MECH. CODE
2022	CALIF. PLUMB'G CODE
2022	CALIF. ELEC. CODE
2022	CALIF. ENERGY CODES
2022	CALIF. GREEN BUILDING CODES
	ANY OTHER APPLICABLE LOCAL & STATE LAWS & REGULATIONS.

PERSONAE

OWNER Chris & Peggy Allan 541 S. Bayview Ave. Sunnyvale, Ca 94086 408-656-5778 chris.allan03@gmail.com	STRUCTURAL ENGINEER NJM CONSULTING ENGINEERING INC. 18488 PROSPECT ROAD SUITE 12 SARATOGA, CA 95070 NATHAN@NJMENGINEERING.COM 415-676-9896
DESIGNER MICHELLE MINER DESIGN MICHELLE MINER 18488 PROSPECT RD. #6 SARATOGA, CA 95070 SHELMINER@AOL.COM 408-396-0984	TITLE 24 FRI ENERGY CONSULTANTS 21 N. HARRISON AVE, SUITE 210 CAMPBELL, CA 95008 NICK@FRICONSULTING.COM 408-866-1620

ANALYSIS

ASSESSOR'S PARCEL #	209-31-036
GROSS LOT AREA: (AFTER DEDICATION)	10,644 S.F.
ZONING:	Ro
TYPE OF CONSTRUCTION:	V-B
OCCUPANCY RATING:	R-3, U
EXISTING USE:	SINGLE FAMILY RES.
SLOPE OF LOT	FLAT LOT
FIRE SPRINKLERS	YES
STORIES	2
FLOOR AREA	
EXISTING LIVING:	1200 S.F.
EXISTING GARAGE:	550 S.F.
BREEZEWAY:	400 S.F.
EXISTING FLOOR AREA	2150 S.F.
NEW LOWER FLOOR	
NEW UPPER FLOOR	2930 S.F.
NEW GARAGE	512 S.F.
REAR COVERED PATIOS	462 S.F.
PROPOSED FLOOR AREA	4838 S.F.
FLOOR AREA RATIO (%)	
EXISTING	20.1%
NEW	45.4%
LOT COVERAGE	
EXISTING LIVING:	1200 S.F.
EXISTING GARAGE:	550 S.F.
BREEZEWAY:	400 S.F.
EXISTING LOT COVERAGE	2150 S.F.
NEW LOWER FLOOR	
NEW GARAGE	512 S.F.
FRONT COVERED PORCH	40 S.F.
REAR COVERED PATIOS	462 S.F.
PROPOSED LOT COVERAGE	3944 S.F.
LOT COVERAGE (%)	
EXISTING	20.1%
NEW	37.0%
PROPOSED SECOND STORY TO FIRST STORY RATIO	
23.6%	
LOWER	
NEW LOWER FLOOR	2930 S.F.
NEW GARAGE	512 S.F.
FRONT COVERED PORCH	40 S.F.
REAR COVERED PATIOS	462 S.F.
LOWER FLOOR AREA	3944 S.F.
UPPER	
NEW UPPER FLOOR	934 S.F.

CITY STAMP AREA

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DEFERRED SUBMITTALS

FIRE SPRINKLERS REQUIRED

1. FIRE SPRINKLERS IN ACCORDANCE WITH NFPA 13D AND STATE AND LOCAL REQUIREMENTS - NOTE THAT PER CRC 313.3.7, A SIGN OR VALVE TAG SHALL BE INSTALLED AT THE MAIN SHUT OFF VALVE TO THE TO THE WATER DISTRIBUTION SYSTEM STATING THE FOLLOWING: " WARNING, THE WATER SYSTEM FOR THIS HOME SUPPLIES FIRE SPRINKLERS THAT REQUIRE CERTAIN FLOWS AND PRESSURES TO FIGHT A FIRE . DEVICES THAT RESTRICT THE FLOW OR DECREASE THE PRESSURE OR AUTOMATICALLY SHUT OFF THE WATER TO THE FIRE SPRINKLER SYSTEM, SUCH AS WATER SOFTENERS, FILTRATION SYSTEMS, AND AUTOMATIC SHUT OFF VALVES, SHALL NOT BE ADDED TO THIS SYSTEM WITHOUT A REVIEW OF THE FIRE SPRINKLER SYSTEM BY A FIRE PROTECTION SPECIALIST. DO NOT REMOVE THIS SIGN."

2. CONSTRUCTION WASTE MANAGEMENT PLAN IN ACCORDANCE WITH CALGREEN 4.408.2

3. PHOTOVOLTAIC ROOFING SYSTEM

SEPARATE PERMIT

REVISIONS

REV #	DATE	BY

DESIGNER'S SIGNATURE

Michelle Miner

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michelle miner

design

18488 Prospect Rd. #6 Saratoga, CA 95070

408.396.0984 Shelminer@aol.com

Chris & Peggy Allan

541 S. Bayview Ave. Sunnyvale, Ca 94086

408-656-5778 chris.allan03@gmail.com

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MM

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DATE

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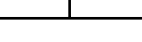
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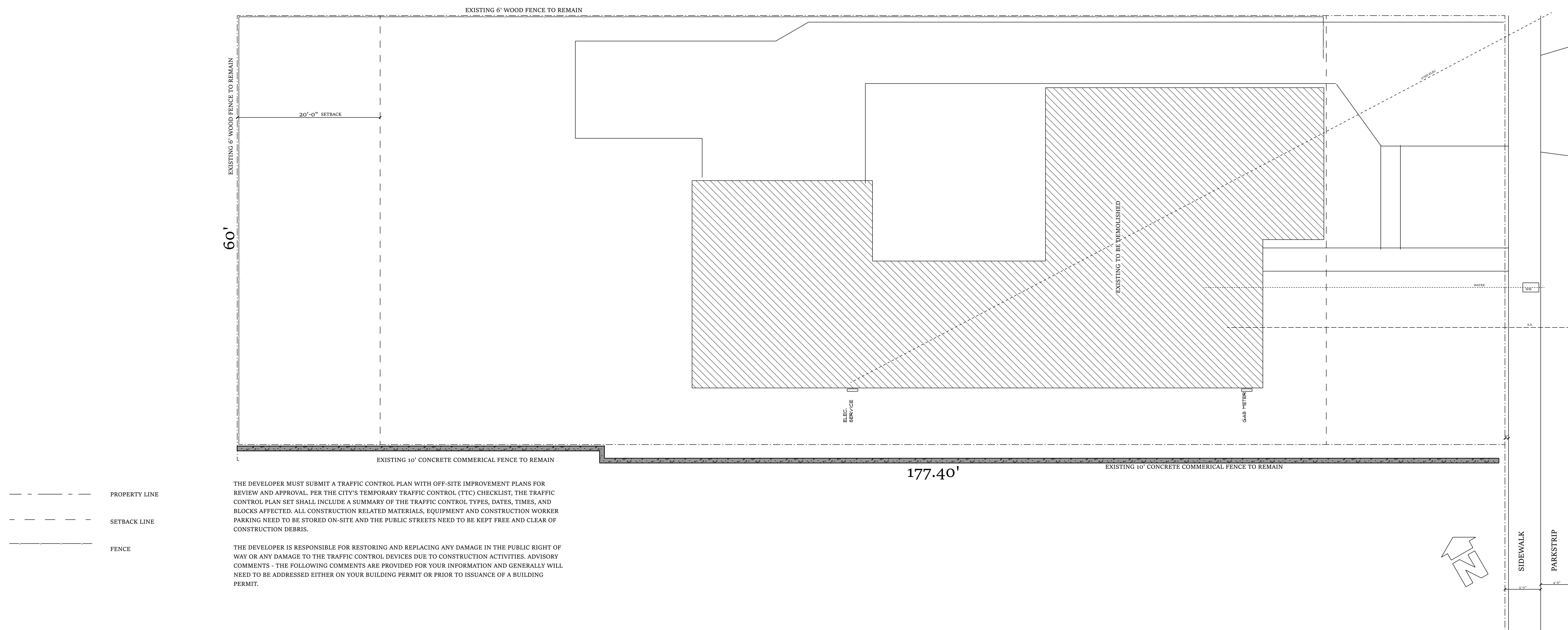
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michelle miner
— design —
18488 Prospect Rd. #6 Saratoga, CA 95070
408.396.0984 shelminer@aol.com

Chris & Peggy Allan
541 S. Bayview Ave. Sunnyvale, Ca 94086
408-656-5778 chris.allan03@gmail.com

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EXISTING SITE PLAN



EXISTING SITE PLAN

SCALE: 1/8" = 1'-0"

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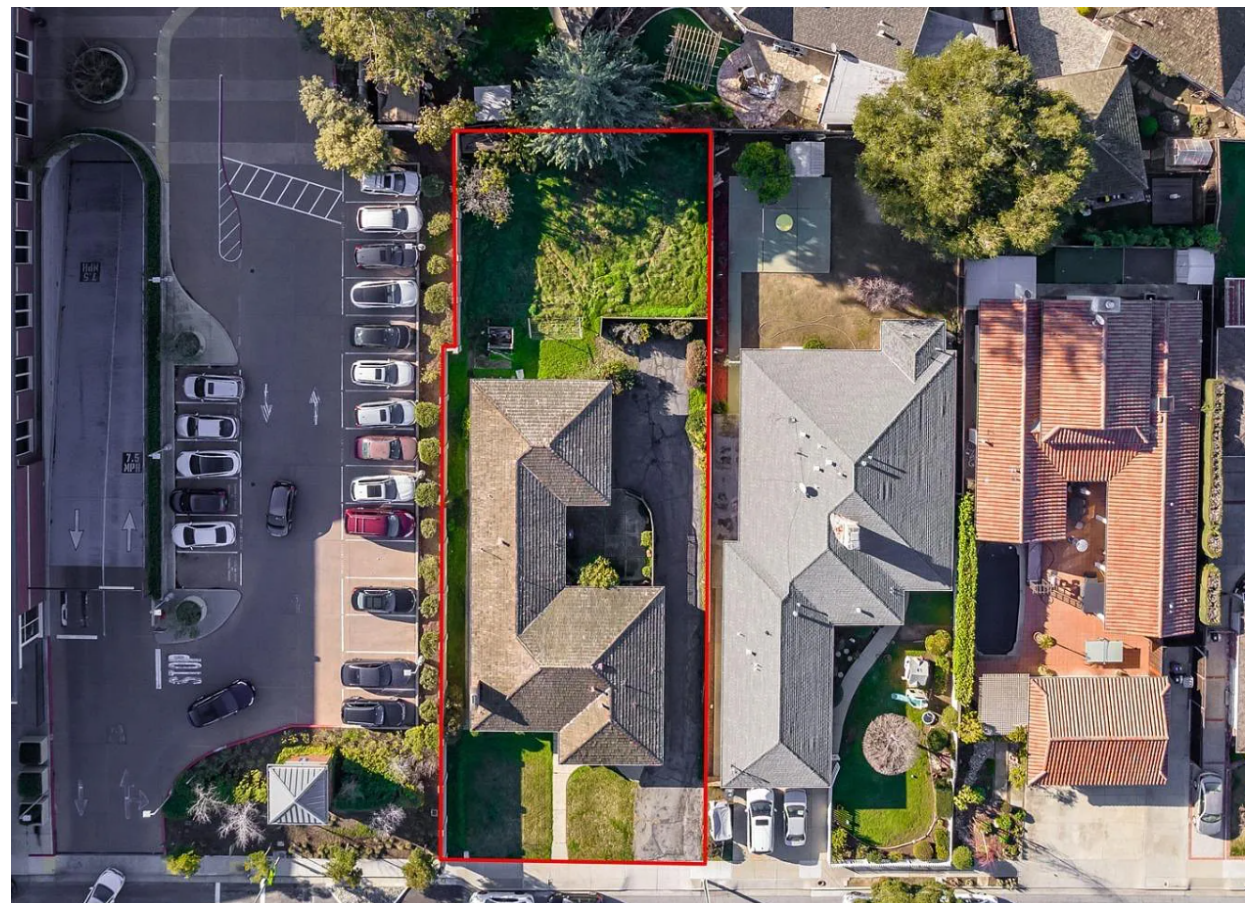
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design

18488 Prospect Rd. #6 Saratoga, CA 95070
408.396.0984 Shelminer@aol.com

Chris & Peggy Allan
541 S. Bayview Ave. Sunnyvale, Ca 94086
408-656-5TT8 chris.allan03@gmail.com

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PHOTOS



541 S. BAYVIEW AVENUE - AERIAL



COMMERCIAL PROPERTY - SUNNYVALE CENTER



541 S. BAYVIEW AVENUE



539 S. BAYVIEW AVENUE



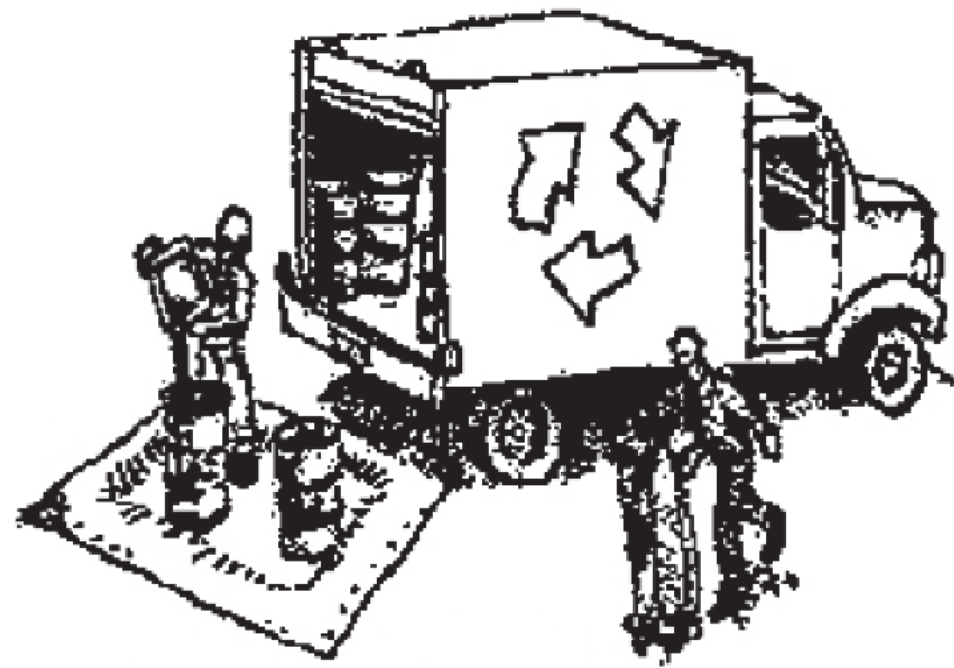
405 KENNEY COURT (ACROSS STREET)

PHOTOS
SCALE: 1/4" = 1'-0"

Construction Best Management Practices (BMPs)

Construction projects are required to implement year-round stormwater BMPs.

Materials & Waste Management



Non-Hazardous Materials

- ☐ Berm and cover stockpiles of sand, dirt or other construction material with tarps when rain is forecast or when they are not in use.
- ☐ Use (but don't overuse) reclaimed water for dust control.
- ☐ Ensure dust control water doesn't leave site or discharge to storm drains.

Hazardous Materials

- ☐ Label all hazardous materials and hazardous wastes (such as pesticides, paints, thinners, solvents, fuel, oil, and antifreeze) in accordance with City, County, State and Federal regulations.
- ☐ Store hazardous materials and wastes in water tight containers, store in appropriate secondary containment, and cover them at the end of every work day or during wet weather or when rain is forecast.
- ☐ Follow manufacturer's application instructions for hazardous materials and do not use more than necessary. Do not apply chemicals outdoors when rain is forecast within 24 hours.
- ☐ Arrange for appropriate disposal of all hazardous wastes.

Waste Management

- ☐ Cover and maintain dumpsters. Check frequently for leaks. Place dumpsters under roofs or cover with tarps or plastic sheeting secured around the outside of the dumpster. A plastic liner is recommended to prevent leaks. Never clean out a dumpster by hosing it down on the construction site.
- ☐ Place portable toilets away from storm drains. Make sure they are in good working order. Check frequently for leaks.
- ☐ Dispose of all wastes and demolition debris properly. Recycle materials and wastes that can be recycled, including solvents, water-based paints, vehicle fluids, broken asphalt and concrete, wood, and cleared vegetation.
- ☐ Dispose of liquid residues from paints, thinners, solvents, glues, and cleaning fluids as hazardous waste.
- ☐ Keep site free of litter (e.g. lunch items, cigarette butts).
- ☐ Prevent litter from uncovered loads by covering loads that are being transported to and from site.

Construction Entrances and Perimeter

- ☐ Establish and maintain effective perimeter controls and stabilize all construction entrances and exits to sufficiently control erosion and sediment discharges from site and tracking off site.
- ☐ Sweep or vacuum any street tracking immediately and secure sediment source to prevent further tracking. Never hose down streets to clean up tracking.

Equipment Management & Spill Control



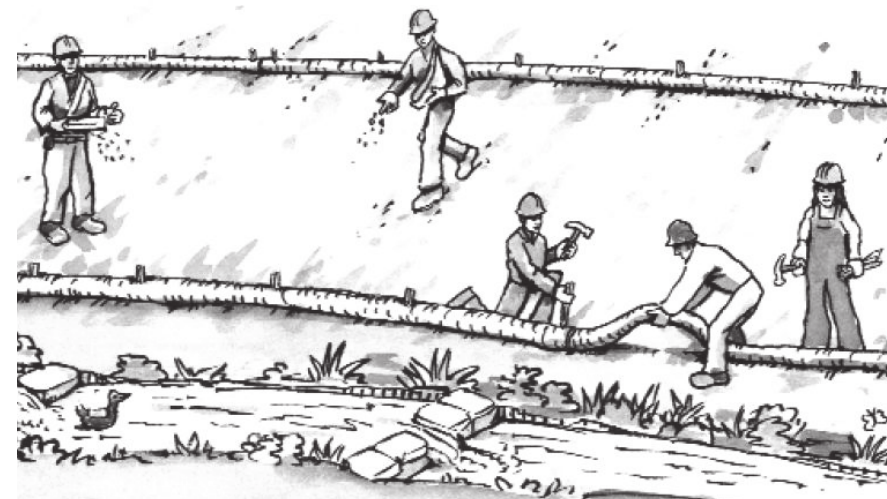
Maintenance and Parking

- ☐ Designate an area of the construction site, well away from streams or storm drain inlets and fitted with appropriate BMPs, for auto and equipment parking, and storage.
- ☐ Perform major maintenance, repair jobs, and vehicle and equipment washing off site.
- ☐ If refueling or vehicle maintenance must be done onsite, work in a bermed area away from storm drains and over a drip pan or drop cloths big enough to collect fluids. Recycle or dispose of fluids as hazardous waste.
- ☐ If vehicle or equipment cleaning must be done onsite, clean with water only in a bermed area that will not allow rinse water to run into gutters, streets, storm drains, or surface waters.
- ☐ Do not clean vehicle or equipment onsite using soaps, solvents, degreasers, or steam cleaning equipment, and do not use diesel oil to lubricate equipment or parts onsite.

Spill Prevention and Control

- ☐ Keep spill cleanup materials (e.g., rags, absorbents and cat litter) available at the construction site at all times.
- ☐ Maintain all vehicles and heavy equipment. Inspect frequently for and repair leaks. Use drip pans to catch leaks until repairs are made.
- ☐ Clean up leaks, drips and other spills immediately and dispose of cleanup materials properly.
- ☐ Use dry cleanup methods whenever possible (absorbent materials, cat litter and/or rags).
- ☐ Sweep up spilled dry materials immediately. Never attempt to "wash them away" with water, or bury them.
- ☐ Clean up spills on dirt areas by digging up and properly disposing of contaminated soil.
- ☐ Report significant spills to the appropriate local spill response agencies immediately. If the spill poses a significant hazard to human health and safety, property or the environment, you must report it to the State Office of Emergency Services. (800) 852-7550 (24 hours).

Earthmoving



Grading and Earthwork

- ☐ Schedule grading and excavation work during dry weather.
- ☐ Stabilize all denuded areas, install and maintain temporary erosion controls (such as erosion control fabric or bonded fiber matrix) until vegetation is established.
- ☐ Remove existing vegetation only when absolutely necessary, plant temporary vegetation for erosion control on slopes or where construction is not immediately planned.
- ☐ Prevent sediment from migrating offsite and protect storm drain inlets, drainage courses and streams by installing and maintaining appropriate BMPs (i.e. silt fences, gravel bags, fiber rolls, temporary swales, etc.).
- ☐ Keep excavated soil on site and transfer it to dump trucks on site, not in the streets.

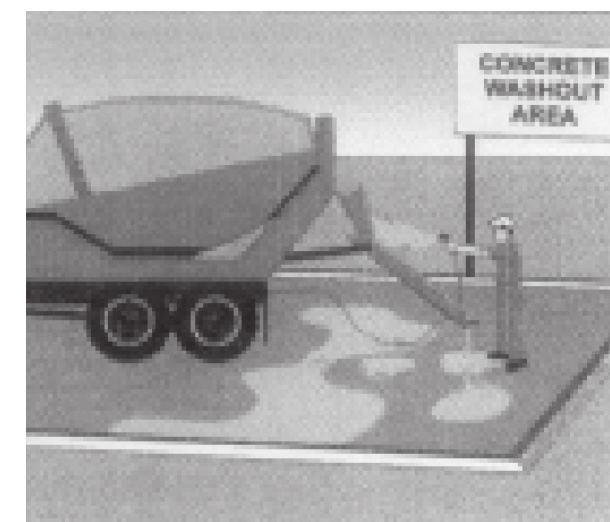
Contaminated Soils

- ☐ If any of the following conditions are observed, test for contamination and contact the Regional Water Quality Control Board:
 - Unusual soil conditions, discoloration, or odor.
 - Abandoned underground tanks.
 - Abandoned wells
 - Buried barrels, debris, or trash.
- ☐ If the above conditions are observed, document any signs of potential contamination and clearly mark them so they are not disturbed by construction activities.

Landscaping

- ☐ Protect stockpiled landscaping materials from wind and rain by storing them under tarps all year-round.
- ☐ Stack bagged material on pallets and under cover.
- ☐ Discontinue application of any erodible landscape material within 2 days before a forecast rain event or during wet weather.

Concrete Management and Dewatering



Concrete Management

- ☐ Store both dry and wet materials under cover, protected from rainfall and runoff and away from storm drains or waterways. Store materials off the ground, on pallets. Protect dry materials from wind.
- ☐ Wash down exposed aggregate concrete only when the wash water can (1) flow onto a dirt area; (2) drain onto a bermed surface from which it can be pumped and disposed of properly; or (3) block any storm drain inlets and vacuum washwater from the gutter. If possible, sweep first.
- ☐ Wash out concrete equipment/trucks offsite or in a designated washout area onsite, where the water will flow into a temporary waste pit, and make sure wash water does not leach into the underlying soil. (See CASQA Construction BMP Handbook for properly designed concrete washouts.)

Dewatering

- ☐ Discharges of groundwater or captured runoff from dewatering operations must be properly managed and disposed. When possible, send dewatering discharge to landscaped area or sanitary sewer. If discharging to the sanitary sewer, call your local wastewater treatment plant.
- ☐ Divert run-on water from offsite away from all disturbed areas.
- ☐ When dewatering, notify and obtain approval from the local municipality before discharging water to a street gutter or storm drain. Filtration or diversion through a basin, tank, or sediment trap may be required.
- ☐ In areas of known or suspected contamination, call your local agency to determine whether the ground water must be tested. Pumped groundwater may need to be collected and hauled off-site for treatment and proper disposal.

Paving/Asphalt Work



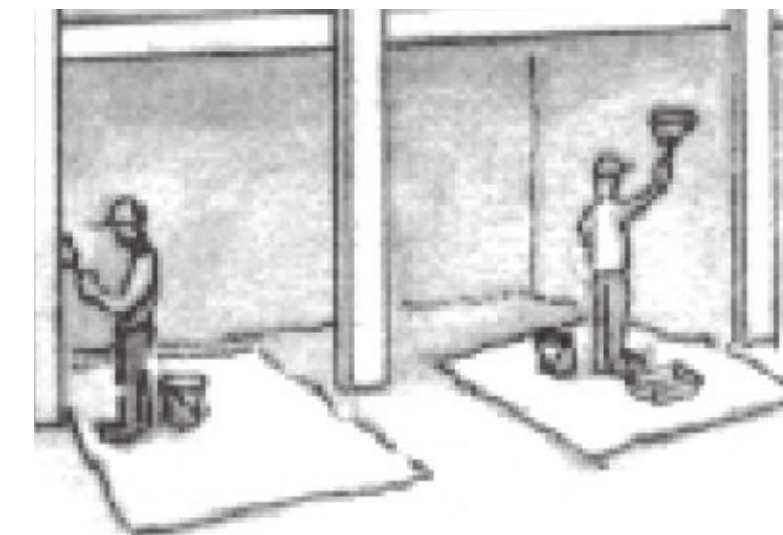
Paving

- ☐ Avoid paving and seal coating in wet weather or when rain is forecast, to prevent materials that have not cured from contacting stormwater runoff.
- ☐ Cover storm drain inlets and manholes when applying seal coat, slurry seal, fog seal, or similar materials.
- ☐ Collect and recycle or properly dispose of excess abrasive gravel or sand. Do NOT sweep or wash it into gutters.

Sawcutting & Asphalt/Concrete Removal

- ☐ Protect storm drain inlets during saw cutting.
- ☐ If saw cut slurry enters a catch basin, clean it up immediately.
- ☐ Shovel or vacuum saw cut slurry deposits and remove from the site. When making saw cuts, use as little water as possible. Sweep up, and properly dispose of all residues.

Painting & Paint Removal



Painting Cleanup and Removal

- ☐ Never clean brushes or rinse paint containers into a street, gutter, storm drain, or stream.
- ☐ For water-based paints, paint out brushes to the extent possible, and rinse into a drain that goes to the sanitary sewer. Never pour paint down a storm drain.
- ☐ For oil-based paints, paint out brushes to the extent possible and clean with thinner or solvent in a proper container. Filter and reuse thinners and solvents. Dispose of excess liquids as hazardous waste.
- ☐ Sweep up or collect paint chips and dust from non-hazardous dry stripping and sand blasting into plastic drop cloths and dispose of as trash.
- ☐ Chemical paint stripping residue and chips and dust from marine paints or paints containing lead, mercury, or tributyltin must be disposed of as hazardous waste. Lead based paint removal requires a state-certified contractor.



Santa Clara Valley
Urban Runoff
Pollution Prevention Program

Storm drain polluters may be liable for fines of up to \$10,000 per day!



NEW HOME RATING SYSTEM, VERSION 8.1

New Home Single Family v. 8.1

CALGreen

Yes	CALGreen Res (REQUIRED)
A. SITE	
A2. Job Site Construction Waste Diversion	
Yes	A2.1 70% C&D Waste Diversion (Including Alternative Daily Cover)
Yes	A2.2 Recycling Rates from Third-Party Verified Mixed-Use Waste Facility
A6. Stormwater Control: Prescriptive Path (section capped at 3 points)	
Yes	A6.1 Permeable Paving Material
Yes	A6.3 Non-Leaching Roofing Materials
B. FOUNDATION	
Yes	B1. Fly Ash and/or Slag in Concrete
B5. Structural Pest Controls	
Yes	B5.2 Plant Trunks, Bases, or Stems at Least 36 Inches from the Foundation
C. LANDSCAPE	
36.15%	Enter the landscape area percentage. Points capped at 6 for less than 15%.
Yes	C1. Plants Grouped by Water Needs (Hydrozoning)
Yes	C2. Three Inches of Mulch in Planting Beds
C3. Resource Efficient Landscapes	
Yes	C3.1 No Invasive Species Listed by Cal-IPC
Yes	C3.2 Plants Chosen and Located to Grow to Natural Size
Yes	C3.3 Drought Tolerant, California Native, Mediterranean Species, or Other Appropriate Species
C4. Minimal Turf in Landscape	
Yes	C4.1 No Turf on Slopes Exceeding 10% and No Overhead Sprinklers Installed in Areas Less Than Eight Feet Wide
≤10%	C4.2 Turf on a Small Percentage of Landscaped Area
Yes	C6. High-Efficiency Irrigation System
Yes	C10. Submeter or Dedicated Meter for Landscape Irrigation
Yes	C13. Reduced Light Pollution
D. STRUCTURAL FRAME AND BUILDING ENVELOPE	
D3. Engineered Lumber	
Yes	D3.1 Engineered Beams and Headers
Yes	D3.2 OSB for Subfloor
Yes	D3.3 OSB for Wall and Roof Sheathing
E. EXTERIOR	
Yes	E4. Durable and Non-Combustible Cladding Materials
	E5. Durable Roofing Materials

Yes	E5.1 Durable and Fire Resistant Roofing Materials or Assembly
F. INSULATION	
F1. Insulation with 30% Post-Consumer or 60% Post-Industrial Recycled Content	
Yes	F1.1 Walls and Floors
Yes	F1.2 Ceilings
G. PLUMBING	
G1. Efficient Distribution of Domestic Hot Water	
Yes	G1.1 Insulated Hot Water Pipes
Yes	G1.2 WaterSense Volume Limit for Hot Water Distribution
G2. Install Water-Efficient Fixtures	
Yes	G2.1 WaterSense Showerheads ≤1.8 gpm with Matching Compensation Valve
Yes	G2.2 WaterSense Bathroom Faucets ≤1.0 gpm
≤1.1 gpf	G2.3 WaterSense Toilets with a Maximum Performance (MaP) Threshold of No Less Than 500 Grams and ≤1.28gpf OR ≤1.1 gpf
Yes	G5. Thermostatic Shower Valve or Auto-Diversion Tub Spout
H. HEATING, VENTILATION, AND AIR CONDITIONING	
H1. Sealed Combustion Units	
Yes	H1.1 Sealed Combustion Furnace
Yes	H1.2 Sealed Combustion Water Heater
H3. Effective Ductwork	
Yes	H3.1 Duct Mastic on Duct Joints and Seams
H6. Whole House Mechanical Ventilation Practices to Improve Indoor Air Quality	
Yes	H6.1 Meet ASHRAE 62.2-2016 Ventilation Residential Standards
H7. Effective Range Hood Design and Installation	
Yes	H7.1 Effective Range Hood Ducting and Design
Yes	H7.2 Automatic Range Hood Control
Yes	H8. High Efficiency HVAC Filter (MERV 16+)
Yes	H10. No Fireplace or Sealed Gas Fireplace
J. BUILDING PERFORMANCE AND TESTING	
J5. Building Performance Exceeds Title 24 Part 6	
Option 1: Mixed Fuel Compliance	J5.1 Home Outperforms Title 24 Part 6
Yes	J6. Title 24 Prepared and Signed by a CABEC Certified Energy Analyst
K. FINISHES	
K1. Entryways Designed to Reduce Tracked-In Contaminants	
Yes	K1.1 Individual Entryways
Yes	K2. Zero-VOC Interior Wall and Ceiling Paints
L. FLOORING	
Yes	L3. Durable Flooring
M. APPLIANCES AND LIGHTING	
Yes	M1. ENERGY STAR® Dishwasher
M2. Efficient Clothes Washing and Drying	
CEE Tier 2	M2.1 CEE-Rated Clothes Washer
Yes	M2.2 ENERGY STAR® Dryer
M4. Permanent Centers for Waste Reduction Strategies	
Yes	M4.1 Built-In Recycling Center
M5. Lighting Efficiency	
Yes	M5.1 High-Efficacy Lighting
N. COMMUNITY	
N1. Smart Development	
Yes	N1.1 Infill Site
N5. Social Interaction	
Yes	N5.1 Residence Entries with Views to Callers

Yes	N5.2 Entrances Visible from Street and/or Other Front Doors
O. OTHER	
Yes	O1. GreenPoint Rated Checklist in Blueprints
O5. Home System Monitors	
Yes	O5.1. Home Energy Monitoring Systems
Yes	O7. Green Appraisal Addendum
Summary	
Total Available Points in Specific Categories	
Minimum Points Required in Specific Categories	
Total Points Targeted	

I Blueprint Scoresheet

Points Targeted: 91
Certification Level Targeted: Silver
Compliance Pathway Targeted: Option 1: Mixed Fuel Compliance
T24 Compliance Targeted: 10 Total EDR Compliance Margin

Points Targeted	Community	Energy	IAQ/Health	Resources	Water	Responsible Party
	Possible Points					
4		1	1	1	1	
2				2		
1				1		
1					1	
1					1	
1				1		
1				1		
1					1	
1					1	
1				1		
1				1		
3					3	
2					2	
2					2	
2					2	
1	1				2	
1				1		
0.5				0.5		
0.5				0.5		
1				1		

1				1		
0.5				0.5		
0.5				0.5		
1			1			
1					1	
2					2	
1					1	
2					2	
1					1	
1			1			
2			2			
1			1			
Y	R	R	R	R	R	R
1			1			
1			1			
1			1			
1			1			
25		25+				
1		1				
1			1			
2			2			
1				1		
1					1	
2		1				2
2		2				
1				1		
2						
2		2				
2	1				1	
1	1					

1	1					
Y	R	R	R	R	R	
1		1				
Y	R	R	R	R	R	
Community	Energy	IAQ/Health	Resources	Water		
315.5	28	97.5	59	82	49	
50	2	25	6	6	6	
91.0	4.0	35.0	11.0	16.0	25.0	

REVISIONS

REV #	DATE	BY

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408.396.0984 Shelnminer@aol.com

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408-656-5118 chris.allan03@gmail.com

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GREEN CHECKLIST

*** NOTE TO CONTRACTOR ***

THE CONTRACTOR AND/OR SUPPLIER OF MATERIALS SHALL NOT SCALE ANY DIMENSIONS FOR CONSTRUCTION PURPOSES. IN THE EVENT A DIMENSION IS REQUIRED THAT DOES NOT OCCUR ON THE DRAWINGS AND/OR A DIMENSION ERROR IS FOUND ON THE DRAWINGS, THE CONTRACTOR AND/OR SUPPLIER OF MATERIALS WILL NOTIFY THE OFFICE OF MICHELLE MINER DESIGN, AND REQUIRES ASSISTANCE AS SOON AS POSSIBLE. IF ANY ERROR IS FOUND ON PLAN OF ANY KIND NOTIFY MICHELLE MINER DESIGN. THE CONTRACTOR AND/OR SUPPLIER OF MATERIALS SHALL BE SOLELY RESPONSIBLE FOR THE RESULTS OF ERRORS, DISCREPANCIES AND OMISSIONS WHICH THE CONTRACTOR AND/OR MATERIAL SUPPLIER FAILED TO NOTIFY THE OFFICE OF MICHELLE MINER DESIGN. PRIOR TO CONSTRUCTION AND/OR FABRICATION OF THE WORK, NO DEVIATION FROM THE PLANS IN ANY WAY SHALL BE MADE WITHOUT THE WRITTEN CONSENT OF MICHELLE MINER DESIGN. APPROVAL BY THE CITY INSPECTOR DOES NOT CONSTITUTE AUTHORITY TO DEViate FROM THE PLANS OR OTHER DOCUMENTS PROVIDED BY THE OFFICE OF MICHELLE MINER DESIGN.

LEGEND

NEW WALLS

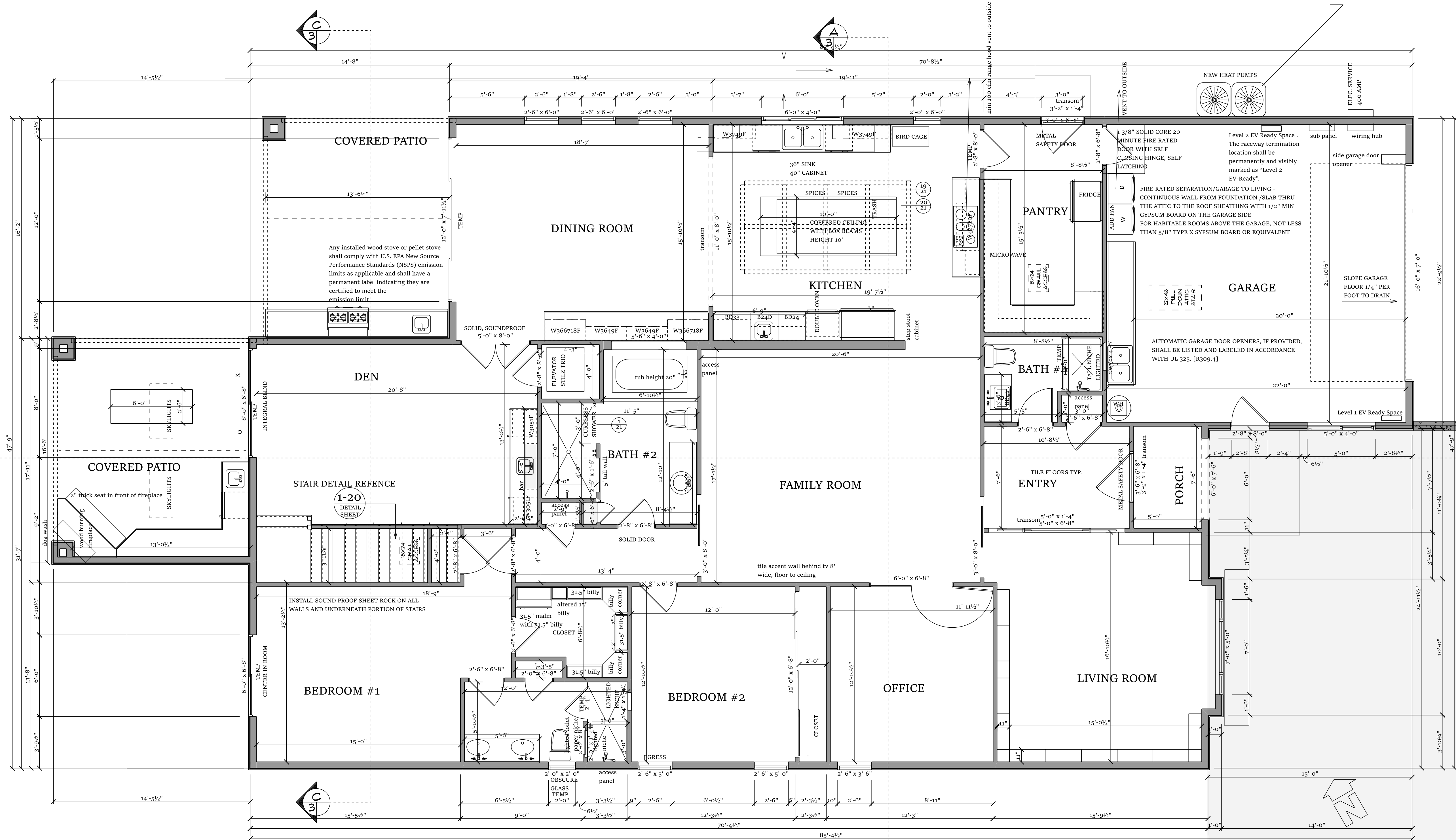
ALL NEW EXTERIOR WALLS TO BE 2X6

ALL NEW INTERIOR WALLS TO BE 2X4

NOTES

SEE WINDOW AND DOOR SHEET FOR MORE DETAILS

CONTRACTOR TO FIELD VERIFY ALL EXISTING OPENINGS SIZES BEFORE ORDERING WINDOWS AND DOORS



NEW LOWER FLOOR PLAN

SCALE: 1/4" = 1'-0"

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design

18488 Prospect Rd. #6 Saratoga, CA 95070
408.396.0984 Shelminer@aol.com

Chris & Peggy Allan

541 S. Bayview Ave. Sunnyvale, Ca 94086
408-656-5118 chris.allan03@gmail.com

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NEW LOWER FLOOR PLAN	

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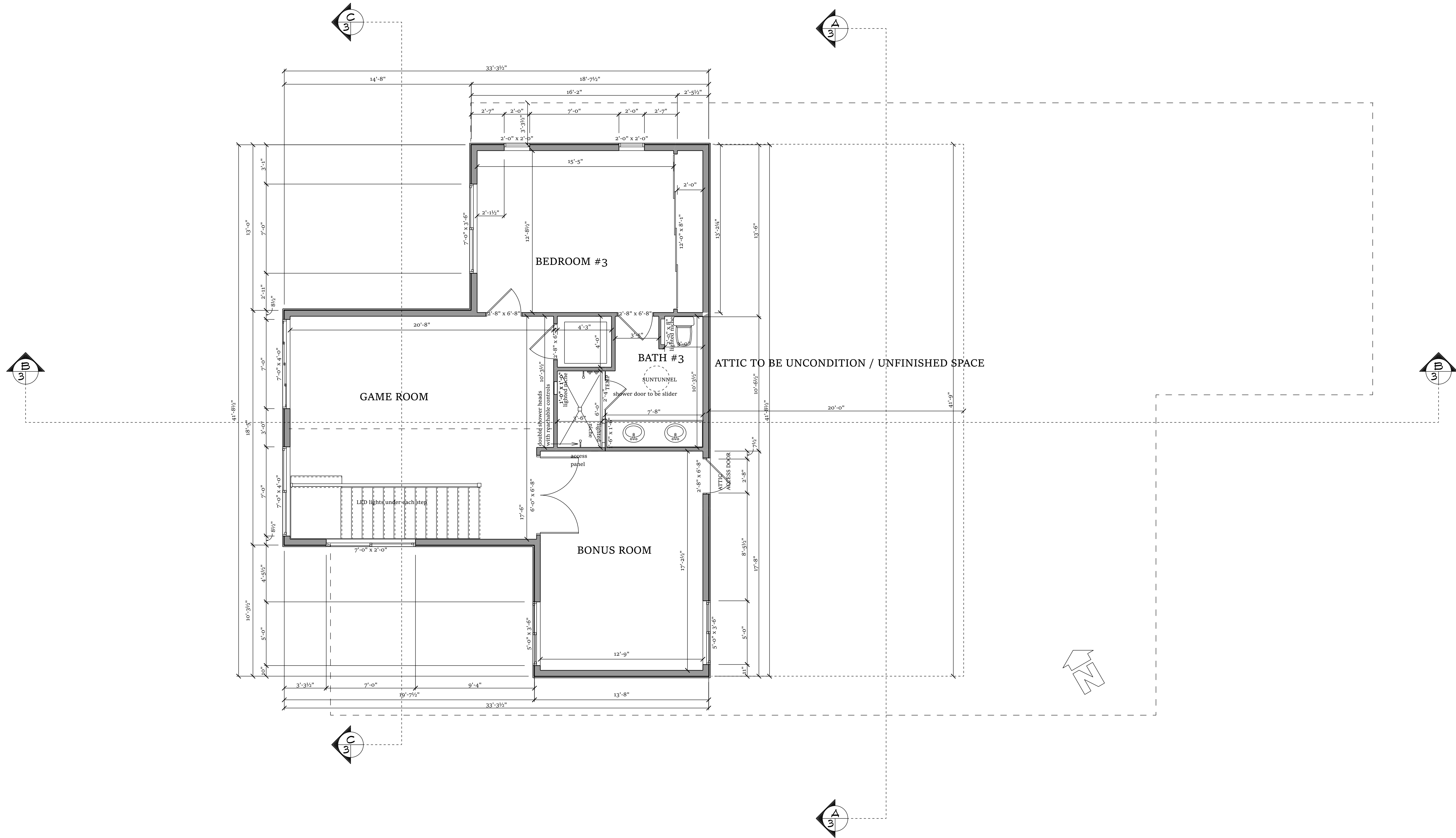


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408.396.0984 Shelminer@aol.com

Chris & Peggy Allan
541 S. Bayview Ave. Sunnyvale, Ca 94086
408-656-5778 chris.allan03@gmail.com

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NEW UPPER FLOOR
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408.396.0984 Shelminer@aol.com

Chris & Peggy Allan

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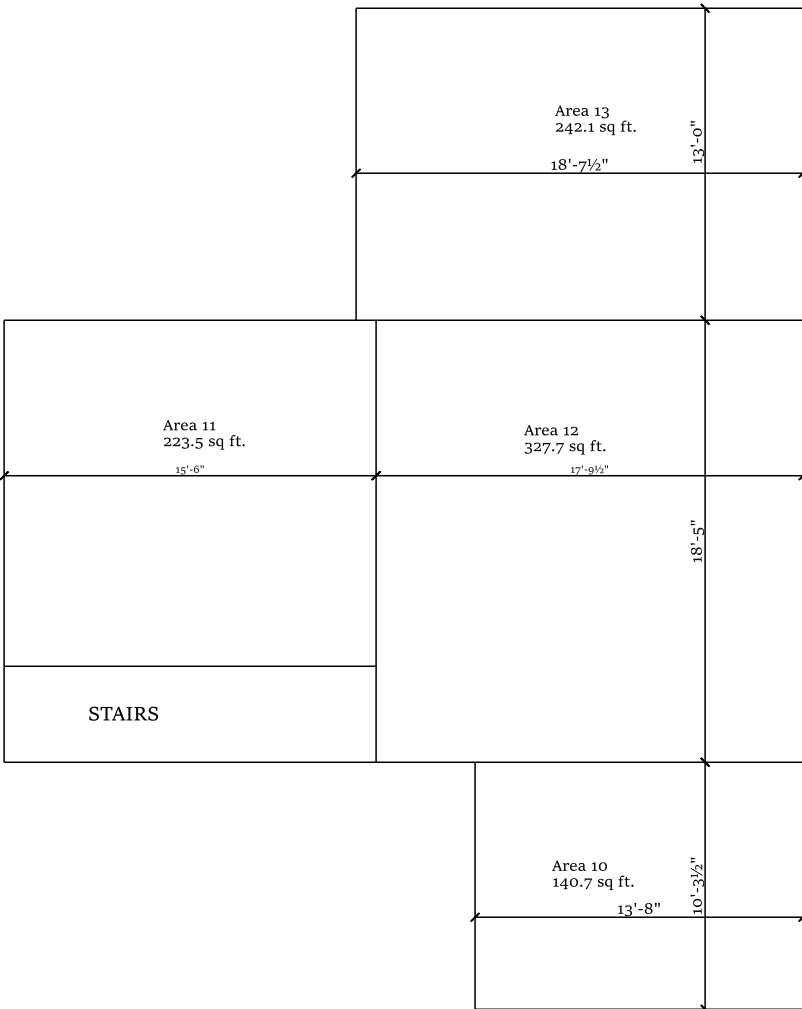
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AREA CALCS



AREA SCHEDULE	
NAME	AREA
Area 1	511.9 sq ft.
Area 2	1099.7 sq ft.
Area 3	376.9 sq ft.
Area 4	979.5 sq ft.
Area 5	10.0 sq ft.
Area 6	493.2 sq ft.
Area 7	222.7 sq ft.
Area 8	238.6 sq ft.
Area 9	40.9 sq ft.
Area 10	140.7 sq ft.
Area 11	223.5 sq ft.
Area 13	242.1 sq ft.
Area 12	327.7 sq ft.

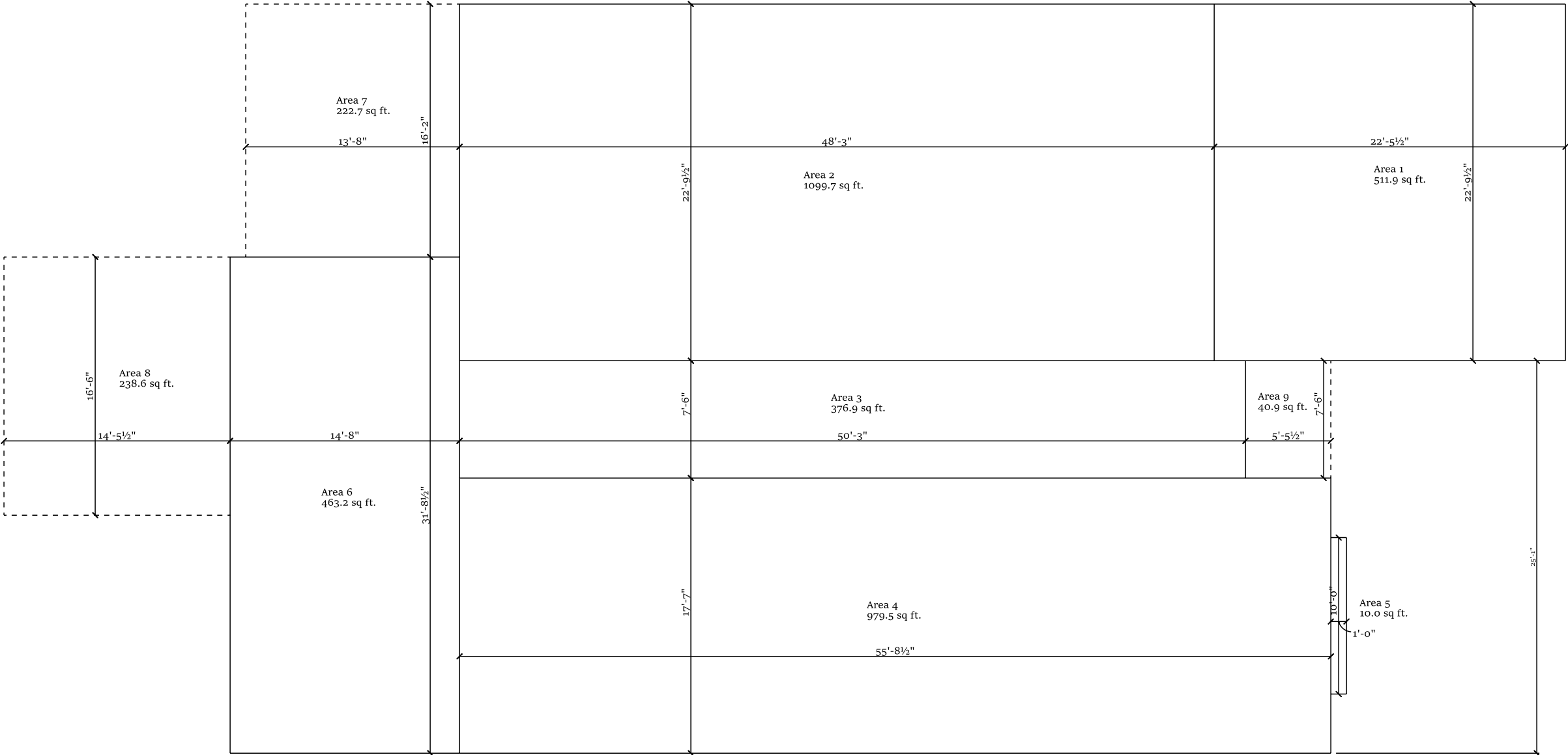
512 S.F. GARAGE

2930 S.F. LOWER FLOOR

502 S.F. COVERED PATIOS AND PORCH

934 S.F. UPPER FLOOR

UPPER FLOOR



UPPER FLOOR

AREA CALCS

SCALE: 1/8" = 1'-0"

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Chris & Peggy Allan
541 S. Bayview Ave. Sunnyvale, Ca 94086
408-656-5118 chris.allan03@gmail.com

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EXTERIOR ELEVATIONS

ELEVATION NOTES

Stucco shall be 7/8" thick and three coat applied over approved wire lath and two layers of grade D building paper over plywood sheathing. Provide 26 gauge galvanized weep screed at foundation plate line at least 4" above grade or 2" above concrete or paving.

Siding shall be applied over one layer grade D building paper. (CBC 1404.2/CRC R703.2)

Provide a spark arrester for any new or existing chimney. (CBC 2113.9.1/CRC 1003.9.1)

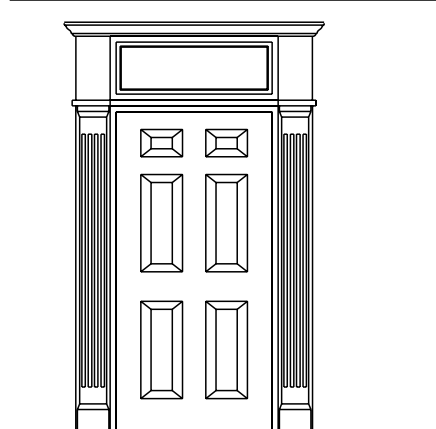
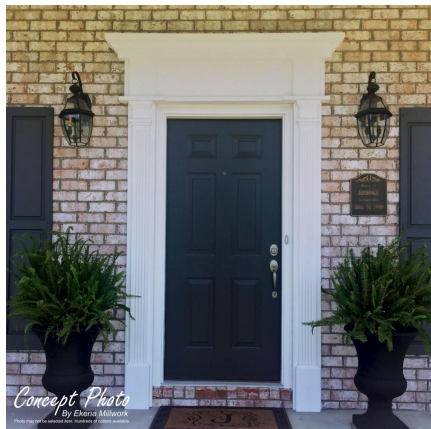
Roof Slopes >2:12 and <4:12 with asphalt shingles shall have two layers of 15 lbs. felt applied shingle style. (CRC 1507.2.2/CRC 905.2.2)

Provide all under-floor areas with cross ventilation at 1/150 for the entire area with one vent within 3' of each corner. Screens over the openings shall have 1/8" to 1/4" openings. (CBC 123/CRC R408)

Attic areas shall have cross ventilation at 1/150 for the entire area with 50% of the required vent area to be ventilators located a minimum of 3' above eave or cornice vents. Screens over the openings shall have 1/8" to 1/4" openings. (CBC 1203/ CRC R806)

Provide attic access (22"x30") and under-floor access (18"x24") for new areas. (CBC 1209/ CRC R408.4)

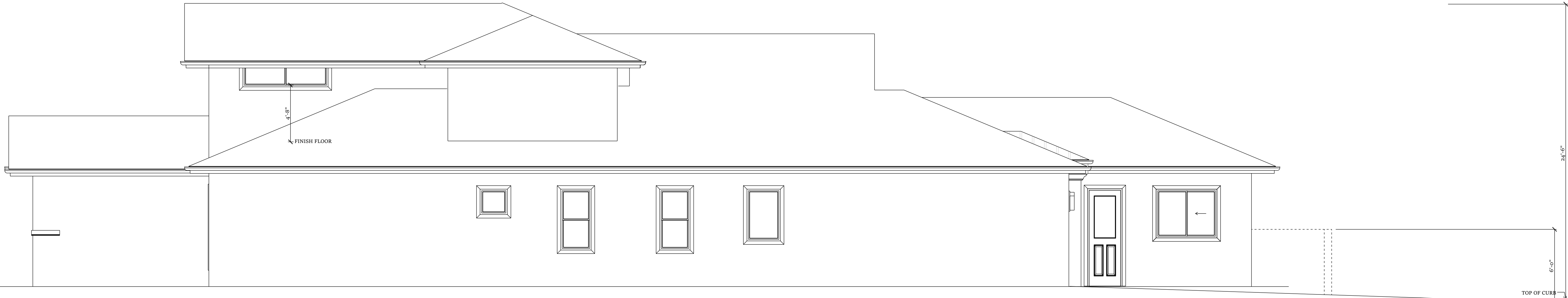
Provide under floor clearance of 18" from joists to earth and 12" clearance from girders to earth.



1/4" FRONT ELEVATION WITH COURTYARD WALL



1/4" FRONT ELEVATION



1/4" LEFT SIDE ELEVATION

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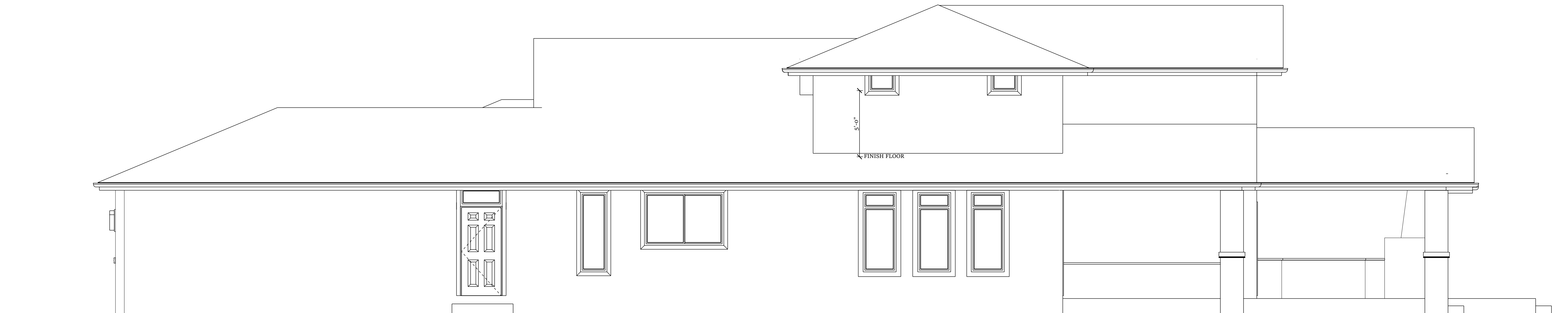
18488 Prospect Rd. #6 Saratoga, CA 95070
408.396.0984 Shelminer@aol.com

Chris & Peggy Allan
541 S. Bayview Ave. Sunnyvale, Ca 94086
408-656-5778 chris.allan03@gmail.com

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1/4" REAR ELEVATION



1/4" RIGHT SIDE ELEVATION

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18488 Prospect Rd. #6 Saratoga, CA 95070
408.396.0984 Shelminer@aol.com

Chris & Peggy Allan
541 S. Bayview Ave. Sunnyvale, Ca 94086
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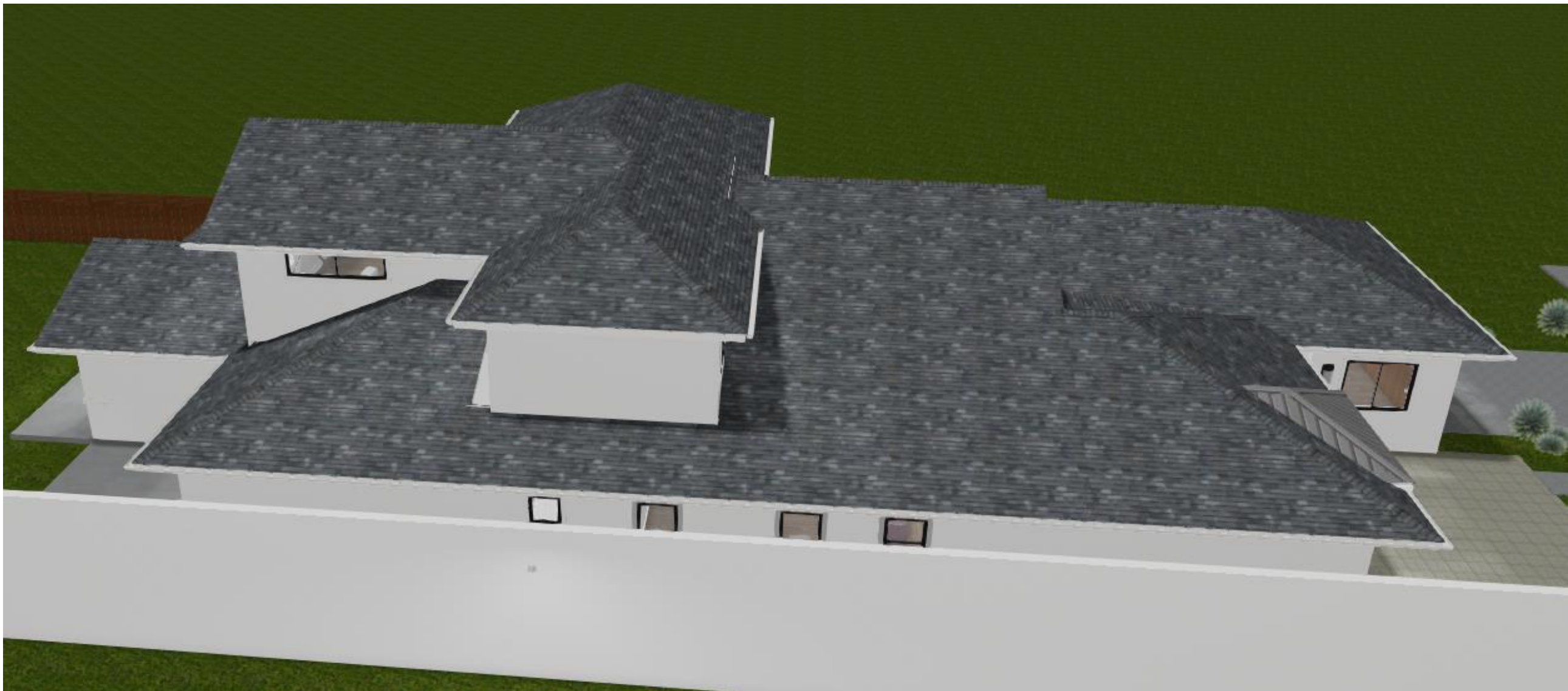
REAR



FRONT



RIGHT SIDE



LEFT SIDE

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Chris & Peggy Allan
541 S. Bayview Ave. Sunnyvale, Ca 94086
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SECTIONS & ROOF PLAN

ROOF PLAN NOTES

ROOFING:
CLASS "A" COMPOSITION SHINGLES, COLOR TO BE SELECTED BY OWNER.
OVER MIN. 15 LB. ROOFING FELT INSTALL PER MANUF. SPECS.
ROOFING FASTENERS ARE TO BE CORROSION RESISTANT IN ACCORDANCE WITH CRC R905.2.5

BUILT-UP ROOF, CLASS "A"
28 LB BASE PLY, ATTACHED MECHANICALLY 9" OC EDGES, AND 18" OC FIELD. INNER PLYS, 11 LB. FELT W/ MIN. 25 LB. MOPPING. 78 LB. MINERAL SURFACE CAP SHEET, MIN. SLOPE AT BUILT-UP ROOF: 1/2" PER FOOT.
(WATER TO DRAIN ONTO SLOPED ROOF.)

FLASHING & COUNTERFLASHING:
26 GAUGE STEEL, U.O.N., AS REQUIRED AT JUNCTURE OF ROOF AND VERTICAL SURFACES. SEE SHEET METAL NOTES ON NOTES SHEET

VALLEY FLASHING:
26 GAUGE GALV. STEEL OVER 15 LB. FELT. CRICKETS SIMILAR.

GUTTERS & DOWNSPOUTS:
5" GALV. STEEL. GUTTER 0 OR MATCH EXISTING STYLE IF REMODEL.
2 1/4" DIA ROUND GALV. STEEL DOWN SPOUTS. PROVIDE BASKET STRAINER AND SPLASH-BLOCK AT EACH DOWNSPOUT AT GROUND LEVEL.

SKYLIGHTS:
USE VELUX RESIDENTIAL, WDMA (WINDOW AND DOOR MANUFACTURING ASSOCIATION) HALLMARK CERTIFICATION #426-H-679 (FOR VENTED) AND #426-H-675 (FOR FIXED). SEE PLAN FOR SKYLIGHT SIZE. PER CRC R308.6.9 TESTING AND LABELING-UNIT SKYLIGHTS SHALL BE TESTED BY AN APPROVED INDEPENDENT LAB, AND BEAR A LABEL IDENTIFYING MANUFACTURER, PERFORMANCE GRADE RATING, AND APPROVED INSPECTION AGENCY TO INDICATE COMPLIANCE WITH THE REQUIREMENTS OF AAMA/WDMA 101/LS-2/NAFS.

PLUMBING VENTS:
PLUMBING WASTE VENTS SHALL TERMINATE NOT LESS THAN 10 FEET FROM, OR NOT LESS THAN 3 FEET ABOVE AN OPENABLE WINDOW, DOOR, OPENING, AIR INTAKE, OR VENT SHAFT OR NOT LESS THAN 3 FEET IN EVERY DIRECTION FROM A LOT LINE, ALLEY AND STREET EXCEPTED. CPC 906.2

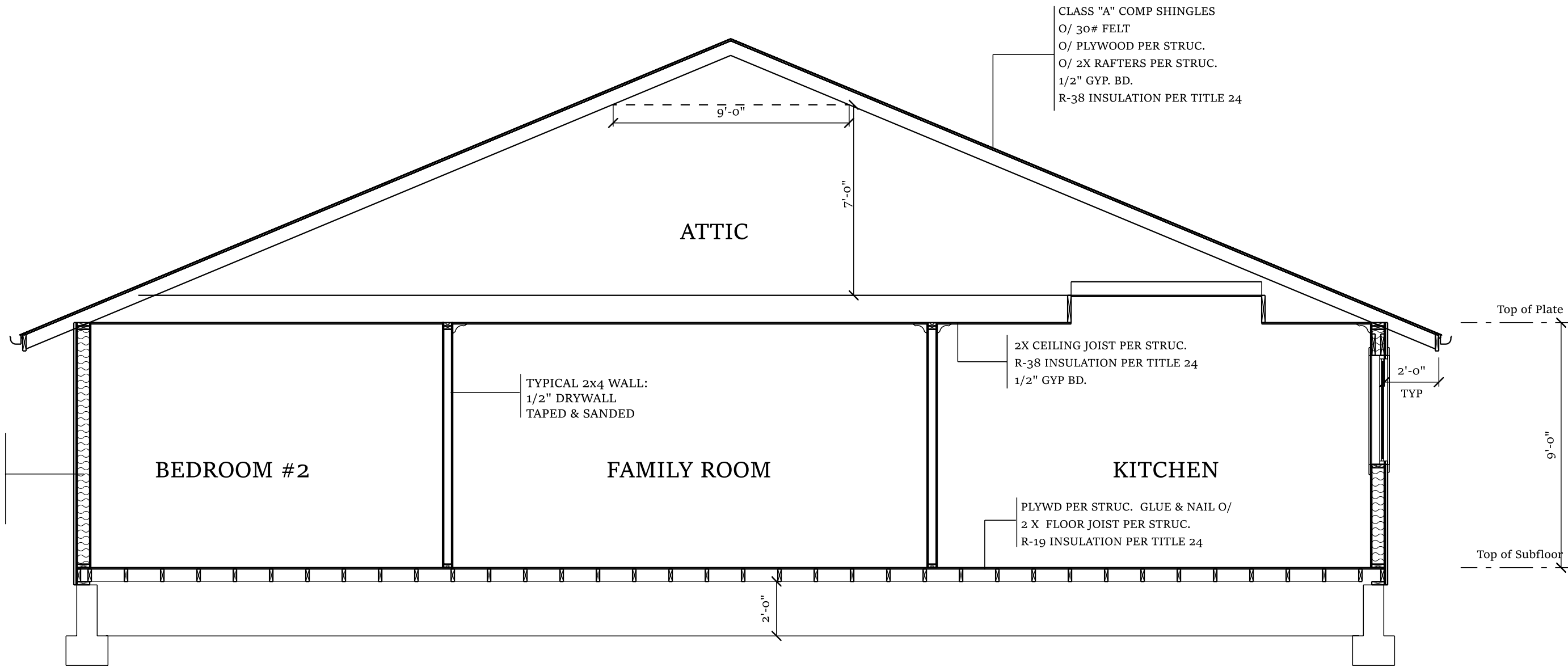
ROOF INSULATION NOTES:
MAINTAIN MIN 1" AIRSPACE BETWEEN THERMAL BATT INSULATION AND ROOF SHEATHING. BAFFLE INSULATION AT VENTS AS NECESSARY.
AT FRAMING CAVITIES THAT CANNOT BE PROPERLY VENTED, FILL CAVITY WITH SPRAY-IN-PLACE MEDIUM-DENSITY SEMI-RIGID CLOSED-CELL POLYURETHANE FOAM INSULATION. COMPLIES AS AIR BARRIER AND THERMAL INSULATION IN COMPLIANCE WITH CRC R806.5

CLASS "A" COMP SHINGLES
O/ 30# FELT
O/ PLYWOOD PER STRUC.
O/ 2X RATTERS PER STRUC.
1/2" GYP. BD.
R-38 INSULATION PER TITLE 24

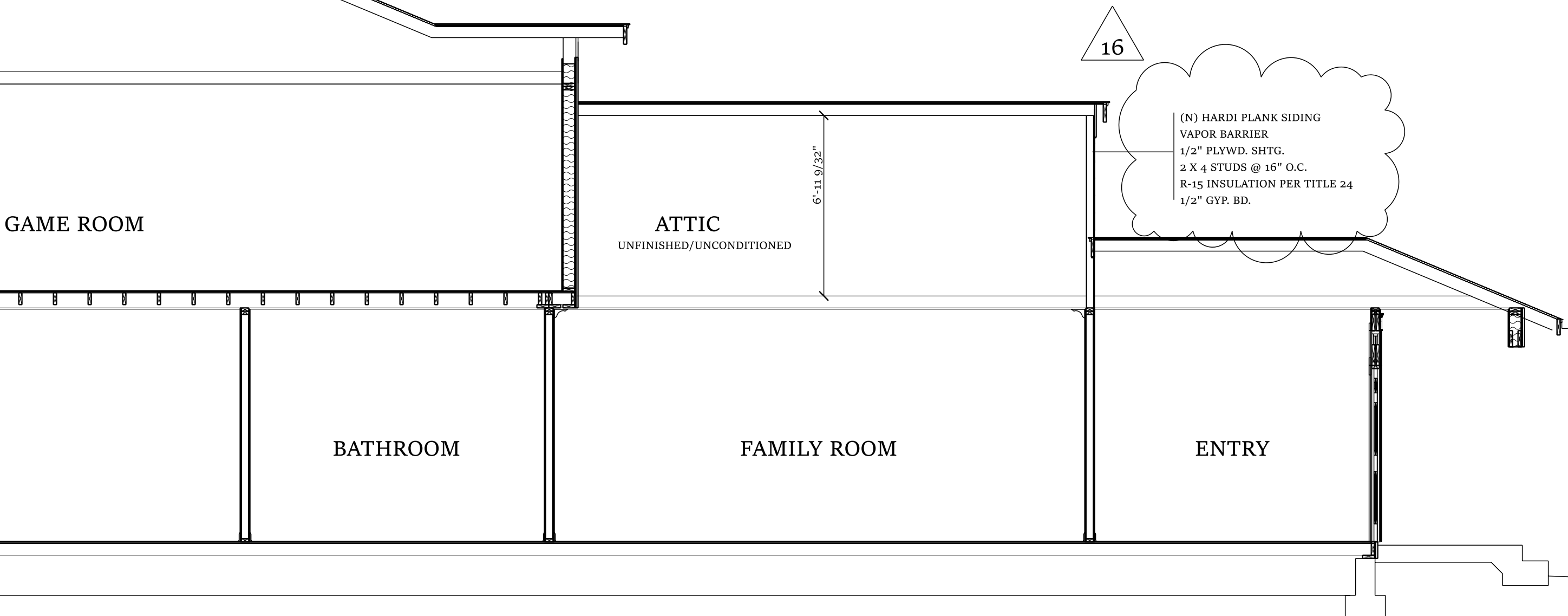
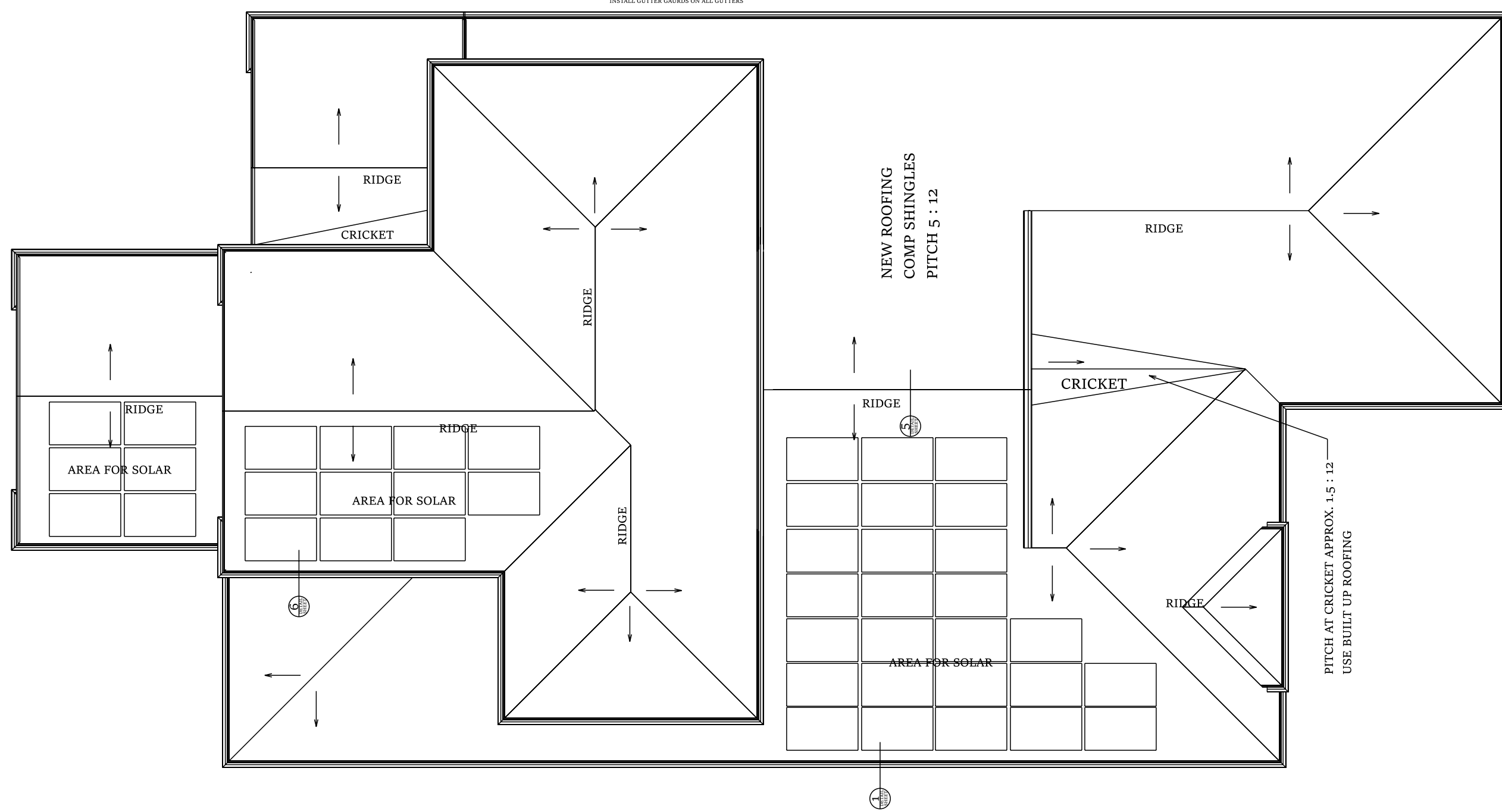
2X CEILING JOIST PER STRUC.
R-38 INSULATION PER TITLE 24
1/2" GYP. BD.

PLYWD PER STRUC. GLUE & NAIL O/
2 X FLOOR JOIST PER STRUC.
R-19 INSULATION PER TITLE 24

CROSS SECTION A
SCALE: 1/4" = 1'-0"



ROOF PLAN
SCALE: 1/8" = 1'-0"



CROSS SECTION B
SCALE: 1/4" = 1'-0"

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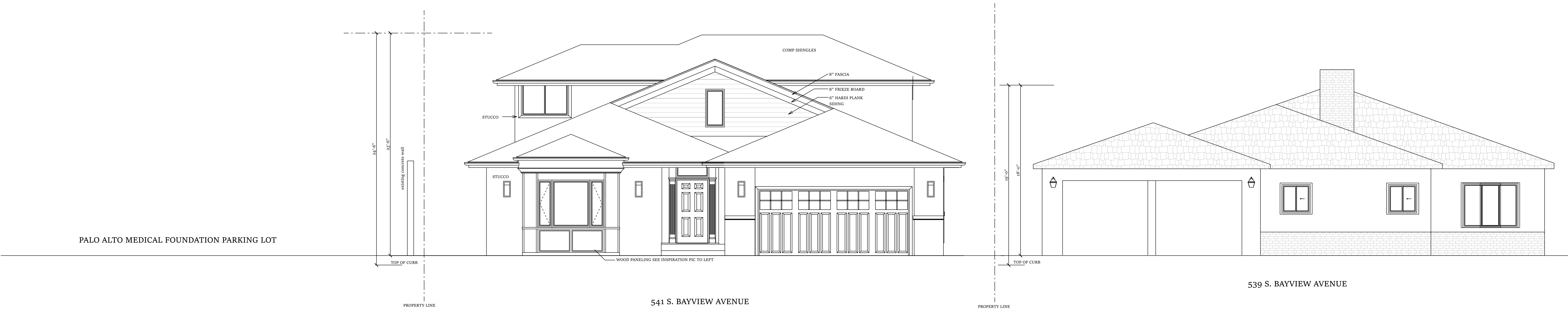
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408.396.0984 Shelminer@aol.com

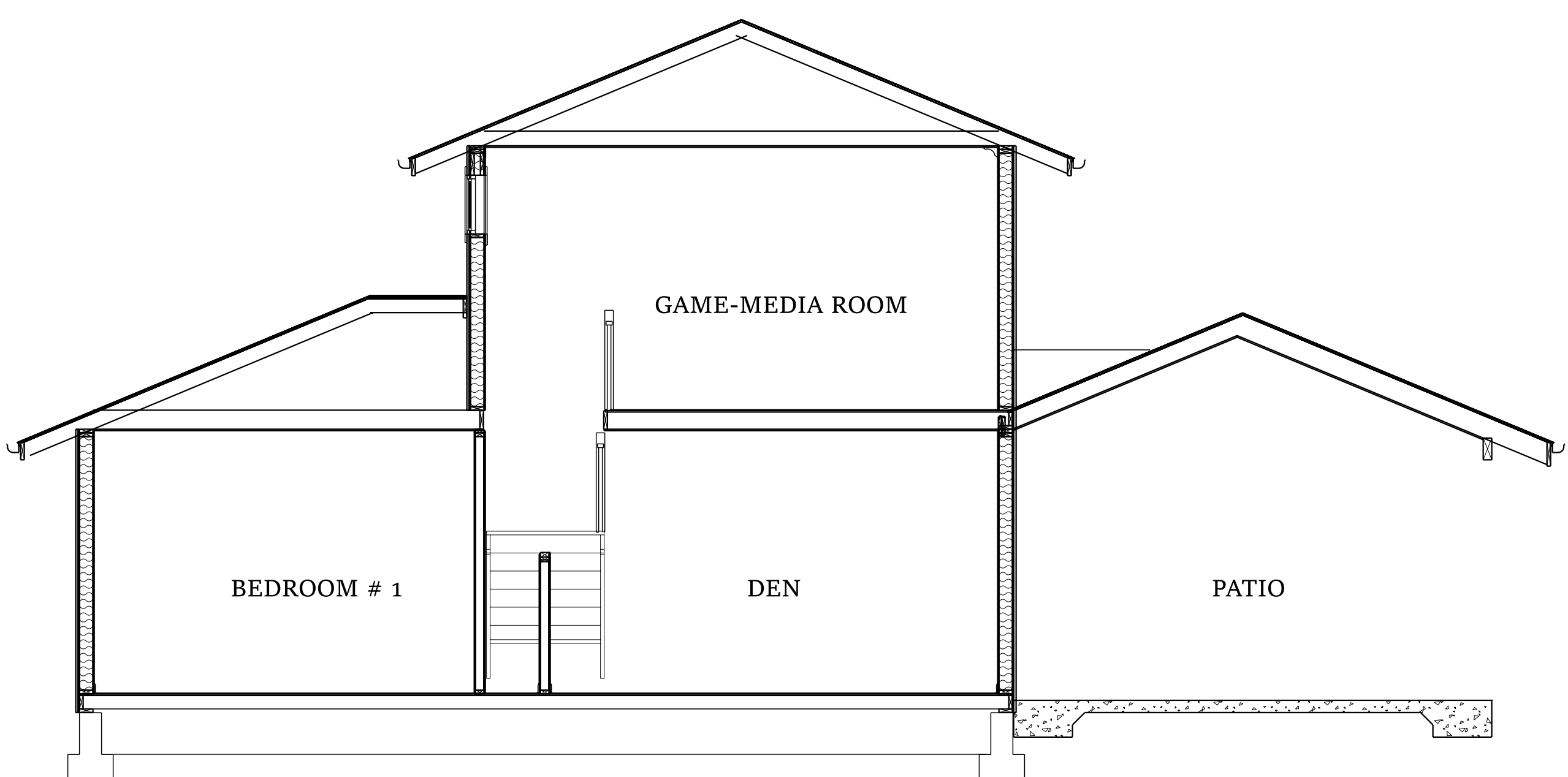
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541 S. Bayview Ave. Sunnyvale, Ca 94086
408-656-5118 chris.allan03@gmail.com

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SECTION - STREETSCAPE




STREETSCAPE
SCALE: 3/16" = 1'-0"



CROSS SECTION C
SCALE: 1/4" = 1'-0"

Address	APN	Stories	Floor Area (s.f.)	Lot Area (s.f.)	FAR
401 E MC KINLEY AV	20911054	1	1804	6250	29%
469 S BAYVIEW AV	20924042	1	6897	21500	32%
296 S BAYVIEW AV	20911055	1	1430	6250	23%
294 S BAYVIEW AV	20911056	1	2028	6250	32%
401 SOUTHWOOD AV	20922016	1	1737	5427	32%
340 S BAYVIEW AV	20922017	1	1199	5346	22%
310 S BAYVIEW AV	20922018	1	1604	5427	30%
403 E OLIVE AV	20923013	1	2645	10320	26%
490 S BAYVIEW AV	20923014	1	2992	9600	31%
476 S BAYVIEW AV	20923015	1	2258	9600	24%
440 S BAYVIEW AV	20923016	1	2440	9350	26%
430 S BAYVIEW AV	20923017	1	1794	5500	33%
420 S BAYVIEW AV	20923018	1	1697	6050	28%
408 S BAYVIEW AV	20923019	1	1687	5400	31%
398 S BAYVIEW AV	20923020	1	1478	5150	29%
390 S BAYVIEW AV	20923021	1	1199	5000	24%
382 S BAYVIEW AV	20923022	1	1521	5000	30%
400 SOUTHWOOD AV	20923023	1	1599	5200	31%
398 E Mc Kinley Av	20924031	1	1164	5850	20%
305 S BAYVIEW AV	20924032	1	2402	6270	38%
335 S Bayview Av	20924034	1	1428	5076	28%
347 S BAYVIEW AV	20924035	1	1764	5076	35%
367 S BAYVIEW AV	20924037	1	1374	5400	25%
369 S Bayview Av	20924038	1	1804	5400	33%
383 S BAYVIEW AV	20924039	1	1225	5400	23%
433 S BAYVIEW AV	20924040	2	2277	5350	43%
445 S BAYVIEW AV	20924041	1	2162	5250	41%
499 S BAYVIEW AV	20924043	1	2051	6435	32%
315 S BAYVIEW AV	20924088	2	2607	4400	59%
325 S Bayview Av	20924089	2	2607	5117	51%
391 S BAYVIEW AV	20924093	2	2494	5761	43%
397 S BAYVIEW AV	20924094	2	2476	5399	46%
407 S BAYVIEW AV	20924095	2	2494	5399	46%
417 S Bayview Av	20924096	2	2476	5399	46%
427 S BAYVIEW AV	20924097	2	2494	5399	46%
398 BISHOP AV	20931032	1	2067	7104	29%
525 S BAYVIEW AV	20931033	1	2520	13130	19%
535 S BAYVIEW AV	20931034	1	3126	9024	35%
539 S BAYVIEW AV	20931035	1	2987	10320	29%
541 S Bayview Av	20931036	2	4838	12120	40%
398 E OLIVE AV	20931055	1	2518	7215	35%
507 S BAYVIEW AV	20931056	1	2220	7104	31%
404 E OLIVE AV	20932001	1	2019	6840	30%
405 KENNEY CT	20932031	1	2310	7625	30%
540 S BAYVIEW AV	20932032	1	2024	8000	25%
534 S BAYVIEW AV	20932033	1	1898	8750	22%

530 S BAYVIEW AV	20932034	1	2252	7875	29%
520 S BAYVIEW AV	20932035	1	1905	7750	25%
508 S BAYVIEW AV	20932036	1	2094	7750	27%
506 S BAYVIEW AV	20932037	2	3615	17400	21%
363 S BAYVIEW AV	20924036	1	1861	5400	34%
		<i>AVERAGE</i>	<i>2174*</i>	<i>7224</i>	<i>32%*</i>

 Proposed Project
Text Adjacent property to Proposed Project
 * Excludes proposed project

Color Board

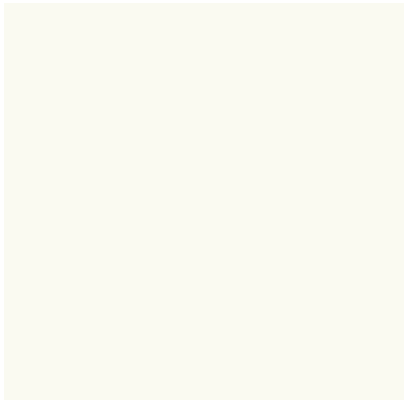
Attachment 7
Page 1 of 1



GRAY COMPOSITE ROOFING



TRIM & FRONT DOOR - BENJAMIN
MOORE - CSP 30 ESPRESSO BEAN



STUCCO - SHERWIN
WILLIAMS- SHOJI WHITE



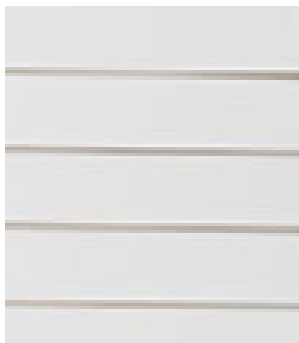
ANDERSON A SERIES & 100
SERIES WINDOWS - BLACK



GARAGE DOOR - WOOD TONE



GRAY METAL ROOFING



SIDING - HARDI PLANK-SW-SHOJI
WHITE



City of Sunnyvale

Agenda Item 2

24-0385

Agenda Date: 2/26/2024

REPORT TO PLANNING COMMISSION

SUBJECT

Proposed Project: Consider actions related to implementation of the 2023-2031 Housing Element to amend the By-Right Approval process for certain parcels within the City:

A. Introduce an Ordinance to Amend By-Right Approvals (Chapter 19.73 of the Sunnyvale Municipal Code) to clarify ministerial review of projects as it relates to proposed subdivisions.

File #: PLNG-2024-0105

Location: Citywide

Applicant: City of Sunnyvale

Environmental Review: Non-discretionary project (Public Resources Code Section 21080(b)(1), CEQA Guidelines Sections 15268, 15357)

Project Planner: Julia Klein, (408) 730-7463, jklein@sunnyvale.ca.gov

REPORT IN BRIEF

General Plan: Various, including Moffett Park Specific Plan, Transit Mixed-Use, El Camino Real Specific Plan, Residential High Density

Existing Zoning: Various, including MP-R, DSP-4, DSP-14, DSP-16, MXD-IV, ECR-MU42, ECR-MU54, Split (ECR-MU54 and ECR-C) and R-4/PD

Existing Site Conditions: Various Uses, including Residential, Office, Commercial and Industrial

Surrounding Land Uses: Various

Issues: Compliance with the Adopted 2023-2031 Housing Element and State Laws and the 60-day review by the State Department of Housing and Community Development.

Staff Recommendation: Recommend to City Council Alternatives 1 and 2:

1. Find that the Action is Exempt from the California Environmental Quality Act (CEQA) pursuant to Public Resources Code Section 21080(b)(1) and CEQA Guidelines Section 15268 and 15357.
2. Introduce an Ordinance in Attachment 2 to amend By-Right Approvals (Chapter 19.73 of the Sunnyvale Municipal Code) to clarify ministerial review of projects as it relates to proposed subdivisions.

BACKGROUND

On January 30, 2024 the City Council adopted an Urgency Ordinance and Introduced an Ordinance that, among other amendments, add By-Right Approvals (Chapter 19.73) to the Sunnyvale Municipal Code. The second reading of the ordinances was held on February 6, 2024 (see link in Attachment 2). The new ordinance section was adopted to address comments from California Department of Housing and Community Development (HCD) and requirements of State Laws as it relates to Housing Element certification. This action was required because the City did not have a certified Housing Element by the statutory deadline of January 31, 2023.

The draft Urgency ordinance was sent to HCD on January 25, 2024 (a few days before the City

Council considered the Urgency and regular ordinances). Staff followed up and sent the adopted Urgency ordinance on January 31, 2024; HCD acknowledged receipt of the ordinance. Overall, the adopted Housing Element met the State criteria. However, on February 12, 2024 HCD provided one comment on the adopted By-Right Zoning regarding the review of ministerial projects as it relates to proposed subdivisions. At this time, the City's Housing Element is not certified as being in compliance. Staff is bringing forward a change to the newly adopted By-Right Approvals (SMC 19.73) section to address HCD's comments.

An Urgency Ordinance will be brought to the City Council on February 27, 2024 with the same changes proposed in Attachment 2. This would be an interim measure until the formal Ordinance contained in this report goes into effect if adopted.

EXISTING POLICY

For more information on applicable City policies see the staff report link in Attachment 3. Below is the most applicable policy from the Housing Element.

HOUSING ELEMENT

- **Policy H-4.5** By-right Housing on Previously Identified Housing Sites. Allow housing developments with at least 20 percent affordable housing by-right, consistent with objective development and design standards, on lower-income sites counted in previous housing cycles, consistent with Government Code Section 65583.2.
- **H2. Rezone Program.** As of the start of the planning period (January 31, 2023), the City has an unmet RHNA of 1,535 lower-income and 3,280 above moderate-income units. The City shall approve a specific plan for the Moffett Park area to accommodate the City's RHNA shortfall by January 31, 2024, and shall ensure that sites rezoned to meet the City's unmet lower-income RHNA meet specific.

ENVIRONMENTAL REVIEW

The proposed action does not require environmental review under the California Environmental Quality Act (CEQA) because it implements a state-mandated program and is therefore not a discretionary project subject to CEQA (Public Resources Code Section 21080(b)(1), CEQA Guidelines Sections 15268, 15357).

DISCUSSION

Housing Element Implementation

As noted above, the adopted Housing Element has not been certified yet. HCD requested one additional change to the By-Right Approval section of the Zoning Code. Specifically, it pertains to the processing of ministerial reviews as they relate to proposed subdivisions. HCD has interpreted the City's adopted code revisions to state that projects that include subdivisions cannot go through the By-Right Approval process. That was not the intent of the City's adopted ordinances. The adopted ordinances do not prevent an applicant from seeking By-Right Approval after a subdivision application is submitted and approved, but the Subdivision Map Act has requirements for public hearings and appeals. Staff have proposed minor changes to adopted ordinances and requested that HCD review the changes before we move forward. City staff has worked with HCD staff to perfect the language of the revisions; the ordinance includes recommended revisions from HCD.

This amendment does not change the list of by-right parcels currently shown in Chapter 19.73; but

would apply to future projects on said parcels. For the list of parcels and maps, refer to January 30, 2024 staff report in Attachment 3.

Adopting Ordinances

The schedule for the Formal Ordinance and Urgency Ordinance is:

- 2/26/2024 - Planning Commission meeting - make recommendation to City Council to amend to Chapter 19.73 (By-Right Approval).
- 2/27/2024 - City Council - Urgency Ordinance; and if adopted, would be effective for 45 days (or thru 4/12/2024).
- 3/19/2024 - City Council meeting - Ordinance introduction to amend Chapter 19.73.
- 3/26/2024 - City Council meeting - 2nd reading of Ordinance to amend Chapter 19.73; and if adopted, would go into effect in 30 days (or on 4/25/2024).
- 4/9/2024 - City Council meeting - Urgency Ordinance Extension; and if adopted, would extend urgency ordinance until 4/25/2024.

The formal Ordinance would go into effect by the time the Urgency Ordinance would expire. These actions would keep the City in compliance with the Adopted Housing Element.

Although it is normally the City's practice to take urgency zoning ordinances to Planning Commission for a recommendation, Government Code Section 65858 allows the City Council to adopt an urgency ordinance "[w]ithout following the procedures otherwise required prior to the adoption of a zoning ordinance." Due to the extreme urgency of this situation, the Planning Commission will only be asked to review the regular (non-urgency) ordinance.

FISCAL IMPACT

No fiscal impact is anticipated due to the recommended action; however, failure to adopt the proposed Ordinances could result in HCD finding the City's Housing Element out of compliance.

PUBLIC CONTACT

Public contact was made by posting the Planning Commission agenda on the City's official-notice bulletin board outside City Hall, Sunnyvale Public Library and Department of Public Safety. Email notifications were sent to interested parties, including housing advocacy organizations and neighborhood groups. The hearing date was included on the City's Housing Element Update webpage and published in the *Mercury News* newspaper. Notices were mailed to property owners and tenants within 300 feet of the sites eligible for By-Right approvals. The agenda and report are available at the Office of the City Clerk and on the City's website.

ALTERNATIVES

Recommend to the City Council:

1. Find that the Action is Exempt from the California Environmental Quality Act (CEQA) pursuant to Public Resources Code Section 21080(b)(1) and CEQA Guidelines Section 15268 and 15357.
2. Introduce an Ordinance in Attachment 2 to amend By-Right Approvals (Chapter 19.73 of the Sunnyvale Municipal Code) to clarify ministerial review of projects as it relates to proposed

subdivisions.

3. Alternative 2 with modifications.
4. Do not Introduce the Ordinance and provide direction to staff on desired changes.

RECOMMENDATION

Recommend to City Council:

1. Find that the Action is Exempt from the California Environmental Quality Act (CEQA) pursuant to Public Resources Code Section 21080(b)(1) and CEQA Guidelines Section 15268 and 15357.
2. Introduce an Ordinance in Attachment 2 to amend By-Right Approvals (Chapter 19.73) to the Sunnyvale Municipal Code.

The proposed Ordinance implements the 2023-2031 Housing Element H2. Rezone Program by amending the By-Right Approval (Chapter 19.73) to clarify that projects with subdivisions would be subject to a ministerial review process and would need to comply with the Subdivision Map Act. Additionally, staff will be bringing a Revised Urgency Ordinance and an Extension to the Urgency Ordinance to City Council to implement the proposed minor changes (as noted above in the Discussion section).

Prepared by: Julia Klein, Principal Planner

Reviewed by: Shaunn Mendrin, Planning Officer

Reviewed by: Trudi Ryan, Director of Community Development

Reviewed by: Connie Verceles, Deputy City Manager

Approved by: Kent Steffens, City Manager

ATTACHMENTS

1. Reserved for Report to Council
2. Draft Ordinance
3. RTC 24-0266, January 30, 2024, By-Right Zoning Introduction
4. HCD Letter Dated February 12, 2024

This page intentionally left blank. Reserved for Report to Council.

ORDINANCE NO. ____

**AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF
SUNNYVALE AMENDING CHAPTER 19.73 (BY-RIGHT
APPROVAL) OF ARTICLE 6 (SPECIAL HOUSING ISSUES)
OF TITLE 19 (ZONING) OF THE SUNNYVALE MUNICIPAL
CODE.**

THE CITY COUNCIL OF THE CITY OF SUNNYVALE FINDS AND DECLARES AS FOLLOWS:

WHEREAS, provisions of the California Government Code require that certain uses be approved as a “use by right” as defined in Government Code Section 65583.2(i); and

WHEREAS, the Housing Element of the City of Sunnyvale provides that the City will adopt an amendment to its zoning code providing for by-right approvals as required by state law; and

WHEREAS, on January 30, 2024, the City Council adopted an urgency ordinance (Ordinance No. 3222-24) and on February 6, 2024, the City Council adopted a regular ordinance (Ordinance No. 3223-24) adding Chapter 19.73 (By-Right Approvals) to the Sunnyvale Municipal Code; and

WHEREAS, the California Department of Housing and Community Development notified that City on February 12, 2024, that the City must make certain changes to the By-Right Approvals ordinance before the City’s Housing element can be certified;

NOW, THEREFORE, THE CITY COUNCIL OF THE CITY OF SUNNYVALE DOES ORDAIN AS FOLLOWS:

SECTION 1. SECTION 19.73.020 AMENDED. Section 19.73.020 (Eligibility) of Chapter 19.73 (By-Right Approval) of Article 6 (Special Housing Issues) of Title 19 (Zoning) of the Sunnyvale Municipal Code is hereby amended as follows.

19.73.020. Eligibility.

The following housing development projects are eligible for by-right zoning approval.

(a) (1) [Text unchanged]

(2) [Text unchanged]

(3) ~~The housing development does not require a subdivision as defined in title 18 of this code or applicable state law.~~

(b) – (c) [Text unchanged]

SECTION 2. SECTION 19.73.030 AMENDED. Section 19.73.030 (Permit requirements) of Chapter 19.73 (By-Right Approval) of Article 6 (Special Housing Issues) of Title 19 (Zoning) of the Sunnyvale Municipal Code is hereby amended as follows.

19.73.030. Permit requirements.

(a) An applicant for a housing development project eligible for by-right zoning approval shall submit a ministerial miscellaneous plan permit for approval of the design pursuant to section 19.82.020(b). No discretionary permit or approval is required. Any subdivision of the sites shall be subject to all laws, including, but not limited to, the local government ordinance implementing the Subdivision Map Act.

(b) Affordable units in the project shall comply with the requirements of chapter 19.67 (Inclusionary Below Market Rate Ownership Housing Program) or chapter 19.77 (Inclusionary Below Market Rate Rental Housing) as applicable.

SECTION 3. CEQA. The adoption of this ordinance does not require environmental review under the California Environmental Quality Act (CEQA) because it implements a state-mandated program and is therefore not a discretionary action subject to CEQA. (Public Resources Code Section 21080(b)(1), CEQA Guidelines Sections 15268, 15357.)

SECTION 4. EFFECTIVE DATE. This ordinance shall be in full force and effect 30 days after adoption.

SECTION 5. PUBLICATION. The City Clerk is directed to cause copies of this ordinance to be posted in three (3) prominent places in the City of Sunnyvale and to cause publication once in The Sun, the official newspaper for publication of legal notices of the City of Sunnyvale, of a notice setting forth the date of adoption, the title of this ordinance, and a list of places where copies of this ordinance are posted, within fifteen (15) days after adoption of this ordinance

Introduced at a regular meeting of the City Council held on March 19, 2024 and adopted as an ordinance of the City of Sunnyvale at a regular meeting of the City Council held on March 26, 2024, by the following vote:

AYES:
NOES:
ABSTAIN:
ABSENT:
RECUSAL:

ATTEST:

APPROVED:

DAVID CARNAHAN
City Clerk
Date of Attestation: _____

LARRY KLEIN
Mayor

(SEAL)

APPROVED AS TO FORM:

REBECCA L. MOON
Interim City Attorney



RTC #: 24-0385

Document Title: RTC 24-0266, January 30, 2024, By-Right Zoning Introduction

Link: <https://sunnyvaleca.legistar.com/LegislationDetail.aspx?ID=6496758&GUID=BF158410-09DA-4C99-8DD1-366B6E3ED789&Options=&Search=>

**DEPARTMENT OF HOUSING AND COMMUNITY DEVELOPMENT
DIVISION OF HOUSING POLICY DEVELOPMENT**

2020 W. El Camino Avenue, Suite 500
Sacramento, CA 95833
(916) 263-2911 / FAX (916) 263-7453
www.hcd.ca.gov



February 12, 2024

Kent Steffens, City Manager
City of Sunnyvale
456 West Olive Ave
Sunnyvale, CA 94086

Dear Kent Steffens:

RE: City of Sunnyvale's 6th Cycle (20213-2031) Adopted Housing Element

Thank you for submitting the City of Sunnyvale's (City) housing element that was adopted on December 12, 2023 and received for review on December 14, 2023. In addition, the California Department of Housing and Community Development (HCD) received Urgency Ordinance 3222-24 for review. Pursuant to Government Code section 65585, HCD is reporting the results of its review.

The adopted element meets the statutory requirements of State Housing Element Law (Gov. Code, § 65580 et seq). The adopted element was found to be substantially the same as the revised draft element that HCD's December 11, 2023 review determined met statutory requirements. However, as noted in the prior review, the housing element cannot be found in substantial compliance until the City has completed necessary rezones that meet statutory requirements as described below.

Specifically, pursuant to Assembly Bill 1398 (Chapter 358, Statutes of 2021), a jurisdiction that did not adopt a compliant housing element within 120 days from the statutory deadline (January 31, 2023) cannot be found in compliance until rezones to make prior identified sites available or accommodate a shortfall of sites to accommodate the regional housing needs allocation (RHNA) are completed pursuant to Government Code sections 65583, subdivision (c)(1)(A) and 65583.2, subdivisions (c), (h) and (i). HCD has reviewed Urgency Ordinance 3222-24 that was completed to temporarily address these statutory requirements. However, the Urgency Ordinance do not appear to meet statutory requirements. For example, the Urgency Ordinance appears to exclude housing developments with a subdivision from by-right approval.

To address these requirements, the City should either submit zoning that meets all requirements pursuant to Government Code section 65583.2, subdivisions (c), (h) and (i) or provide additional documentation to demonstrate recent rezoning complies with these statutory requirements. HCD will review the documentation and issue correspondence identifying the updated status of the City's housing element compliance.

Kent Steffens, City Manager
Page 2

Several federal, state, and regional funding programs consider housing element compliance as an eligibility or ranking criteria. For example, the CalTrans Senate Bill (SB) 1 Sustainable Communities grant, the Strategic Growth Council and HCD's Affordable Housing and Sustainable Communities program, and HCD's Permanent Local Housing Allocation consider housing element compliance and/or annual reporting requirements pursuant to Government Code section 65400. With a compliant housing element, the City will meet housing element requirements for these and other funding sources.

For your information, some general plan element updates are triggered by housing element adoption. HCD reminds the City to consider timing provisions and welcomes the opportunity to provide assistance. For information, please see the Technical Advisories issued by the Governor's Office of Planning and Research at: <https://www.opr.ca.gov/planning/general-plan/guidelines.html>.

HCD appreciates the hard work and dedication the City's housing element team provided throughout the housing element review. HCD is committed to assisting the City in addressing all statutory requirements of State Housing Element Law. If you have any questions or need additional technical assistance, please contact Shawn Danino, of our staff, at Shawn.Danino@hcd.ca.gov.

Sincerely,



Paul McDougall
Senior Program Manager



City of Sunnyvale

Agenda Item 3

24-0141

Agenda Date: 2/26/2024

REPORT TO PLANNING COMMISSION

SUBJECT

Forward a Recommendation to the City Council to Evaluate the Minimum Automobile Off-Street Parking Requirements for Residential Uses (Study Issue CDD 19-07)

File #: 2022-7404

Environmental Review: The action is exempt from the California Environmental Quality Act (CEQA) pursuant to CEQA Guidelines Section 15378(a).

Project Planner: George Schroeder, (408) 730-7443, gschroeder@sunnyvale.ca.gov

REPORT IN BRIEF

The Planning Commission sponsored this study issue in 2018. The original Study Issue was to explore potential reductions in residential automobile parking requirements and future conversions of parking to other uses; the study did not include potential modifications to bicycle parking requirements. In February 2022, the City Council supported the study with a scope limited to reporting Sunnyvale's standards vs. other cities' standards.

Recent changes in state legislation have already impacted the amount of parking that the City can require for certain projects. A reduced parking formula must be applied to any housing project seeking state density bonus provisions. No parking is required for Accessory Dwelling Units (ADU) or any type of residential development within a half mile of major transit stops. Only one covered parking space is required for each new single-family home in Dual Urban Opportunity (DUO) housing projects, including those utilizing an Urban Lot Split (ULS). DUO projects are also exempted from minimum parking requirements if within a half mile of El Camino Real, considered a high-quality transit corridor.

Data for minimum automobile parking requirements were collected for 14 other incorporated cities and towns within Santa Clara County, as well as the County's regulations. Additionally, eight other Bay Area peer jurisdictions were surveyed based on similarities in population and regional context to those of Sunnyvale. Based on the information gathered, the following observations were made:

- Current minimum parking requirements for single and two-family homes in Sunnyvale are higher on average than other jurisdictions, though many require additional uncovered parking through minimum front yard setbacks and required driveways.
- Minimum parking standards for multifamily residential in Sunnyvale are more closely aligned with other jurisdictions, however Sunnyvale's regulations include specific requirements that could increase the total number of spaces required.
- Parking requirements in Area Plans/Specific Plans (referenced collectively as specific plans in this report) are slightly higher than other jurisdictions (however lower than the Citywide standards).

- Demographic data reveals that Sunnyvale has a higher percentage of households *without* cars, fewer households with multiple cars, and fewer residents per household than the cumulative average values of Santa Clara County jurisdictions.

Reasonable reductions in the amount of required parking could assist property owners, and could also:

- contribute to decreased housing costs;
- increase the number of available units;
- avoid an oversupply of parking spaces;
- reduce impervious surface area;
- discourage higher rates of vehicle ownership; and,
- promote climate-friendly modes of transportation such as walking, bicycling, carpooling, and using public transit.

Staff Recommendation: Recommend Alternative 1 to the City Council to direct staff to conduct community outreach and explore amendments to the Sunnyvale Municipal Code (SMC) and applicable Specific Plans parking regulations, to be considered at future hearings. The amendments to be studied should include:

- a. Clarifying parking standards to be objective and without discretionary review.
- b. Single/Two-Family Residential:
 - i. Lowering minimum off-street parking spaces per property, with flexibility in type and arrangement (e.g., covered/uncovered, tandem).
 - ii. Exploring maximum number of parking spaces and allowing tandem parking.
- c. Multi-Family Residential:
 - i. Introducing unbundled parking
 - ii. Adjusting/simplifying the unassigned space requirement (currently dependent on the type of covered assigned space provided)
 - For example, require the minimum parking standards to be a flat rate of 1 space per zero to one-bedroom unit; 1.5 spaces per two to three-bedroom unit; and 2 spaces for units with four or more bedrooms.
 - iii. Examining maximum amount of parking required, except for areas within a half-mile of major transit stops.
 - iv. Allowing flexibility in type (covered, uncovered) and arrangement, including a tandem allowance for up to 100% of units.
 - v. Evaluating use of compact parking spaces.
 - vi. Updating the Limited Street Parking Provisions to specify objective parking requirements in certain conditions.

EXISTING POLICY

Key Goals and Policies are provided below. A more complete list of goals and policies from the Housing Element, Land Use and Transportation Chapter, and other elements of the General Plan which pertain to residential parking are detailed in Attachment 3.

GENERAL PLAN

Housing Element

Goal H-4 Minimize the impact of governmental constraints on the maintenance, improvement, and development of housing.

Land Use and Transportation Element

Goal LT-3.13 Move progressively toward eliminating direct and hidden subsidies of motor vehicle parking and driving, making the true costs of parking and driving visible to motorists.

COUNCIL POLICY**7.3.01 Legislative Management - Goals and Policies**

Policy 7.3B.3 - Prepare and update ordinances to reflect current community issues and concerns in compliance with state and federal laws.

BACKGROUND**Study Issue History**

The Planning Commission first sponsored this study issue (CDD 19-07) on November 12, 2018 with the title of, "Develop Citywide Guidelines or Criteria for Allowing Reduced Parking for Development Projects and for Future Conversions of Parking to other Uses." The Planning Commission asked whether there are circumstances where reduced automobile parking could be appropriate, such as: a multifamily project that may be able to increase the total number of units if parking requirements are reduced, or on a single-family property where the size of an existing one-car garage restricts the total allowable square footage of the house, thereby potentially restricting large or extended families from living together in one dwelling. The study issue did not include potential modifications to bicycle parking requirements.

The study issue was deferred for several years until the City Council's Study Issues/Budget Proposals Workshop on February 17, 2022, when they referred the study issue and reduced its scope to evaluate single-family and multifamily parking standards and to compare the standards to other jurisdictions and direct staff to identify policies from other cities that are worthy of further study. See the study issue paper in Attachment 2.

Brief History of Residential Parking Requirements in Sunnyvale (see Attachment 4 for more details)

- 1946** No parking requirements for any uses.
- 1951** First adoption of parking requirements: one space per residential unit (could be covered or uncovered).
- 1959** Single-family: 2 spaces per unit, plus 1 for each employee living at another location.
Multifamily: 1-1/2 spaces per unit (depending on unit size), plus 1 for each employee living at another location.
- 1975** At least one space per unit in multifamily must be covered.
- 1986** Compact spaces allowed in multifamily developments.
- 1996** Increased parking requirements for both single-family and multifamily.
- 2003** Upgrades for non-conforming single-family parking required when home exceeds four bedrooms or 1,800 square feet.
- 2015** Reduction in unassigned parking, for multifamily, when 2 covered spaces per unit provided; tandem parking allowed for all residential (with limitations on percentage of units).

ENVIRONMENTAL REVIEW

The action being considered does not constitute a project pursuant to CEQA Guidelines Section 15378(a) as it has no potential for resulting in either a direct physical change in the environment or a

reasonably foreseeable indirect physical change in the environment. There are no changes proposed to the SMC or Area/Specific Plan parking requirements at this time. If the Council directs staff to prepare amendments, the amendments would be considered a project, but would still likely be exempt from CEQA because it could be seen with certainty that there is no possibility that the action will have a significant environmental effect. [CEQA Guidelines Section 15061(b)(3)]. Additionally, as affirmed in recent legal precedent, parking is generally not considered an impact on the environment. Future development projects that are subject to the requirements of the amended chapters will be environmentally evaluated on an individual basis.

DISCUSSION

This report reviews the City's current parking standards, provides information on what other cities require, presents demographic information on vehicle availability by household, and provides options to consider, moving forward.

History of Parking Regulations in Sunnyvale

A component of this study issue is to review the historic parking requirements for residential uses in Sunnyvale. Sunnyvale has a long history of reviewing its residential parking requirements. The City's minimum residential automobile parking requirements have trended upward since the original requirement of one space per housing unit in 1951, resulting in greater quantities of required parking; the exception being specific plan areas with more transportation options which have lower parking requirements. Residential parking requirements have also become more complex, depending on different factors such as: bedroom count; covered/uncovered; assigned/unassigned; part of a mixed-use project; and, tandem and mechanical parking structures. Only until recently has a slight degree of flexibility been introduced in the quantity and arrangement of parking spaces, mostly for multifamily residential.

In 2012, the City introduced maximum parking for non-residential uses only. The last update to citywide residential parking requirements was in response to State legislation, in January 2023.

Refer to Attachment 4 for a detailed timeline of the City's modifications to residential parking requirements.

Sunnyvale's Current Residential Parking Requirements

The general parking standards in Chapter 19.46 of the Sunnyvale Municipal Code (SMC) establish minimum required automobile parking for residential properties. There are also separate residential parking standards in most Area Plans and Specific Plans (collectively referred to as "specific plans" in this report). Minimum residential parking requirements are based on a variety of factors depending on land use. The parking requirements are very dependent on many factors which can be confusing to a property owner or developer. More details of the parking requirements for Sunnyvale are provided below. Comparisons of Sunnyvale and other cities are provided in Attachments 5 and 6.

SUMMARY OF PARKING REQUIREMENTS*(excludes State regulations on parking)*

	Sunnyvale
Single family	2 covered +2 uncovered
Upgrade for additions	Y
Tandem	w/limits
Multifamily	1.5 - 2.65+
Tandem	w/limits on %
Type of Parking	Y
# of Bedrooms	Y
Special Housing	0.25 - 2.15
Tandem	Y
# of Bedrooms	Y
Senior (not deed restricted)	1
SRO	0.25 - 1.0
MHP	2
Tandem	Y
DSP Multifamily	0.5-1.0
LSAP	1.0-1.7
ECRSP	1.0 - 2.25
PPSP	Citywide
MPSP	Max only: 0.75-1.0

AB 2097 and 2023 SMC Amendments

Assembly Bill (AB) 2097 is the latest major change in state parking law which went into effect on January 1, 2023, and was incorporated into the SMC shortly after. The law prohibits local agencies from imposing any minimum automobile parking requirements, except for electric vehicle and accessible spaces, for all residential and nonresidential developments (with some exceptions for hotels and event centers) located within one-half mile in a straight-line distance of a major transit stop. In Sunnyvale, the major transit stops include the two Caltrain stations, eight Santa Clara Valley Transportation Authority (VTA) light rail stations (three of which are in Mountain View), and one bus stop in Santa Clara that has a one-half mile buffer that encroaches into Sunnyvale.

El Camino Real meets the state's definition of a high-quality transit corridor but does not currently meet the definition of a major transit stop. The VTA 22 and 522 bus lines have 15-minute headways in the morning and afternoon peak commute periods, but they do not intersect with bus lines going in other directions that also have 15-minute or less headways. If that should ever change, large swaths of single-family, multifamily, and commercial areas would be exempted from minimum automobile parking requirements.

Senate Bill (SB) 330 and SB 8 Objective Standards Requirement

The Housing Crisis Act (SB 330), approved in 2019, and its extension, SB 8, approved in 2021, applies to all-residential developments; mixed-use developments consisting of residential and nonresidential uses with at least two-thirds of the square footage designated for residential use; and

transitional or supportive housing. The law states that where housing is an allowable use, the City cannot impose or enforce design standards on or after January 1, 2020, that are not objective design standards. Objective design standards involve no personal or subjective judgment by a public official and are uniformly verifiable by reference to a uniform criterion available and knowable by both the development applicant and public official before submittal of an application. Any new residential parking requirements established would need to be “objective.”

Single/Two-Family Parking Requirements

Required parking for single-family and two-family dwellings generally follow the citywide standard of four spaces total, two of which must be covered (garage or carport) and not in tandem unless specified existing conditions occur (narrow lot width or significant structural modifications needed to accommodate a side-by-side arrangement). Two uncovered parking spaces on a driveway with minimum dimensions of 17 feet wide by 20 feet in depth and located in front of a garage/carport count as two of the four spaces required.

AB 1308: Parking for Single-Family Home Remodels

The governor approved AB 1308 in 2023, which provides that if an owner of a single-family home seeks to remodel, renovate, or add to the single-family dwelling, and it will not exceed any local maximum size limitations (e.g. height, lot coverage, floor area ratio), then cities cannot increase the minimum parking requirement as a condition of approval.

The current SMC requirement in Section 19.46.050(b) seems to conflict with the new law by requiring dwellings with less than two covered parking spaces to provide an upgrade to two covered spaces when an addition to the home results in four or more bedrooms; or an addition results in a total living area of 1,800 square feet or more. This requirement is not based on exceeding a maximum size limitation for the property.

Accessory Dwelling Unit (ADU) Parking Requirements

In 2019 AB 881 was approved. AB 881 established that, statewide, parking is not required for accessory dwelling units (ADUs) nor could replacement parking be required for conversion of a main dwelling's covered parking (garage or carport) into an ADU. This provision has effectively reduced the total required parking provided (for the main residence and any ADUs) on the same property to two driveway spaces. Some older homes in Sunnyvale only have a one-car driveway, which could be the only parking for those sites, even with the addition of ADUs.

Dual Urban Opportunity (DUO) Housing Parking Requirements

One covered parking space is required for each dual urban opportunity (DUO) housing unit. In addition to waiving minimum parking requirements if the site is within one-half mile of a major transit stop (per AB 2097), minimum parking is also not required if within one-half mile of a high-quality transit corridor. El Camino Real is considered a high-quality transit corridor because it has fixed route bus service with service intervals no longer than 15 minutes during peak commute hours. A map showing properties within one-half mile of major transit stops or bus stops along El Camino Real is available on the City's website at:

<https://gis.sunnyvale.ca.gov/portal/apps/webappviewer/index.html?>

Multifamily Residential Parking Requirements

The Sunnyvale Municipal Code general parking requirement is at least one covered parking space

for each multifamily unit in Sunnyvale. Additionally, unassigned (resident or guest) parking spaces are required based on the number of bedrooms in a unit and the type of parking structure (i.e., private enclosure or open parking available to different residents). The minimum parking requirements range from 1.5/2.25 spaces per studio to one-bedroom unit to 2.15/2.65 spaces per four-bedroom unit, with an additional 0.15 unassigned spaces for each additional bedroom. Up to 10% of the unassigned spaces may be compact.

Tandem parking is allowed for up to 50% of the units in a development and must be assigned to the same unit. Required unassigned spaces must be in a side-to-side arrangement. Mechanical lifts, stackers or other similar means of independent mechanized parking may also satisfy covered assigned space requirements.

Reductions to multifamily residential parking standards require approval of a Variance or Special Development Permit (for sites in a specific plan area or specified Combining District). Parking adjustments are not allowed for residential uses except special housing developments (100% affordable housing developments, senior housing, and housing for persons with disabilities).

Limited Street Parking Provision

There are requirements in SMC 19.46 to provide additional off-street parking when there is limited on-street parking. New single/two-family developments require an additional 0.4 unassigned parking spaces per unit, not on a driveway. For multifamily residential the approval body may also require additional unassigned parking spaces. There is no definition for "limited street parking" and thus is not considered an objective standard as it involves a subjective determination by City staff or the approval body. Historically, staff has calculated adjacent on-street parking and if it is determined to be fewer than 0.4 spaces per unit, additional guest spaces are required as a condition of project approval.

Single-Room Occupancy and Special Housing Development Parking Requirements

Lower parking rates are established for single-room occupancy facilities and special housing developments. The minimum parking requirements for single-room occupancy developments are based on the square footage of the unit, ranging from 0.25 spaces per units less than 200 square feet to 1 space for each unit greater than 250 square feet.

The minimum parking requirements for 100% affordable housing developments range from 1 space per one-bedroom unit to 2.15 spaces per four- or more bedroom unit. Units of any size for seniors or persons with disabilities in all-affordable housing developments require 0.6 spaces. Otherwise, in standard housing, 1 space per unit is required for units for senior citizens or persons with disabilities. Units of any size in assisted living developments require 0.25 spaces per resident.

Rates for special housing developments can be further reduced by making findings for a parking adjustment as specified in Section 19.46.130 of the SMC.

Mobile Home Park Parking Requirements

The minimum parking requirement for mobile home parks is 2 spaces/unit plus 1 space per employee living offsite plus 1 space per special purpose vehicle. The provided parking can be in a tandem parking arrangement.

Automatic Parking Reductions for Projects Qualifying for a State Density Bonus

In order to facilitate the construction of affordable units, the California State Density Bonus Law entitles developers providing affordable housing in accordance with the criteria of the law to automatic reduced parking ratios. The reduced requirements account for lower rates of car ownership in lower-income households and aim for cost reductions in affordable projects. The reduced parking ratios do not count towards the allowable incentives/concessions to development standards that depend on the percentage and type of affordable units provided. Most multifamily residential projects in the City qualify for a density bonus and automatic parking reductions because the City's affordable housing requirement meets or exceeds the thresholds in the density bonus law.

Qualifying projects with a percentage of affordable units are entitled to reduced parking ratios of 1 space per studio and one-bedroom unit, 1.5 spaces per two to three-bedroom unit, and 2.5 spaces per four or more bedroom unit. Further reductions to 0.5 spaces per unit, regardless of bedroom size, are allowed for projects with at least 11% very low-income units or 20% low-income units within one-half mile of a major transit stop with unobstructed access to the stop. If a development includes at least 40% moderate income units within one-half mile of a major transit stop with unobstructed access, the allowable ratio is 0.5 spaces per bedroom. Furthermore, no parking is required for 100% affordable projects (exclusive of manager's units) within one-half mile of a major transit stop with unobstructed access. No parking requirements also apply to special needs housing and rental senior housing (55 years or older) that have either paratransit service or unobstructed access, within one-half mile to fixed bus route service that operates at least eight times per day.

Required spaces under the state density bonus law may be provided through tandem or uncovered parking. Additionally, an applicant may request parking incentives or concessions beyond the reduced parking allowances specified in the law.

Area Plan and Specific Plan Parking Requirements

Specific plans either have their own minimum residential automobile parking requirements or default to the general standards in Chapter 19.46 of the SMC. Parking details not addressed in plan areas (such as parking space dimensions and drive aisle widths) revert to the general standards.

Downtown Specific Plan (DSP)

The DSP has its own minimum residential automobile parking requirements. For both single-family and multifamily, one covered assigned space is required per unit. One uncovered space is also required for single-family. Depending on the bedroom type of a multifamily unit, additional unassigned spaces are required, ranging from 0.5 per studio/one bedroom unit to 1 per three bedroom or larger unit. When two assigned spaces are provided for multifamily units, the number of unassigned spaces may be reduced by 25% for each unit. Tandem is allowed for up to 100% of units in multifamily, provided they are assigned to the same unit.

El Camino Real Specific Plan (ECRSP)

The ECRSP has minimum and maximum parking requirements, differentiated by the two types of locations - nodes (near transit, and where greater activity is expected) and segments, which connect the nodes with fewer transit connections. For node properties, the minimum for studio/one-bedroom units is 1 space/unit with a maximum of 1.5 spaces/unit. The minimum for two-bedroom units is 1.25 spaces/unit, the minimum for three-bedroom units and greater is 1.7 spaces/unit, and the maximum for two-bedrooms and greater is 2 spaces/unit. The studio/one-bedroom unit requirement for segment properties is the same as the nodes. The minimum requirement for two-bedroom units within segments is 1.5 spaces/unit and 2 spaces/unit for three bedroom and greater units. The

maximum allowance for two-bedroom and greater units within segments is 2.25 spaces/unit.

Lakeside Specific Plan

This plan applies to the site on the south side of Lakeside Drive, north of the artificial lake behind properties on Oakmead Parkway. The residential portion of this plan was recently built out, and no additional residential development is expected to occur. The plan's residential parking requirement defaults to the SMC parking standards.

Lawrence/101 Specific Plan

This plan applies to the site to the west of the Lakeside Specific Plan, bounded by Lawrence Expressway to the west, US-101 to the north, Lakeside Drive to the east, and Oakmead Parkway to the south. This plan is also built out with no significant new development to occur. The plan requires 2.25 spaces per residential unit, regardless of the bedroom type. There are also limitations on provided surface parking.

Lawrence Station Area Plan (LSAP)

The LSAP includes minimum and maximum parking requirements with no separate requirement for unassigned spaces. The standards are the same for node properties within the ECRSP.

Moffett Park Specific Plan (MPSP)

The MPSP includes different residential automobile parking requirements than the rest of the City and is also unique in that the plan does not require minimum parking; it only sets maximums on parking supply. Projects may exceed maximums by up to 50% of the maximum ratio, provided that all of the additional spaces over the maximum are shared with the public or other entities at all times. Parking maximums are to be phased in over time where more parking would be built up front through a tiered approach to maximums. The maximum residential parking ratio at plan adoption is 1 space per unit with future maximums of 0.75 spaces per unit. Tandem is allowed for up to 100% of units in multifamily, provided they are assigned to the same unit or an automated vehicle release is provided at all hours of operation.

Peery Park Specific Plan (PPSP)

The PPSP residential parking requirements generally default to the SMC standard, with allowances for reductions in minimum parking through a transportation demand management (TDM) plan or other adequate survey data as requested by the City.

MTC Transit-Oriented Communities (TOC) Policy for Parking Maximums

In 2022, the Metropolitan Transportation Commission (MTC), the Bay Area's regional transportation planning agency, adopted the Transit-Oriented Communities (TOC) Policy. The policy is rooted in Plan Bay Area 2050, the region's Long Range Transportation Plan/Sustainable Communities Strategy and in Sunnyvale it applies to areas within one-half mile of Caltrain and VTA light rail stations. One component of the policy is parking management to support higher transit ridership. For new residential development, the policy specifies parking maximums of 1 space per unit or lower. This requirement has not been reflected in the SMC, DSP, or LSAP, but it was addressed in the recent comprehensive update to the MPSP. The City may be eligible for certain types of funding from MTC if in compliance with the MTC TOC policy.

Shared Parking

Shared parking is the use of a parking space to serve two or more individual land uses on the same

property or across different properties without conflict, typically when the peak parking period for each use does not overlap. This arrangement reduces the number of parking spaces provided. For example, office uses see peak parking demand during the weekday daytime whereas residential parking is most needed in the evening, night and weekends. Rather than providing distinct parking spaces for each land use, the same parking supply can be used by office employees during the day and residents in the evenings and at night.

Shared parking is permitted on a citywide basis with a discretionary permit such as a Use Permit, Plan Review Permit, or Special Development Permit. Shared parking is also allowed in each of the major specific plans with varying nuances. A parking analysis and management plan are typically required for shared parking proposals.

The DSP, ECRSP, LSAP, and PPSP require or give the deciding body discretion to require a shared parking agreement for shared parking proposals. However, the discretionary procedures are not objective.

AB 894: Shared Parking

The governor approved AB 894 in 2023, which allows underused parking lots to satisfy parking requirements on a different site. There is specified criteria for when cities must accept shared parking agreements between sites. Prior to acting on a shared parking agreement, if the parties to the agreement include developments of at least 10 residential units, cities must notify all property owners within 300 feet, and if a request for a meeting is received within 14 days, hold a public meeting. However, if cities adopt a shared parking agreement ordinance, the noticing and hearing requirements would be avoided. Cities may also include requirements for recording the shared parking agreements against the affected parcels and for the content of the agreements. Prior to AB 894 Sunnyvale has approved off-site locations for required parking through the Variance procedure (probably less than 10 instances).

Parking Management Plans

Parking management plans are designed to manage the use of parking on a property. They can include demand management strategies such as valet parking, off-site employee parking, and shared parking agreements. These plans are required for all new multifamily and mixed-use development throughout the City, and existing developments when shared or mechanized parking is proposed. Parking management plans are reviewed by the Director of Community Development with a staff-level miscellaneous plan permit (MPP) or by the hearing body reviewing the associated permit for the development.

Unbundled Parking

Unbundling parking separates the cost to rent or own a parking space from the cost to own or lease the property or unit. The MPSP requires unbundled parking, stating all parking spaces shall be leased or sold separately from the rental or purchase fees for the individual units in perpetuity. Affordable units which include financing requirements may be granted an exception. There is also a policy in the LSAP to allow unbundling, provided that every unit has the option to access at least one parking space.

AB 1317: Unbundled Parking

The governor approved AB 1317 in 2023 which requires new residential rental properties with 16 or more units that are completed after January 1, 2025 to unbundle parking from rent. Tenants of these

properties are given right of first refusal to parking spaces provided for their unit. The law applies to ten specified counties, including Santa Clara County. The requirement does not apply to 100% affordable housing projects or to multifamily projects with individual garages such as townhomes and rowhouses. Cities are not required to enforce this law as it is a civil matter between tenants and landlords.

Electric Vehicles

The City's Reach Codes are intended to exceed the state's energy codes to accelerate the reduction of greenhouse gases. The Reach Codes specify electric vehicle parking requirements for new construction. For each single-family, duplex, and townhome unit, one Level 1 outlet (standard outlet) and one Level 2 ready circuit (outlet for an EV charger) is required. Seventy percent of unassigned spaces for townhomes must have Level 1 ready circuits and the remaining 30% shall have Level 2 ready circuits. All other multifamily units follow the same requirement of 70% Level 1 outlets and 30% Level 2 ready circuits.

Pre-wiring (Level 1 or 2 EV-ready) is required for all new construction, with charging stations not necessarily required. For single/two-family and townhomes with private garages, a Level 2 EV-ready space is required for one space with another Level 1 EV-ready space if two spaces are provided per unit. Thirty percent of unassigned spaces are required to be provided with at least one Level 2 EV-ready space. In addition, each remaining unassigned parking spaces must be provided with at least a Level 1 EV ready space. Multifamily shared parking facilities are required to provide at least one Level 2 EV-ready space for 30% of the dwelling units with parking spaces. In addition, each remaining dwelling unit with parking space(s) shall be provided with at least a Level 1 EV-ready space.

Specific plan areas default to the SMC standard, except for the MPSP. The MPSP specifies Level 2 EV Ready parking spaces for 30% of unbundled spaces with an additional 30% Level 1 EV Ready spaces; or in accordance with the CBC Tier 2, whichever is more stringent.

Compact Parking

The City's general parking requirements in SMC Chapter 19.46 allow multifamily residential uses with parking lots of ten or more spaces to provide compact spaces (7.5 feet wide by 15 feet in depth) for up to ten percent of the total number of unassigned spaces. However, compact parking is prohibited in mobile home parks, non-residential and mixed-use developments.

Overview of Other Jurisdictions' Parking Requirements

Data for minimum parking requirements were collected for the 14 other incorporated cities and towns within Santa Clara County, as well as the County's regulations for unincorporated areas, using online accessible municipal codes. Eight other Bay Area peer jurisdictions were selected based on population and regional similarities to Sunnyvale; these were Concord, Daly City, Fremont, Hayward, Redwood City, Richmond, San Mateo, and Walnut Creek. Data for their minimum parking requirements were also collected using online accessible municipal codes. All included agencies were contacted to confirm the preliminary data collected for their jurisdictions, and revisions were made based on their reviews.

Minimum parking requirement data for other jurisdictions' single/two-family and multifamily dwellings are presented in the tables below. Many other jurisdictions do not have a specified

covered/uncovered parking space requirement like Sunnyvale, only total spaces per unit. Similarly, many other jurisdictions do not have an assigned/unassigned multifamily residential requirement like Sunnyvale. Other jurisdictions' parking requirements also do not follow Sunnyvale's requirements based on parking structure type (e.g. individual garages or open parking). To better compare the data, average and minimum/maximum total spaces per unit are shown in the tables below. Refer to Attachment 5 (single/two-family) and Attachment 6 (multifamily) for data from each jurisdiction, as well as notes further explaining intricacies in the requirements.

Table 1 – Single/Two-Family Residential Minimum Parking Requirements

	Total Spaces per Unit (Averages)	Total Spaces per Unit (Min/Max)
Sunnyvale	4.00	4.00
Other SCC Jurisdictions	2.27-2.60	0.00-6.00
Peer Jurisdictions	1.88-3.25	1.00-6.00

Table 2 - Multi-Family Residential Minimum Parking Requirements (Citywide)

	Total Spaces per Unit (Averages)	Total Spaces per Unit (Min/Max)
Sunnyvale	1.50-2.65	1.00-2.65
Other SCC Jurisdictions	1.31-1.96	0.00-3.00
Peer Jurisdictions	1.28-2.13	0.50-2.50

Table 3 - Multi-Family Residential Minimum Parking Requirements (Specific Plans)

	Total Spaces per Unit (Averages)	Total Spaces per Unit (Min/Max)
Sunnyvale	1.17-2.08	1.00-2.25
Other SCC Jurisdictions	0.95-1.63	0.00-3.00
Peer Jurisdictions	1.09-1.49	0.00-2.00

The results indicate that, on average, Sunnyvale's minimum residential parking requirements are higher for both single/two-family residential and multifamily residential developments than jurisdictions both within Santa Clara County and comparable Bay Area peer jurisdictions. The discrepancy is most pronounced for single/two-family development where Sunnyvale maintains a requirement of four total spaces per unit, while most other agencies within the county require a total of two spaces. Only Cupertino, Los Altos Hills, and Monte Sereno require four or more spaces for every single-family home.

While Sunnyvale's minimum residential parking standards for single/two-family homes are higher on average, it is not uncommon for the other studied jurisdictions to impose other requirements which effectively provide additional parking spaces above the minimum. Cities in Santa Clara County require an average 23-foot front yard setback, which allows space for front driveway parking. Though, many other jurisdictions specify that required parking spaces are not counted within the required front yard, negating credit for front driveway parking. This pattern is generally true of the studied peer jurisdictions as well. In Walnut Creek, for example, only two covered parking spaces are required per single/two-family home, but their regulations effectively provide four spaces. This is because a minimum 18-foot long driveway is also required in addition to the two covered spaces, allowing space for two uncovered parking spaces on the driveway. However, credit is not given for front driveway parking because Walnut Creek specifies the required parking spaces may not occupy any portion of a required yard.

Sunnyvale's total parking requirements for multifamily residential units are closer to the average range for Santa Clara County and peer jurisdictions, however there is greater variation in other jurisdictions' multifamily parking requirements. Most require at least one assigned or covered space per unit, but the requirements for additional guest/unassigned spaces often vary based on the zoning, floor area, unit bedrooms, or total number of units in the development. Sunnyvale's parking regulations are unique in that the number of required parking spaces depends on the type of covered assigned space provided. Less unassigned parking is required when two assigned spaces are provided per unit, or when one assigned space per unit is provided in a carport or open parking structure. More unassigned parking is required when one assigned space is provided per unit in an individual fully-enclosed garage. This is because carports and open parking are more likely to be used to park cars than individual garages, which often are used for storage. Most other jurisdictions simply prescribe required parking ratios based on the bedroom count of a unit and then add on a percentage or additional ratio for guest/unassigned spaces.

Demographic Data

Demographic data was obtained from the U.S. Census Bureau's American Community Survey (ACS) to examine vehicle ownership rates and average household sizes for Sunnyvale and the other studied jurisdictions. Compared with other jurisdictions, Sunnyvale has a higher percentage of households with no vehicles available, fewer households with more than one vehicle, and fewer people occupying housing units. The comparison of this demographic data and minimum parking requirements between the different jurisdictions may warrant a reduction to Sunnyvale's minimum residential parking requirements. Table 5 below shows the overall summary. The full demographic data broken down by jurisdiction is included in Attachments 7 and 8.

Table 5 – Demographic Data

	Households with No Vehicle Available	Households with More than One Vehicle Available	Average Persons per Household
Sunnyvale	6.5%	58.3%	2.69
Other SCC Jurisdictions	4.1%	71.2%	2.84
Peer Jurisdictions	6.5%	61.6%	2.84

Source: ACS Community Survey, 5-year Averages, 2021

State Law's Impact on Residential Parking Requirements

As noted in the Background section, the State of California passed recent legislation with limitations on the quantity of parking local agencies can require in certain circumstances, such as when sites are near major transit stops or particular housing products (affordable housing, ADUs and DUOs). Additionally, any new requirements local agencies impose on residential parking standards must be "objective," where reasonable people could not reach different conclusions as to how to comply with the standard. Construction of parking spaces can be costly to developers, limits single-family home expansions, and has been cited as a deterrent to housing construction throughout the state. The state has also set targets for carbon emission reduction which will be difficult to meet if driving gas-powered vehicles are still the predominant form of transportation.

Options to Consider

Moderately reduce required parking minimums for residential development

Code updates could be considered (Citywide standards and within specific plan areas) for reducing minimum residential parking standards. A reduction would make Sunnyvale's standards more comparable with other jurisdictions, reduce housing costs, reflect lower vehicle ownership rates, and encourage use of active and public transportation. The updates would also include other revisions in accordance with recent state law (shared parking - AB 894 and unbundled parking - AB 1317) and refinements or deletions of various standards to ensure they are objective. Amendments would also address the three Housing Element programs associated with parking (Programs H21, H22 and H23, see Attachment 3 for more details).

For single-family and two-family developments, the City could consider a minimum of two total off-street spaces. These spaces could be configured in an uncovered or covered format, or in a combination thereof (such as a one-car garage and one driveway space). The two parking spaces might also be in a side-by-side or tandem arrangement. This approach would more closely align with other jurisdictions and allow more flexibility for property owners. Such a revised standard would be higher than the DUO requirement of one covered space per unit, or it could also result in no covered parking at all. The reduction would remove the current requirement for properties with less than two covered parking spaces to upgrade for certain remodels, which would also align with recent AB 1308. Staff further recommends not pursuing a maximum amount of parking for single/two-family developments.

The consideration that staff is suggesting for multifamily closely follows the flat-rate reduced parking already allowed by the State Density Bonus Law, with a preliminary recommendation for four or more bedrooms as slightly lower than the State maximum (i.e., 2.0 vs State 2.5). Most multifamily projects would qualify for reduced parking under the State Density Bonus Law because the City's affordable requirements exceed the percentages needed to qualify for a density bonus. No additional unassigned parking would be required in addition to the flat rate and parking spaces would not be specified to be assigned or covered. Preliminarily, the requirements could be:

Size of Unit	Minimum Number of Parking Spaces
Zero to one-bedroom	1.0
Two to three-bedroom	1.5
Four or more bedroom	2.0*

**instead of two and one-half spaces per State Density Bonus law*

The reductions would also apply to Specific Plans, single-room occupancy, and special housing developments that currently require more parking. Compact parking spaces would be evaluated in the analysis with options to consider, such as eliminating the use of compact spaces, reducing the allowed amount, and only allowing compact through an adjustment or variance. Staff also recommends exploring an increase to the tandem parking allowance from 50% to 100%, which is what is currently in place for DSP properties. The recommended requirements would simplify the City's multifamily parking standards and reflect the trend of reduced parking minimums in the interest of increasing the supply of housing.

Eliminate required parking minimums for residential development

The Council may want to consider eliminating required parking for residential uses altogether. This approach has been utilized by the City of San Jose, which instead requires Transportation Demand Management (TDM) plans for all new projects, exempting most small projects including single-family homes. Alameda, Berkeley, Emeryville, and San Francisco have eliminated minimum parking requirements for most uses and implemented parking maximums instead. Many residential properties in Sunnyvale already qualify for no parking requirements under AB 2097, and many more will be exempted if bus frequency along El Camino Real someday meets the definition of a major transit stop. An elimination or reduced requirement for minimum parking does not prevent additional parking from being built at the applicant's discretion. It is possible that a reduction or elimination of required off-street parking may increase demand for street parking or the need for permit parking. Permit parking has added enforcement costs to the City. While perfectly legal, many Sunnyvale residents have complained about parked cars in their neighborhood belonging to residents of other neighborhoods (or belonging to non-residents); the concerns are that it impacts the availability of street parking for visitors, deliveries and personal vehicles (that cannot be parked on their property). The City could also consider phasing in the elimination of parking based on the availability of other transportation and lower vehicle ownership rates.

Maintain current parking regulations

The collected data indicates that Sunnyvale's minimum residential parking requirements are higher on average than those of other jurisdictions in Santa Clara County and the greater Bay Area, despite having lower vehicle ownership rates and lower household sizes. However, if the City Council finds

that current minimum parking requirements are sufficient, recent state law reductions already in effect, and the potential impacts of reducing the amount of required parking, the Council may decide to make no changes to minimum residential parking requirements at this time.

FISCAL IMPACT

No fiscal impact is anticipated due to the recommended action. However, if the City Council directs changes that require detailed parking studies, a consultant that specializes in parking requirements may need to be hired, necessitating a budget modification.

PUBLIC CONTACT

Public contact was made by posting the Planning Commission agenda on the City's official notice bulletin board outside of City Hall, Sunnyvale Public Library, and Department of Public Safety. Notices were also posted in the *Sun* newspaper and the City's website.

ALTERNATIVES

Recommend that the City Council:

1. Direct staff to conduct community outreach and prepare amendments to the Sunnyvale Municipal Code (SMC) and applicable Specific Plans parking regulations, to be considered at future hearings. The amendments to be studied include:
 - a. Clarifying parking standards to be objective and without discretionary review.
 - b. Single/Two-Family Residential:
 - i. Lowering minimum off-street parking spaces per property, with flexibility in type and arrangement.
 - ii. Exploring maximum number of parking spaces and allowing tandem parking.
 - c. Multi-Family Residential:
 - i. Introducing unbundled parking.
 - ii. Adjusting/simplifying the unassigned space requirement (currently dependent on the type of covered assigned space provided).
 1. For example, require the minimum parking standards to be a flat rate of 1 space per zero to one-bedroom unit; 1.5 spaces per two to three-bedroom unit; and 2 spaces for units with four or more bedrooms.
 - iii. Examining maximum amount of parking required, except for areas within a half-mile of major transit stops.
 - iv. Allowing flexibility in type (covered, uncovered) and arrangement, including a tandem allowance for up to 100% of units.
 - v. Evaluating continued use of compact parking spaces.
 - vi. Updating the Limited Street Parking Provisions to specify objective parking requirements in certain conditions.
2. Alternative 1 with modifications.
3. Do not modify parking requirements.

STAFF RECOMMENDATION

Recommend Alternative 1 to the City Council to direct staff to conduct community outreach and prepare amendments to the Sunnyvale Municipal Code (SMC) and applicable Specific Plans parking regulations, to be considered at future hearings. The amendments to be studied include:

- a. Clarifying parking standards to be objective and without discretionary review.
- b. Single/Two-Family Residential:

- i. Lowering minimum off-street parking spaces per property, with flexibility in type and arrangement.
- ii. Exploring maximum number of parking spaces and allowing tandem parking
- c. Multi-Family Residential:
 - i. Introducing unbundled parking.
 - ii. Adjusting/simplifying the unassigned space requirement (currently dependent on the type of covered assigned space provided).
 - 1. For example, require the minimum parking standards to be a flat rate of 1 space per zero to one-bedroom unit; 1.5 spaces per two to three-bedroom unit; and 2 spaces for units with four or more bedrooms.
 - iii. Examining maximum amount of parking required, except for areas within a half-mile of major transit stops.
 - iv. Allowing flexibility in type (covered, uncovered) and arrangement, including a tandem allowance for up to 100% of units.
 - v. Evaluating continued use of compact parking spaces.
 - vi. Updating the Limited Street Parking Provisions to specify objective parking requirements in certain conditions.

Reasonable reductions in the amount of required parking not only assists property owners, but also contributes to decreased housing costs, increases the number of available units, avoids an oversupply of parking spaces, reduces impervious surface area, discourages higher rates of vehicle ownership, and promotes climate-friendly modes of transportation such as walking, bicycling, carpooling, and taking transit.

Directing staff to study policy updates based on Alternative 1 would allow property owners of single/two-family dwellings to more easily expand and remodel without the constraint of accommodating a total of four parking spaces onsite. Reducing the area dedicated to parking allows more space available for ADUs and extended families living together in one house. It also would align the City's standards with those of other jurisdictions within Santa Clara County and throughout the Bay Area.

The recommended reduction in minimum requirements for multifamily developments would have a limited impact overall, since reduced parking requirements are already in place for certain Specific Plans, areas within one half-mile of transit, and projects utilizing density bonus reduced parking allowances. The tradeoff for such a reduction is likely fewer unassigned/guest parking spaces onsite.

Housing Element programs related to parking (Programs H21, H22 and H23) could be combined with the follow up study items on parking.

Prepared by: George Schroeder, Principal Planner
Reviewed by: Shaunn Mendrin, Planning Officer
Reviewed by: Trudi Ryan, Director of Community Development
Reviewed by: Connie Verceles, Deputy City Manager
Approved by: Kent Steffens, City Manager

ATTACHMENTS

- 1. Reserved for Report to Council

2. Study Issue Paper, CDD 19-07
3. General Plan Goals and Policies
4. Residential Parking Requirements Timeline
5. Single/Two-Family Parking Data for Other Jurisdictions
6. Multifamily Parking Data for Other Jurisdictions
7. Demographic Data for Santa Clara County Jurisdictions
8. Demographic Data for Peer Jurisdictions

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City of Sunnyvale

Agenda Item

22-0353

Agenda Date: 3/22/2022

2022 COUNCIL STUDY ISSUE

NUMBER

CDD 19-07

TITLE Evaluate the Minimum Parking Requirements for Residential Uses

BACKGROUND

Lead Department: Community Development

Support Departments: Office of the City Manager
Office of the City Attorney

Sponsor(s): Planning Commission

History: 1 year ago: Deferred by City Council
2 years ago: Deferred by Planning Commission

SCOPE OF THE STUDY

What precipitated this Study?

The general parking standards in the Sunnyvale Municipal Code (SMC) establish required parking for residential and non-residential development based on a variety of factors. For residential uses, the number of bedrooms, the number of assigned spaces to a dwelling unit, and the type (i.e., private enclosure or open) also affect the requirements for parking. Lower parking space rates or potential adjustments are established for specified locations in the City, affordable housing, senior housing, and housing for persons with disabilities.

Reductions (if not covered by an adjustment) to the parking standards require approval of a Variance or approval of a Special Development Permit (only allowed within specified zoning districts). The Planning Commission has asked if there are circumstances where reduced parking could be appropriate, such as: a multi-family project that may be able to increase the total number of units if parking requirements are reduced, or on a single-family property where the size of an existing one-car garage restricts the total allowable square footage of the house, thereby potentially restricting large or extended families from living together in one dwelling.

At the February 17, 2022, Study Issues/Budget Proposals Workshop, the City Council supported reducing the scope of the original Study Issue CDD 19-07, *Develop Citywide Guidelines or Criteria for Allowing Reduced Parking for Development Projects and for Future Conversions of Parking to Other Uses*, to evaluate only the parking standards for residential uses and to compare them to similar jurisdictions.

What are the key elements of the Study?

There are certain areas within the City where parking standards are reduced compared to the generic citywide standards (e.g., Downtown Specific Plan, Lawrence Station Area Plan). Generally, the areas with reduced parking standards are located near major transit stations, but reduced parking

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standards have also been considered in other areas if a project can demonstrate other trip reduction strategies. Changes in State legislation, coupled with new types of building allowances in single-family zoning districts, also make it an appropriate time to evaluate and compare all residential parking standards to determine if the City has other general parking standards that may be appropriate to reduce.

This reduced Study would include:

- Evaluation of the City's current residential parking regulations in comparison to other jurisdictions;
- Review of the historic parking requirements for residential uses in Sunnyvale;
- Examination of the covered parking requirement for single-family zoning districts; and,
- Consideration of the pros and cons of reducing parking requirements for residential uses.

The analysis and information from this reduced study would be presented to the City Council for further direction and potential future phasing of other parking related studies.

Estimated years to complete study: 1 year

FISCAL IMPACT

Cost to Conduct Study

Level of staff effort required (opportunity cost):	Moderate
Funding Required for Non-Budgeted Costs:	\$0
Funding Source:	N/A

The comparison and evaluation of residential parking standards with other comparable jurisdictions would be completed by staff and would not require outside consultant assistance.

Cost to Implement Study Results

Unknown. Future phases and studies may require the hiring of a consultant who specializes in parking requirements, parking policies, design guidelines, and has specialized knowledge in the parking industry

EXPECTED CITY COUNCIL, BOARD OR COMMISSION PARTICIPATION

Council-Approved Work Plan: No

Council Study Session: No

Reviewed by Boards/Commissions: Bicycle and Pedestrian Advisory Committee, Planning Commission

STAFF RECOMMENDATION

Support. This policy issue merits discussion at the 2022 Study Issues Workshop.

Staff believes that evaluating the existing residential parking regulations and comparing the City's regulations with other jurisdictions could be a valuable study and will provide useful information to the Planning Commission and City Council for consideration in future studies and recommendations on parking reductions.

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Prepared by: Amber Blizinski, Principal Planner
Reviewed by: Shaunn Mendrin, Planning Officer
Prepared by: Trudi Ryan, Director, Community Development
Reviewed by: Teri Silva, Assistant City Manager
Approved by: Kent Steffens, City Manager

Key goals, objectives, and policies from the General Plan and Council Policy Manual are listed below:

General Plan Housing Element

Goal H-4 Minimize the impact of governmental constraints on the maintenance, improvement, and development of housing.

The sixth cycle Housing Element contains several policy programs intended to implement reductions to parking requirements including:

Program H21 – Missing Middle Housing Reduce or eliminate parking minimums for projects within a half mile of public transit, pursuant to Government Code Section 65863.2.

Program H22 – Adaptive Reuse Consider regulatory incentives or waivers related to parking for adaptive reuse projects.

Program H23 – Zoning Code Amendments Establish appropriate parking standards for residential care homes and identify clear parking requirements for emergency shelters consistent with Government Code 65583. Provide clear and transparent parking standards for mixed-use developments outside of specific plan areas. Review and revise Single Room Occupancy (SRO) regulations to remove the minimum parking requirement and remove the maximum occupancy limitation, as necessary for consistency with the California Building Code. Reduce parking requirements for studio and efficiency units to no more than one parking space per unit, inclusive of guest parking. Remove minimum parking requirements for most developments within one-half mile of transit, pursuant to Government Code Section 65863.2.

General Plan Land Use and Transportation Element

Policy LT-1.1c Actively monitor and participate in activities of nongovernmental organizations that influence regional land use and transportation planning such as the Silicon Valley Leadership Group, Sustainable Silicon Valley, and the Bay Area Economic Forum. Consider more standardized land use policies in the region, such as parking standards, to promote equity between cities.

Goal LT-2 Environmentally Sustainable Land Use and Transportation Planning and Development – Support the sustainable vision by incorporating sustainable features into land use and transportation decisions and practices.

Policy LT-3.1b Establish reduced parking requirements for transit, corridor, and village mixed-use developments and for developments with comprehensive TDM programs that are consistent with the City's established goals.

Goal LT-3.9 As parking is the temporary storage of transportation vehicles, do not

consider parking a transport use of public streets.

Goal LT-3.10 Prioritize street space allocated for transportation uses over parking when determining the appropriate future use of street space.

Goal LT-3.13 Move progressively toward eliminating direct and hidden subsidies of motor vehicle parking and driving, making the true costs of parking and driving visible to motorists.

Policy LT-3.13a Pursue opportunities for user fees such as paid parking, paid parking permits at workplaces, and paid parking places for on-street parking in transit-rich residential neighborhoods, and promote corporate parking cash-out programs.

Policy LT-3.13c Advocate at the regional, state, and federal levels for actions that increase the visibility of the true costs of parking and driving to motorists and improve the cost return attributable to driving.

Council Policy Manual

Council Policy 7.3.01 Legislative Management – Goals and Policies

Policy 7.3B.3 - Prepare and update ordinances to reflect current community issues and concerns in compliance with state and federal laws.

Sunnyvale Residential Parking Requirements Timeline

Year	Ordinance	Summary of Changes
1946	251-46	No parking requirements for any uses.
1951	324-51	First adoption of parking requirements: one space per residential unit (could be covered or uncovered)
1959	714-59	<ul style="list-style-type: none"> Single-family: 2 spaces per unit, plus 1 for each employee living at another location. Multifamily: 1.5 spaces per unit (depending on unit size), plus 1 for each employee living at another location.
1963	1085-63	Required two-car garages for single-family.
1966	1319-66	<ul style="list-style-type: none"> Increased multifamily parking requirement to include one space per each employee living at another location. Increased parking for two-bedrooms and above to 2 parking spaces per unit.
1972	1661-72	Multifamily parking regulations modified (1.5 spaces per studio/one-bedroom, 1.75 per two-bedroom, 2 per three or more bedrooms).
1975	1786-75	Specified that at least one required parking space for multifamily residential be covered.
1979	1949-79	Clarified that parking is allowed in front and side yards of single-family residential provided it is for licensed operable vehicles and on a stabilized permanent surface. Parking areas limited to no more than 50% of the front yard.
1986	2165-86	<ul style="list-style-type: none"> Text clarifications to multifamily parking requirements, but no increases in spaces per unit. Allowance of up to 35% of total unassigned spaces for multifamily residential may be compact.
1988	2231-88	Updated multifamily parking regulations (2 spaces per two-bedroom unit).
1988	2236-88	<ul style="list-style-type: none"> Clarified that 2 covered parking spaces are required for single-family in either a garage or carport. Required one-for-one covered replacement parking for converted garages. Required 2 covered parking spaces per unit in condominiums and planned unit developments, with at least one covered and assigned space. Extended allowance to condominium and planned unit residential projects that up to 35% of total unassigned spaces may be compact.
1991	2356-91	Reduced parking requirements for senior housing (one space per unit)
1992	2411-92	<ul style="list-style-type: none"> Increased required parking for condominiums and planned unit developments to 2.25 spaces per unit, with at least one covered and assigned parking space. Included stipulations for the conversion and replacement of covered parking.

1996	2549-96	<ul style="list-style-type: none"> • Multifamily parking updates (added greater minimum parking for individual garages as opposed to carports/underground open garages, added unassigned and guest parking for each bedroom type, and prohibited tandem parking arrangements) • Required a two-car wide driveway for each single/two-family dwelling to provide two uncovered parking spaces. • For developments with limited or no on-street parking, required .4 unassigned spaces per unit for single/two-family residential and .33 unassigned spaces per unit for multifamily residential.
2003	2716-03	Upgrades for non-conforming single-family parking required when home exceeds four bedrooms or 1,800 square feet.
2008	2877-08	<ul style="list-style-type: none"> • Parking Management Plans required for all new mixed-use or multifamily residential developments. • Clarified unassigned/guest parking requirements for multifamily bedroom types. No change to the total number of parking spaces that were established in 1996. • Reduced percentage of compact spaces allowed for all multifamily residential projects to 10%.
2012	2988-12	Complete reorganization of the parking code and changes to nonresidential parking requirements. No substantive changes to residential and mixed-use parking requirements.
2015	3056-15	<ul style="list-style-type: none"> • Reduction in required unassigned spaces for multifamily when two covered spaces per unit are provided • Tandem parking allowed for single-family and multifamily. • Allowance for stacker/independent mechanized parking in multifamily.
2023	3207-23	<ul style="list-style-type: none"> • Updated requirements per AB 2097 (2022) which prohibit requiring minimum parking for residential and nonresidential developments (except for bicycle, electric vehicle, and accessible spaces) that are located within one-half mile of a major transit stop. Developers may charge for the use of any parking spaces.

City		Required Covered	Required Uncovered	Total Required Spaces	Required Front Setback	Driveway Regulations	Trigger for Parking Compliance	Notes
Sunnyvale		2	2	4	20	17x20	4+ bedrooms or 1,800 gross sf	400 sf garage
Campbell		1	1	2	20	At least 8x25 feet. Offstreet parking may not be located in required yards	N/A if use remains SFR	
County of Santa Clara		1	1	2	25	At least 8 feet wide. May not be wider than 40% of lot width. 2 parking spaces may be within front yard	N/A if use remains SFR	2 additional parking spaces required if no street parking available within 100 feet. 1 if street parking on one side of street.
Cupertino		2	2	4	20	At least 10 feet wide. Vehicles may be parked in required front yard.	N/A if use remains SFR or existing parking does not become affected. s.f. must be setaside to accommodate a 2 car garage if one does not currently exist	Small lot SFR requires 2 covered, .8 open
Gilroy		1	1	2	26	Garages require minimum 18-foot long driveway. No required off-street parking in front setback.	Upon change of use	Tandem would be allowed in some cases such as for ADUs and parking downtown (voluntary or outside of AB2097).
Los Altos		1	1	2	25	At least 12 feet wide. Parking may be allowed in front setback		
Los Altos Hills		2	2	4	30	At least 12 feet wide. Parking not allowed in front setback	New residence, rebuild, addition over 900 sq.ft., ADU, and SB 9 unit	Except for ADU parking, all covered and uncovered parking spaces need to meet the 40-foot front, and 30-foot side and rear setbacks.
Los Gatos		0	2	2	25	Garages require 18x18 driveway. Parking not allowed in any front yard.	Second story, increase to bedrooms, 50% expansion	Additional parking required in hillsides (https://www.losgatosca.gov/1146/Los-Gatos-Hillside-Specific-Plan)
Milpitas		0	2-3+	2-3+	20	Minimum 14-foot width. Parking may be allowed in a required front yard.	New residence, addition or remodel resulting in more "bedrooms", SB 9, some ADUs	3 or fewer bedrooms require 2 spaces, 4 or more require 3 per unit, plus 1 per each additional bedroom. For purposes of calculating off-street parking requirements, staff may count other labeled rooms as "bedrooms" per Planning Code "Bedroom" definition - https://library.municode.com/ca/milpitas/codes/code_of_ordinances?nodeId=TITXIZOPLAN_CH10ZO_S2DE_XI-10-2.03DE
Monte Sereno		2	2-4	4-6	30	Minimum 20-foot width.	New Construction (includes addition over 50% of the existing house square footage)	2-2 with on-street parking, up to 2-6 without on-street parking in certain zoning

Single- and Two-Family Parking Data

Morgan Hill		2	0	2	15	Minimum 18-foot width. Parking allowed for operative cars in front yard driveway.	"New" buildings require parking compliance. Expansions require parking only for expanded area.	Requires 1 guest space per every four units?
Mountain View		1	0	2	20	9x20. Front yard driveway may be used for parking.	When a structure is enlarged, increased in capacity, or when a change in use requires more parking, compliance is required	For each dwelling in any single-family residential zoning district, a garage or carport shall be provided and permanently maintained for parking. 2 spaces required, only one must be covered
Palo Alto		1	1-3	2-4	20	Minimum 10-foot width. Required front yard may not be used for parking.	"New" buildings require parking compliance. Expansions require parking only for expanded area.	OS district requires 4 total, all other single-family require 2
Santa Clara		2	0	2	20	Minimum 20-foot driveway length. Uncovered parking may be allowed in required front yard driveway.	Expansion in gross floor area or capacity requires conformance, unless home has existing NC two-car garage with at least 17x17 dimension	2 covered spaces required plus minimum 20 foot driveway
San Jose		0	0	0	20	Minimum 10x18 driveway. Parking allowed in front yard setback.	No parking required	
Saratoga		2	0	2	25	Minimum 12-foot width. Driveway may be used for parking.	Compliance required at the time of occupancy or expansion	2 spaces required in an enclosed garage
TOTALS (Averaged)				2.27-2.60	23			Low end: 1 city = 0, 11 cities = 2, 3 cities = 4, (2.27). High end: 1 city = 0, 9 cities = 2, 1 city = 3, 3 cities = 4, 1 city = 6, (2.73).
Concord		1-3	1	2-4	20	Minimum 12x20 driveway. Operational vehicles allowed to park on driveways	Increase in number of bedrooms requires parking compliance	Existing homes require 2 spaces with one covered. At maximum, new homes with more than 6 bedrooms require 4 spaces with at least 3 enclosed.
Daly City		0	0	2-6	15	Minimum 19-foot length, may be used for required parking	Any addition requires compliance	0-1,500 sf = 2 spaces; 1,500-2,000 sf = 3 spaces; 2,000-2,500 sf = 4 spaces; 2,500+ sf = 6 spaces
Fremont		0	0	2-3	20	Minimum 18-foot length. Required parking may not occupy any part of required yard	Only if a change of use is involved	4 or fewer bedrooms = 2 spaces, 5 or more bedrooms = 3 spaces
Hayward		1-2	0-2	1-4	20	Minimum 16x20. Required parking must be outside of required front yard area.	Addition by more than 50% of original floor area or increase in bedrooms triggers compliance	Only 1 covered required if dwelling with single car garage built before 3/24/1959. 2 uncovered required if lot abuts street that has no parking lane on either side and is posted with no parking signs.

Redwood City		1	0	2	20 (garages and carports)	Minimum 10-foot width. May not be located in required front yard.	Floor area addition resulting in 2,000 sf total, or Use Permit is required.	
Richmond		0	0	2	20	At least 9-foot width. One uncovered space may be provided in the required front yard.	Expansion of use with nonconforming parking requires compliance	Parking spaces below the minimum or above the maximum may be allowed through a conditional use permit. Required parking within front half of lot shall be covered unless enclosed parking is converted to living space.
San Mateo		2	0	2-3	15	At least 10-foot width. Required parking no allowed in required front yard	Increase in intensity of use (including addition) requires parking compliance.	Under 3,000 sf requires 2 garage spaces. 3,000-3,749 sf requires 2 garage plus one additional (may be uncovered). 3,750 sf or greater requires 1 additional space for every 750 sf in addition to the 2+1 (may be uncovered).
Walnut Creek		2	0	2	20	Minimum 18-foot length. Required parking may not occupy any part of required yard	At time of construction of a structure or Major alteration (50% expansion) requires compliance	If the required parking is removed due to the construction of an accessory dwelling unit, no replacement parking is required.
TOTALS (Averaged)				1.88-3.25	19			Low end: 7 cities = 2, 1 city = 1, (1.88). High end: 3 cities = 2, 2 city = 3, 2 cities = 4, 1 city = 6, (3.25).

City	Area Plan	Required Covered	Required Uncovered	Total Required Spaces	Notes
Sunnyvale	Citywide	1	0	1 assigned per unit, additional unassigned per bedroom. 0.25 to 1.55 added per unit by space type.	
	Downtown Specific Plan	1	0	1 assigned per unit; .5 unassigned per studio/1-bed; .7 unassigned per 2-bed; 1 unassigned per 3+bed	When two assigned spaces are provided a multi-family residential unit (including tandem), the number of unassigned spaces may be reduced by 25% for each unit with more than one assigned space.
	Lawrence Station Area Plan	0	0	1-1.5 per studio/1-bed; 1.25-2 per 2-bed; 1.7-2 per 3+ bed	
	El Camino Real Specific Plan	0	0	Nodes: 1-1.5 per studio/1-bed; 1.25-2 per 2-bed; 1.7-2 per 3+ bed. Segments: 1-1.5 per studio/1-bed; 1.5-2.25 per 2-bed; 2-2.25 per 3+ bed	
Total				Citywide: 1.50-2.65 per unit. Specific Plans: 1.17-2.08 per unit.	Citywide low end: 1+0.5. Citywide high end: 2+0.65. Specific Plan low end: (1.5+1+1)/3. Specific Plan high end: (2+2+2.25)/3
Campbell	Citywide	0	0	1 per studio/1-bed up to 625 sf; 2 per studio/1-bed over 625 sf; 2.5 per 2+ beds.	Parking Reduction Strategies: Motorcycle; Mixed Use: Shared Parking with guest, and Carshare Parking.
	Transit Oriented + Walkable	0	0	1 per studio/1-bed up to 625 sf; 1.5 per studio/1-bed over 625 sf; 2 per 2+ beds	Parking Reduction Strategies: Motorcycle; Mixed Use: Shared Parking with guest, and Carshare Parking.
	San Tomas Area Plan	2-2.5	0-1	PD zones require 2 additional guest spaces above standard zoning requirements	Driveway spaces shall not be included as guest parking. Plan requirements superceded when proposing a housing development project with two or more units.
County of Santa Clara	Countywide	0	0	1.5 per unit	
Cupertino	Citywide	1	0	2 per unit	If the City, at its cost, has conducted an area-wide or City-wide parking study in the last seven years, then the City may find, based on substantial evidence, that a higher parking ratio is required than shown in Table 19.56.040C. In no event, may the required parking be greater than the ratio shown in Table 19.56.040B. The parking study must conform to the requirements of Government Code Section 65915(p)(7).
Gilroy	Citywide	1	0	1.5 per 1-/2-bed units; 2 per 3+beds. 1 guest space for every 4 units	Tandem would be allowed in some cases such as for ADUs and parking downtown (voluntary or outside of AB2097).
	Downtown Specific Plan	0	0	<800sf = 1 per unit plus 1 guest space per 6 units. >800sf = 1.5 per unit plus 1 guest space per 4 units.	A majority, if not all, Downtown Specific Plan parcels are located within ½ mile of Monterey so AB 2097 applies. In-lieu fees have not been established and would not be applicable under AB 2097.

Multi-Family Parking Data

Los Altos	Single Family Zoned	1	1	2	
	Multiple Family	0	0	1.5 per unit w/ 2 or fewer bedrooms. 2 per unit w/ more than 2 bedrooms. 1 visitor space for every 4 units	
Los Altos Hills	Citywide	N/A	N/A	N/A	No multi-family zoning. Proposed to rezone multi-family by 2026 with new objective standards
Los Gatos	Citywide	0	0	1.5 per living unit plus 1 visitor space for each unit subject to PC	Required parking may be waived if the lot does not have adequate area to provide required spaces. If this finding is made by the deciding body, parking shall be provided to the maximum extent possible.
Milpitas	Citywide	"1-3"	0	1 covered per studio; 1.5 covered per 1-bed; 2 covered per 2-3 beds; 3 stalls (min. 2 covered) per 4-bed and 1 additional stall per bedroom in 4+ bedroom unit. Guest parking required	Guest Parking - Projects with Parking structures, 15% of the total required any may be uncovered. Projects with private garages, 20% of the total required and may be uncovered. Bicycle parking - 5% of the total required
	Metro Specific Plan	0	0	.5 to 1.5 spaces per unit. May be reduced by parking reduction point system or in-lieu fee.	Point reduction point system includes near VTA/Bart, shared parking agreements, shuttle service, dedicated parking for shared car, micro-mobility devices, paid employee parking, car- van-pooling, or other measures as proposed by the applicant. Up to 100% parking reduction. Minimum parking requirements may be reduced through in-lieu fee if approved by City Council and must be used on multimodal programmatic or infrastructure improvements.
	Midtown Specific Plan	"1-3"	0	R4/MXD Zoning: 1 covered per studio; 1.5 covered per 1-bed; 2 covered per 2-3 beds; 3 stalls (min. 2 covered) per 4-bed and 1 additional stall per bedroom in 4+ bedroom unit. Guest parking required for both R4/MXD	TOD zoning allows for up to 20% reduction in required parking. Guest parking for R4 zoning - projects with parking structures, 15% of the total required any may be uncovered. Projects with private garages, 20% of the total required and may be uncovered. Guest parking for MXD zoning - 15% of total required stalls covered or uncovered. Bicycle parking - 5% of the total required. Note: an updated and renamed version of the Midtown Specific Plan "Gateway-Main Street Specific Plan" is underway and may be adopted around end of 2023 or in 2024.
Monte Sereno	Citywide	N/A	N/A	N/A	No multi-family zoning
Morgan Hill	Citywide	0-1	0-1.5	MU-D & RAH: 1 per unit 600sf or less, 1.5 per unit 600-1350sf; 2 per unit 1350+sf. Other zoning: 1 covered + .5 uncovered per studio/1-bed; 1+1 per 2 bedroom; 1 covered + 1.5 uncovered per 3-bed	1 guest space per every 3 units required.
	Downtown Specific Plan	0	0	1 space per unit <600 sf; 1.5 spaces per unit between 600-1350 sf; 2 spaces per unit >1350 sf.	

Mountain View	Citywide	1	0	1.5 spaces per studio/1-bed under 650sf; 2 spaces per 1-bed 650+sf or 2+beds. At least 1 covered for all. Additional guest spaces required	15 percent of required parking shall be located for guest parking. The City also implements Model Parking Standard for High Density Residential (over 43 DU/acre) projects: one space per one-bedroom and two spaces per 2+ bedrooms, without additional spaces for guests.
	460 Shoreline Boulevard Precise Plan	0	0	1.5 per unit + "convenient" guest parking	Special attention shall be given to minimization of paving, screening of parking from Shoreline Boulevard, safe access to the site.
	San Antonio Station Precise Plan	1	0	Same as city standards, unless parking study determines a reduction is warranted. A portion of the 200 spaces reserved for Caltrain may be shared.	Parking for housing should be depressed at least partially below grade. Open parking for residential buildings shall be limited to 10% of the total parking required. ZA may grant approval of aboveground parking structure for residential or transit purposes.
	Villa-Mariposa Area Precise Plan	0	0	1 space per bedroom, up to 2 spaces per unit	
	Evelyn Avenue Corridor Precise Plan	1	0	R-1: 2 spaces per unit, at least one shall be in a garage. Mixed Use: 2 per 2-bed, 1 per 1-bed, 15% guest. Further reduction may be approved by ZA with a parking study.	Special consideration shall be given to design of shared and/or common lots or garages. ZA may require a parking study to determine if further reduction in parking is warranted.
	Downtown Precise Plan	0	0	1.5 per studio/1-bed + .3 guest per unit. 2 spaces per 2+ bed + .3 guest per unit.	In-lieu fees allowed in Areas E&H up to 100% of required guest parking. ZA may allow density increase in situations where on-site parking has been reduced. Most of the downtown is within 0.5 miles of the Caltrain station. Currently per AB2097 no minimum parking is required for residential developments. The downtown precise plan is in the process of being modified and the parking standards in the document may change.
	1101 Grant Road Precise Plan	0	0	.35 cars per unit for senior housing	Special attention shall be given to providing guest parking.
	Whisman Station Precise Plan	1	0	Two private spaces per unit, one of which shall be covered, +.5 guest spaces per unit.	
	El Camino Real Precise Plan	0	0	1 per studio/1-bed; 2 for 2+ beds. 15% of required parking must be available to guests	Applicants may meet minimum parking requirements through use of designated off-site facilities within 600 feet walking distance and not including street parking.
	North Bayshore Precise Plan	0	0	Maximum .25 per micro unit, .5 per 1-bed, 1 per 2-3 bed.	No minimum amount of parking will be required in North Bayshore. Residential projects requesting a higher parking maximum than permitted by the Plan shall submit a parking study completed by a traffic engineer. Guest parking determined by ZA
	San Antonio Precise Plan	0	0	1 per studio/1-bed; 2 for 2+ beds. 15% of required parking must be available to guests	Applicants may meet minimum parking requirements through use of designated off-site facilities within 600 feet walking distance and not including street parking.

Multi-Family Parking Data

	East Whisman Precise Plan	0	0	Outside buffer zone: maximum 1 per studio/1-bed, 2 per 2+ beds. Inside buffer zone: minimum 1 per studio/1-bed, 2 per 2+ beds. 1 carshare space per 50-200 parking spaces, 2 per 201+ spaces.	One carshare space per 80 units may be exempt from off-street parking maximums.
Palo Alto	Citywide	1	0	1 per micro unit, studio, or 1-bed; 2 per 2+ beds	For residential mixed-use developments in the CD-C zone, CC(2) zone, and on CN and CS zoned sites abutting El Camino Real, the first 1,500 square feet of ground-floor retail uses shall not be counted toward the vehicle parking requirement. Various parking adjustments can be made by Director - 18.52.050 - https://codelibrary.amlegal.com/codes/paloalto/latest/paloalto_ca/0-0-0-81045
	South of Forest Area (Phase 1)	0	0	Mixed Use Overlay: 1.25 per studio; 1.5 per 1-bed; 2 per 2-bed. One guest space plus 10% of total units. MU-1/MU-2: 1.5 per unit.	For any site or sites with multiple uses the parking requirement may be reduced if in the judgement of the Director of Planning and Community Environment the joint facility will serve all existing, proposed and potential uses as effectively as separate parking facilities
	South of Forest Area (Phase 2)	0	0	TRT-35/RT-50 w/TDM program: 1 per studio; 1.2 per 1-bed; 1.5 per 2+ beds. Guest parking per Chapter 18.83	Multiple-family residential use may receive a parking reduction with an approved TDM program that shall include, for each unit, membership in a car share program and subsidized transit passes in an amount to be determined by the Director, or an equivalent alternative
Santa Clara	Citywide	1	0	1 covered +1 for R3-18D, R3-25D, R3-36D; 1 per studio & 1-bed, 1.5 per 2+ beds in Mixed Use	
	Lawrence Station Area Plan	0	0	1 per studio/1-bed; 2 per 2/3 bed	
	Patrick Henry Drive Specific Plan	0	0	.5 per unit <550 sf; 1 per unit >550 sf. Plus .05 visitor spaces per unit.	All parking must be structured or underground.
San Jose	Citywide	0	0	0	
	Downtown Parking Management Zone	0	0	0	
	Pedestrian Oriented Zoning Districts	0	0	0	
	Urban Village Plans	0	0	0	
Saratoga	Citywide	1	0	1 covered garage plus 1.5 for each additional unit on-site	

Multi-Family Parking Data

TOTALS (Averaged)				Citywide: 1.31-1.96. Specific Plans: 0.95-1.63.	Citywide low end: (1+1.5+2+1.5+1.5+2.5+1+1+1.5+1+1+0+1.5)/13. Citywide high end: (2.5+1.5+2+2+2+2.5+3+2.5+2+2+2+0+1.5)/13. Specific Plan low end: (1+1+.5+1+1+1.5+1.5+1+1+1.8+2.5+1+.25+1+1+1.25+1+1+.5+0+0+0)/22. Specific Plan high end: (2+1.5+1.5+3+2+1.5+2+2+2+2.3+2.5+2+1+2+2+2+1.5+2+1+0+0+0)/22.
Concord	Citywide	1	0	1 per studio; 1.5 per 1 bed; 2 per 2-3 beds, add .5 space per bedroom for 4+ beds. 1 guest space per 3 units.	Projects within Downtown Parking District may accept in-lieu fees for required parking subject to a Use Permit decided by the Planning Commission.
	Transit Station Overlay District	0	0	Required parking may be reduced by up to 25 percent by approval of a Use Permit	Downtown Specific Plan contains recommended parking standards which do not appear to have been incorporated into Municipal Code
Daly City	Citywide	0	0	1 per studio; 1.5 per 1-bed; 2 per 2+ bed	In no event shall an administrative variance be issued which reduces the overall off-street parking standard by more than twenty percent.
	Bart Station Area Specific Plan	0	0	1.25 to 2 per unit based on architecture	Applies only to areas near Colma BART.
Fremont	Citywide	1	0	1 covered per studio/1-bed; 1 covered + .5 per 2+ beds. Add .5 per unit guest parking.	
	Downtown Community Plan	0	0	.75 per dwelling non-exclusive use; 1 per dwelling exclusive use; maximum 2 per unit	Optional in-lieu fees
	City Center Community Plan	0	0	Office and Urban Neighborhoods: minimum .75 nonexclusive, 1 exclusive, maximum 2. Transit Neighborhoods: .5 nonexclusive, .75 exclusive, maximum 1.	Required parking may be located off site up to 600 feet from the site. No minimum parking is required for a supportive housing development within one-half mile of a public transit stop.
	TOD Overlay Parking District	1	0	Minimum 1 covered plus .25 guest. Maximum 1.5 plus .25 guest.	No minimum parking is required for a supportive housing development within one-half mile of a public transit stop.
	Warm Springs/South Fremont Community Plan	0	0	Within .25 miles of Bart: 1.5 per unit. Otherwise 2 per unit	

Hayward	Citywide	1	.5-1.1	1 covered + .5 open per studio; 1 covered + .7 open per 1-bed; 1 covered and 1.1 open per 2+ beds	10% of spaces shall be marked for visitor parking, except when less than 10 spaces are required, when one visitor space shall be required. Any uncovered space may be covered instead. Except for uses established in the Central Parking District, parking space requirements for multi-family residential may be reduced by the Director of Community and Economic Development/Planning Director up to 15 percent provided public rail transportation is available within 1,000 feet of the site.
	Central Parking District	1	0.5	1 covered and .5 open spaces per dwelling unit	The Central Parking District requirements may be satisfied by providing payments in lieu of providing all or part of the required non-residential parking
	Downtown Core Area Specific Plan	0	0	1 per dwelling unit provided that total buildout of parking supply is 1.5 per dwelling unit in plan area	Residential parking requirements may be met in locations other than on the development sites, subject to the approval of the reviewing authority.
	Mission Boulevard Code	0	0	0 required, maximum of 2 allowed except within .5 miles of BART stations where maximum is 1.	
	Citywide	1	0	2 spaces per unit, 1 covered, plus one space for every 4 units for guest parking.	Required parking may be reduced to 1.5 spaces per studio/1-bed units (1 covered) plus guest parking if within 500 feet of ECR or Woodside Road. ZA may reduce required guest parking if adequate street parking is available and/or if only one parking space is reserved per unit for residents and/or if the site is in close proximity to retail shopping.

Multi-Family Parking Data

Redwood City	Downtown Parking Zone	0	0	.75 per studio, 1 per 1-bed, 1.5 per 2+ beds.	May be reduced by ZA if it can be shown that fewer than those required are necessary. Factors shall include project design, location, affordability, unit size. May be waived up to 100% through in-lieu fee.
	Mixed Use Zones	0	0	1.5 per studio/1-bed; 2 per 2+ beds; 1 additional space for every 4 units guest parking.	Guest parking may be reduced when adequate street parking is available, only one space per unit is reserved for residents, and/or the site is close to retail shopping.
	North Main Street Precise Plan	0	0	Average of 2 stalls per unit including guest stalls. See RCZC 30.2.E.1 and .2	
	Downtown Medical Campus Precise Plan	0	0	1 per studio/1-bed, 1.5 per 2+ beds.	
Richmond	Citywide	0	0	.5 per efficiency unit; 1 per 1-bed; 1.5 per 2-bed; 2 per 3+ bed. Must be covered if within front half of lot, or within 25 feet of side street on a corner lot.	
	Bart Station Area	0	0	Minimum required shall be reduced by 50%. May be further reduced or eliminated through a Conditional Use Permit	
	City Center Specific Plan	0	0	1.5 per unit on site.	
	Tiscornia Estate Specific Plan	0	0	2 per unit on site.	
San Mateo	Citywide	1	0	1.3 per studio; 1.6 per 1-bed; 1.8 per 2-bed; 2 per 3+ beds or 1400 sf. +0.2 guest spaces per unit for all.	
	Central Parking and Improvement District	0	0	New projects shall either fund a City-commissioned project-specific parking demand study or meet standard requirements	Projects within the City's Central Parking Improvement District and outside of the Limited Parking Zone must satisfy a minimum of 25% of the project's required parking through provision of on-site parking. May increase to 50% when nearby parking occupancy is high.
	Downtown Specific Planning Area	0	0	1 per studio; 1.3 per 1-bed; 1.5 per 2-bed; 1.8 per 3+ beds. +0.2 guest spaces per unit for all.	All open at-grade parking stalls shall be covered with a deck, or trellis, and may be allowed in rear yards only within R4-D zoning.
	Hillsdale Station Area Plan	0	0	1 per studio; 1.2 per 1-bed; 1.5 per 2-bed; 1.8 per 3 bed.	
	Citywide	1	0	1.25 per studio; 1.5 per 1-bed; 2 per 2-bed; 2.25 per 2+ bed. At least one covered each.	In-lieu fees allowed for Pedestrian Retail Zoning District subject to Council approval.

Multi-Family Parking Data

Walnut Creek	Alma Avenue Area Specific Plan	0	0	1 space per bedroom, but not fewer than 1.25 per unit.	Off-site parking allowed if located up to 200 feet from the residential building and if site is under same ownership. Parking standard may be reduced if submitted parking studies justify such reduction.
	North Downtown Specific Plan	0	0	.5 per studio; .75 per 1-bed; 1 per 2-bed; 1.25 per 3+ bed. Reduced to .5 per unit within Makers Row	May be further reduced by TDM
	Parking Reduction Zones	0	0	Subject to minor use permit, a reduction of up to 20% of standard required parking according to area formula.	
	Low Income/Bart Proximate Housing	0	0	1-2 per unit based on income level and proximity to BART	Bart proximate is within 1/2 mile of Walnut Creek of Pleasant Hill stations.
TOTALS (Averaged)				Citywide: 1.28-2.13. Specific Plans: 1.09-1.49	Citywide low end: (1+1+1.5+1.5+2+.5+1.5+1.25)/8. Citywide high end: (2.5+2+2+2.1+2+2+2.2+2.25)/8. Specific Plan low end: (1.25+.75+.5+1.25+1.5+1.5+1+0+.75+1.5+2+1+.25+1.5+2+1.2+1+1.25+.5+1)/20. Specific Plan high end: (2+1+.75+1.25+2+1.5+1.5+0+1.5+2+2+1.5+1+1.5+2+2+1.8+1.25+1.25+2)/20.

	Santa Clara County, California		Campbell city, California		Cupertino city, California		Gilroy city, California	
	Occupied housing units	Percent occupied housing units	Occupied housing units	Percent occupied housing units	Occupied housing units	Percent occupied housing units	Occupied housing units	Percent occupied housing units
Label	Estimate	Estimate	Estimate	Estimate	Estimate	Estimate	Estimate	Estimate
Occupied housing units	646,847	646,847	17,346	17,346	20,963	20,963	17,023	17,023
Household size	2.93		2.52		2.88		3.43	
VEHICLES AVAILABLE								
No vehicle available	34,930	5.4%	1,091	6.3%	927	4.4%	654	3.8%
1 vehicle available	181,895	28.1%	5,317	30.7%	5,191	24.8%	3,456	20.3%
2 vehicles available	258,298	39.9%	7,301	42.1%	9,844	47.0%	6,438	37.8%
3 or more vehicles available	171,724	26.5%	3,637	21.0%	5,001	23.9%	6,475	38.0%

	Los Altos city, California		Los Altos Hills town, California		Los Gatos town, California		Milpitas city, California	
	Occupied housing units	Percent occupied housing units	Occupied housing units	Percent occupied housing units	Occupied housing units	Percent occupied housing units	Occupied housing units	Percent occupied housing units
Label	Estimate	Estimate	Estimate	Estimate	Estimate	Estimate	Estimate	Estimate
Occupied housing units	10,805	10,805	3,125	3,125	13,036	13,036	23,681	23,681
Household size	2.9		2.7		2.53		3.26	
VEHICLES AVAILABLE								
No vehicle available	227	2.1%	27	0.9%	458	3.5%	1,269	5.4%
1 vehicle available	1,975	18.3%	239	7.6%	3,607	27.7%	6,121	25.8%
2 vehicles available	5,131	47.5%	1,288	41.2%	5,833	44.7%	9,333	39.4%
3 or more vehicles available	3,472	32.1%	1,571	50.3%	3,138	24.1%	6,958	29.4%

	Monte Sereno city, California		Morgan Hill city, California		Mountain View city, California		Palo Alto city, California	
	Occupied housing units	Percent occupied housing units	Occupied housing units	Percent occupied housing units	Occupied housing units	Percent occupied housing units	Occupied housing units	Percent occupied housing units
Label	Estimate	Estimate	Estimate	Estimate	Estimate	Estimate	Estimate	Estimate
Occupied housing units	1,219	1,219	14,721	14,721	34,316	34,316	26,007	26,007
Household size			3.05		2.39		2.62	
VEHICLES AVAILABLE								
No vehicle available	0	0.0%	445	3.0%	2,369	6.9%	1,740	6.7%
1 vehicle available	153	12.6%	3,016	20.5%	14,885	43.4%	8,740	33.6%
2 vehicles available	567	46.5%	6,340	43.1%	12,589	36.7%	10,397	40.0%
3 or more vehicles available	499	40.9%	4,920	33.4%	4,473	13.0%	5,130	19.7%

	San Jose city, California		Santa Clara city, California		Saratoga city, California		Sunnyvale city, California	
	Occupied housing units	Percent occupied housing units	Occupied housing units	Percent occupied housing units	Occupied housing units	Percent occupied housing units	Occupied housing units	Percent occupied housing units
Label	Estimate	Estimate	Estimate	Estimate	Estimate	Estimate	Estimate	Estimate
Occupied housing units	324,376	324,376	45,830	45,830	11,039	11,039	57,565	57,565
Household size	3.08		2.68		2.81		2.69	
VEHICLES AVAILABLE								
No vehicle available	17,786	5.5%	2,463	5.4%	269	2.4%	3,718	6.5%
1 vehicle available	86,570	26.7%	16,135	35.2%	1,624	14.7%	20,271	35.2%
2 vehicles available	127,316	39.2%	19,352	42.2%	4,804	43.5%	23,328	40.5%
3 or more vehicles available	92,704	28.6%	7,880	17.2%	4,342	39.3%	10,248	17.8%

	Concord city, California		Daly City city, California		Fremont city, California		Hayward city, California	
	Occupied housing units	Percent occupied housing units	Occupied housing units	Percent occupied housing units	Occupied housing units	Percent occupied housing units	Occupied housing units	Percent occupied housing units
Label	Estimate	Estimate	Estimate	Estimate	Estimate	Estimate	Estimate	Estimate
Occupied housing units	45,471	45,471	31,787	31,787	74,629	74,629	49,524	49,524
Household size	2.74		3.28		3.08		3.21	
VEHICLES AVAILABLE								
No vehicle available	2,832	6.2%	2,778	8.7%	3,152	4.2%	2,882	5.8%
1 vehicle available	14,006	30.8%	9,421	29.6%	18,603	24.9%	12,973	26.2%
2 vehicles available	17,349	38.2%	10,960	34.5%	32,706	43.8%	18,451	37.3%
3 or more vehicles available	11,284	24.8%	8,628	27.1%	20,168	27.0%	15,218	30.7%

	Redwood City city, California		Richmond city, California		San Mateo city, California		Walnut Creek city, California	
	Occupied housing units	Percent occupied housing units	Occupied housing units	Percent occupied housing units	Occupied housing units	Percent occupied housing units	Occupied housing units	Percent occupied housing units
Label	Estimate	Estimate	Estimate	Estimate	Estimate	Estimate	Estimate	Estimate
Occupied housing units	30,020	30,020	39,493	39,493	40,233	40,233	32,126	32,126
Household size	2.74		2.9		2.58		2.15	
VEHICLES AVAILABLE								
No vehicle available	1,687	5.6%	3,390	8.6%	2,232	5.5%	2,475	7.7%
1 vehicle available	9,156	30.5%	13,207	33.4%	14,889	37.0%	13,589	42.3%
2 vehicles available	12,623	42.0%	13,322	33.7%	15,735	39.1%	11,616	36.2%
3 or more vehicles available	6,554	21.8%	9,574	24.2%	7,377	18.3%	4,446	13.8%