



City of Sunnyvale

Notice and Agenda - Final Planning Commission

Monday, April 13, 2015

7:00 PM

Council Chambers and West Conference
Room, City Hall, 456 W. Olive Ave.,
Sunnyvale, CA 94086

7:00 P.M. PLANNING COMMISSION - STUDY SESSION - WEST CONFERENCE ROOM

1 [15-0416](#)

File #: 2014-8019

Location: 750 Lakeway Drive (APNs: 216-43-014)

Zoning: M-S (Industrial and Service) Zoning District

Proposed Project:

SPECIAL DEVELOPMENT PERMIT for the redevelopment of a 232-room Residence Inn with partial demolition of 32 rooms and construction of a new 7-story hotel with 111 guest rooms (79 net new rooms), including 32 underground parking spaces and associated site modifications on a 6.15-acre site.

Applicant / Owners: DLR Group / Grand Prix Sili I LLC

Environmental Review: Mitigated Negative Declaration

Project Planner: Margaret Netto, (408) 730-7628,
mnetto@sunnyvale.ca.gov

2 [15-0417](#)

File #: 2014-7633

Location: 861 E. El Camino Real (APN: 211-16-021)

Zoning: C-2/ECR (Highway Business/El Camino Real Precise Plan)
Zoning District

Proposed Project:

SPECIAL DEVELOPMENT PERMIT to allow a 161-room hotel (Hampton Inn), including underground parking on a 1.5 acre site (formerly Summer Winds Nursery).

Applicant / Owners: Myhre Group Architects / InterMountain
Management LLC

Environmental Review: Mitigated Negative Declaration

Project Planner: Margaret Netto, (408) 730-7628,
mnetto@sunnyvale.ca.gov

3 Public Comment on Study Session Agenda Items

4 Comments from the Chair

5 Adjourn Study Session

Any agenda related writings or documents distributed to members of the Planning Commission regarding any open session item on this agenda will be made available for public inspection in the Planning Division office located at 456 W. Olive Ave., Sunnyvale CA 94086 during normal business hours, and in the Council Chambers on the evening of the Planning Commission meeting pursuant to Government Code §54957.5.

8:00 P.M. PLANNING COMMISSION - PUBLIC HEARING - COUNCIL CHAMBERS

CALL TO ORDER

SALUTE TO THE FLAG

ROLL CALL

PUBLIC ANNOUNCEMENTS/PUBLIC COMMENTS

Speakers are limited to 3 minutes for announcements of related commission events, programs, resignations, recognitions, acknowledgments.

CONSENT CALENDAR

- 1.A [15-0415](#) Approval of the Draft Minutes of the Planning Commission Meeting of March 23, 2015
- 1.B [15-0447](#) Approval of the 2015 Planning Commission Master Work Plan

PUBLIC HEARINGS/GENERAL BUSINESS

- 2 [15-0381](#) **File #:** 2015-7086
Location: 394 South Pastoria Avenue (APN: 165-12-002)
Zoning: R2 (Low-Medium Density Residential) Zoning District
Proposed Project:
 DESIGN REVIEW: for a new two-story, single-family dwelling with a floor area of 2,727 square feet (2,325-square foot living area and 402-square foot garage) and 49.8% FAR. The existing one-story, single-family home would be demolished.
Applicant / Owner: SC Design Group (applicant) / Kaiwei Yao and Rao Shen (owner)
Environmental Review: Categorically Exempt Class 3
Project Planner: Momoko Ishijima, (408) 730-7532, mishijima@sunnyvale.ca.gov

- 3 [15-0386](#) **File #:** 2014-8023
Location: 787 N. Mary Avenue (APN: 165-40-003)
Zoning: MS
Proposed Project: Application for a project on a 4.02-acre site:
 USE PERMIT to allow a new 54-foot tall wireless facility
 (mono-eucalyptus) in the parking lot of an industrial site.
Applicant / Owner: Verizon Wireless (applicant) / North Mary, LLC
 (owner)
Environmental Review: Negative Declaration
- 4 [15-0167](#) Peery Park Specific Plan - Draft Project Description, Draft Vision,
 Guiding Principles, Goals and Policies and Conceptual Policy
 Framework Diagrams and Maps
Staff Contact: Amber El-Hajj, (408) 730-2723,
 ael-hajj@sunnyvale.ca.gov
- 5 [15-0414](#) **Standing Item:** Potential Study Issues for 2016

NON-AGENDA ITEMS AND COMMENTS

-Commissioner Comments

-Staff Comments

INFORMATION ONLY ITEMS

ADJOURNMENT

Any agenda related writings or documents distributed to members of the Planning Commission regarding any open session item on this agenda will be made available for public inspection in the Planning Division office located at 456 W. Olive Ave., Sunnyvale CA 94086 during normal business hours, and in the Council Chambers on the evening of the Planning Commission meeting pursuant to Government Code §54957.5.

Agenda information is available by contacting The Planning Division at (408) 730-7440. Agendas and associated reports are also available on the City's web site at sunnyvale.ca.gov or at the Sunnyvale Public Library, 665 W. Olive Ave., Sunnyvale, 72 hours before the meeting.

*Planning a presentation for a Planning Commission meeting?
To help you prepare and deliver your public comments, please review the "Making Public Comments During City Council or Planning Commission Meetings" document available at Presentations.inSunnyvale.com.*

PLEASE TAKE NOTICE that if you file a lawsuit challenging any final decision on any public hearing item listed in this agenda, the issues in the lawsuit may be limited to the issues which were raised at the public hearing or presented in writing to the City at or before the public hearing.

PLEASE TAKE FURTHER NOTICE that Code of Civil Procedure section 1094.6 imposes a 90-day deadline for the filing of any lawsuit challenging final action on an agenda item which is subject to Code of Civil Procedure section 1094.5.

Pursuant to the Americans with Disabilities Act, if you need special assistance in this meeting, please contact the Planning Division at (408) 730-7440. Notification of 48 hours prior to the meeting will enable the City to make reasonable arrangements to ensure accessibility to this meeting. (29 CFR 35.106 ADA Title II)



City of Sunnyvale

Meeting Minutes - Draft Planning Commission

Monday, March 23, 2015

7:00 PM

Council Chambers and West Conference
Room, City Hall, 456 W. Olive Ave.,
Sunnyvale, CA 94086

7:00 P.M. PLANNING COMMISSION - STUDY SESSION - WEST CONFERENCE ROOM

- 1 Highlights of 2015 Planning Commissioners Academy**
- 2 Public Comment on Study Session Agenda Items**
- 3 Comments from the Chair**
- 4 Adjourn Study Session**

8:00 P.M. PLANNING COMMISSION - PUBLIC HEARING - COUNCIL CHAMBERS

CALL TO ORDER

Chair Melton called the meeting to order at 8:00 p.m.

SALUTE TO THE FLAG

Chair Melton led the salute to the flag.

ROLL CALL

Present: 7 - Chair Russell Melton
Vice Chair Ken Olevson
Commissioner Ralph Durham
Commissioner Sue Harrison
Commissioner Larry Klein
Commissioner Ken Rheaume
Commissioner David Simons

PUBLIC ANNOUNCEMENTS/PUBLIC COMMENTS

CONSENT CALENDAR

1.A [15-0330](#) Approval of the Draft Minutes of the Planning Commission Meeting of March 9, 2015

Comm. Klein moved to approve the draft minutes with minor edits given to staff earlier. Comm. Simons seconded. The motion carried by the following vote:

Yes: 7 - Chair Melton
 Vice Chair Olevson
 Commissioner Durham
 Commissioner Harrison
 Commissioner Klein
 Commissioner Rheaume
 Commissioner Simons

No: 0

1.B [15-0340](#) **File #:** 2015-7063
Location: 1268 Townsend Terrace (APN: 202-37-025)
Zoning: R-1.5/PD (Low Medium Density Residential / Planned Development) Zoning District
Proposed Project:
 SPECIAL DEVELOPMENT PERMIT: for a first and second floor addition of 152 square feet to an existing two-story, single-family residence resulting in a building size of 2,368.5 square feet and 54.9% floor area ratio (FAR). The project also includes the expansion of the front porch.
Applicant / Owner: Flanders Bay Company (applicant) / Tyson Leistiko (owner)
Environmental Review: Categorically Exempt Class 1
Project Planner: Momoko Ishijima, (408) 730-7532, mishijima@sunnyvale.ca.gov
NOTE: *Noticed in error. No action required.*

No action was required.

1.C [15-0316](#)**File #:** 2015-7028**Location:** Las Palmas townhome development (under construction) behind 660 W. El Camino Real (APNs: 201-40-043 through 078)**Zoning:** C-2/PD**Proposed Project:** Modification to approved **Special Development****Permit #**2012-7170 (mixed use development with 103 townhomes and 145-room hotel) to remove the requirement for a planned pedestrian connection from the Las Palmas townhome development to Cherry Glen Plaza.**Applicant / Owner:** SummerHill 660 W. El Camino Real LLC (applicant) / (owner)**Environmental Review:** Categorically Exempt Class 1**Project Planner:** Ryan Kuchenig, (408) 730-7431, rkuchenig@sunnyvale.ca.gov**NOTE:** *The applicant has requested an indefinite continuance.*

Comm. Klein moved to approve the continuance. Comm. Simons seconded. The motion carried by the following vote:

Yes: 7 - Chair Melton
 Vice Chair Olevson
 Commissioner Durham
 Commissioner Harrison
 Commissioner Klein
 Commissioner Rheaume
 Commissioner Simons

No: 0

1.D [15-0306](#)**File #:** 2015-7048**Location:** 1601 Longspur Avenue (APN: 309-49-017)**Zoning:** R-0 (Low-Density Residential)**Proposed Project:** Related applications on a 6,634 square-foot site:

DESIGN REVIEW for a first-story addition of 551 square feet and new second story of 1,007 square feet to an existing one-story single-family home, resulting in 3,256 square feet (2,820 square feet living area and 436 square feet garage) and 49% floor area ratio.

Applicant / Owner: Ali's Construction and Remodeling (applicant) / Bijish Raveendran (owner)

Environmental Review: Class 1 Categorical Exemption

Project Planner: Timothy Maier, (408) 730-7257, tmaier@sunnyvale.ca.gov

Comm. Harrison pulled this item from the consent calendar.

Trudi Ryan, Planning Officer, announced that no presentation on the staff report was prepared as this item was on the Consent Calendar and said that Tim Maier, Assistant Planner, was in attendance to answer any questions.

Comm. Harrison discussed the frequency with which staff approves second story additions that are over the 35 percent second-to-first floor ratio, and discussed nearby homes exceeding this ratio.

Comm. Klein confirmed with Mr. Maier that the applicant did not want to reduce the size of the proposed second story any further, and discussed with staff the features of the proposed home that are factors of the staff plate height recommendation.

Comm. Rheaume confirmed with Ms. Ryan that the proposed second-to-first floor ratio is not a deviation and that the reason this project was brought for review by the Planning Commission is due to the proposed Floor Area Ratio (FAR) being beyond 45 percent.

Comm. Simons and Ms. Ryan further discussed features of homes that may influence a staff decision to allow a second-to-first floor ratio that exceeds the 35 percent guideline, and confirmed that the zoning code does not require the stepping in of the second floor.

Chair Melton opened the public hearing.

Bijish Raveendran, the project applicant, discussed reasons for applying for the Design Review.

Comm. Harrison confirmed with Mr. Raveendran that he and his architect did not look at options for plans with a second-to-first floor ratio at or below 35 percent or an FAR below 45 percent.

Chair Melton closed the public hearing.

Vice Chair Olevson moved Alternative 1 to approve the Design Review with the Conditions in Attachment 4 of the staff report.

Comm. Rheaume seconded.

Vice Chair Olevson thanked the applicant for making an impassioned plea for approval, and said this is a neighborhood well into the transition from all single-story ranch style homes to multiple styles. He said there are very attractive single-family homes on this street and on the neighboring street, that there are numerous homes that have been remodeled to second story and an area of all second story homes not too far away. He said he is persuaded that the design submitted will not have an overpowering look in the neighborhood, and that he can make the findings that this project is compatible with the General Plan and meets the guidelines we are trying to achieve for keeping our neighborhoods vital and constantly upgraded.

Comm. Rheaume said he will be supporting the motion and can make the findings that this project meets all of the basic design principles. He noted that the only reason the project requires Planning Commission review is because it exceeds 45 percent FAR, and said the 35 percent target ratio of second-to-first floor is just a recommendation. He said he was faced with a similar dilemma, and that when you try working in all of the different numbers and percentages sometimes you step back and look at what you have and see that it is not feasible. He said he sees these percentages as guidelines and not facts, which is why they are listed as recommendations, and that what is important is to step back and ask if this is really what we want to build in our neighborhoods. He said he can make the findings that this is a nice design that will add high sill windows on the second floor in the back, that the project meets all the setback requirements, and said the applicant is investing in our community.

Comm. Klein said he will not be supporting the motion and that while he applauds the applicant for working with City staff to come with up a good plan, the mass of the proposed home is a major issue. He said one reason we have guidelines in place is to try to reduce the general bulk of homes built in our neighborhoods, and

that there are several nearby two-story homes with a second-to-first floor area ratio of 37 percent and that if this home was closer to that he feels it would be more applicable to the neighborhood overall. He noted that this home is on a corner lot and not progressively larger than those in the neighborhood and that with a little bit of work the applicant could reduce the second story to be within the City guidelines for single-family homes. He said he applauds the applicant for increasing the setbacks for the second story beyond the minimum requirement, and that he oftentimes sees people who are barely meeting the minimum requirements, but that meeting those minimums does not guarantee approval of a second story that is as big as the applicant wants. He said in general the design looks good but he worries about the massing and how visible it will be on the corner, and that the issue with plate height will make this home seem much taller than nearby homes. He added that he cannot make the findings due to the bulk and that with more work this home could fit well within the community.

Comm. Simons said he understands the various perspectives of the Commissioners on this project, and that he is trying to be consistent with different neighborhood issues and along with what is happening in Sunnyvale. He said this is becoming an Apple neighborhood which means people who want to live close to work are coming in and we will see more changes in the neighborhood, some of which have started and he expects will continue. He said the larger homes in this neighborhood are going to be potentially moderated by the Design Guidelines, and that the trend is not going to be people with growing families living in 1,300 square foot homes. He noted that the original intent for maximum sizes of homes on different lots in Sunnyvale was to allow homeowners to reinvest into their homes and not be forced to purchase a new home. He said he knows the massing of this home is bigger than many homes in this neighborhood, which are predominantly single story, and that there are a handful of older two-story homes that would not fit the Design Guidelines today, but that this one has been moderated. He said he understands what the applicant is trying to do and has spent the time necessary to make a number of changes, and that he would like to see people with growing families who would like to stay in their homes be able to expand them if they are compatible with the neighborhood. He added that this is a neighborhood in transition, that more space is needed and we will be seeing more of these homes, so he will be supporting the motion and can make the findings.

Comm. Harrison said she will not be supporting the motion, and she cannot make finding 2.2.2 that discusses respecting the scale, bulk and character in the adjacent neighborhood, specifically with regard to the second floor to first floor ratio and the ten foot plate heights on the first and second story. She said she also cannot make finding 2.2.3 regarding designing homes to respect the immediate neighbors.

Comm. Durham said he will be supporting the motion, and that although he wishes the plate heights were nine feet each, which would cut the bulk, the roofline will reduce the appearance of the bulkiness. He said that while we do want people to stay in their neighborhoods and invest and upgrade their houses, many of these houses are fairly old and in need of upgrade and people coming in will be pushing the limits. He said that this is going to be a good increase to the housing stock in the area, and he can make the findings for this project.

Chair Melton said he can make the findings and is supporting the motion, and that as long as he has been a Planning Commissioner, the second-to-first story ratio guideline has been a great topic of debate amongst the Commissioners on various projects and is one he holds near and dear to his heart. He said he has voted no on a number of projects along the way because the ratios were out of whack with the neighborhood, and that this does not seem to be the case in this particular instance, which leads him to the conclusion that the findings are made on this project.

MOTION: Vice Chair Olevson moved Alternative 1 to approve the Design Review with the Conditions in Attachment 4 of the staff report.

Comm. Rheaume seconded. The motion carried by the following vote:

Yes: 5 - Chair Melton
Vice Chair Olevson
Commissioner Durham
Commissioner Rheaume
Commissioner Simons

No: 2 - Commissioner Harrison
Commissioner Klein

PUBLIC HEARINGS/GENERAL BUSINESS

2

[15-0321](#)**File #:** 2014-8084**Location:** 1323 Sunnyvale Saratoga Road (APN: 323-10-015)**Applicant / Owner:** Café Stop, ABI Consulting Inc. / DRB Investment**Proposed Project:** Call for Review by the Planning Commission of a decision by the Zoning Administrator approving a **SPECIAL DEVELOPMENT PERMIT** for a new café with additional covered outdoor seating and associated site improvements..**Reason for Permit:** A Special Development Permit is required for the proposed project in the C-1/PD Zoning District.**Project Planner:** Shétal Divatia, (408) 730-7637, sdivatia@sunnyvale.ca.gov**Issues:** Front setback, landscaping, on-site circulation, and conformance with previously approved Special Development Permits (related to parking area).**Recommendation:** Approve the Special Development Permit subject to recommend Conditions of Approval.

Shetal Divatia, Senior Planner, presented the staff report, and noted a correction in the staff report.

Comm. Klein and Trudi Ryan, Planning Officer, discussed issues with the driveway apron and handicap parking stall that were not caught during first review of this project, and Ms. Ryan said this new project is an opportunity to correct those issues. Comm. Klein and Ms. Ryan also discussed the process by which the issue of overflowing garbage would be addressed.

Vice Chair Olevson and Ms. Divatia discussed the lack of shading in the parking lot, and confirmed with Ms. Ryan that this lot was developed for retail use before the 50 percent shading requirement was in place and that this is a legal nonconforming site. Vice Chair Olevson commented on the area available to the public having very little landscaping, and discussed with Ms. Ryan increasing the number of trees on the site. He also confirmed with Ms. Ryan that it is standard to ask for a title report to confirm that the property owner has signed off on the project.

Comm. Simons discussed with Ms. Ryan the possibility of using pervious paving for the lot.

Chair Melton verified with Ms. Ryan that City Council has appeal and call for review options available to review decisions of the Planning Commission. He discussed with staff the consequences of changing Condition of Approval (COA) PS-19(f) to have a non-removable fence, and changing PS-3 to have the applicant move the illegal outdoor storage container. Chair Melton confirmed with Ms. Ryan that AT-1(b) can be removed.

Comm. Klein discussed with Ms. Ryan the site improvements proposed when the business was changed from a flower shop to the restaurant, and discussed with Ms. Divatia the history of bicycle parking on the site. He also discussed potentially doubling the number of bike parking spaces with Ms. Ryan.

Chair Melton opened the public hearing.

Jonathan Miller, the project applicant, said he is available to answer any questions.

Vice Chair Olevson discussed with Mr. Miller potential options to improve the visual appearance of the site with additional landscaping, and options for resolving the noncompliant site issues.

Comm. Rheume stated that he visited the site and the applicant gave him a tour, and discussed with Mr. Miller his plans for restoring the brick of the building to its original color. Comm. Rheume also discussed the lack of landscaping on the site, and confirmed with Mr. Miller that he is amenable to adding planters along the edge of the property facing the direction of the high school.

Comm. Simons confirmed with Mr. Miller that the surface of the parking lot will be repaved and restriped, and discussed further Mr. Miller's plan to sandblast the brick.

Comm. Durham discussed with Mr. Miller whether the wood storage area can potentially overflow and take up a number of parking spaces, and confirmed with Mr. Miller that he is amenable to adding more bicycle parking spaces.

Chair Melton stated that he visited the site and was given a tour by the applicant, and confirmed with Mr. Miller that the old vehicles that were stored on the site have been removed and will not be stored on the streets or parked on the site in the future. Chair Melton also confirmed with Mr. Miller that there is no fence between his property and the adjacent street acting as a driveway to his property.

Comm. Rheume verified with Mr. Miller the amount of seating inside and outside of the proposed restaurant.

Comm. Harrison confirmed with Mr. Miller the purpose of the storage shed next to the Falafel Stop restaurant.

Biljana Simsic, a nearby Sunnyvale resident, discussed her concerns with the

project.

Allen Bertrand, with ABI Consulting, discussed the need for outdoor seating, and Mr. Miller addressed the concerns of the member of the public.

Comm. Klein confirmed with Mr. Miller that there are no standing areas for people to eat and that the counter is for food service only and not seating. Comm. Klein discussed further with Mr. Miller the neighbor's issue with the gate and fence.

Ms. Ryan noted that the plans show that the brick and wall around the seating area are to remain blue, and that what the applicant is describing is removing the paint. Chair Melton confirmed with Mr. Miller that his preference is to return the brick to its natural color.

Chair Melton closed the public hearing.

Comm. Durham discussed with Ms. Ryan the issue of patrons parking offsite on Belfry and additional enforcement measures that would limit the amount of parking on Belfry.

Comm. Klein verified with Ms. Ryan that there are two employee-only parking spaces on the site, and discussed COA BP-15 that would require employees to park onsite. Comm. Klein commented on two designated employee parking spaces being the bare minimum for two simultaneously operating restaurants, and discussed the option of requiring a Parking Management Plan (PMP). Comm. Klein and Ms. Ryan also discussed potentially requiring that the gate be operable only to nearby residents.

Comm. Simons discussed further the option for having the pedestrian gate operated only by residents to gain access to the park, and confirmed that staff is comfortable coordinating the color of the brick with the applicant.

Comm. Simons moved Alternative 2 to approve the Special Development Permit with modifications:

- 1) If the color of the brick is going to be natural it must be in good condition and that the colors of that part of the building are coordinated with staff in the future;
- 2) The parking lot surface will be consistent at the end of the project except for the concrete trash skirt;
- 3) Have a minimum of two marked employee parking spots;
- 4) Remove Condition of Approval AT-1;
- 5) Have a minimum of eight Class I bicycle parking spots for the site; and

6) Have limited access for the Belfry gate via a key or passcode.

Comm. Rheume seconded and offered a friendly amendment to remove the illegal second storage bin. Comm. Simons stated that he is having difficulty with that amendment. Chair Melton noted that the applicant would go through the Miscellaneous Plan Permit process to make it legal. Comm. Rheume withdrew that friendly amendment and offered another amendment to ensure that a PMP is in place.

Comm. Simons accepted.

Comm. Simons thanked Chair Melton for calling this project up for review, and said it has the potential to be improved for the City and the applicant and that the changes will be make the site more usable. He said he has noticed that restaurants with a larger amount of bike parking bring in a whole new crowd to the area, and he thinks that adding bike parking will be a useful thing since there is limited land at this site. He said other issues with this development include parking but that the Parking Management Plan will be extremely useful. He said he thought about stopping in and had to drive around because he could not find parking on the site and ended up parking on the street, and said bike parking and upgrades to the building will be a big improvement.

Ms. Ryan and Comm. Simons corrected the modification to have a minimum of eight Class II bicycle parking spaces, and Ms. Ryan noted that a PMP is already required in COA BP-15.

Comm. Rheume said he appreciates Chair Melton bringing this project up for review to the Planning Commission, and that this is a good plan. He said he had some initial concerns, but that going onsite and seeing what the applicant will do with this part of the property and seeing what he did with the falafel restaurant, he sees a quality building put together there and he has proof that the applicant does quality work. He said he is glad staff recommended the limited access gate and that hopefully it is a good compromise with the neighbors and the applicant so that we will not be cutting off easy walkable space to the parks and schools, especially for the kids. He added that this is a win-win situation for everyone, that he can make the findings and that the project meets the General Plan goals and objectives.

Comm. Klein said his issue with the PMP is that it requires the employees to park onsite, and offered a friendly amendment to allow offsite employee parking.

Comms. Simons and Rheume accepted.

Comm. Klein offered another friendly amendment to have staff review the PMP six months after the opening of the second restaurant.

Comms. Simons and Rheaume accepted.

Comm. Klein said he will be supporting the motion, and that this business is in a key location that is close to the high school and has a lot of traffic going through it. He said the modifications will alleviate some of the issues with the neighbors and deal with the parking problems, and that this plan is better than how it started. He said he looks forward to the applicant refurbishing and refurnishing this dated building which can ultimately become a destination. He said he is hoping the parking issue will improve with the removal of some of the current cars onsite and the PMP trying to deal with where employees will be parking and with the business owner paying attention to how many employees will be working during different shifts. He added that all of these changes will improve the functionality of the new and existing businesses.

Vice Chair Olevson said he will be supporting the motion, and that he is disappointed that we do not have the opportunity to impose more landscaping requirements that would improve the visual appearance of this lot from the neighborhoods. He noted that up and down Sunnyvale-Saratoga Avenue and the north and south passageways through the City landscaping on the center strip has done a great job of improving the entrance into Sunnyvale, and that many homes to the south have additional landscaping in the form of trees and vines. He said the proposed landscaping is minimal and will not enhance Sunnyvale but that recognizing the constraints we have with the existing building and permitting, he will be reluctantly supporting the motion.

Comm. Durham said he will be supporting the motion, that he can make the findings and is glad to see an increase in bicycle parking. He said the gate access is just a little too much because he sees it as a good place allowing people to get to the restaurant in terms of walkability and bikeability and keeps people off of the busy streets of Fremont and Sunnyvale-Saratoga. He said he is unsure about how to get a passkey out to residents of the Nimitz, Zurich, Exeter neighborhoods, and that some people would readily want to go to Belfry to get to the restaurant if it is as good as it is supposed to be. He said overall this is a good plan and thanked the applicant for bringing it forward.

Chair Melton said he will be supporting the motion, and thanked the applicant for coming forth and making a capital improvement to a pretty outdated building. He

congratulated the applicant on the success of the Falafel Stop restaurant and said part of the traffic issues we are seeing is due to that success. He said he is pleased that the PMP will work through that, and he thanked the member of the public for coming out and voicing her concerns and thanked staff and the Zoning Administrator. He noted that this call for review is part of a new process that starts with staff and goes to the Zoning Administrator, and that while no particular reason is required for calling an item up for review, his motivation was that a lot was going on with this project which is on a complicated site. He said he is also pleased with where the project is heading, that he can make the findings and is looking forward to it coming to fruition.

Comm. Simons said the lack of landscaping was an issue to many Commissioners and asked if Vice Chair Olevson had any recommendations, to which Vice Chair Olevson responded that he would like to see greater landscaping in the parking area, including trees for shading. Chair Melton confirmed with Vice Chair Olevson that he is offering a friendly amendment to have the applicant will work with staff to explore opportunities for additional tree landscaping in the parking lot.

Comms. Simons and Rheume accepted.

MOTION: Comm. Simons moved Alternative 2 to approve the Special Development Permit with modifications:

- 1) If the color of the brick is going to be natural it must be in good condition and that the colors of that part of the building are coordinated with staff in the future;
- 2) The parking lot surface will be consistent at the end of the project except for the concrete trash skirt;
- 3) Have a minimum of two marked employee parking spots;
- 4) Remove Condition of Approval AT-1;
- 5) Have a minimum of eight Class II bicycle parking spots for the site;
- 6) Have limited access for the Belfry gate;
- 7) Allow offsite employee parking;
- 8) Review the parking management plan six months after the opening of the second restaurant; and
- 9) Applicant will work with staff to explore opportunities for additional tree landscaping in the parking lot.

Comm. Rheume seconded. The motion carried by the following vote:

Yes: 7 - Chair Melton
Vice Chair Olevson
Commissioner Durham
Commissioner Harrison
Commissioner Klein
Commissioner Rheaume
Commissioner Simons

No: 0

- 3 [15-0305](#) **File #:** 2014-7985
Location: 1050-1060 Helen Avenue (APNs: 213-35-009, 213-35-010)
Zoning: C-2/ECR (Commercial Highway Business/Precise Plan for El Camino Real)
Proposed Project: Related applications on a 0.59-acre site:
 SPECIAL DEVELOPMENT PERMIT: to construct 7 two-story homes (3 duets and 1 detached home)
 VESTING TENTATIVE MAP: to subdivide 2 lots into 8 lots, including 7 ownership lots and one common lot.
Applicant / Owner: Fred Azarm (applicant) / FMA Development LLC, D'Ambrosio Brothers Investments Company (owners)
Environmental Review: Mitigated Negative Declaration
Project Planner: Timothy Maier, (408) 730-7257, tmaier@sunnyvale.ca.gov

Timothy Maier, Assistant Planner, presented the staff report and noted one correction to the Conditions of Approval (COA).

Comm. Klein verified with Mr. Maier which COA had been corrected and the location of the fence referenced in COA PS-1(a). Comm. Klein also discussed with Mr. Maier potentially adding a COA that would require a trellis or other options to reduce the visual impact from lot 4 of the adjacent commercial buildings.

Comm. Simons confirmed with staff the intent of the decorative pavers in COAs BP-11(e) and (f), and confirmed that the appearance of the address 199 N. Sunnyvale Ave. in the report is a mistake. Comm. Simons also confirmed with staff that a COA could be added to require the veneer stone to wrap around the proposed masonry wall until it intersects another plane.

Comm. Durham noted typos on pages 4 and 8 of the staff report, and clarified with Mr. Maier the setback requirements for the proposed homes.

Comm. Harrison discussed with Mr. Maier how the commercial zoning of the property allows for a smaller residential density of the project, and Comm. Harrison commented on this property having the potential to fulfill General Plan policy LT-3.4A. Trudi Ryan, Planning Officer, stated that if the Planning Commission can provide further direction or deny the request if uncomfortable with this property being used for residential or with the proposed density.

Vice Chair Olevson and Mr. Maier discussed the intent of the statement on page 3 of the staff report acknowledging the loss of potential commercial space and whether the use of the R-3 zoning district as a comparison allows the applicant to avoid Below Market Rate (BMR) requirements.

Chair Melton verified with Mr. Maier only one exterior color rendering of the project was provided.

Chair Melton opened the public hearing.

Mehdi Sadri, a representative of FMA Development, LLC and D'Ambrosio Brothers Investments Company, gave a presentation on the proposed project.

Comm. Harrison discussed with Mr. Sadri the cost for which the owners anticipate selling the proposed properties.

Chair Melton closed the public hearing.

MOTION: Comm. Harrison moved Alternative 3 adopt the Mitigated Negative Declaration and deny the Special Development Permit and Tentative Map.

The motion failed for lack of a second.

Comm. Rheume moved Alternative 2 to adopt the Mitigated Negative Declaration and approve the Special Development Permit and Tentative Map with a modified condition to have the applicant work with staff on the color palette.

Comm. Klein seconded and offered a friendly amendment to add PS-1(h) to work with staff to add a trellis, ivy and appropriate landscaping to mask the view of the commercial buildings from lot 4.

Comm. Rheume accepted, and said he can make the findings for the project. He said it brings more housing to the community, that it is a nice, quality design and that seven units is sufficient. He said to just have two long rows of garages looking down the street would not be desirable, and that these two units facing Helen makes it more welcoming and a better transition for the street.

Comm. Klein noted that he was on the Planning Commission in 2007 when this site was initially approved for a mixed use project and that he wishes it went forward. He said he has some reservations regarding the density but that he supports the addition of single family homes, of which we do not see many new ones, and that we normally see larger scale multi-family projects. He said redeveloping two single family homes into seven is a move forward as far as having more residences within the city, but that whether or not the project should be a series of townhomes is a question in his mind. He said redoing this site and the site to the south into a full

mixed use project is the right transition to El Camino Real, but now that the business on the corner of El Camino and Helen is doing much better that possibility is basically out of way. Comm. Klein said this is an improvement in trying to get more density and creating a better fit with the community to the north that has a higher density. He said he still worries about the noise and he is hoping the condition to add large trees around the east, north and south of the site will help deal with some of the noise issues, although he worries about those trees and the number of them achieving the appropriate barrier. He said the trellis will add some visual masking to the businesses to the east and will provide some aesthetic covering until the landscaping is better grown, and that he also worries about the proposed colors being too similar to the two tones of the buildings to the north and hopes staff can deal with the final design. He added that he likes the additions of project, that the design is a good use of the space and the homes are generally a good design for the neighborhood.

Comm. Simons offered a friendly amendment to include the modification for the veneer stone to wrap to a logical depth.

Comms. Rheame and Klein accepted.

Comm. Simons discussed with staff exploring with the applicant choosing a species of tree that would mitigate noise, and discussed the appropriate language to use to in the color palette modification that would indicate that colors should contrast with adjacent buildings and be more lively.

Vice Chair Olevson said he will be supporting the motion, and can make the findings in the strange way we are approaching this project by choosing the zoning on the fly. He said we are not a policy-making group, and he hopes that if the City Council reviews this session they give thought to how we choose zoning when it is not clear cut for our purposes and for applicants. He said this is a well thought out project, that over the course of the last several weeks the owners of this property have made several attempts to make a useful project and that it appears that this one should be very successful.

Comm. Harrison said she will not be supporting the motion, not because she does not appreciate the applicant's journey and the actual product and size of the site, but because it does not meet the General Plan with regard to locating higher density housing near transportation corridors, employment and commercial services.

Comm. Durham said he will be supporting the motion, and that while he would like

to see a slightly greater density in this place he cannot imagine putting another townhouse type unit in here. He said just having two walls of garages would get two other buildings and that we are going from two houses to seven which is a 250 percent increase. He said that if the site remains commercial we end up losing housing, and that this is a quality product that is slightly different than what we normally see, it being two stories instead of three.

Chair Melton said he will be supporting the motion and can make the findings. He said this is a good project and he looks forward to it coming to fruition.

FINAL MOTION: Comm. Rheaume moved Alternative 2 to adopt the Mitigated Negative Declaration and approve the Special Development Permit and Tentative Map with modified conditions:

- 1) Colors proposed for the exteriors of buildings must be modified to introduce additional hues and/or greater contrast, resulting in a lively appearance; applicant shall coordinate with City staff to determine a suitable color palette, subject to review and approval by the Director of Community Development;
- 2) Include a trellis, landscaping, and/or similar feature(s) for the purpose of preventing visibility of adjacent commercial buildings from proposed Lot 4;
- 3) Any wainscoting to be placed on any building facade wrap a minimum of four feet around any facade not facing a public street or drive aisle, or wrap to a natural termination point such as a fence, gate, or similar structure or feature; and
- 4) Landscaping plans must be modified such that evergreen (non-deciduous) trees which generate minimal sound when disturbed by wind and which maintain canopies of thick foliage year-round, providing assistance with mitigation of noise impacts, be placed on the northerly, southerly, and easterly portions of the property's perimeter.

Comm. Klein seconded. The motion carried by the following vote:

Yes: 6 - Chair Melton
Vice Chair Olevson
Commissioner Durham
Commissioner Klein
Commissioner Rheaume
Commissioner Simons

No: 1 - Commissioner Harrison

- 4 [15-0208](#) Design Guidelines for mixed-use projects, known as the Toolkit for Mixed-use Developments; Find that the project is exempt under CEQA pursuant to Guidelines 15060(3) and 15378(b)(5) (Study Issue)
Project Planner: Andrew Miner, (408) 730-7707,
aminer@sunnyvale.ca.gov

Comm. Durham handed out an information only document for this item.

Andrew Miner, Principal Planner, presented the staff report.

Comm. Simons discussed with staff adding a reference section to the Santa Clara Valley Transportation Authority (VTA) pedestrian and bicycle design guidelines.

Comm. Klein discussed with staff the reasoning behind the recommendation to have up to 15 percent of the building facade stepped back beyond the setback, and discussed having a loading zone for residential. Comm. Klein suggested having varied building heights when the length of the side of a building exceeds a certain limit, and discussed with Mr. Miner strengthening the language that recommends constructing electric charging stations.

Comm. Harrison and Mr. Miner discussed the inclusion of pocket parks in the toolkit, and clarified with staff the parking requirement.

Vice Chair Olevson confirmed with Mr. Miner that the staff report is not intending to use the Sierra Club's guidelines as City policy. Mr. Miner added that City staff wrote the guidelines and the Sierra Club's guidelines are for reference only.

Comm. Rheume suggested articulating further in the guidelines directions about obtaining an organic look by recommending using different window types, colors, styles and sizes, and different rooflines.

Comm. Durham discussed the handout he provided and suggested including in the guidelines regarding separation of uses and staggered delivery times to be off commute hours and limiting the size of vehicles. He also suggested adding language to ensure bicycle parking is installed correctly and in thoughtful locations. Mr. Miner added that these issues are part of Citywide policy.

Comm. Klein discussed with Mr. Miner specifically noting an option for vertical bike racks along the walls in the guidelines.

Trudi Ryan, Planning Officer, suggested including under the resources section a list of additional resources such as the VTA guidelines and Citywide guidelines that

discuss some of the features commented on by the Commissioners.

Chair Melton opened the public hearing and upon seeing no speakers for this item, closed the public hearing.

Comm. Klein moved Alternatives:

- 1) Find that the project is exempt from CEQA pursuant to CEQA Guidelines 15060(3) and 15378(b)(5); and
- 2) Approve the Toolkit for Mixed-use Developments (Attachment 2) for mixed-use projects in all zoning districts with recommendations discussed.

Comm. Simons seconded.

Comm. Klein thanked Mr. Miner for his hard work writing the toolkit, and said he knows the need and desire for mixed use is a high goal for the City moving forward, and for most cities it is a placard placed upon all developments as something to strive for. He said the toolkit puts some of the guidelines in place that help developers understand the give and take and what they should be looking for when designing projects, and that too often they are told to go build projects and can potentially come up with a plan that is unusable and does not take into consideration all of the requirements of mixed use projects. He said what staff has done is try to put the guidelines in place to define the things that should be considered and the trade-offs that need to be made. He added that he thinks this is a good step forward for future, well designed mixed use projects, and makes it easier for staff to work with developers and easier for the Planning Commission and City Council to give feedback when looking at projects.

Comm. Simons verified with Mr. Miner that the motion includes the Commissioners' comments, and said adding in extra references for different specific guidelines will make it more of a one-stop product for mixed use for business and developers, and he sees it as a beginning in helping Sunnyvale continue to develop more intelligently with more integrated projects in the future.

Chair Melton said he will be supporting the motion, and appreciates staff for a job well done on this document, which will be a tremendous value to the City, applicants, staff, the Zoning Administrator, Planning Commission and City Council.

MOTION: Comm. Klein moved Alternatives:

- 1) Find that the project is exempt from CEQA pursuant to CEQA Guidelines 15060(3) and 15378(b)(5); and
- 2) Approve the Toolkit for Mixed-use Developments (Attachment 2) for mixed-use

projects in all zoning districts with the following recommendations:

- a) Add a reference section to VTA pedestrian and bicycle design guidelines;
- b) Clarify guidelines relating to building heights for longer buildings, amount of building facade setback, loading areas and pocket park reference.

Comm. Simons seconded. The motion carried by the following vote:

Yes: 7 - Chair Melton
Vice Chair Olevson
Commissioner Durham
Commissioner Harrison
Commissioner Klein
Commissioner Rheaume
Commissioner Simons

No: 0

5 [15-0331](#) **Standing Item:** Potential Study Issues for 2016

None.

NON-AGENDA ITEMS AND COMMENTS

-Commissioner Comments

None.

-Staff Comments

Ms. Ryan discussed Planning related City Council items, and announced the proposal for creation of a Community Advisory Committee for the update to the Precise Plan for El Camino Real and that staff will be looking for representatives from the various Commissions to serve on that Committee. Ms. Ryan also noted that staff has received an appeal of the recently heard Design Review on Lois Avenue.

INFORMATION ONLY ITEMS

None.

ADJOURNMENT

With no further business Chair Melton adjourned the Planning Commission meeting at 11:51 p.m.



City of Sunnyvale

Agenda Item

15-0447

Agenda Date: 4/13/2015

Approval of the 2015 Planning Commission Master Work Plan

2015 Master Work Plan

Planning Commission Annual Calendar

List all regularly scheduled board/commission meetings, specific issues identified in the Tentative Council Meeting Agenda Calendar, and routine assignments specific to each board or commission.

Meeting Date	Agenda Item/Issue
January 12	SS <i>Joint Study Session w/ HHSC re: Rental Housing Impact Fee for Market-Rate Rental Development</i>
	PH Routine Planning Items
January 26	SS Routine Planning Items
	PH Routine Planning Items
	PH <i>Moffett Park Specific Plan Amendment Initiation</i>
February 9	SS Routine Planning Items
	PH Routine Planning Items
	PH <i>Consideration of a Rental Housing Impact Fee for Market-Rate Rental Development</i>
	PH Study Issue – <i>Tandem and Stacker Parking</i>
February 23	SS Routine Planning Items
	PH Routine Planning Items
February 24	<i>Joint Study Session w/City Council re:</i> 1) <i>Review of Draft Lawrence Station Area Plan</i> 2) <i>Peery Park Specific Plan</i>
<i>March 4-6</i>	<i>Training - League of CA Cities Planning Commissioners Academy Newport Beach, CA</i>
March 9	SS Routine Planning Items
	PH Routine Planning Items

Meeting Date	Agenda Item/Issue
March 23	SS Routine Planning Items PH Routine Planning Items PH Quarterly Consideration of GPIs <i>PH Toolkit for Mixed Use Developments</i>
April 13	SS Routine Planning Items PH Routine Planning Items PH Approve Master Work Plan¹ <i>PH Peery Park Specific Plan</i>
April 27	SS Routine Planning Items PH Routine Planning Items
May 11	SS Routine Planning Items PH Routine Planning Items PH Review Recommended Budget²
May 12	Joint Study Session w/City Council re: <i>Regulating Short-term Residential Rental Units (e.g. AirBnB)</i>
May 19	Joint Study Session w/City Council re: <i>Land Use and Transportation Element (LUTE)</i>
May 27 (Wednesday)	SS Routine Planning Items PH Routine Planning Items PH Alternative Date for Review of Recommended Budget²
June 8	SS Routine Planning Items PH Routine Planning Items PH Quarterly Consideration of GPIs
June 22	SS Routine Planning Items Recognition of Commission members PH Routine Planning Items <i>PH Study Issue – Design Guidelines for Parking Structures</i>

Meeting Date	Agenda Item/Issue
July 13	SS Routine Planning Items PH Routine Planning Items PH Election of Officers³ PH Study Issue – <i>Appropriate Locations for Child Care Facilities</i>
July 27	SS Routine Planning Items PH Routine Planning Items PH Study Issue – <i>Policies Regarding Private Security Cameras</i>
August 10	SS Routine Planning Items PH Routine Planning Items
August 24	SS Routine Planning Items PH Routine Planning Items PH Study Issue – <i>Regulating Short-term Residential Rental Units (e.g. AirBnB)</i>
September 14	SS Routine Planning Items PH Routine Planning Items
September 28	SS Routine Planning Items PH Routine Planning Items PH Final Month to Propose Study Issues⁴ PH Quarterly Consideration of GPIs
October 12	SS Routine Planning Items PH Routine Planning Items PH Study Issue – <i>Evaluate Timing of Park Dedication In-lieu Fee Calculation and Payment</i>
October 26	SS Routine Planning Items PH Routine Planning Items

Meeting Date	Agenda Item/Issue
November 9	SS Routine Planning Items
	PH Routine Planning Items
	PH Final Month to Rank Study Issues⁵
	PH Study Issue – Consider Multi-family Residential Transportation Demand Management Programs
November 23	SS Routine Planning Items
	PH Routine Planning Items
December 14	SS Routine Planning Items
	PH Routine Planning Items
	PH Quarterly Consideration of GPIs
	PH Final month for Annual Review of Code of Ethics and Conduct for Elected and Appointed Officials⁶

Additional items yet to be scheduled:

1. Lawrence Station Area Plan
2. Peery Park Specific Plan
3. Land Use and Transportation Element (LUTE)
4. Update to Precise Plan for El Camino Real

- *Training will be scheduled based on workload of routine planning applications*
- *Schedules are subject to change*

Footnotes

1

Council Policy requires that all boards and commissions create an annual work plan, defined as a 12-month calendar of the policy issues the body will be acting on during the year. Since Council does not approve until February the issues it will ask each of the boards and commissions to study, these work plans cannot be adopted by the boards and commissions until March.

2

May is the month for boards and commissions to perform an official review and to comment on the City Manager's recommended budget to Council—i.e., make recommendations to Council regarding priorities and service levels, fees, and/or capital projects. Given the budgetary process, the City Manager's recommended budget will typically not be ready for boards and commissions to review until shortly before the time that a recommendation is necessary from the advisory body---in other words, there will be a very short turnaround time

for boards and commissions---in fact, often there is not enough time to perform a thorough review and get the results to Council without conducting a special meeting of the board/commission and subsequently sending the Chair or his/her designated representative to Council's public hearing to convey recommendations in person (waiting for the normal board/commission minutes to reach Council won't work). This underscores the importance of a board/commission being prepared to conduct its review—to understand its role, to know how to navigate the budget and be familiar with its contents---*before* the issue is brought before it in May.

3

Unless otherwise dictated by City Charter, each board and commission shall, within the month of July each year, or during the next regularly scheduled meeting if a July meeting is not otherwise necessary, elect one of its members as presiding officer, to serve commencing after the end of the meeting, upon completion of mandatory chair training and ideally in time to advise staff on the agenda for the next regularly-scheduled meeting. The Board of Building Code Appeals must meet in July to select a Chair and Vice Chair if no meetings if no meetings are scheduled in the future. The selection of Chair and Vice Chair shall be the last item on the agenda at the scheduled meeting. All boards and commissions shall select their chair and vice chair in accordance with practices and procedures outlined by the Office of the City Clerk.

4

Boards and Commissions may propose study and budget issues throughout the year. In order to be considered by Council for study in the following year, however, all study issues must be proposed by boards and commissions by no later than September. Any Study Issues proposed by a board/commission after September shall roll over to the next year's process (the only other recourse a b/c member has is to get Council to sponsor the Study Issue Paper). Study Issue papers must be signed by the City Manager prior to being ranked by a board or commission.

5

Boards and Commissions must rank their study issues by no later than November of each year so that staff can complete the work necessary to add these issues to the list Council will consider for ranking early in the next calendar year. Chairs of boards and commissions are encouraged to attend the Council's public hearing on study/budget issues to champion the issues ranked and prioritized by their body.

6

All Boards and Commissions must review the City's Code of Ethics and Conduct for Elected and Appointed Officials annually and by no later than December.



City of Sunnyvale

Agenda Item

15-0381

Agenda Date: 4/13/2015

REPORT TO PLANNING COMMISSION

SUBJECT

File #: 2015-7086

Location: 394 South Pastoria Avenue (APN: 165-12-002)

Zoning: R2 (Low-Medium Density Residential) Zoning District

Proposed Project:

DESIGN REVIEW: for a new two-story, single-family dwelling with a floor area of 2,727 square feet (2,325-square foot living area and 402-square foot garage) and 49.8% FAR. The existing one-story, single-family home would be demolished.

Applicant / Owner: SC Design Group (applicant) / Kaiwei Yao and Rao Shen (owner)

Environmental Review: Categorically Exempt Class 3

Project Planner: Momoko Ishijima, (408) 730-7532, mishijima@sunnyvale.ca.gov

REPORT IN BRIEF

General Plan: Low Medium Density Residential (7-14 du/ac)

Existing Site Conditions: Single-family residence

Surrounding Land Uses

North: Duplex

South: Single-family residence

East: Single-family residence

West: Single-family residence and Sunnyvale Unified School District property

Issues: Neighborhood compatibility and Floor Area Ratio

Staff Recommendation: Approve the Design Review with conditions

BACKGROUND

The existing residence was constructed in 1938. A Tree Removal Permit was approved in January 2015 for the removals of a protected walnut tree in the front yard (poor condition) and a protected maple tree in the rear yard (diseased).

Description of Proposed Project

The applicant proposes to demolish the existing 1,040-square foot single-story, single-family residence and to construct a new two-story, single-family residence on a 5,475-square foot lot. The proposed building size would total 2,727 square feet including a 402-square foot garage with a resulting floor area ratio (FAR) of 49.8%. A Design Review is required for construction of a new residence to evaluate compliance with development standards and with the Single Family Home

Design Techniques. Planning Commission review is required for homes that exceed 45% FAR. See Attachment 1 for a map of the vicinity and mailing area for notices and Attachment 2 for the Data Table of the project.

ENVIRONMENTAL REVIEW

A Class 3 Categorical Exemption relieves this project from the California Environmental Quality Act (CEQA) provisions. Class 3 Categorical Exemptions includes new construction or conversion of small structures.

DISCUSSION

Architecture: The existing neighborhood is comprised of both one- and two-story residences with a mix of architectural styles including ranch, cottage, bungalow and contemporary Mediterranean with earlier homes constructed in the 1920's and 30's and newer homes constructed more recently. The applicant proposes to demolish the existing cottage style home, which does not have a garage. The new two-story residence would be a traditional style home with stucco exterior, stone base, a gabled entry porch and a two-vehicle garage facing South Pastoria Avenue. The roof has a mix of hipped and gabled roof forms with composition shingle roof material.

The proposed second floor, consisting of three bedrooms and two bathrooms, would generally be located over the first floor to the south with increased setbacks on all sides. A 45-square foot balcony is proposed off of the master bedroom to the rear.

Floor Area Ratio: A residential project with an FAR greater than 45% requires review by the Planning Commission. The neighborhood contains a mix of one- and two-story homes and both adjacent homes on South Pastoria Avenue are two story. The FAR of the neighboring residences range from 13.6% to 72.7% with an average of 31.9%. The older homes are smaller. Some have no garages. There are a number of properties that were developed with duplexes and apartments around mid-century and the more recent developments of new homes have been approved with FAR exceeding 45%. The proposed FAR of 49.8% is greater than the average FAR in the neighborhood, however, the proposed design uses increased setbacks and varied setbacks in the front to help reduce the bulk and mass of the home.

Privacy: The proposed setbacks on the second floor exceed the minimum setback required. The closest home is a two-story duplex on the left side (to the north); however, the second floor will be more than 17 feet from the side property line where the minimum requirement is seven feet. The second floor on the south side is setback eight feet with a bathroom and three bedroom windows. The two-story neighbor on the south side has small windows on the second floor with a significant setback to the second floor and does not have conflicting privacy issues.

Solar Access: Sunnyvale Municipal Code (SMC) 19.56.020 states that no permit may be issued for any construction which would interfere with solar access by shading more than 10% of the roof of any structure on a nearby property. The proposed second story addition is set on the south side of the structure to minimize the shadowing on the adjacent left side neighbor to the north. The project plans demonstrate shading would not exceed the maximum level permitted thereby complying with current solar standards.

Landscaping: A protected walnut tree and maple tree were removed as part of a separate permit

(2015-7005), which required replacement trees to be planted. A condition of approval is recommended for the applicant to provide a replacement tree in the front yard and one in the rear yard (refer to Attachment 4) of 24-inch box sizes as part of this permit.

Applicable Design Guidelines and Policy Documents: The proposed home is consistent with the adopted Single-Family Design Techniques since the proposed design incorporates traditional design elements and positively adds to the streetscape. Staff has included findings for the Single-Family Design Techniques in Attachment 3.

Development Standards: The proposed project complies with the applicable Development Standards as set forth in the Sunnyvale Municipal Code, such as lot coverage, parking, height and setbacks.

Fiscal Impact: No fiscal impacts other than normal fees and taxes are expected.

Notice of Public Hearing, Staff Report and Agenda

- Published in the Sun newspaper
- Posted on the site
- 72 notices mailed to property owners and residents within 300 feet of the project site
- Posted on the City of Sunnyvale's Website
- Provided at the Reference Section of the City of Sunnyvale's Public Library
- Agenda Posted on the City's official notice bulletin board

Public Contact: One letter of support has been received from the neighbor across the street (Attachment 7).

Conclusion

Findings and General Plan Goals: Staff was able to make the required Findings based on the justifications for the Design Review and the Recommended Conditions of Approval (Attachment 4). Recommended Findings and General Plan Goals are located in Attachment 3.

Alternatives

1. Approve the Design Review with the conditions in Attachment 4.
2. Approve the Design Review with modified conditions.
3. Deny the Design Review and provide direction to staff and the applicant where changes should be made.

Recommendation

Recommended Alternative 1 in accordance with the Findings in Attachment 3 and Conditions of Approval in Attachment 4.

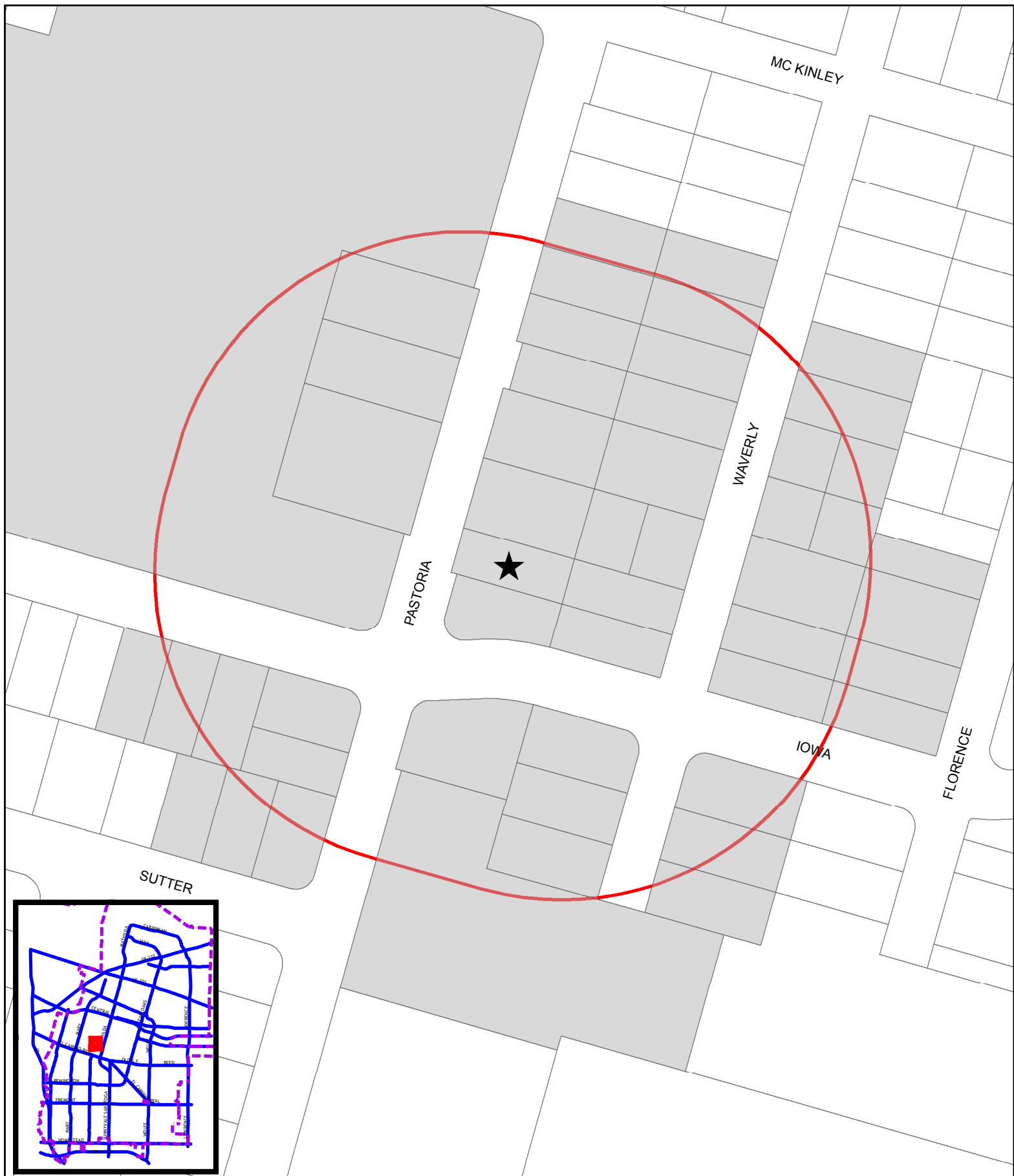
Prepared by: Momoko Ishijima, Associate Planner

Approved by: Gerri Caruso, Principal Planner

ATTACHMENTS

1. Noticing and Vicinity Map

2. Project Data Table
3. Recommended Findings
4. Recommended Conditions of Approval
5. Site and Architectural Plans
6. Floor Area Ratio Comparison
7. Letters from Neighbors



2015-7086
394 S Pastoria Ave. (APN: 165-12-002)
Design Review - 300 foot area Map

0 37.5 75 150 Feet



PROJECT DATA TABLE

	EXISTING	PROPOSED	REQUIRED/ PERMITTED
General Plan	Residential Low Medium Density	Same	Residential Low Medium Density
Zoning District	R-2	Same	R-2
Lot Size (s.f.)	5,475	Same	5,475
Gross Floor Area (s.f.)	1,040	2,727	2,464 (Threshold for Planning Commission Review)
Lot Coverage (%)	18.9%	35.4%	40% max.
Floor Area Ratio (FAR)	18.9%	49.8%	45% threshold (Threshold for Planning Commission Review)
Building Height (ft.)	14'	24' 11"	30' max.
No. of Stories	1	2	2 max.
Setbacks			
Front:			
1st Floor	23' 0"	20' 0"	20' min.
2nd Floor	N/A	25' 0"	25' min.
Right Side:			
1st Floor	4' 0"	6' 0"	6' min.
2nd Floor	N/A	8' 0"	7' min.
Left Side:			
1st Floor	12' 0"	4' 0"	4' min.
2nd Floor	N/A	17' 8"	7' min.
Rear:			
1st Floor	50' 0"	41' 0"	20' min.
2nd Floor	N/A	41' 0"	20' min.
Parking			
Total Spaces	2	4	4 min.
Covered Spaces	0	2	2 min.

Starred items indicate deviations from Sunnyvale Municipal Code requirements.

RECOMMENDED FINDINGS

Design Review

The proposed project is desirable in that the project's design and architecture conforms with the policies and principles of the Single Family Home Design Techniques.

Basic Design Principle	Comments
2.2.1 Reinforce prevailing neighborhood home orientation and entry patterns	The project home's entry and garage, and new front porch are oriented towards South Pastoria Avenue in a scale that is compatible with homes in the neighborhood.
2.2.2 Respect the scale, bulk and character of homes in the adjacent neighborhood.	The proposed home has been designed to reduce the apparent scale and bulk through increased setbacks and modest plate heights. The proposed home is within the allowable height of 30 feet.
2.2.3 Design homes to respect their immediate neighbors	The proposed design respects the privacy of adjacent neighbors by including second floor setbacks and reduced massing. Homes on both sides of the proposed homes are two stories.
2.2.4 Minimize the visual impacts of parking.	Two covered and two uncovered parking spaces are proposed as is the requirement.
2.2.5 Respect the predominant materials and character of front yard landscaping.	The exterior materials are similar to those found in the neighborhood and applied in a manner consistent with the architecture.
2.2.6 Use high quality materials and craftsmanship.	The proposed design includes high quality horizontal siding materials. These materials are consistent with the City's adopted Design Techniques and the surrounding neighborhood.
2.2.7 Preserve mature landscaping.	No protected trees are proposed to be removed with the project.

**RECOMMENDED
CONDITIONS OF APPROVAL AND
STANDARD DEVELOPMENT REQUIREMENTS
April 13, 2015**

Planning Application 2015-7086
394 South Pastoria Avenue

Design Review to allow the demolition of an existing single-story, single-family residence and the construction of a new two-story, single-family residence with a floor area of 2,727 square feet (2,325 square feet living area and 402 square feet garage) for a total floor area ratio of 49.8%.

The following Conditions of Approval [COA] and Standard Development Requirements [SDR] apply to the project referenced above. The COAs are specific conditions applicable to the proposed project. The SDRs are items which are codified or adopted by resolution and have been included for ease of reference, they may not be appealed or changed. The COAs and SDRs are grouped under specific headings that relate to the timing of required compliance. Additional language within a condition may further define the timing of required compliance. Applicable mitigation measures are noted with "Mitigation Measure" and placed in the applicable phase of the project.

In addition to complying with all applicable City, County, State and Federal Statutes, Codes, Ordinances, Resolutions and Regulations, Permittee expressly accepts and agrees to comply with the following Conditions of Approval and Standard Development Requirements of this Permit:

GC: THE FOLLOWING GENERAL CONDITIONS AND STANDARD DEVELOPMENT REQUIREMENTS SHALL APPLY TO THE APPROVED PROJECT.
--

GC-1. CONFORMANCE WITH APPROVED PLANNING APPLICATION:

All building permit drawings and subsequent construction and operation shall substantially conform with the approved planning application, including: drawings/plans, materials samples, building colors, and other items submitted as part of the approved application. Any proposed amendments to the approved plans or Conditions of Approval are subject to review and approval by the City. The Director of Community Development shall determine whether revisions are considered major or minor. Minor changes are subject to review and approval by the Director of Community Development. Major changes are subject to review at a public hearing. [COA] [PLANNING]

GC-2. PERMIT EXPIRATION:

The permit shall be null and void two years from the date of approval by the final review authority at a public hearing if the approval is not exercised, unless a written request for an extension is received prior to expiration date and is approved by the Director of Community Development. [SDR] [PLANNING]

GC-3. ENCROACHMENT PERMIT:

Prior to any work in the public right-of-way, obtain an encroachment permit with insurance requirements for all public improvements including a traffic control plan per the latest California Manual on Uniform Traffic Control Devices (MUTCD) standards to be reviewed and approved by the Department of Public Works. [COA] [PUBLIC WORKS]

BP: THE FOLLOWING CONDITIONS SHALL BE ADDRESSED ON THE CONSTRUCTION PLANS SUBMITTED FOR ANY DEMOLITION PERMIT, BUILDING PERMIT, GRADING PERMIT, AND/OR ENCROACHMENT PERMIT AND SHALL BE MET PRIOR TO THE ISSUANCE OF SAID PERMIT(S).

BP-1. CONDITIONS OF APPROVAL:

Final plans shall include all Conditions of Approval included as part of the approved application starting on sheet 2 of the plans. [COA] [PLANNING]

BP-2. RESPONSE TO CONDITIONS OF APPROVAL:

A written response indicating how each condition has or will be addressed shall accompany the building permit set of plans. [COA] [PLANNING]

BP-3. BLUEPRINT FOR A CLEAN BAY:

The building permit plans shall include a "Blueprint for a Clean Bay" on one full sized sheet of the plans. [SDR] [PLANNING]

BP-4. LANDSCAPE PLAN:

If the project is modified to include new landscaping over 1,000 square feet, separate review of landscape and irrigation plans is required. Landscape and irrigation plans shall be prepared by a certified professional, and shall comply with Sunnyvale Municipal Code Chapter 19.37 requirements. Landscape and irrigation plans are subject to review and approval by the Director of Community Development through the submittal of a Miscellaneous Plan Permit (MPP). [COA] [PLANNING]

BP-5. GREEN BUILDING:

All new residential construction must complete the GreenPoint Rated Checklist and achieve a minimum 80 points.

BP-6. BEST MANAGEMENT PRACTICES:

The project shall comply with the following source control measures as outlined in the BMP Guidance Manual and SMC 12.60.220. Best management practices shall be identified on the building permit set of plans and shall be subject to review and approval by the Director of Public Works:

- a) Storm drain stenciling. The stencil is available from the City's Environmental Division Public Outreach Program, which may be reached by calling (408) 730-7738.

- b) Landscaping that minimizes irrigation and runoff, promotes surface infiltration where possible, minimizes the use of pesticides and fertilizers, and incorporates appropriate sustainable landscaping practices and programs such as Bay-Friendly Landscaping.
 - c) Appropriate covers, drains, and storage precautions for outdoor material storage areas, loading docks, repair/maintenance bays, and fueling areas.
 - d) Covered trash, food waste, and compactor enclosures.
 - e) Plumbing of the following discharges to the sanitary sewer, subject to the local sanitary sewer agency's authority and standards:
 - i) Discharges from indoor floor mat/equipment/hood filter wash racks or covered outdoor wash racks for restaurants.
 - ii) Dumpster drips from covered trash and food compactor enclosures.
 - iii) Discharges from outdoor covered wash areas for vehicles, equipment, and accessories.
 - iv) Swimming pool water, spa/hot tub, water feature and fountain discharges if discharge to onsite vegetated areas is not a feasible option.
 - v) Fire sprinkler test water, if discharge to onsite vegetated areas is not a feasible option. [SDR] [PLANNING]
- BP-7. ENCROACHMENT CONDITIONS:
The building permit plans shall include all encroachment permit items. [COA] [PUBLIC WORKS]
- BP-8. CONSTRUCTION MATERIAL AND STAGING:
All construction related materials, equipment, and construction workers parking need to be managed on-site and not located in the public right-of-ways or public easements. [COA] [PUBLIC WORKS]
- BP-9. REPLACEMENT TREES:
One replacement tree in the front and one replacement tree in the rear yard shall be planted for the removed protected trees and shall be a specimen tree of at least 24-inch box size.
- BP-10. TREE PROTECTION PLAN:
The applicant shall submit a tree protection plan for staff review. The tree protection plan shall be prepared by a certified arborist and shall include recommendations for ensuring the viability of existing protected trees during development, grading, and excavation. The arborist should prepare recommendations for construction methods and protection of the deodar cedar tree and its roots, located at 396 S. Pastoria Avenue.

EP: THE FOLLOWING CONDITIONS SHALL BE ADDRESSED AS PART OF AN ENCROACHMENT PERMIT APPLICATION.

- EP-1. WATER METER:
Install new radio-read water meter and box. [SDR] [PUBLIC WORKS]
- EP-2. SEWER CLEANOUT:
Install new sewer cleanouts at the property lines for all existing and proposed sanitary sewer laterals to be used for the project. [SDR] [PUBLIC WORKS]
- EP-3. DRIVEWAY APPROACHES:
Remove existing driveway approach and install new driveway approach to comply with Americans with Disabilities Act (ADA) requirements and per city standard details and specifications. [COA] [PUBLIC WORKS]
- EP-4. STREET TREE:
Add a note to plant City street tree in park strip. [SDR] [PUBLIC WORKS]
- EP-5. DAMAGE TO EXISTING PUBLIC IMPROVEMENTS:
Developer shall be responsible to rectify any damage to the existing public improvements fronting and adjacent to the project site as a result of project construction to City's satisfaction by the Director of Public Works. [COA] [PUBLIC WORKS]
- EP-6. PUBLIC WORKS DEVELOPMENT FEES:
Developer shall pay all applicable Public Works development fees associated with the project, including but not limited to, utility frontage and/or connection fees, off-site improvements plan check and inspection fees, prior to map recordation or any permit issuance, whichever occurs first. The exact fee amount shall be determined based upon the fee rate at the time of fee payment. [COA] [PUBLIC WORKS]

DC: THE FOLLOWING CONDITIONS SHALL BE COMPLIED WITH AT ALL TIMES DURING THE CONSTRUCTION PHASE OF THE PROJECT.

- DC-1. BLUEPRINT FOR A CLEAN BAY:
The project shall be in compliance with stormwater best management practices for general construction activity until the project is completed and either final occupancy has been granted. [SDR] [PLANNING]
- DC-2. CLIMATE ACTION PLAN – OFF ROAD EQUIPMENT REQUIREMENT:
a. Idling times will be minimized either by shutting equipment off when not in use or reducing the maximum idling time to 5 minutes (as required by the California airborne toxics control measure Title 13, Section 2485 of California Code of Regulations [CCR]), or less. Clear signage will be

provided at all access points to remind construction workers of idling restrictions.

- b. Construction equipment must be maintained per manufacturer's specifications.
- c. Planning and Building staff will work with project application to limit GHG emissions from construction equipment by selecting one of the following measures, at a minimum, as appropriate to the construction project:
 - i. Substitute electrified or hybrid equipment for diesel- and gasoline-powered equipment where practical.
 - ii. Use alternatively fueled construction equipment on-site, where feasible, such as compressed natural gas (CNG), liquefied natural gas (LNG), propane, or biodiesel.
 - iii. Avoid the use of on-site generators by connecting to grid electricity or utilizing solar-powered equipment.
 - iv. Limit heavy-duty equipment idling time to a period of 3 minutes or less, exceeding CARB regulation minimum requirements of 5 minutes.
[COA] [PLANNING]

DC-3. DUST CONTROL:

At all times, the Bay Area Air Quality Management District's CEQA Guidelines and "Basic Construction Mitigation Measures Recommended for All Proposed Projects", shall be implemented. [COA] [PLANNING]

END OF CONDITIONS

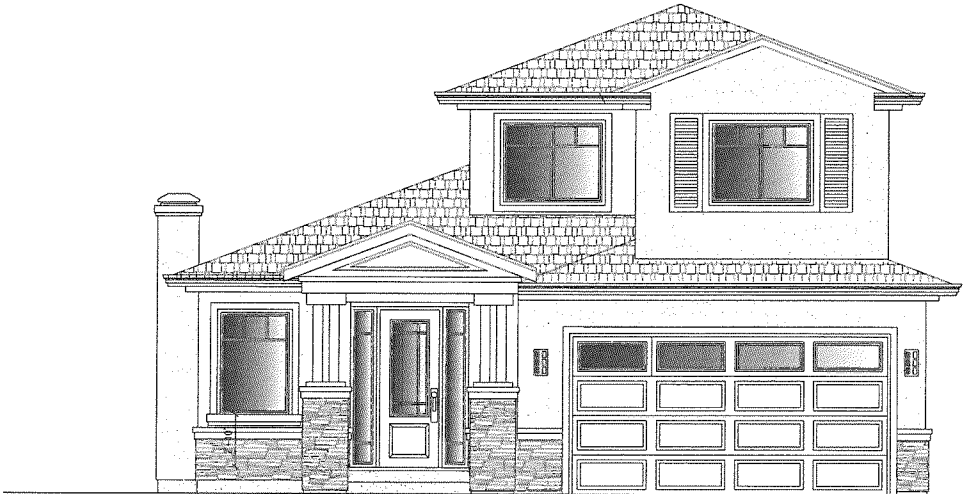
YAO PROJECT

YAO
PROJECT

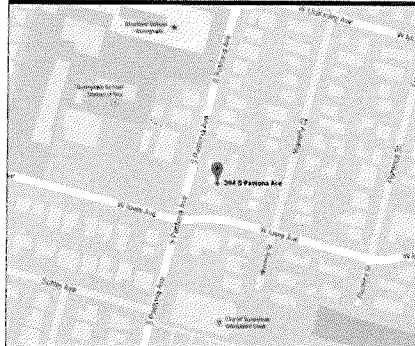
394 S. PASTORIA AVE,
SUNNYVALE, CA 94086



20370 TOWN CENTER LN
SUITE 139
CUPERTINO, CA 95014
408.865.0577



SUNNYVALE . CALIFORNIA

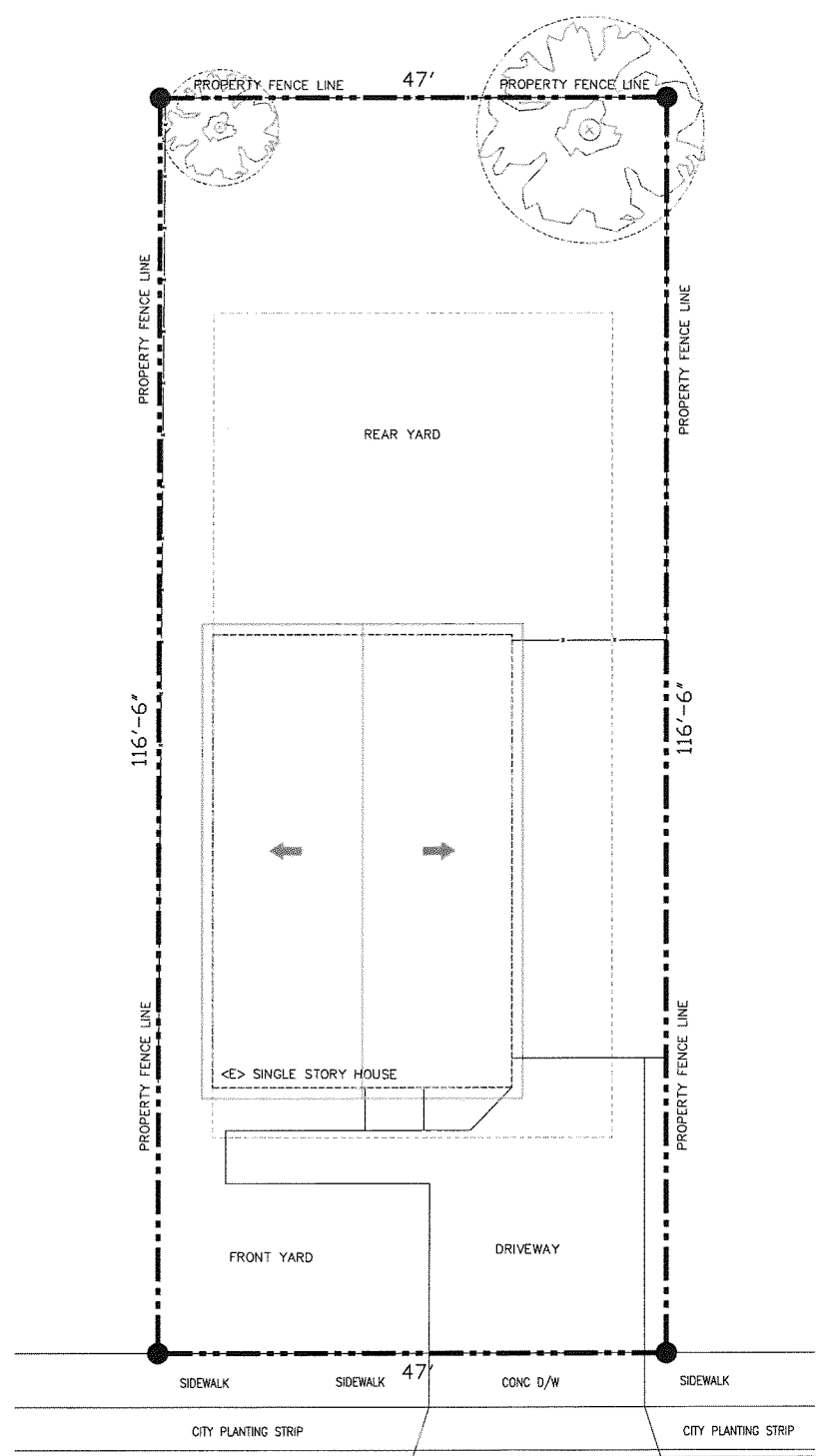
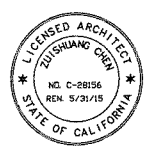
PROJECT INFO			PROJECT DATA		SHEET INDEX		GENERAL NOTES																
			FLOOR CALCULATION		ARCHITECTURAL		ADDITIONAL NOTES:																
VICINITY MAP			EXISTING HOUSE		A-0.0 COVER SHEET		PUBLIC WORKS NOTES: 1. WATER METER MAY BE UPGRADED / UPSIZED TO 1" RADIO READ PER CITY'S APPROVAL FIRE SPRINKLER SYSTEM DESIGN PLAN 2. NEW SEWER CLEANOUT PER CITY'S STANDARD DETAIL 15A PER AVAILABLE CITY'S UTILITY BLOCK MAP. PLEASE FIELD VERIFY THE EXISTING SEWER BEFORE INSTALLATION. 3. REMOVE THE EXISTING DRIVEWAY APPROACH AND RECONSTRUCT A NEW DRIVEWAY APPROACH PER CURRENT CITY'S STANDARD REQUIREMENTS AND NOT COMPLY WITH ADA REQUIREMENTS. 4. CITY STREET TREE IN PARK-STRIP																
			FIRST FLOOR 1,040 SQ.FT. F.A.R. / LOT COV CALD: 18.9% 1,040 SQ.FT.		A-1.0 SITE PLAN A-2.0 PROPOSED FLOOR PLAN A-3.0 PROPOSED ELEVATION A-3.1 CROSS SECTIONS A-4.0 BLOCK DIAGRAM / SOLAR STUDY																		
GENERAL INFORMATION			CODE & REGULATION		STRUCTURAL		FIRE ADDITION NOTES:																
			FIRST FLOOR 1,379 SQ.FT. SECOND FLOOR 946 SQ.FT. GARAGE 402 SQ.FT.																				
PROPERTY ADDRESS: 394 S. PASTORIA AVE, SUNNYVALE CA. 94086 APN: 165-12-002 DESCRIPTION OF WORK: <N> 2 STORY HOUSE WITH GARAGE ARCHITECT OF RECORD: SUSAN CHEN 20370 TOWN CENTER LN, SUITE 139 CUPERTINO, CA 95070 SChen@sgroup.com 408.865.0577 ZONING: R-2 OCCUPANCY GROUP: R-3 TYPE OF CONSTRUCTION: TYPE V-B STORIES: TWO STORY'S			EXCLUDED SQUARE FOOTAGE NOTE COVERED		ADDITIONAL NOTES:																		
			FRONT COVERED ENTRY 56 SQ.FT. CHIMNEY 10 SQ.FT. F.A.R. CALD: 45% 2,463.75 SQ.FT. F.A.R. MODIFICATION 49.8% @ 2,727 SQ.FT. (N) TOTAL BUILDING AREA ON SITE 2,727 SQ.FT.																				
LOT SIZE: 5.475 SQ.FT.			LOT COVERAGE		THE ROOF WILL COMPLY WITH COOL ROOF REQUIREMENTS OF THE 2013 C.E.C. 2013 110.8																		
SET-BACK INFORMATION			(N) FIRST FLOOR FOOT PRINT 1,781 SQ.FT. (N) PORCH 56 SQ.FT. (N) SECOND BALCONY OVERHANG 45 SQ.FT. (N) CHIMNEY 10 SQ.FT. TOTAL: 1,892 SQ.FT. TOTAL LOT COVERAGE 34.5% @ 1,892 SQ.FT.																				
SETBACK: <table><tr><td></td><td>REQUIRED:</td><td>PROPOSED:</td></tr><tr><td>FRONT</td><td>20'-0"</td><td>20'-0"</td></tr><tr><td>SIDE</td><td>5'-0"</td><td>5'-0"</td></tr><tr><td>REAR</td><td>20'-0"</td><td>44'-2"</td></tr><tr><td>MAX. HEIGHT</td><td>30'-0"</td><td>24'-11"</td></tr></table>				REQUIRED:	PROPOSED:	FRONT	20'-0"	20'-0"	SIDE	5'-0"	5'-0"	REAR	20'-0"	44'-2"	MAX. HEIGHT	30'-0"	24'-11"	LANDSCAPING		NFPA 13D AUTOMATIC SPRINKLER SYSTEM WILL BE INSTALLED. PLANS TO BE DEFERRED		REVISION 01.22.15 PLANNING SUBMITAL	
	REQUIRED:	PROPOSED:																					
FRONT	20'-0"	20'-0"																					
SIDE	5'-0"	5'-0"																					
REAR	20'-0"	44'-2"																					
MAX. HEIGHT	30'-0"	24'-11"																					
NOTES			(N) PAVED AREA 806 SQ.FT. (N) GRASS AREA 999 SQ.FT. (N) IVY AND GROUND 1,767 SQ.FT. TOTAL LANDSCAPED AREA: 999 SQ.FT. EXCLUDED SQ.FT. BAY WINDOWS / FRONT PORCH / REAR 56 SQ.FT. CHIMNEY 10 SQ.FT. REAR BALCONY 45 SQ.FT.		FINISHED ROOFING MATERIAL SHALL BE INSTALLED AND COMPLETED PRIOR TO FRAME INSPECTION.																		
ANY HIDDEN CONDITIONS THAT REQUIRE WORK TO BE PERFORMED BEYOND THE SCOPE OF THE BUILDING PERMIT ISSUED FOR THESE PLANS MAY REQUIRE FURTHER CITY APPROVALS INCLUDING REVIEW BY THE PLANNING COMMISSION. THE BUILDING OWNER, PROJECT DESIGNER, AND/OR CONTRACTOR MUST SUBMIT A REVISION TO THE CITY FOR ANY WORK NOT GRAPHICALLY ILLUSTRATED ON THE JOB COPY OF THE PLANS PRIOR TO PERFORMING THE WORK.			CODE & REGULATION		INSTALLATION INSTRUCTIONS FOR ALL LISTED EQUIPMENT SHALL BE PROVIDED TO THE BUILDING INSPECTOR AT ROUGH INSPECTION. (2010 CMC 303.1 AND 2013 CPC 310.4)		PROJECT NO. 1449 DATE 01.21.15																
			ALL WORK TO COMPLY WITH THE 2013 C.R.C., C.B.C., C.M.C., C.P.C. & 2013 C.E.C., CALIFORNIA TITLE 24 AMENDMENTS, AND CITY OF SUNNYVALE MUNICIPAL CODE. BUILDING CODE USED PERFORM THE WORK ACCORDING TO THE BUILDING CODES, ORDINANCES AND LAWS OF THE AUTHORITY HAVING JURISDICTION OF THE PROJECT WHICH INCLUDE BUT ARE NOT LIMITED TO: A. 2013 CALIFORNIA BUILDING CODE B. 2013 CALIFORNIA RESIDENTIAL CODE C. 2013 CALIFORNIA MECHANICAL CODE D. 2013 CALIFORNIA PLUMBING CODE E. 2013 CALIFORNIA ELECTRICAL CODE F. 2013 CALIFORNIA FIRE CODE G. 2013 CALIFORNIA GREEN BUILDING STANDARD CODE F. CITY OF SUNNYVALE MUNICIPAL CODE		PROPERTY LINE SURVEY WILL BE COMPLETED BY A LICENSED SURVEYOR AND PROVIDED TO THE BUILDING INSPECTOR PRIOR TO FOUNDATION INSPECTION.																		
					BUILDING HEIGHT VERIFICATION WILL BE COMPLETED BY A LICENSED SURVEYOR AND PROVIDED TO THE BUILDING INSPECTOR PRIOR TO FRAMING INSPECTION.		SITE PLAN																
							A-0.0																

YAO
PROJECT

394 S. PASTORIA AVE,
SUNNYVALE, CA 94086

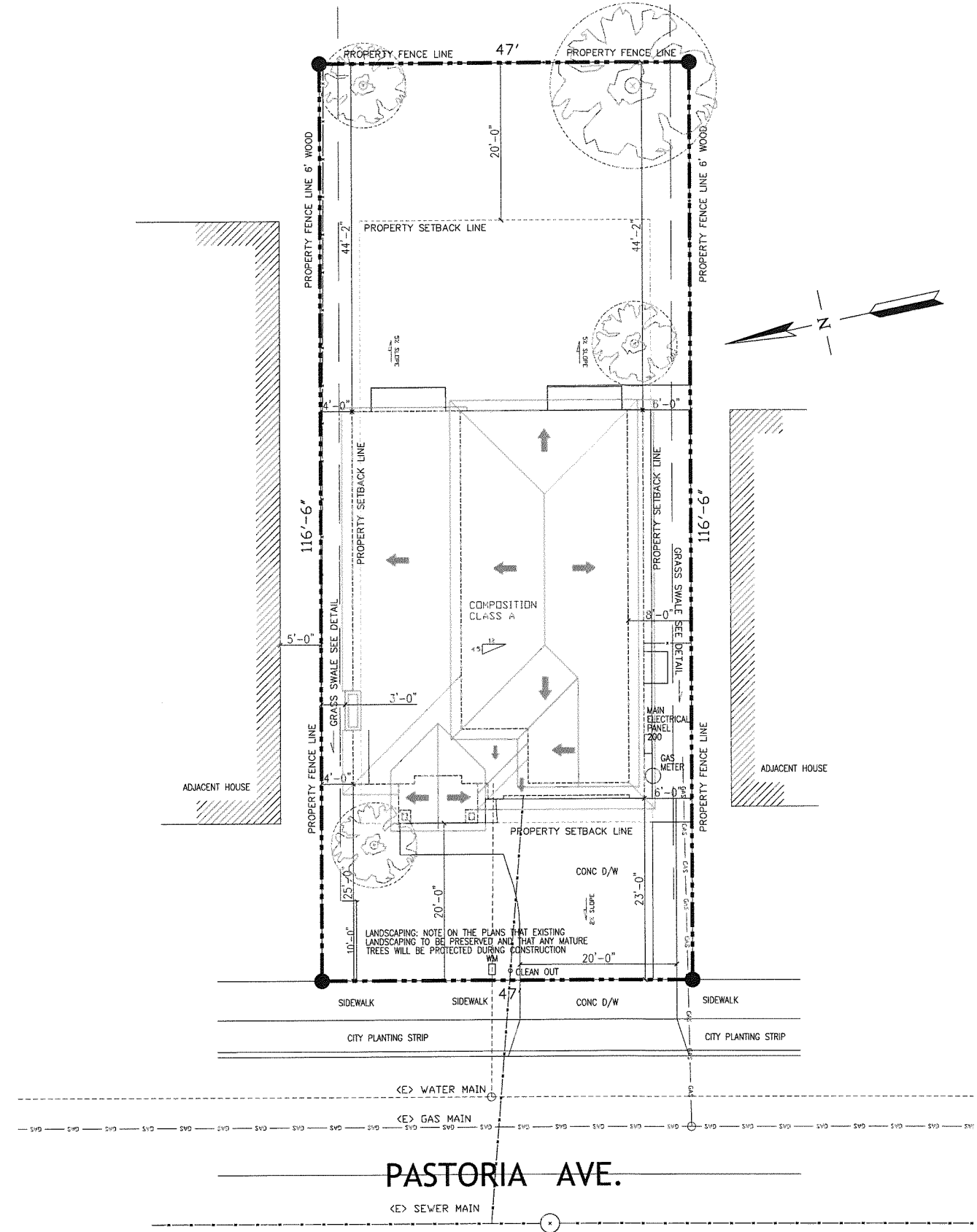


20370 TOWN CENTER LN
SUITE 139
CUPERTINO, CA 95014
408.865.0577



PASTORIA AVE.

-0-1 EXISTING SITE PLAN
SCALE: 1/8" = 1'-0"



PASTORIA AVE.

-1-0 PROPOSED SITE PLAN
SCALE: 1/8" = 1'-0"

SITE NOTES:
ALL DIMENSIONS AND SETBACK POINTS NEED TO BE CHECKED BEFORE THE ADDITION TAKES PLACE. PLEASE VERIFY ALL SETBACKS FROM THE BOUNDARY OF THE PROPERTY TO THE <E> HOUSE AND <N> ADDITIONS PRIOR TO DIGGING NEW FOUNDATION AND POURING OF THE CONCRETE OR FORM WORK SETUP.
IF THE SETBACK AT THE SITE DOES NOT MATCH UP TO THE DRAWING OR IS OVER THE ALLOWABLE SETBACK PLEASE CONTACT THE PROJECT ARCHITECT FOR FURTHER GUIDANCE ON THE PROJECT.

REVISION
01.22.15 PLANNING SUBMITAL

PROJECT NO. 1448 DATE 01.21.15

SITE PLAN

A-1.0

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PROJECT

394 S. PASTORIA AVE,
SUNNYVALE, CA 94086



2037Q TOWN CENTER LN
SUITE 139
CUPERTINO, CA 95014
408.865.0577

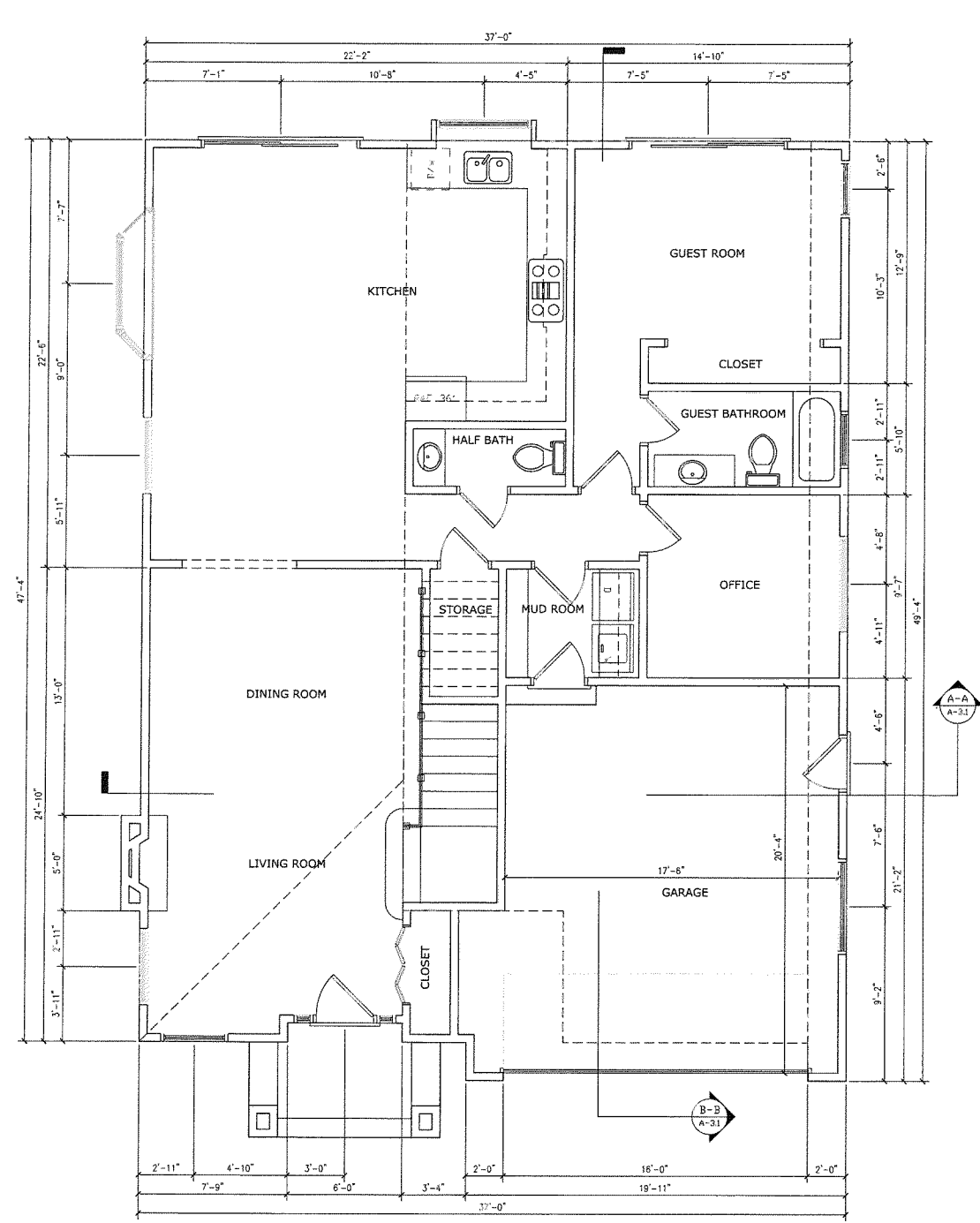


REVISION
01.22.15 PLANNING SUBMITTAL

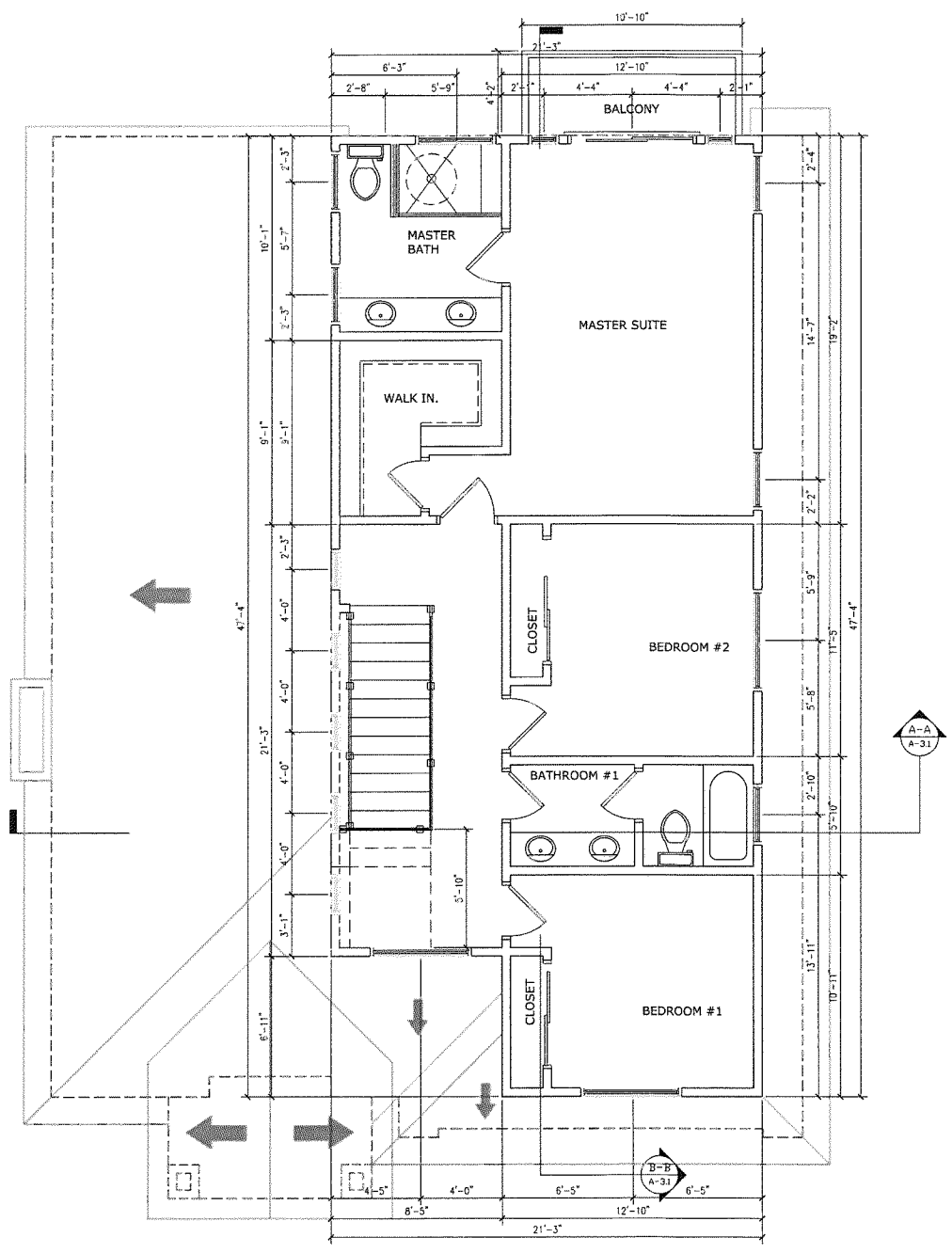
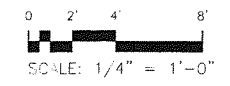
PROJECT NO. 1449 DATE 01.21.15

PROPOSED
FLOORPLAN
ELECTRICAL

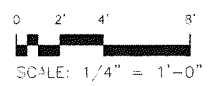
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-1-0 PROPOSED FIRST FLOOR



-2-0 PROPOSED SECOND FLOOR

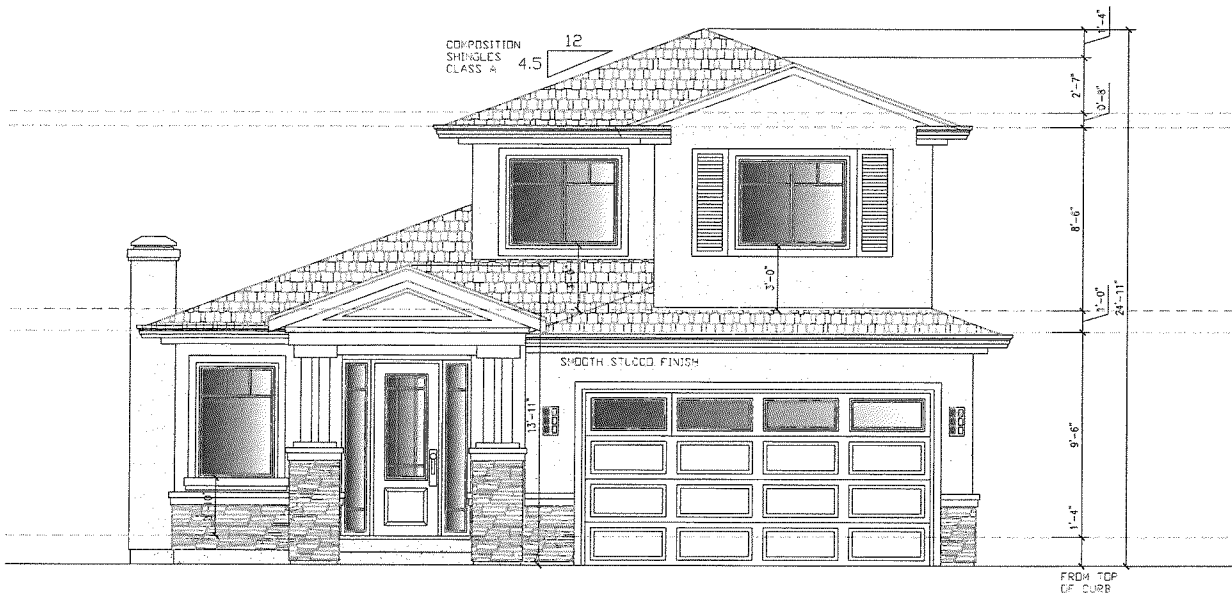
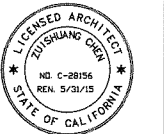


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PROJECT

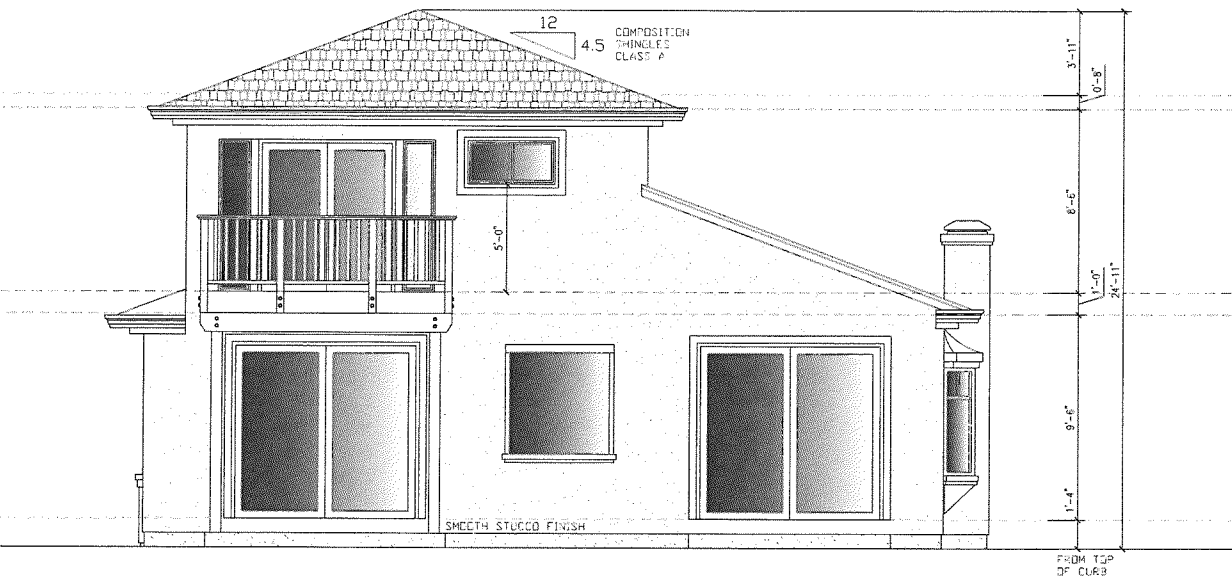
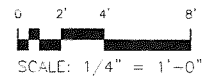
394 S. PASTORIA AVE,
SUNNYVALE, CA 94086



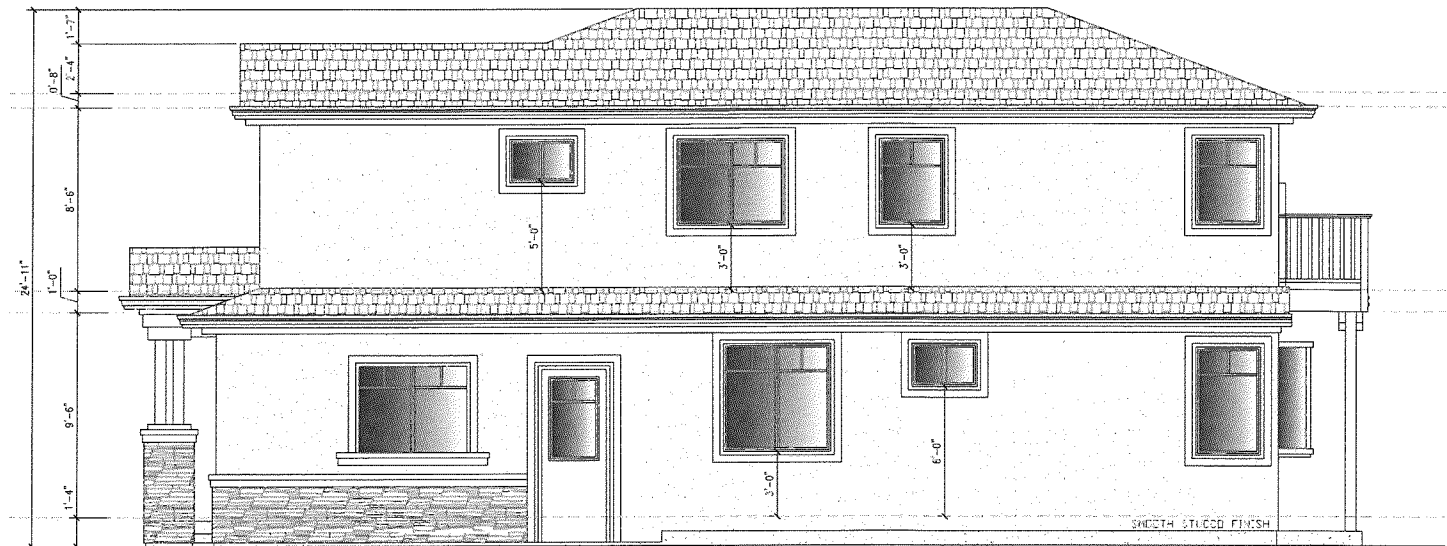
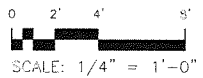
20370 TOWN CENTER LN
SUITE 139
CUPERTINO, CA 95014
408.865.0577



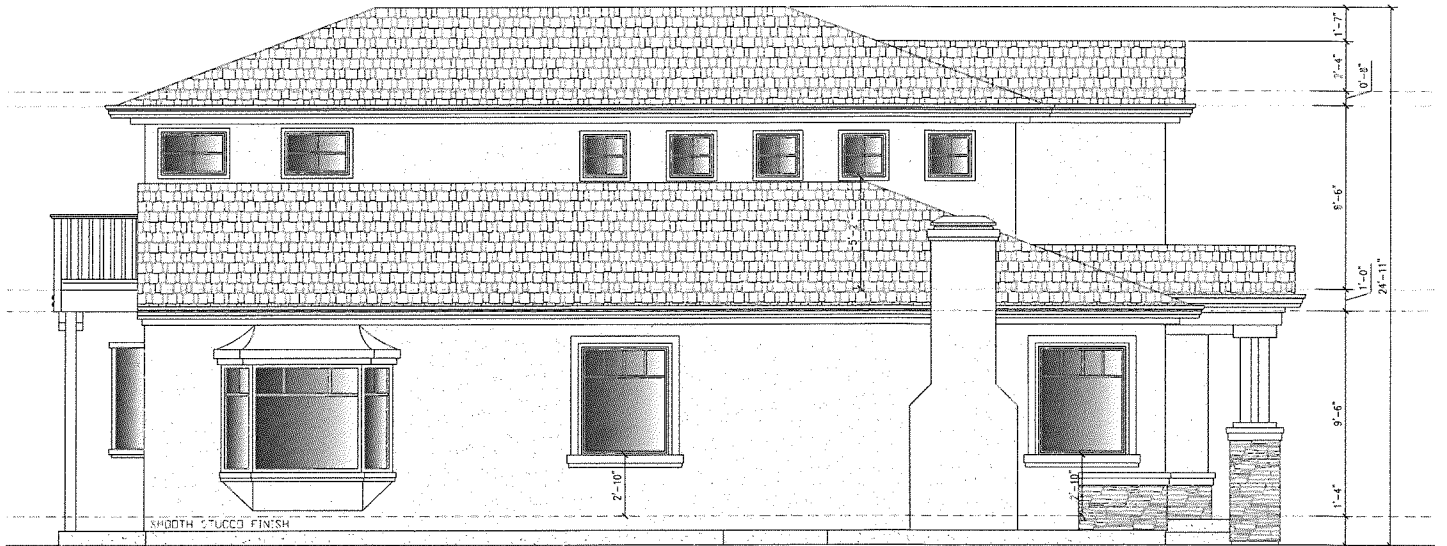
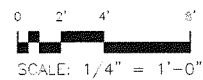
1-0 PROPOSED ELEVATION FRONT



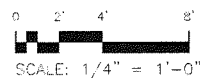
3-0 PROPOSED ELEVATION REAR



2-0 PROPOSED ELEVATION RIGHT SIDE



4-0 PROPOSED ELEVATION LEFT SIDE



REVISION
01.22.15 PLANNING SUBMITTAL

PROJECT NO. 1449 DATE 01.21.15

PROPOSED
ELEVATIONS

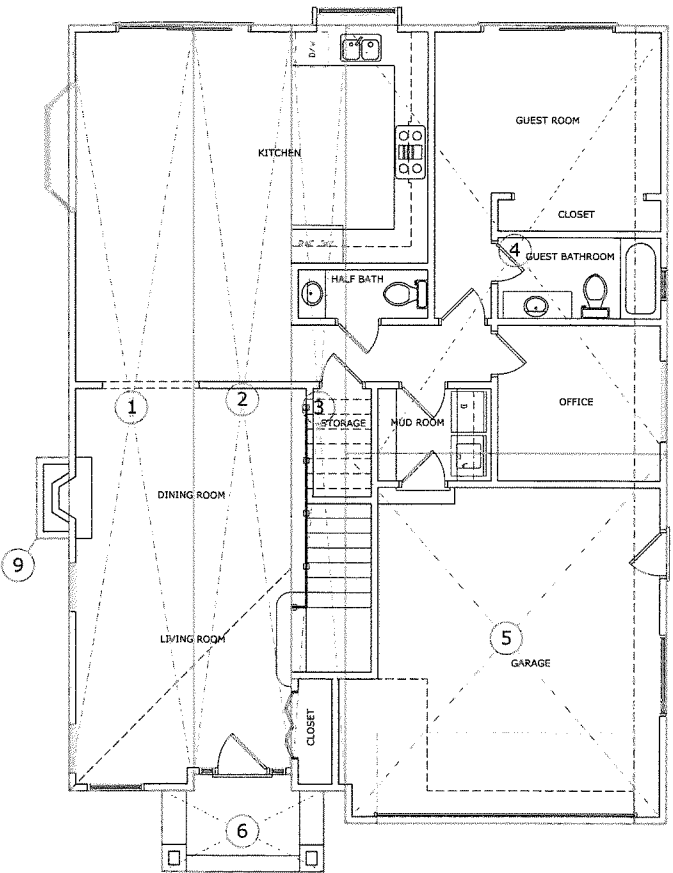
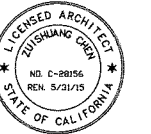
A3.0

YAD
PROJECT

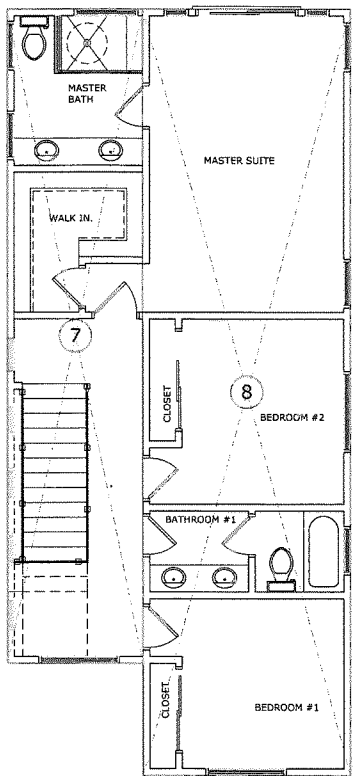
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SUNNYVALE, CA 94086



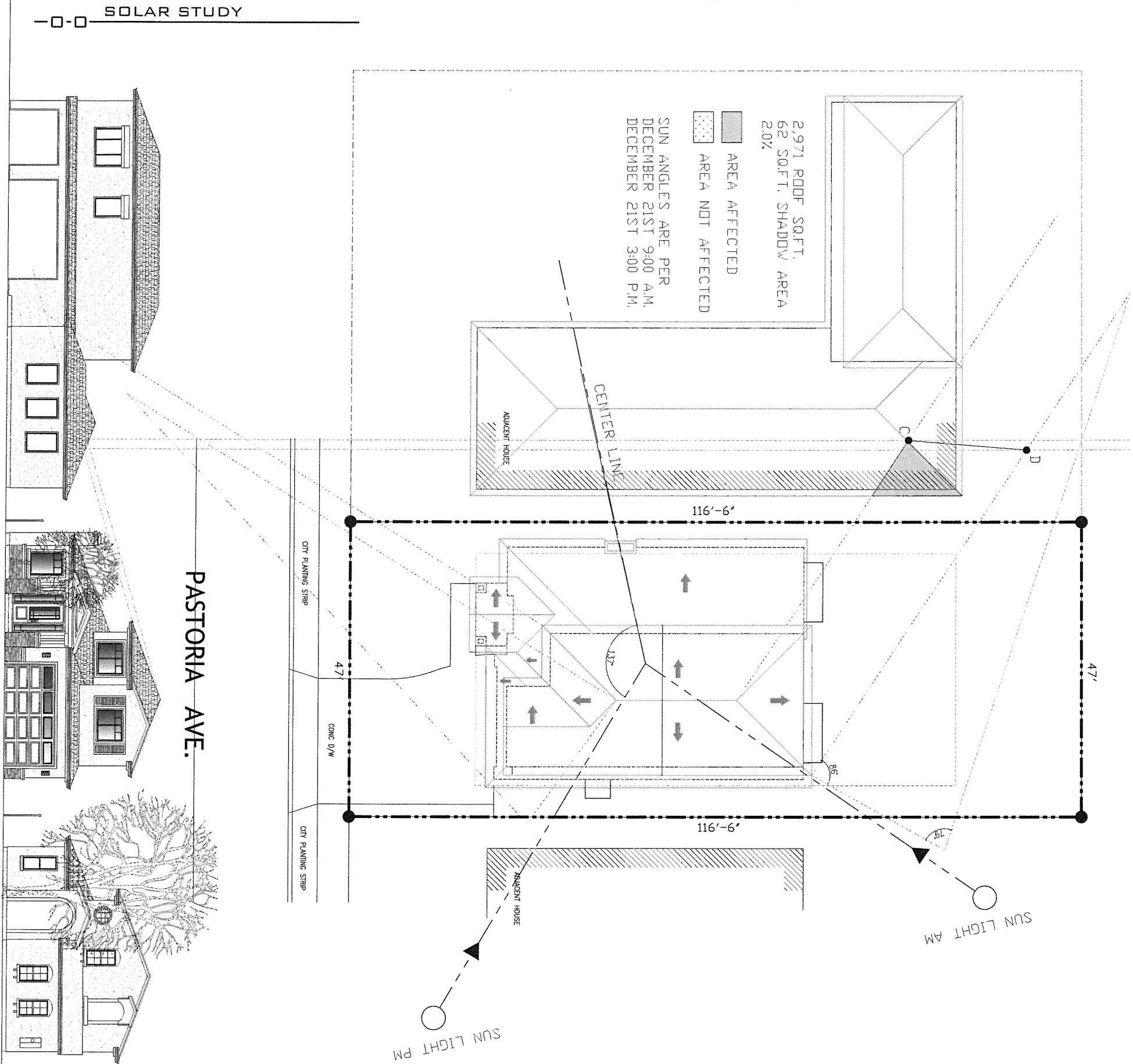
20370 TOWN CENTER LN
SUITE 139
CUPERTINO, CA 95014
408.865.0577



- | | |
|--------------------------|-----------|
| (1) = KITCHEN / OFFICE | 366 SQFT. |
| (2) = ENTRY / LIVING | 277 SQFT. |
| (3) = STAIRCASE / LIVING | 157 SQFT. |
| (4) = GREAT ROOM | 527 SQFT. |
| (5) = GARAGE / LAUNDRY | 454 SQFT. |
| (6) = COVERED ENTRY | 56 SQFT. |
| (7) = MASTER / BEDROOM | 339 SQFT. |
| (8) = BEDROOM 1 AND 2 | 607 SQFT. |
| (9) = CHIMNEY | 10 SQFT. |
| TOTAL: | 2,793 |



—0-1— BLOCK DIAGRAM
SCALE: N/A



PASTORIA AVE.

REVISION
01.22.15 PLANNING SUBMITTAL

PROJECT NO. 1449 DATE 01.21.15

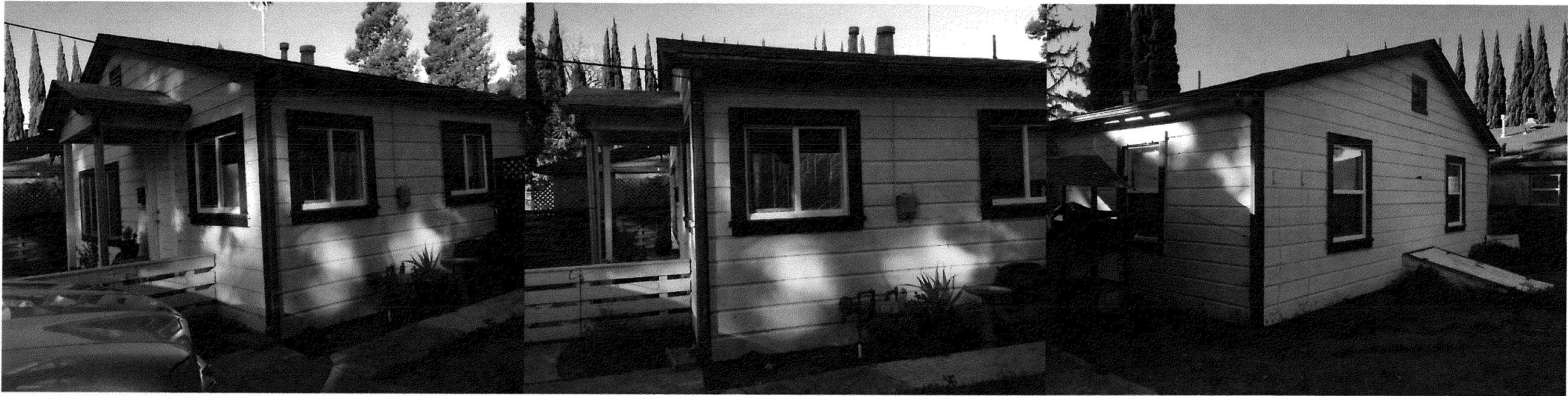
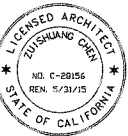
BLOCK
DIAGRAM
SOLAR STUDY

YAO
PROJECT

394 S. PASTORIA AVE.
SUNNYVALE, CA 94086



20370 TOWN CENTER LN
SUITE 139
CUPERTINO, CA 95014
408.865.0377



FRONT RIGHT

RIGHT SIDE

RIGHT REAR



RIGHT SIDE

REAR

LEFT SIDE

REVISION
01.22.15 PLANNING SUBMITTAL

PROJECT NO. 1449 DATE 01.21.15

EXISTING
HOUSE
PHOTOS

A5.0

SiteNumber	SiteStreet		APN	Zoning	CombiningDistrict	Heritage	YearBuilt	CountyLandSqFt	CountyBuildingSqFt	GarageSqFt	Total Bldg	FAR
433	Waverly	St	16504005	O	PD		1952	6380	1640	0	1640	25.7%
437	Waverly	St	16504006	O	PD		1952	6380	1512	0	1512	23.7%
440	Waverly	St	16504007	O	PD		1953	6600	1652	0	1652	25.0%
434	Waverly	St	16504008	O	PD		1952	6600	1512	0	1512	22.9%
656	W Iowa	Av	16504009	O	PD		1953	7200	1524	0	1524	21.2%
830	W Iowa	Av	16505016	R0			1948	5000	1682	380	2062	41.2%
826	W Iowa	Av	16505017	R0			1992	5000	1403	1294	2697	53.9%
822	W Iowa	Av	16505018	R0			1948	5557	4041		4041	72.7%
818	W Iowa	Av	16505019	R0			1948	5000	1157	399	1556	31.1%
812	W Iowa	Av	16505020	R0			1948	5000	1714	380	2094	41.9%
802	W Iowa	Av	16505021	R0			1949	6666	1084	400	1484	22.3%
399	S Pastoria	Av	16505022	R0			1949	6000	1043	440	1483	24.7%
385	S Pastoria	Av	16511003	R2			1942	10220	2128	400	2528	24.7%
389	S Pastoria	Av	16511004	R2			1920	9860	2689	440	3129	31.7%
395	S Pastoria	Av	16511005	R2			1937	16988	2790	440	3230	19.0%
394	S Pastoria	Av	16512002	R2			1938	5475	2325	402	2727	49.8%
392	S Pastoria	Av	16512003	R2			1951	8352	2745	0	2745	32.9%
388	S Pastoria	Av	16512004	R2			1999	8468	2503	420	2923	34.5%
374	S Pastoria	Av	16512006	R2			1940	5800	1698	216	1914	33.0%
360	S Pastoria	Av	16512007	R2			1940	5800	860	216	1076	18.6%
346	S Pastoria	Av	16512008	R2			1950	5922	2118	451	2569	43.4%
330	S Pastoria	Av	16512009	R2			1952	5800	1405	253	1658	28.6%
312	S Pastoria	Av	16512010	R2			1947	5800	1992	360	2352	40.6%
792	W Mc Kinley	Av	16512011	R2			1986	7392	3567	426	3993	54.0%
702	W Mc Kinley	Av	16512036	R2			1955	7920	1840	0	1840	23.2%
319	Waverly	St	16512037	R2			1988	5800	2500	0	2500	43.1%
321	Waverly	St	16512038	R2			1939	5800	1264	324	1588	27.4%
345	Waverly	St	16512039	R2			1928	5800	956	400	1356	23.4%
359	Waverly	St	16512040	R2			1920	5800	816	320	1136	19.6%
381	Waverly	St	16512041	R2		HR	1936	5800	873	308	1181	20.4%
385	Waverly	St	16512042	R2			1928	5800	992	280	1272	21.9%
397	Waverly	St	16512043	R2			1940	8468	750	400	1150	13.6%
403	Waverly	St	16512044	R2			1930	4104	805	252	1057	25.8%
405	Waverly	St	16512045	R2			1930	4248	1109	228	1337	31.5%
409	Waverly	St	16512046	R2			1927	5452	1204	400	1604	29.4%
415	Waverly	St	16512047	R2			1930	5452	626	216	842	15.4%
414	Waverly	St	16512048	R2			1996	5640	2000	402	2402	42.6%
408	Waverly	St	16512049	R2			1926	5593	1250	0	1250	22.3%
402	Waverly	St	16512050	R2			1925	5593	1008	432	1440	25.7%
382	Waverly	St	16512052	R2			1958	6000	1800	0	1800	30.0%
376	Waverly	St	16512053	R2			1938	5950	1516	0	1516	25.5%
346	Waverly	St	16512054	R2			1930	5950	1032	216	1248	21.0%
330	Waverly	St	16512055	R2			1930	5950	954	216	1170	19.7%
320	Waverly	St	16512056	R2			2002	5950	2941	358	3299	55.4%
664	W Mc Kinley	Av	16512057	R2			1959	7260	1284	884	2168	29.9%
398	Waverly	St	16512095	R2	PD		1996	3692	1390	400	1790	48.5%
392	Waverly	St	16512097	R2	PD		1996	3692	1390	400	1790	48.5%
388	Waverly	St	16512098	R2	PD		1996	3692	1390	400	1790	48.5%
396	S Pastoria	Av	16512100	R2			2000	6085	2207	453	2660	43.7%
386	S Pastoria	Av	16512105	R2			1939	5800	1010	280	1290	22.2%
							AVG	6291			1932	31.9%

Feedback for 394 S Pastoria Ave

To Sunnyvale Planning Commission,

We have been informed of upcoming second story house at 394 S Pastoria Ave.
We have seen this project's drawing as submitted to you.

We are supportive as we believe this project will enhance our neighborhood.

Sincerely,

Richard A. Duarte
395 S. Pastoria Ave

Audrey A. Duarte
395 S. Pastoria Ave
Sunnyvale Ca



City of Sunnyvale

Agenda Item

15-0386

Agenda Date: 4/13/2015

REPORT TO PLANNING COMMISSION

SUBJECT

File #: 2014-8023

Location: 787 N. Mary Avenue (APN: 165-40-003)

Zoning: MS

Proposed Project: Application for a project on a 4.02-acre site:

USE PERMIT to allow a new 54-foot tall wireless facility (mono-eucalyptus) in the parking lot of an industrial site.

Applicant / Owner: Verizon Wireless (applicant) / North Mary, LLC (owner)

Environmental Review: Negative Declaration

Project Planner: Timothy Maier, (408) 730-7257, tmaier@sunnyvale.ca.gov

REPORT IN BRIEF

General Plan: Industrial

Existing Site Conditions: Industrial/Office

Surrounding Land Uses

North: CA-237/US-101 Interchange

South: Industrial/Office

East: Industrial/Office

West: Sunnyvale Golf Course

Issues: Aesthetics, neighborhood compatibility

Staff Recommendation: Approve with conditions

BACKGROUND

Description of Proposed Project

The applicant requests approval of a Use Permit to locate a new wireless telecommunications facility in the parking lot of an existing industrial/office property.

See Attachment 1 for a map of the vicinity and mailing area for notices and Attachment 2 for the Data Table of the project.

Previous Actions on the Site

A Use Permit was approved in 1980, allowing high-intensity employment at the subject site and permitting construction of the existing office building. No other significant land-use application has

been reviewed in conjunction with the subject property.

DISCUSSION

Background

The proposed project is a new wireless telecommunications tower that will be camouflaged as a eucalyptus tree (applicant references this tower as a mono-eucalyptus). The proposed structure would be 54 feet tall as measured from adjacent grade to the maximum height of the tower, and would accommodate nine (9) panel antennas and nine (9) remote radio units (RRUs). The associated ground equipment is proposed to be sited at the base of the proposed tower and screened with a chain-link fence incorporating redwood slats. The facility is proposed to improve wireless telephone access in the CA-237/US-101 interchange area, as well as to homes and businesses within the vicinity.

Use Permit

City of Sunnyvale Municipal Code (SMC) 19.54 requires a Use Permit for freestanding wireless telecommunication facilities, including monopoles, lattice towers and other towers up to a maximum of 65' in height. Planning Commission review is required for adoption of the Negative Declaration for CEQA purposes. City Council review is required for projects requiring a Use Permit in the Peery Park Specific Plan District; the date for the City Council hearing has not been scheduled.

ANALYSIS

Development Standards

The proposed project complies with the applicable Development Standards as set forth in SMC Section 19.54. The following items are those which have been typically identified by the Planning Commission for requested clarification:

Site Layout

Tower - The proposed facility would be located immediately adjacent to the rear property line, 57 feet from the left-hand property line, and 134 feet from the right-hand property line. The proposed location is within a landscaped area in the rear portion of a parking lot, directly adjacent to the CA-237/US-101 interchange, surrounded by a line of trees similar in height to the proposed facility. The facility has been granted approval from the Federal Aviation Administration for a height of 54 feet (Attachment 10). The proposed location would result in minimal disturbance to the site.

Ground equipment - Ground equipment, including a diesel-powered generator and associated mechanical equipment, would be sited at the base of the proposed wireless facility. Submitted plans propose a screening fence of six (6) feet in height to surround the equipment, incorporating redwood slats. The equipment area would require modifications be made to existing on-site paving and landscaping. Recommended conditions of approval require submittal of a landscape plan to be reviewed with the building permit application.

Design

The proposed telecommunication tower would be camouflaged as a eucalyptus tree (mono-

eucalyptus) to minimize the visibility of the structure. The applicant has provided photosimulations to illustrate the appearance of the complete project (Attachment 8) when placed among existing surrounding trees. Photosimulations indicate that the proposed antennas would be primarily undetectable among the existing trees. However, based on the plans submitted, the proposed design of the facility necessitates aesthetic modifications, as listed below (Attachment 4):

- The wireless facility must be camouflaged to appear as a eucalyptus tree;
- The lowest elevation of simulated foliage must measure 15 feet above ground level;
- The simulated trunk of the wireless facility must be covered completely in faux tree bark;
- Exterior materials shall be compatible in color and pattern with existing adjacent trees located on the project site;
- Tree pole design and specifications, including branch design and density of foliage exhibited by material samples, must be provided, and are subject to review by the Director of Community Development;
- The antennas must be compatible in color with all other portions of the wireless facility.

Timeframe

Per information provided by the applicant, construction of the proposed wireless facility would take place over a period of approximately 45 to 60 days, likely commencing in late July or early August 2015.

Parking

The proposed project would result in the loss of four (4) parking spaces. Parking capacity provided for all uses on the subject property would still comply with City of Sunnyvale Municipal Code requirements.

Landscaping and Tree Preservation

The proposed location would not result in the loss of any protected trees but will likely necessitate removal of a small amount of vegetation adjacent to the proposed location. If necessary, any loss of vegetation would occur at the rear of the property, would be very minor, and would not alter the overall character of landscaping at the subject lot. Landscaping proposed for possible removal is not designated as “protected” by code. Recommended conditions of approval require submittal of a landscape plan to be reviewed with the building permit application.

Wireless Telecommunications Ordinance

The following sections of the Wireless Telecommunication Ordinances of the Sunnyvale Municipal Code apply to the proposed project:

19.54.140(a) - Wherever technically feasible, wireless telecommunication service providers are encouraged to co-locate telecommunication facilities in order to reduce adverse visual impacts; however, the city discourages the development of “antenna farms” or the clustering of multiple antennas on a single monopole, tower or other elevation, unless the site is determined to be suitable based on the following factors:

- (1) Compliance with all FCC RF emission standards;

This project meets all FCC RF emissions standards (Attachment 9).

(2) Visibility from residentially zoned property;

The proposed structure would not be visible from residentially-zoned property.

(3) Visibility from El Camino Real or the right of way of a freeway, expressway or other major arterial street;

This project facility would be visible from CA-237, US-101, and neighboring City streets; however, the proposed aesthetic treatment would screen much of the telecommunications equipment.

(4) Visibility from the Downtown Specific Plan area or other areas declared by the Director of Community Development to be visually sensitive; and

This project is not visible from the Downtown Specific Plan area or other areas identified in the Telecommunications code as being sensitive.

(5) Lack of aesthetically preferable feasible alternatives.

The area needing coverage is predominately industrial/office. A height of 54 feet is proposed in order to provide wireless telecommunications coverage for the area. Alternative sites were found to be unsuitable and/or unavailable, and the use of existing structures would be prohibitive due to their low profile. The use of camouflage to resemble a eucalyptus tree on a site that bears existing similar trees will help to provide visual compatibility and screening.

ENVIRONMENTAL REVIEW

A Negative Declaration has been prepared in compliance with California Environmental Quality Act (CEQA) provisions and City guidelines (Attachment 7). An Initial Study has determined that construction of the proposed project would not have the potential to result in significant environmental effects on the subject or surrounding properties.

FISCAL IMPACT

No fiscal impact other than normal fees and taxes are expected.

PUBLIC CONTACT

At the time of preparation of this report, staff had not received any comment from members of the public related to the application.

Notice of Negative Declaration and Public

- Published in the Sun newspaper
- Posted on site
- 280 notices mailed to property owners and tenants within 2,000 feet of project site

Staff Report

- Posted on the City of Sunnyvale website
- Provided at the Reference Section of the City of Sunnyvale Public Library

Agenda

- Posted on the City's official notice bulletin board
- Posted on the City of Sunnyvale's website

CONCLUSION**Findings and General Plan Goals**

Staff was able to make the required Findings based on justifications for the Use Permit. Findings and General Plan Goals are located in Attachment 3.

Conditions of Approval

Conditions of Approval are located in Attachment 4.

ALTERNATIVES

1. Approve the Special Development Permit with Recommended Conditions of Approval found in Attachment 4.
2. Approve the Special Development Permit with modified Conditions of Approval.
3. Deny the Special Development Permit.

RECOMMENDATION

Recommend Alternative 1: Approve the Special Development Permit with Recommended Conditions of Approval found in Attachment 4.

Prepared by: Timothy Maier, Project Planner

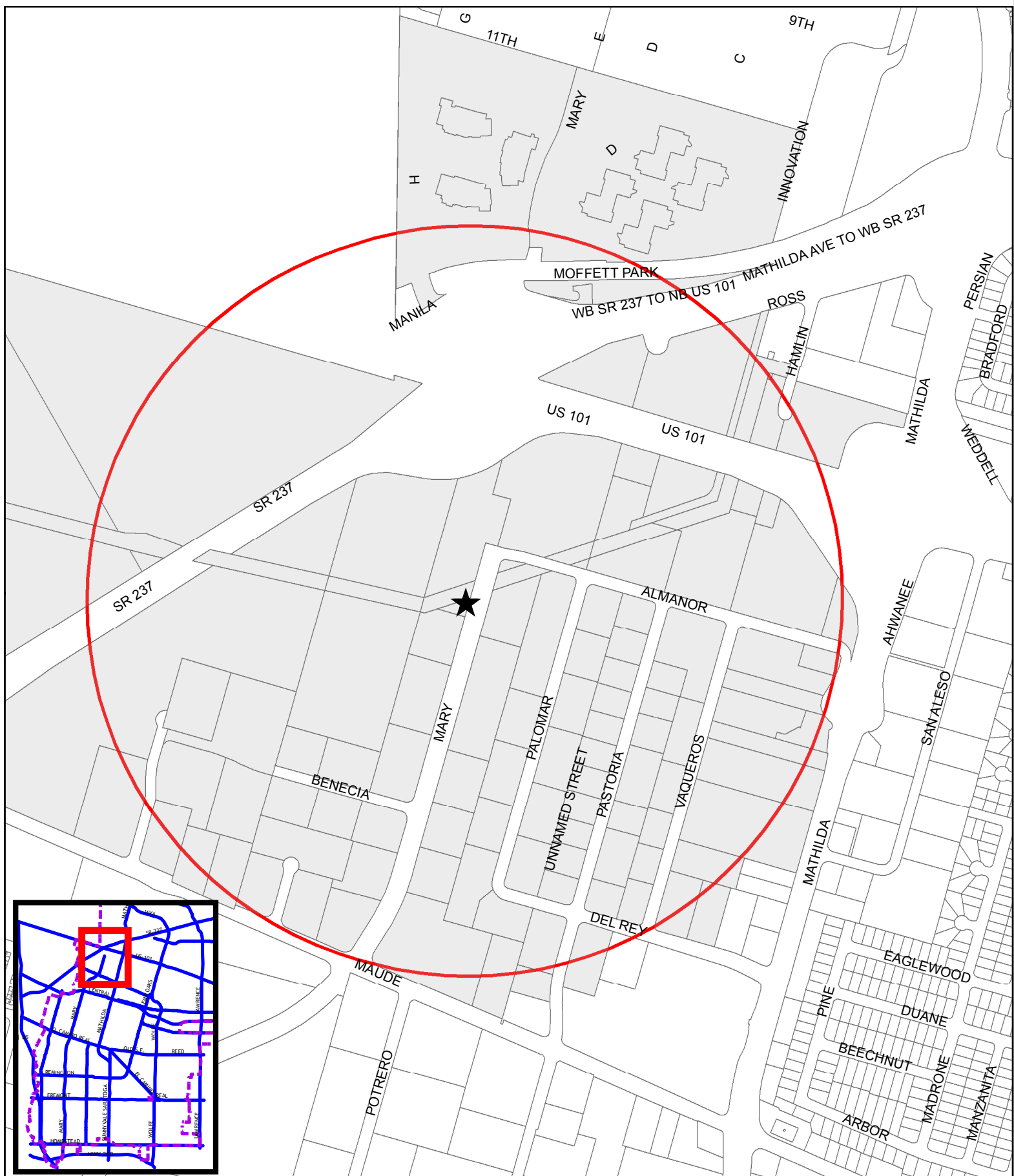
Reviewed by: Gerri Caruso, Principal Planner

Reviewed by: Trudi Ryan, Planning Officer

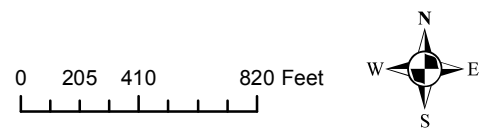
Reviewed by: Hanson Hom, Director of Community Development

Attachments:

1. Vicinity and Noticing Map
2. Project Data Table
3. Recommended Findings
4. Recommended Conditions of Approval
5. Project Description Letter
6. Site and Architectural Plans
7. Initial Study
8. Photosimulations
9. Radiofrequency (RF) Study
10. FAA Clearance Letter



2014-8023
 787 N Mary Ave. (APN: 165-40-003)
 USE PERMIT
 2000- foot area



PROJECT DATA TABLE

	EXISTING	PROPOSED	REQUIRED/ PERMITTED
General Plan	Industrial	Same	Industrial
Zoning District	M-S	Same	Industrial
Lot Size (s.f.)	175,111 (4.02 acres)	Same	22,500
Height of Antenna Pole	N/A	54' to top of facility	90'
Antenna Setbacks			
Front (N. Sunnyvale Ave.)	N/A	Approx. 763'	25' min.
Left Side	N/A	57'	20' combined side setback min.
Right Side	N/A	134'	
Rear	N/A	0'	N/A



Starred items indicate deviations from Sunnyvale Municipal Code requirements.

RECOMMENDED FINDINGS

Findings-Special Development Permit

1. The proposed use attains the objectives and purposes of the General Plan of the City of Sunnyvale as the City of Sunnyvale Municipal Code requires that the location of such facilities be designed with sensitivity to the surrounding area. The proposed facility, as conditioned, will be screened as a tree resembling a eucalyptus tree and located in close proximity to a line of existing trees. Associated equipment will be screened with aesthetic treatment applied to offset adverse aesthetic impact. Staff considers the application for the facility to be consistent with the goals and policies of the Telecommunication Policy adopted by the City of Sunnyvale in June 1996 and updated in 2005.
2. The proposed use ensures that the general appearance of proposed structures, or the uses to be made of the property to which the application refers, will not impair either the orderly development of, or the existing uses being made of, adjacent properties. As conditioned, the proposed location complies with visual standards established by the City of Sunnyvale for telecommunications facilities. The project meets all FCC radiofrequency (RF) emissions standards. The proposed use is desirable, and will not be materially detrimental to the public welfare or injurious to the property, improvements or uses within the immediate vicinity and within the Zoning District.

Council Policy Manual-Telecommunications (7.2.16)

The City of Sunnyvale's Council Policy Manual (CPM) is a compendium of policies established by City Council resolution or motion which provide guidelines for current or future City action. Such policies, when implemented, assist in achieving General Plan goals.

- **Policy Statement 1.A.5** – Support retention of local zoning authority for cellular towers, satellite dish antennas, and other telecommunications equipment, facilities, and structures.
- **Policy Statement 2** – Promote universal access to telecommunications services for all Sunnyvale residents.

The proposed facility would be designed and sited appropriately through the City's zoning authority as conditioned, and the facility would provide improved telecommunication services in an area which is currently underserved.

**RECOMMENDED
CONDITIONS OF APPROVAL AND
STANDARD DEVELOPMENT REQUIREMENTS
4/13/15**

Planning Application **2014-8023**
787 N. Mary Ave.

Use Permit to allow a new 54-foot tall wireless facility (mono-eucalyptus)
in the parking lot of an industrial site

The following Conditions of Approval [COA] and Standard Development Requirements [SDR] apply to the project referenced above. The COAs are specific conditions applicable to the proposed project. The SDRs are items which are codified or adopted by resolution and have been included for ease of reference, they may not be appealed or changed. The COAs and SDRs are grouped under specific headings that relate to the timing of required compliance. Additional language within a condition may further define the timing of required compliance. Applicable mitigation measures are noted with "Mitigation Measure" and placed in the applicable phase of the project.

In addition to complying with all applicable City, County, State and Federal Statutes, Codes, Ordinances, Resolutions and Regulations, Permittee expressly accepts and agrees to comply with the following Conditions of Approval and Standard Development Requirements of this Permit:

GC: THE FOLLOWING GENERAL CONDITIONS AND STANDARD DEVELOPMENT REQUIREMENTS SHALL APPLY TO THE APPROVED PROJECT.
--

GC-1. CONFORMANCE WITH APPROVED PLANNING APPLICATION:

All building permit drawings and subsequent construction and operation shall substantially conform with the approved planning application, including: drawings/plans, materials samples, building colors, and other items submitted as part of the approved application. Any proposed amendments to the approved plans or Conditions of Approval are subject to review and approval by the City. The Director of Community Development shall determine whether revisions are considered major or minor. Minor changes are subject to review and approval by the Director of Community Development. Major changes are subject to review at a public hearing. [COA] [PLANNING]

GC-2. COMPLY WITH APPLICABLE REGULATIONS:

The facility must comply with any and all applicable regulations and standards promulgated or imposed by any state or federal agency, including but not

limited to the Federal Communications Commission and Federal Aviation Agency.[SDR] [PLANNING]

GC-3. PERMIT EXPIRATION:

The permit shall be null and void two years from the date of approval by the final review authority at a public hearing if the approval is not exercised, unless a written request for an extension is received prior to expiration date and is approved by the Director of Community Development. [SDR] (PLANNING)

GC-4. TESTING WITHIN 15 DAYS:

The applicant shall test any wireless telecommunications site installed in the City of Sunnyvale within 15 days of operating the tower. The test shall confirm that any Emergency 911 wireless call made through the wireless telecommunications site shall provide Enhanced 911 capability (including phase 2 information when available from the caller's device) and direct the call to the City of Sunnyvale Department of Public Safety dispatcher, ensuring phase 2 information is transferred. If the call is to be directed elsewhere pursuant to State and Federal law the applicant shall ensure that the Enhanced 911 information transfers to that dispatch center. This capability shall be routinely tested to ensure compliance as long as the approved wireless telecommunications site is in service. [SDR] [PLANNING]

GC-5. HOLD HARMLESS:

The wireless telecommunication facility provider shall defend, indemnify, and hold harmless the city or any of its boards, commissions, agents, officers, and employees from any claim, action or proceeding against the city, its boards, commission, agents, officers, or employees to attack, set aside, void, or annul, the approval of the project when such claim or action is brought within the time period provided for in applicable state and/or local statutes. The city shall promptly notify the provider(s) of any such claim, action or proceeding. The city shall have the option of coordinating in the defense. Nothing contained in this stipulation shall prohibit the city from participating in a defense of any claim, action, or proceeding if the city bears its own attorney's fees and costs, and the city defends the action in good faith. [SDR] [PLANNING]

GC-6. LIABILITY:

Facility lessors shall be strictly liable for any and all sudden and accidental pollution and gradual pollution resulting from their use within the city. This liability shall include cleanup, intentional injury or damage to persons or property. Additionally, lessors shall be responsible for any sanctions, fines, or other monetary costs imposed as a result of the release of pollutants from their operations. Pollutants include any solid, liquid, gaseous or thermal irritant or contaminant, including smoke, vapor, soot, fumes, acids, alkalis,

chemicals, and waste. Waste includes materials to be recycled, reconditioned or reclaimed. [SDR] [PLANNING]

GC-7. NO THREAT TO PUBLIC HEALTH:

The facility shall not be sited or operated in such a manner that is poses, either by itself or in combination with other such facilities, a potential threat to public health. To that end, the subject facility and the combination of on-site facilities shall not produce at any time power densities in any inhabited area that exceed the FCC's Maximum Permissible Exposure (MPE) limits for electric and magnetic field strength and power density for transmitters or any more restrictive standard subsequently adopted or promulgated by the federal government. [SDR] [PLANNING]

GC-8. CONFORMANCE WITH PREVIOUS PLANNING PERMIT:

The subject site shall comply with all conditions of approval and requirements of planning application (INSERT PLANNING APPLICATION NUMBER). [PLANNING] [COA]

PS: THE FOLLOWING CONDITIONS SHALL BE MET PRIOR TO SUBMITTAL OF BUILDING PERMIT, AND/OR GRADING PERMIT.

PS-1. REQUIRED REVISIONS TO PROJECT PLANS:

The plans shall be revised to address comments from the Administrative Hearing Officer, Planning Commission or City Council including the following:

- a) The wireless facility must be camouflaged to appear as a eucalyptus tree;
- b) The lowest elevation of simulated foliage must be 15 feet above ground level;
- c) The simulated trunk of the wireless facility must be covered completely in faux tree bark;
- d) Exterior materials shall be compatible in color and pattern with existing adjacent trees located on the project site;
- e) Tree pole design and specifications, including branch design and density of foliage exhibited by material samples, must be provided, and are subject to review by the Director of Community Development;
- f) The wireless facility and antennas must be painted to be compatible with surrounding vegetation located on the property. [COA] [PLANNING]

PS-2. EXTERIOR MATERIALS REVIEW:

Final exterior building materials and color scheme are subject to review and approval by the Planning Commission/Director of Community Development prior to submittal of a building permit. [COA] [PLANNING]

BP: THE FOLLOWING CONDITIONS SHALL BE ADDRESSED ON THE CONSTRUCTION PLANS SUBMITTED FOR ANY DEMOLITION PERMIT, BUILDING PERMIT, GRADING PERMIT, AND/OR ENCROACHMENT PERMIT AND SHALL BE MET PRIOR TO THE ISSUANCE OF SAID PERMIT(S).

BP-1. CONDITIONS OF APPROVAL:

Final plans shall include all Conditions of Approval included as part of the approved application starting on sheet 2 of the plans. [COA] [PLANNING]

BP-2. RESPONSE TO CONDITIONS OF APPROVAL:

A written response indicating how each condition has or will be addressed shall accompany the building permit set of plans. [COA] [PLANNING]

BP-3. NOTICE OF CONDITIONS OF APPROVAL:

A Notice of Conditions of Approval shall be filed in the official records of the County of Santa Clara and provide proof of such recordation to the City prior to issuance of any City permit, allowed use of the property, or Final Map, as applicable. The Notice of Conditions of Approval shall be prepared by the Planning Division and shall include a description of the subject property, the Planning Application number, attached conditions of approval and any accompanying subdivision or parcel map, including book and page and recorded document number, if any, and be signed and notarized by each property owner of record.

For purposes of determining the record owner of the property, the applicant shall provide the City with evidence in the form of a report from a title insurance company indicating that the record owner(s) are the person(s) who have signed the Notice of Conditions of Approval. [COA] [PLANNING]

BP-4. BLUEPRINT FOR A CLEAN BAY:

The building permit plans shall include a "Blueprint for a Clean Bay" on one full sized sheet of the plans. [SDR] [PLANNING]

BP-5. TREE PROTECTION PLAN:

Prior to issuance of a Demolition Permit, a Grading Permit or a Building Permit, whichever occurs first, obtain approval of a tree protection plan from the Director of Community Development. Two copies are required to be

submitted for review. The tree protection plan shall include measures noted in Title 19 of the Sunnyvale Municipal Code and at a minimum:

- a) An inventory shall be taken of all existing trees on the plan including the valuation of all 'protected trees' by a certified arborist, using the latest version of the "Guide for Plant Appraisal" published by the International Society of Arboriculture (ISA).
- b) All existing (non-orchard) trees on the plans, showing size and varieties, and clearly specify which are to be retained.
- c) Provide fencing around the drip line of the trees that are to be saved and ensure that no construction debris or equipment is stored within the fenced area during the course of demolition and construction.
- d) The tree protection plan shall be installed prior to issuance of any Building or Grading Permits, subject to the on-site inspection and approval by the City Arborist and shall be maintained in place during the duration of construction and shall be added to any subsequent building permit plans. [COA] [PLANNING/CITY ARBORIST]

PF: THE FOLLOWING CONDITIONS SHALL BE ADDRESSED ON THE CONSTRUCTION PLANS AND/OR SHALL BE MET PRIOR TO RELEASE OF UTILITIES OR ISSUANCE OF A CERTIFICATE OF OCCUPANCY.

PF-1. RF EMISSIONS STUDIES:

The applicant shall submit to the Director of Community Development Radio Frequency Emissions at least two reports of field measurements showing: 1.) The ambient level of RF emissions before construction of the facility and 2.) The actual level of emissions after the facility is in place and operating at or near full capacity. [COA] [PLANING]

PF-2. NOISE STUDIES:

The applicant shall submit to the Director of Community Development Noise Analysis with at least two reports of field measurements showing: 1.) The noise measurement before construction of the facility and 2.) The actual noise measurement after the facility is in place and operating at or near full capacity. [COA] [PLANING]

DC: THE FOLLOWING CONDITIONS SHALL BE COMPLIED WITH AT ALL TIMES DURING THE CONSTRUCTION PHASE OF THE PROJECT.

DC-1. BLUEPRINT FOR A CLEAN BAY:

The project shall be in compliance with stormwater best management practices for general construction activity until the project is completed and either final occupancy has been granted. [SDR] [PLANNING]

DC-2. TREE PROTECTION:

All tree protection shall be maintained, as indicated in the tree protection plan, until construction has been completed and the installation of landscaping has begun. [COA] [PLANNING]

AT: THE FOLLOWING CONDITIONS SHALL BE COMPLIED WITH AT ALL TIMES THAT THE USE PERMITTED BY THIS PLANNING APPLICATION OCCUPIES THE PREMISES.

AT-1. CERTIFICATION:

Before January 31 of each even numbered year following the issuance of any authorizing establishment of a wireless telecommunication facility, an authorized representative for each wireless carrier providing service in the City of Sunnyvale shall provide written certification to the City executed under penalty of perjury that (i) each facility is being operated in accordance with the approved local and federal permits and includes test results that confirm the facility meets city noise requirements and federal RF emissions standards; (ii) each facility complies with the then-current general and design standards and is in compliance with the approved plans; (iii) whether the facility is currently being used by the owner or operator; and (iv) the basic contact and site information supplied by the owner or operator is current.. [SDR] [PLANNING]

AT-2. 10 YEAR RENEWAL:

Every owner or operator of a wireless telecommunication facility shall renew the facility permit at least every ten (10) years from the date of initial approval. If a permit or other entitlement for use is not renewed, it shall automatically become null and void without notice or hearing ten (10) years after it is issued, or upon cessation of use for more than a year and a day, whichever comes first. Unless a new use permit or entitlement of use is issued, within one hundred twenty (120) days after a permit becomes null and void all improvements, including foundations and appurtenant ground wires, shall be removed from the property and the site restored to its original pre-installation condition within one hundred eighty (180) days of nonrenewal or abandonment. [SDR] [PLANNING]

AT-3. MINIMIZE NOISE:

The facility shall be operated in such a manner so as to minimize any possible disruption caused by noise. Backup generators shall only be operated during periods of power outages, and shall not be tested on weekends or holidays, or between the hours of 10:00 p.m. and 7:00 a.m. on weekday nights. At no time shall equipment noise from any source exceed an exterior noise level of 60 dB at the property line. [SDR] [PLANNING]

AT-4. RF EMISSIONS:

Certification must be provided that the proposed facility will at all times comply with all applicable health requirements and standards pertaining to RF emissions. [SDR] [PLANNING]

AT-5. MAINTAIN CURRENT INFORMATION:

The owner or operator shall maintain, at all times, a sign mounted on the outside fence showing the operator name, site number and emergency contact telephone number. The owner or operator of the facility shall also submit and maintain current at all times basic contact and site information on a form to be supplied by the city. The applicant shall notify city of any changes to the information submitted within thirty (30) days of any change, including change of the name or legal status of the owner or operator. This information shall include, but is not limited to the following:

- a) Identity, including name, address and telephone number, and legal status of the owner of the facility including official identification numbers and FCC certification, and if different from the owner, the identity and legal status of the person or entity responsible for operating the facility.
- b) Name, address and telephone number of a local contact person for emergencies.
- c) Type of service provided. [SDR] [PLANNING]

AT-6. GOOD REPAIR:

All facilities and related equipment, including lighting, fences, shields, cabinets, and poles, shall be maintained in good repair, free from trash, debris, litter and graffiti and other forms of vandalism, and any damage from any cause shall be repaired as soon as reasonably possible so as to minimize occurrences of dangerous conditions or visual blight. Graffiti shall be removed from any facility or equipment as soon as practicable, and in no instance more than forty-eight (48) hours from the time of notification by the city. [SDR] [PLANNING]

AT-7. RESPONSIBILITY TO MAINTAIN:

The owner or operator of the facility shall routinely and regularly inspect each site to ensure compliance with the standards set forth in the Telecommunications Ordinance. [SDR] [PLANNING]

AT-8. NO INTERFERENCE WITH CITY COMMUNICATION SYSTEMS:

The facility operator shall be strictly liable for interference caused by the facility with city communication systems. The operator shall be responsible for all labor and equipment costs for determining the source of the interference, all costs associated with eliminating the interference, (including but not limited to filtering, installing cavities, installing directional antennas, powering down

systems, and engineering analysis), and all costs arising from third party claims against the city attributable to the interference. [SDR] [PLANNING]

Supplement Response – February 12, 2015

The Project Description-Proposal – REVISED

Site Name: Hwy 101 & 237
Proposed Verizon Wireless facility located at
787 No. Mary Avenue Sunnyvale, CA 94085
Owner: North Mary LLC
APN: 165-40-003

Introduction

Verizon and its affiliates have acquired licenses from the Federal Communication Commission (“FCC”) and the CPUC. These licenses include Santa Clara County, California. The regional system operates under the name GTE Mobilnet of California Limited Partnership, a California limited partnership, d/b/a Verizon Wireless.

Applicant’s Request

Verizon Wireless formally requests pursuant to the City of Sunnyvale Wireless Telecommunications Policy approval of a Use Permit for an unmanned telecommunications facility located at 787 No. Mary Avenue in Sunnyvale, California.

Project Description

Installation of an unmanned Verizon Wireless telecommunications facility to be located at 787 No. Mary Avenue, in Sunnyvale, California. The proposed facility will consist of the installation of nine panel antennas, and nine remote radio units mounted on new 60 foot MonoEucalyptus, an approximate 11’-1.6” x 16’-10 1/2” equipment shelter and a *standby 30 KW Diesel generator with a 132 gallon UL 142 listed fuel tank, located with a 21’x 41’ lease area within a chain link fence with redwood slats.

*Verizon will include a new stand-by 30KW diesel generator with a 132-gallon diesel fuel tank. This generator will supply power in emergency situations only. This is part of Verizon’s homeland security initiative. Verizon wants the entire network to be able to sustain itself in the event of blackout situations. The generator will meet all noise standards of the City of Sunnyvale.

Maintenance

The facility will be unmanned and will be visited only monthly for routine maintenance. The facility will emit no glare, odor or noise above acceptable levels, and will not have any signage other than those required for identification as mandated by the FCC and FAA, which are designed to protect public safety. To ensure structural integrity of the facility, Verizon Wireless will construct and maintain the site in compliance with all federal, state and local building codes and standards. In addition, each facility is monitored 24 hours a day, electronically for intrusion and environmental disruption. The facility will also contain a sign identifying a 1-800 number to call in case of an emergency (manned 24 hours a day by Verizon employees) and identifying it as a Verizon facility. Verizon will be in compliance with all FCC regulations regarding signage at the facility.

Need for Site and Location Justification

Wireless phone systems operate on a “grid” system, whereby overlapping “cells” mesh to form a seamless wireless network. The technical criteria for establishing cell sites are very exacting as to both the height and location of the telecommunication facility. Based on a computerized engineering study, which takes into account, among other things, local population density, traffic patterns, and topography, Verizon Wireless’s RF engineers have identified this location as being a necessary and appropriate location for a cell site in order to provide coverage along the Highway 101 and Hwy 237 interchange and to the surrounding businesses and community of Sunnyvale.

Collocation Opportunity

The proposed MonoEucalyptus pole will be structurally designed to hold up to two additional carriers. The applicant is unaware at this time of other carriers intention to collocate on the proposed tower.

Alternative Site Analysis

Verizon Wireless investigated existing structures, towers and buildings high enough to accommodate the coverage objectives.

220 W. Ahwanee Avenue – Sunnyvale – Lock Storage

Antennas would be mounted to rooftop, no ground space for equipment – would require equipment to be placed inside storage units not currently available. Placement of generator would require taking at least one parking space. Limited parking.

St. Jude’s – 645 Almanor – Sunnyvale

Had site visit with RF and facilities manager. LL rejected- management not interested.

675 Almanor Avenue – Sunnyvale Parkinson Offices

Access crosses several parcels and water district easement.

525 Almanor Avenue – Sunnyvale

Menrock ownership – non-responsive property owner

919 Hamlin Court – Sunnyvale

Existing Treepole – Rad center available too low to meet Verizon coverage objective, Insufficient available ground space for equipment shelter and generator.

Radio Frequency

The proposed facility will be designed and constructed to meet applicable governmental and industry safety standards. Verizon Wireless continues to comply with all FCC governing construction requirements, technical standards, interference protection, power limitations, and radio frequency standards. Any and all RF emissions are subject to the exclusive jurisdiction of the FCC. (See EMF Evaluation dated December 27, 2013)

Standby Generator for emergency back up power supply

In order for Verizon to maintain the site's operational capability in the event of an emergency or extended power outage, a 30 kW diesel fired generator will be installed at time of construction. The generator itself is enclosed in a sound attenuated enclosure, utilizes a muffler with the exhaust pipe directed vertically approximately 8 feet above ground level. The generator would run for extended periods of time only in the event of a natural disaster, other emergency or prolonged power outage. Sound test results are available for the proposed generator and are attached for review.

Safety

The proposed site will be entirely self-monitored by sophisticated computers which connect directly to a central office and which alert personnel to equipment malfunction or breach of security. Moreover, no smoke, debris or other nuisance will be generated by the proposed facility.

The proposed facility will not be detrimental to nor will it endanger the public health, safety, morals, comfort, or general welfare of the community. The proposed facility will not pose a risk of explosion, fire or other danger to life or property due to proximity to other materials and the facility will be designed and a State of California qualified engineer will certify that the proposed facility will be structurally sound.

In Conclusion

Everyday, more than 296,000 "911" calls are made from wireless phones. According to the National Center for Health Statistics Interview Survey January – June 2010, 26.6% of U.S. Households are Wireless "Only" households. The proposed Verizon Wireless Telecommunications Facility enhances the general welfare of the community by providing the infrastructure for these calls, as well as providing vital means of communication during times of emergency when traditional land lines are not available or in cases of power failure. The carefully selected and designed facility allows these calls to occur while remaining a site that meets the needs of the community now and in the future.

For the purpose and duration of this application, the project manager is **NSA Wireless, Inc. located at 2010 Crow Canyon Place Suite 355, San Ramon CA 94583, contact Pamela Nobel direct at (707) 486-7252, email: pdnobel@earthlink.net** or NSA Wireless at (925) 244-1890, and Fax: (925) 355-0672.

Verizon Wireless long-term responsible party and agent for service of process is:

GTE Mobilnet of California Limited Partnership,
dba Verizon Wireless
180 Washington Valley Road
Bedminster, New Jersey 07921
Attention: Network Real Estate



SITE NAME: HWY 101 & 237

PSL NUMBER: 277666

SITE ADDRESS: 787 NORTH MARY AVENUE
SUNNYVALE, CA 94085

SITE ADDRESS:

787 NORTH MARY AVE
SUNNYVALE, CA 94085

PROPERTY OWNER:

NORTH MARY LLC
3715 HAVEN AVENUE SUITE 210
MENLO PARK, CA 94025
CONTACT: TJ BIANCHI AND TITO BIANCHI
PHONE: (650) 298-0080

PROJECT ARCHITECT:

DIAMOND ENGINEERING SERVICES
4255 PARK ROAD
BENICIA, CA 94510
CONTACT: BRUCE LYON, ARCHITECT

PROJECT ENGINEER:

DIAMOND ENGINEERING SERVICES, INC
4255 PARK ROAD
BENICIA, CA 94510
CONTACT: ERIC UHRENHOLT, PE

CONSTRUCTION MANAGER:

NSA WIRELESS, INC.
2010 CROW CANYON PL., STE. #355
SAN RAMON, CA 94583
CONTACT: SCOTT COWAN
PHONE: (707) 689-4144

ZONING/LEASING AGENT:

NSA WIRELESS, INC.
2010 CROW CANYON PL., STE. #355
SAN RAMON, CA 94583
PAMELA NOBEL
PHONE: (707) 486-7252

JURISDICTION:

CITY OF SUNNYVALE

COUNTY:

SANTA CLARA COUNTY

APN NUMBER

165-40-003

ELEVATION (NAVD88):

40.2'± AMSL

APPLICANT:

verizon wireless



2785 MITCHELL DRIVE
WALNUT CREEK, CA 94598

LATITUDE (NAD83):

37° 23' 56.5" N

LONGITUDE (NAD83):

122° 02' 09.2" W

POWER COMPANY:

P.G.&E.
PHONE: (800) 743-5000

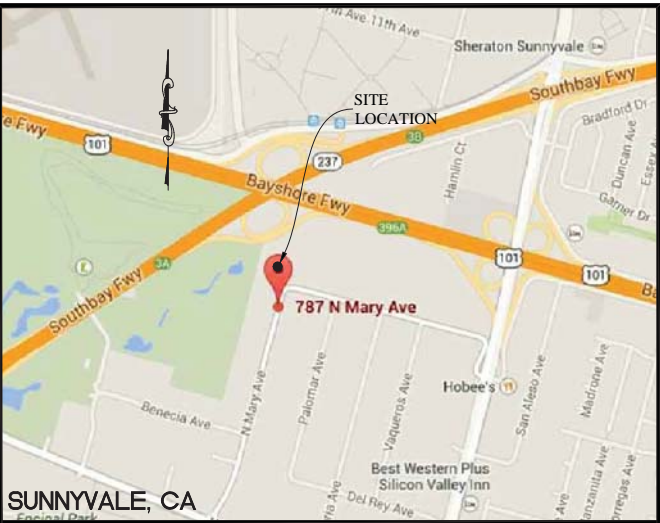
TELCO COMPANY:

AT&T

ALL WORK AND MATERIALS SHALL BE PERFORMED AND INSTALLED IN ACCORDANCE WITH THE CURRENT EDITIONS OF THE FOLLOWING CODES AS ADOPTED BY THE LOCAL GOVERNING AUTHORITIES. NOTHING IN THESE PLANS IS TO BE CONSTRUED TO PERMIT WORK NOT CONFORMING TO THE LATEST APPLICABLE VERSION OF THESE CODES.

1. 2013 CALIFORNIA BUILDING CODE (CBC)
2. WORK PERFORMED SHALL COMPLY WITH THE FOLLOWING:
 - CALIFORNIA FIRE CODE 2013 EDITION UFC
 - CALIFORNIA BUILDING CODE 2013 EDITION CBC
 - CALIFORNIA MECHANICAL CODE 2013 EDITION IAPMO
 - CALIFORNIA PLUMBING CODE 2013 EDITION IAPMO
 - CALIFORNIA ELECTRICAL CODE 2013 EDITION 2008 NEC
 - CAL GREEN CODE 2013 EDITION CGC
3. CALIFORNIA ENERGY EFFICIENCY STANDARDS CODE (CEES) 2013 EDITION REVISED JULY 2013, AND ALL APPLICABLE LOCAL & STATE ORDINANCES, CODES AND REGULATIONS AND 2013 CALIFORNIA STATE STANDARDS CODE AMENDMENTS.
4. LOCAL BUILDING CODE
5. CITY/COUNTY ORDINANCES
6. NFPA 76
7. EQUIPMENT SHELTER INSPECTED AND APPROVED BY STATE OF CALIFORNIA. NOT SUBJECT FOR LOCAL INSPECTION
8. COMPLY W/ SUNNYVALE MUNICIPAL CODE.
9. FUEL TANK SUPPORTING GENERATOR SHALL COMPLY THE REQUIREMENTS OF CHAPTER 57 CFC AND THE SUNNYVALE MUNICIPAL CODE

BUILDING CODES



VICINITY MAP

FACILITY IS UNMANNED AND NOT FOR HUMAN HABITATION. HANDICAPPED ACCESS REQUIREMENTS ARE NOT REQUIRED IN ACCORDANCE WITH THE 2013 CALIFORNIA BUILDING CODE.

ADA COMPLIANCE

SIGNATURES

VERIZON WIRELESS EQUIPMENT ENGINEER:

VERIZON WIRELESS REAL-ESTATE:

SIGNATURE:

DATE:

SIGNATURE:

DATE:

VERIZON WIRELESS CONSTRUCTION:

VERIZON WIRELESS RF ENGINEER:

SIGNATURE:

DATE:

SIGNATURE:

DATE:

PROPERTY OWNER:

AGENT - LEASING:

SIGNATURE:

DATE:

SIGNATURE:

DATE:

AGENT - CONSTRUCTION:

AGENT - ZONING:

SIGNATURE:

DATE:

SIGNATURE:

DATE:

SIGNATURE OF APPROVAL

1. INSTALL NEW VERIZON WIRELESS SHELTER LOCATED IN THE (E) PARKING AREA.
2. INSTALL NEW 30kW DIESEL GENERATOR, W/ 132 GALLON, UL 142 LISTED TANK ON NEW CONCRETE PAD INSIDE FENCED COMPOUND.
3. INSTALL NEW 54'-0" EUCALYPTUS INSIDE NEW FENCED COMPOUND.
4. INSTALL (3) NEW ANTENNAS PER SECTOR TO THE NEW MONO EUCALYPTUS, (3) SECTORS TOTAL, FOR A TOTAL OF (9) ANTENNAS.
5. INSTALL (3) NEW RRUS PER SECTOR TO THE NEW MONOPINE, (3) SECTORS TOTAL, FOR A TOTAL OF (9) RRUS.
6. INSTALL (2) NEW RAYCAPS/SURGE SUPPRESSORS TO THE NEW EUCALYPTUS.

PROJECT DESCRIPTION



2785 MITCHELL DRIVE
WALNUT CREEK, CA 94598
TEL.: (925) 904-3533
FAX: (925) 904-3513

NSA Wireless, Inc.

2010 CROW CANYON PL., STE. #355
SAN RAMON, CA 94583
(925) 244-1890



PROJECT TEAM

SHEET NO	DESCRIPTION
T1	PROJECT INFORMATION & SHEET INDEX
G1	GENERAL NOTES AND ABBREVIATIONS
C-1	SITE SURVEY
C-2	SITE SURVEY
A1	SITE PLAN
A2	EQUIPMENT & ANTENNA PLAN
A3	NORTH EAST ELEVATION
A4	SOUTH EAST ELEVATION
A5	SOUTH WEST ELEVATION
A6	NORTH WEST ELEVATION
A7	EQUIPMENT & CABLE TRAY LAYOUTS
AD1	ANTENNA, RRU & RAYCAP DETAILS
AD2	ANTENNA COLOR CODES

SITE INFORMATION

ZONING:
ZONING CLASSIFICATION:
BUILDING TYPE:
OCCUPANCY:

MS-INDUSTRIAL/SERVICE
MS-INDUSTRIAL/SERVICE
V-B
S-2, UNMANNED WIRELESS
TELECOMMUNICATIONS FACILITY
AN AUTOMATED FIRE SUPPRESSION
SYSTEM (FIRE SPRINKLERS) IS NOT
REQUIRED.

FIRE SPRINKLES:

SITE COMPOUND LEASE AREA:
SHELTER LEASE AREA:
TOTAL LEASE AREA:

895± SQ. FT. (21'-10" x 41'-0")
194± SQ. FT. (INCLUDED IN COMPOUND AREA)
895± SQ. FT.

BUILDING DATA



DIAMOND ENGINEERING SERVICES

4255 PARK ROAD
BENICIA, CA 94510
TEL: 707-304-3351



wireless

2785 MITCHELL DRIVE
WALNUT CREEK, CA
94598TEL: (925) 904-3533
FAX: (925) 904-3513

VERIZON WIRELESS
HWY 101 & 237 PSL#277666
787 NORTH MARY AVENUE
SUNNYVALE, CA 94085

PROJECT INFORMATION
& SHEET INDEX

REVISIONS	DESCRIPTION	DATE	No.	D	E	F	G	H	J
	FOR REVIEW	07-24-2014							
	FOR REVIEW	08-01-2014							
	FOR REVIEW	08-04-2014							
	FOR REVIEW	08-08-2014							
	PER PLAN COMMENT	02-09-2015							
	PER PLAN COMMENT	02-17-2015							

Job No.:
N14027

Draw/Check By:
BLL / EKV


T1

ABBREVIATIONS			
A/C	AIR CONDITIONING	GYP BD	GYPSUM BOARD
AB	ANCHOR BOLT		
AC	ASPHALTIC CONCRETE	HDR	HEADER
ACI	AMERICAN CONCRETE INSTITUTE	HDG	HOT DIP GALVANIZED
AD	AREA DRAIN	HORIZ	HORIZONTAL
ADDL	ADDITIONAL	HSS	HOLLOW STRUCTURAL SECTION
ADH	ADHESIVE		
ADJ	ADJACENT	IBC	INTERNATIONAL BUILDING CODE
ADJ	ADJUSTABLE	ID	INSIDE DIAMETER
AFF	ABOVE FINISHED FLOOR	ISO	INTERNATIONAL STANDARDS ORGANIZATION
AFG	ABOVE FINISHED GRADE		
AFS	ABOVE FINISHED SLAB		
AGL	ABOVE GRADE LEVEL	LBS	POUND
AISC	AMERICAN INSTITUTE OF STEEL CONSTRUCTION	LF	LINEAR FEET (FOOT)
		LONG	LONGITUDINAL
ANSI	AMERICAN NATIONAL STANDARDS INSTITUTE	MAX	MAXIMUM
APA	AMERICAN PLYWOOD ASSOCIATION	MB	MACHINE BOLT
APPROX	APPROXIMATE	MFR	MANUFACTURER
ASSY	ASSEMBLY	MIN	MINIMUM
ASTM	AMERICAN SOCIETY FOR TESTING AND MATERIALS	MISC	MISCELLANEOUS
AUX	AUXILIARY	mm	MILLIMETER
AVG	AVERAGE	MTL	METAL
AWS	AMERICAN WELDING SOCIETY		
		N/A	NOT APPLICABLE
B PL	BASE PLATE	NEC	NATIONAL ELECTRICAL CODE
BLDG	BUILDING	NFPA	NATIONAL FIRE PROTECTION ASSOCIATION
BM	BEAM		
BGL	BELOW GRADE LEVEL	NIC	NOT IN CONTRACT
BOS	BOTTOM OF STEEL	NO	NUMBER
BOT	BOTTOM	NTS	NOT TO SCALE
C	CHANNEL	OC	ON CENTER
CB	CARRIAGE BOLT	OD	OUTSIDE DIAMETER
CB	CATCH BASIN	OPP	OPPOSITE
CIP	CAST-IN-PLACE	OSHA	OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION
CJ	CONSTRUCTION JOINT		
CJ	CONTROL JOINT	PERP	PERPENDICULAR
CL	CENTER LINE	PL	PROPERTY LINE
CLG	CEILING	PLYWD	PLYWOOD
CLR	CLEAR	PN	PART NUMBER
CMP	CORRUGATED METAL PIPE	POC	POINT OF CONNECTION
CMU	CONCRETE MASONRY UNIT	PRELIM	PRELIMINARY
CO	CLEANOUT	PREP	PREPARATION
COL	COLUMN	PT	PRESSURE TREATED
CONFIG	CONFIGURATION		
CONN	CONNECT	QTY	QUANTITY
CONT	CONTINUE		
CSK	COUNTER SUNK	R	RADIUS
CU	CUBIC	REINF	REINFORCE
CU FT	CUBIC FEET	REQD	REQUIRED
CU IN	CUBIC INCH	REV	REVISION
CU YD	CUBIC YARD	RM	ROOM
DBL	DOUBLE	SCHED	SCHEDULE
DEG	DEGREE	SF	SQUARE FOOT (FEET)
DET	DETAIL	SHT	SHEET
DIA	DIAMETER	SIM	SIMILAR
DIAG	DIAGONAL	SPEC	SPECIFICATION
DIM	DIMENSION	SQ	SQUARE
DIR	DIRECTION	SQ IN	SQUARE INCH
DJ	DOUBLE JOIST	SQ YD	SQUARE YARD
DL	DEAD LOAD	SS	STAINLESS STEEL
DP	DIAMOND PLATE	ST	STREET
DS	DOWNSPOUT	STA	STATION
DWG	DRAWING	STD	STANDARD
E	EAST	T&B	TOP AND BOTTOM
EA	EACH	T&G	TONGUE AND GROOVE
EL	ELEVATION	TEMP	TEMPORARY
ELEV	ELEVATION	THK	THICKNESS
EMBED	EMBEDMENT	TOB	TOP OF BEAM
EP	EDGE OF PAVEMENTS	TOC	TOP OF CONCRETE
EQ	EQUAL	TOFF	TOP OF FINISH FLOOR
EQUIP	EQUIPMENT	TOF	TOP OF FOOTING
ETC	ET CETERA	TOPO	TOPOGRAPHY
EW	EACH WAY	TOS	TOP OF SLAB
		TOS	TOP OF STEEL
FDN	FOUNDATION	TOW	TOP OF WALL
FIN	FINISH	TS	TUBE STEEL
FIN .FLR.	FINISH FLOOR	TYP	TYPICAL
FIN GR	FINISH GRADE		
FLR	FLOOR	UON	UNLESS OTHERWISE NOTED
FOC	FACE OF CONCRETE		
FOS	FACE OF SLAB	VIF	VERIFY IN FIELD
FOS	FACE OF STUD		
FOW	FACE OF WALL	W	WATT
FRP	FIBERGLASS REINFORCED PLASTIC	W/	WITH
FASTNR	FASTENER	W/O	WITHOUT
FT	FOOT (FEET)	WF	WIDE FLANGE
FTG	FOOTING	WP	WORKING POINT
		WWF	WELDED WIRE FABRIC
GA	GAGE		
GALV	GALVANIZED	℄	CENTERLINE
GL	GRADE LEVEL / GROUND LEVEL	℄	PLATE
GLZ	GLAZING	±	PLUS OR MINUS
GSM	GALVANIZED SHEET METAL	(E)	EXISTING

GENERAL:	
THESE PLANS ARE A BUILDER'S SET. DES BUILDERS WAS CONTRACTED TO PROVIDE A BUILDER'S SET WITH THE AGREEMENT THAT AN EXPERIENCED AND KNOWLEDGEABLE CONTRACTOR SHALL CONSTRUCT THIS PROJECT. THE PLANS CONTAIN INFORMATION FOR GENERAL CONSTRUCTION AND BUILDING PERMIT PURPOSES ONLY AND ARE NOT EXTENSIVELY DETAILED NOR ARE SPECIFICATIONS PROVIDED. FOR ITEMS, METHODS AND/OR MATERIALS NOT SHOWN, THE MINIMUM REQUIREMENT OF THE 2013 CBC SHALL GOVERN. ANY OR PART OF ALL SYSTEMS, MATERIALS, CONNECTIONS, AND DETAILS NOT SPECIFICALLY PROVIDED IN THESE PLANS ARE THE SOLE AND COMPLETE RESPONSIBILITY OF THE CONTRACTOR TO PROPERLY VERIFY AND INSTALL. THE ENGINEER DOES NOT PROVIDE CONTINUOUS CONSTRUCTION OBSERVATION. THE CONTRACTOR IS SOLELY RESPONSIBLE FOR THE QUALITY AND CONSTRUCTION STANDARDS FOR THIS PROJECT. THE ENGINEER AND ARCHITECT ARE NOT RESPONSIBLE FOR CONSTRUCTION COST DUE TO REVISIONS TO THE DRAWINGS.	
JOB SAFETY:	
THE ENGINEER IS NOT RESPONSIBLE FOR THE FABRICATION, ERECTION, AND/OR JOB SAFETY. THE CONTRACTOR SHALL COMPLY WITH ALL LOCAL, STATE AND FEDERAL SAFETY REGULATIONS. THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR THE DESIGN AND INSTALLATION OF ALL SHORING, BRACING, FORM WORK, ETC., AS REQUIRED FOR THE PROTECTION OF LIFE AND PROPERTY DURING THE CONSTRUCTION OF THE BUILDING.	
SHOP DRAWINGS:	
SHOP DRAWINGS ARE AN AID FOR FIELD PLACEMENT AND ARE SUPERSEDED BY THE ARCHITECTURAL DRAWINGS. IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO MAKE CERTAIN THAT THE SHOP DRAWINGS AND CONSTRUCTION ARE IN CONFORMANCE WITH THE LATEST ARCHITECTURAL AND STRUCTURAL DRAWINGS. AT LEAST 3 WEEKS BEFORE FABRICATION, THE CONTRACTOR SHALL SUBMIT ALL SHOP DRAWINGS TO THE ENGINEER / ARCHITECT FOR REVIEW TO DETERMINE GENERAL COMPLIANCE WITH THE DRAWINGS. OUR REVIEW DOES NOT GUARANTEE IN ANY WAY THAT THE SHOP DRAWINGS ARE IN COMPLIANCE WITH THE LATEST DRAWINGS.	
SITE OBSERVATIONS:	
THE CONTRACTOR SHALL GIVE ENGINEER 72 HOURS MINIMUM NOTICE AS TO THE TIME OF THE SITE OBSERVATION OR SPECIAL INSPECTION FOR ANY OBSERVATION REQUIRED BY C.B.C. 2013 CHAPTER 17A. ALL WRITTEN SITE OBSERVATIONS / INSPECTIONS REPORTS BY THE REGULATORY AGENCY SHALL BE COPIED TO THE ENGINEER.	
FOUNDATION:	
DESIGN WAS BASED ON THE MINIMUM REQUIREMENTS OF CHAPTER 18 IN THE CBC.	
1. DIMENSIONS SHOWN ON THE DRAWINGS TAKE PRECEDENCE OVER SCALED MEASUREMENTS. DO NOT SCALE DRAWINGS.	
2. PLAN DIMENSIONS ARE TAKEN FROM FACE OF SURFACE TO FACE OF SURFACE OR GRID LINE.	
3. PLAN DIMENSIONS ARE TAKEN FROM FACE OF STRUCTURE, FINISH OR GRID LINE, UNLESS OTHERWISE NOTED, TYPICAL FOR FLOOR PLANS AND DETAILS.	
4. DETAILS ARE DIAGRAMMATICAL AND MAY DIFFER IN SCALE FROM PROPOSED CONDITIONS.	
5. DETAILED DRAWINGS AND LARGER SCALE DRAWINGS TAKE PRECEDENCE OVER SMALL SCALE DRAWINGS.	
6. DETAILS LABELED TYP (TYPICAL) SHALL BE FABRICATED AT ALL LIKE CONDITIONS THROUGHOUT THE PROJECT UNLESS OTHERWISE NOTED. DETAILS LABELED SIM (SIMILAR) SHALL BE FABRICATED WITH MINOR DIFFERENCES AS SHOWN IN THE DETAIL.	
7. FOR DETAILS NOT SHOWN, USE THE MANUFACTURER'S STANDARD DETAILS OR APPROVED SHOP DRAWINGS/DATA SHEETS IN ACCORDANCE WITH THE PROJECT SPECIFICATIONS.	
8. SHOP DRAWINGS ARE SUPERSEDED BY THESE DRAWINGS UNLESS APPROVED BY ENGINEER.	
9. THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR ALL DIMENSIONS AND CONDITIONS ON THE JOB AND SHALL NOTIFY THE ARCHITECT/ENGINEER OF ANY DISCREPANCIES BEFORE COMMENCING ANY WORK.	
10. CONTRACTOR SHALL BE RESPONSIBLE FOR ALL METHODS OF FABRICATION. SHORING, SCAFFOLDING, FORMWORK, ETC., MAY REQUIRE ADDITIONAL STRUCTURAL DESIGN.	
11. THE CONTRACTOR SHALL PROVIDE, LOCATE, AND INSTALL ACCESS PANELS AS REQUIRED AFTER INSTALLATION OF MECHANICAL AND ELECTRICAL WORK.	
12. ALL EQUIPMENT, DEVICES, FIXTURES AND MATERIALS SHALL BE LISTED BY AN APPROVED TESTING AGENCY.	
13. ALL MATERIAL SYSTEMS SHALL BE NON-COMBUSTIBLE WITH A FLAME SPREAD RATIO OF 25 OR LESS, SMOKE DEVELOPING RATING OF 50 OR LESS THAN A FUEL CONTRIBUTING OF ZERO.	
14. CONNECTING HARDWARE SHALL BE RATED FOR OPERATION UNDER AMBIENT CONDITIONS OF -40 TO 140 DEGREES FAHRENHEIT AND IN THE RANGE OF 0 TO 100 PERCENT RELATIVE HUMIDITY.	
15. THE SITE PLAN IS NOT BASED ON A SURVEY AND SHALL ONLY BE USED FOR THE SCOPE OF WORK SHOWN IN THESE DRAWINGS.	
16. FABRICATED STEEL PARTS, PIPE, BOLTS, PLATE WASHERS AND STEEL SECTIONS SHALL BE HOT DIPPED GALVANIZED. SHEET METAL SHALL BE GALVANIZED.	
GENERAL NOTES	
1. ADDITIONAL CONTRACTOR SUBMITTALS: UPON REQUEST, CERTIFICATION THAT THE MATERIALS BEING USED MEET THE REQUIREMENTS SPECIFIED.	
2. MANUFACTURER'S DATA FOR: GROUTS AND EPOXIES, PREFAB. WOOD TRUSSES, CALCULATIONS AND DRAWINGS.	
3. OBSERVATION: THE ENGINEER WILL VISIT THE PROJECT SITE AS DICTATED BY CONSTRUCTION PROGRESS TO MAKE GENERAL OBSERVATIONS ON THE WORK FOR GENERAL CONFORMANCE TO THE DESIGN INTENT.	
4. GENERAL: THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE QUALITY OF ALL MATERIALS USED ON THE PROJECT AND SHALL BEAR THE BURDEN OF PROOF THAT THE MATERIALS USED COMPLY WITH THE PROJECT REQUIREMENTS.	
QUALITY ASSURANCE	


CODE	CALIFORNIA BUILDING CODE (CBC), 2013 EDITION.		
RESOURCE	ASCE/SEI 7-10 (FOR SEISMIC)		
BUILDING DATA:	TABLE VALUE		
OCCUPANCY GROUP:	S-2		
CONSTRUCTION TYPE	N/A		
FIRE SPRINKLERS	FIRE SPRINKLERS ARE NOT REQUIRED		
DESIGN LOADS			
FLOOR LOAD	N/A		
ROOF LOAD	N/A		
GROUND SNOW LOAD P_g =	N/A		
BASIC WIND SPEED	110		
WIND EXPOSURE	C		
SEISMIC DESIGN CATEGORY	D		
SITE CLASS	D		
FLOOD DESIGN LOAD			
SEISMIC IMPORTANCE FACTOR=	I=1.0		
SEISMIC OCCUPANCY GROUP	III TABLE 1-1		
MAPPED SPECTRAL RESPONSE ACCELERATIONS			
$S_s=1.500\text{ g}$	$S_{ms}=1.500\text{ g}$	$S_{ds}=1.000\text{ g}$	$S_1=0.600\text{ g}$
$SM_1=0.900\text{ g}$	$S_{d1}=0.600\text{ g}$		
SPECTRAL RESPONSE COEFFICIENTS			
		$F_a=1.0$	$F_v=1.5$

DESIGN CRITERIA



DIAMOND ENGINEERING SERVICES

4255 PARK ROAD
BENICIA, CA 94510
TEL: 707-304-3351



wireless

2785 MITCHELL DRIVE
WALNUT CREEK, CA
94598
TEL: (925) 904-3533
FAX: (925) 904-3513

VERIZON WIRELESS
HWY 101 & 237 PSL#277666
787 NORTH MARY AVENUE
SUNNYVALE, CA 94085

GENERAL NOTES AND
ABBREVIATIONS

REVIEWS	DESCRIPTION	DATE		No.	D	E	F	G	H	J							
		FOR REVIEW	07-24-2014														
		FOR REVIEW	08-01-2014														
		FOR REVIEW	08-04-2014														
		FOR REVIEW	08-08-2014														
		PER PLAN COMMENT	02-09-2015														
		PER PLAN COMMENT	02-17-2015														

Job No.:
N14027

Draw/Check By:
BLL / EKU

G1

NOTES

OWNER(S): NORTH MARY LLC, A CALIFORNIA LIMITED LIABILITY COMPANY
APN: 165-40-003

THIS DRAWING DOES NOT REPRESENT A BOUNDARY SURVEY OF ANY PARCEL OF LAND, NOR DOES IT IMPLY OR INFER THAT A BOUNDARY SURVEY WAS PERFORMED. THIS IS A SPECIALIZED TOPOGRAPHIC MAP WITH PROPERTY AND EASEMENTS BEING A GRAPHIC DEPICTION BASED ON INFORMATION GATHERED FROM VARIOUS SOURCES OF RECORD AND AVAILABLE MONUMENTATION. PROPERTY LINES AND LINES OF TITLE WERE NEITHER INVESTIGATED NOR SURVEYED AND SHALL BE CONSIDERED APPROXIMATE ONLY. NO PROPERTY MONUMENTS WERE SET.

THE EASEMENTS (IF ANY) THAT APPEAR ON THIS MAP HAVE BEEN PLOTTED BASED SOLELY ON INFORMATION CONTAINED IN THE PRELIMINARY TITLE REPORT BY: FIDELITY NATIONAL TITLE COMPANY, TITLE NO. 14-5016193, DATED APRIL 14, 2014. WITHIN SAID TITLE REPORT THERE ARE FIFTEEN (15) EXCEPTIONS LISTED, SIX (6) OF WHICH ARE EASEMENTS, AND TWO (2) OF WHICH CANNOT BE PLOTTED.

THE UNDERGROUND UTILITIES (IF ANY) THAT APPEAR ON THIS MAP HAVE BEEN LOCATED BY FIELD OBSERVATION. THE SURVEYOR MAKES NO GUARANTEE THAT THE UNDERGROUND UTILITIES SHOWN COMPRISE ALL SUCH UTILITIES IN THE AREA, EITHER IN SERVICE OR ABANDONED. THE SURVEYOR FURTHER DOES NOT WARRANT THAT THE UNDERGROUND UTILITIES SHOWN ARE IN THE EXACT LOCATION INDICATED ALTHOUGH HE DOES STATE THAT THEY ARE LOCATED AS ACCURATELY AS POSSIBLE FROM THE INFORMATION AVAILABLE.

The information shown above meets or exceeds the requirements set forth in FAA order 8260.19D for 1-A accuracy (± 20' horizontally and ± 3' vertically). The horizontal datum (coordinates) are expressed as degrees, minutes and seconds, to the nearest hundredth of a second. The vertical datum (heights) are expressed in feet and decimals thereof and are determined to the nearest 0.1 foot.

THE LATITUDE AND LONGITUDE AS SHOWN WAS DETERMINED BY GPS OBSERVATIONS.

LAT. 37° 23' 57.35" N. NAD 83
LONG. 122° 02' 08.67" W. NAD 83
ELEV. 40.2' NAVD 88 - GROUND (BASIS OF DRAWING)

LESSOR'S PROPERTY LEGAL DESCRIPTION PER TITLE REPORT:

THE LAND REFERRED TO HEREIN BELOW IS SITUATED CITY OF SUNNYVALE, COUNTY OF SANTA CLARA, STATE OF CALIFORNIA, AND IS DESCRIBED AS FOLLOWS:

ALL OF PARCEL A, AS SHOWN UPON THAT CERTAIN MAP ENTITLED, "PARCEL MAP BEING A RESUBDIVISION OF LOTS 6 THRU 9 OF THAT RECORD OF SURVEY RECORDED IN BOOK 306 OF MAPS, AT PAGE 42 SANTA CLARA COUNTY RECORDS", WHICH MAP WAS FILED FOR RECORD IN THE OFFICE OF THE RECORDER OF THE COUNTY OF SANTA CLARA, STATE OF CALIFORNIA, ON MARCH 15, 1974 IN BOOK 337 OF MAPS, AT PAGE 23.

EXCEPTING THEREFROM THAT PORTION THEREOF AS CONVEYED TO THE CITY AND COUNTY OF SAN FRANCISCO, A MUNICIPAL CORPORATION, BY DEED RECORDED AUGUST 6, 1951 IN BOOK 2262, OF OFFICIAL RECORDS, AT PAGE 31.
APN: 165-40-003

LEGEND

- SITE BOUNDARY LINE
- OVERHEAD POWER LINE
- PROPERTY BOUNDARY (PER TITLE REPORT)
- EDGE OF PAVEMENT
- FENCELINE
- POWER POLE
- GROUND ELEVATION
- CONCRETE PAD
- POINT OF BEGINNING
- POINT OF COMMENCEMENT

PARKING SPACE NOTE:

EXISTING PARKING SPACES AVAILABLE:
PARKING SPACES = 164
ACCESSIBLE SPACES = 4

PROPOSED PARKING SPACES AVAILABLE AFTER RE-STRIPING:
PARKING SPACES = 160
ACCESSIBLE SPACES = 4

(5) PARKING SPACES SHALL BE REMOVED TO ACCOMMODATE THE NEW VERIZON WIRELESS LEASE AREA. HOWEVER, THERE WILL BE ONLY A NET LOSS OF (4) SPACES AFTER RE-STRIPING.

APN: 165-40-004
OWNER(S): IRVINE CO.

PARCEL B
P.M. BK. 337,
PG. 23

80' WIDE STRIP GRANTED TO THE CITY AND COUNTY OF SAN FRANCISCO PER BOOK 2262, PAGE 31 O.R.

APN: 165-40-006
80' WIDE STRIP GRANTED TO THE CITY AND COUNTY OF SAN FRANCISCO PER BOOK 2262, PAGE 31 O.R.

APN: 165-41-007
OWNER(S): IRVINE CO.

APN: 165-41-008
OWNER(S): WONG/KIM

APN: 165-40-007
OWNER(S): IRVINE CO.

LINE	LENGTH	BEARING
L1	0.91	S11°56'04"E
L2	2.50	N74°52'14"E
L3	3.60	S74°52'14"W

DETAIL 'A'
SCALE: 1" = 10'

OVERALL SITE MAP
SCALE: 1" = 60'

NSA Wireless, Inc.

2010 Crow Canyon Place, Ste. 355
San Ramon, CA 94583
Office: 925-244-1890 Fax: 925-355-0672

SMITHCO
SURVEYING ENGINEERING

P.O. BOX 81626 BAKERSFIELD, CA 93380
PHONE: (661) 393-1217 FAX: (661) 393-1218

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REVISION

NO.	DESCRIPTION	BY	DATE
0	PRELIM. ISSUE	SL	05/21/14
1	LEASE/ESMNTS	SL	07/23/14
2	TITLE REVIEW	SL	07/31/14
3	LEASE/ESMNTS	SL	08/06/14
4	ACCESS/FIRE	SL	01/06/15

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DRAWN BY: SL
CHECKED BY: GJS
DATE DRAWN: 05/21/14
SMITHCO JOB #: 88-001

SITE NAME

277666
HWY 101 & 237

SITE ADDRESS

787 NORTH MARY AVENUE
SUNNYDALE, CA 94085
SANTA CLARA COUNTY

SHEET TITLE

SITE SURVEY

FOR EXAMINATION ONLY

SHEET

C-1

(T(S) PER TITLE REPORT:

ERS CONTAINED IN THAT CERTAIN DOCUMENT
GRANT DEED
JULY 30, 1951
D BY: HENRY H. BULL, UNMARRIED MAN AND CITY AND COUNTY OF SAN FRANCISCO, A MUNICIPAL
TION
V6 DATE: AUGUST 1, 1951
V6 NO.: BOOK 2259, PAGE 187 OF OFFICIAL RECORDS
CE IS HEREBY MADE TO SAID DOCUMENT FOR FULL PARTICULARS.
ITTED AS SHOWN HEREON ***

ERS CONTAINED IN THAT CERTAIN DOCUMENT
GRANT DEED
JULY 31, 1951
D BY: LOUISE C.CHACE AND J. D. CHACE, HER HUSBAND AND THE CITY AND COUNTY OF SAN FRANCISCO, A
L CORPORATION
V6 DATE: AUGUST 6, 1951
V6 NO.: BOOK 2262, PAGE 31 OF OFFICIAL RECORDS
CE IS HEREBY MADE TO SAID DOCUMENT FOR FULL PARTICULARS.
ITTED AS SHOWN HEREON ***

WNERSHIP OF SAID LAND DOES NOT INCLUDE RIGHTS OF ACCESS TO OR FROM THE STREET, HIGHWAY,
WAY ABUTTING SAID LAND, SUCH RIGHTS HAVING BEEN RELINQUISHED BY THE DOCUMENT,
V6 DATE: JANUARY 26, 1958
V6 NO.: BOOK 4302, PAGE 186 OF OFFICIAL RECORDS
ITTED AS SHOWN HEREON ***

MENT(S) FOR THE PURPOSE(S) SHOWN BELOW AND RIGHTS INCIDENTAL THERETO AS DELINEATED OR AS
FOR DEDICATION, ON THE MAP OF SAID TRACT/PLAT;
INGRESS AND EGRESS
AS SHOWN ON MAP
ITTED AS SHOWN HEREON ***

MENT(S) FOR THE PURPOSE(S) SHOWN BELOW AND RIGHTS INCIDENTAL THERETO AS RESERVED IN A DOCUMENT;
D BY: RICHARD T. PERRY, TRUSTEE OF SUCCESSOR TRUSTEE UNDER THE TRUST AGREEMENT DATED JULY 20,
HARD T. PERRY SEPARATE PROPERTY TRUST) AND AS AMENDED AS TO AN 1/2 INTEREST AND JOHN
E, TRUSTEE OR SUCCESSOR TRUSTEE UNDER THE TRUST AGREEMENT DATED JULY 20, 1977 (JOHN ARRILLAGA
E PROPERTY TRUST) AND AS AMENDED AS TO AN UNDIVIDED 1/2 INTEREST.
INGRESS AND EGRESS
V6 DATE: JANUARY 13, 1978
V6 NO.: BOOK D404, PAGE 425 OF OFFICIAL RECORDS
ITTED AS SHOWN HEREON ***

INRECORDED LEASE WITH CERTAIN TERMS, COVENANTS, CONDITIONS AND PROVISIONS SET FORTH THEREIN AS
D BY THE DOCUMENT
SUBORDINATION, NON-DISTURBANCE AND ATTORNMEN AGREEMENT
NORTH MARY LLC, A CALIFORNIA LIMITED LIABILITY COMPANY
APPLE INC, A CALIFORNIA CORPORATION
V6 DATE: AUGUST 24, 2012
V6 NO.: INSTRUMENT NO. 21820556 OF OFFICIAL RECORDS
EMENT RECORDED AUGUST 24, 2012 AT INSTRUMENT NO. 21820556 WHICH STATES THAT SAID LEASE HAS
CE SUBORDINATE TO THE DOCUMENT
DEED OF TRUST
V6 DATE: JUNE 1, 1990
V6 NO.: INSTRUMENT NO. 10543742, BOOK L375, PAGE 978 OF OFFICIAL RECORDS
T PLOTTABLE-EXACT LOCATION IS NOT DEFINED BY RECORD ***

INRECORDED LEASE WITH CERTAIN TERMS, COVENANTS, CONDITIONS AND PROVISIONS SET FORTH THEREIN AS
D BY THE DOCUMENT
NOTICE OF NONRESPONSIBILITY
NORTH MARY LLC, A CALIFORNIA LIMITED LIABILITY COMPANY
APPLE CORPORATION, A CALIFORNIA CORPORATION
V6 DATE: JANUARY 17, 2014
V6 NO.: INSTRUMENT NO. 22500548 OF OFFICIAL RECORDS
T PLOTTABLE-EXACT LOCATION IS NOT DEFINED BY RECORD ***

D LESSEE LEASE/LICENSE AREA DESCRIPTION:

T PORTION OF THE HEREON DESCRIBED LESSOR'S PROPERTY, SITUATE IN THE CITY OF SUNNYVALE, COUNTY OF
LARA, STATE OF CALIFORNIA, MORE PARTICULARLY DESCRIBED AS FOLLOWS:

CING AT THE NORTHWESTERLY MOST CORNER OF SAID PROPERTY, THENCE ALONG A CURVE, CONCAVE TO THE
HAVING A RADIUS OF 548.03 FEET, THROUGH A CENTRAL ANGLE OF 3°20'19", FOR AN ARC DISTANCE OF 31.93
ENCE S 11°56'04" E, A DISTANCE OF 0.91 FEET TO THE POINT OF BEGINNING;

1) N 74°52'14" E, A DISTANCE OF 41.00 FEET;
2) S 15°07'46" E, A DISTANCE OF 12.83 FEET TO POINT 'A';
3) CONTINUING S 15°07'46" E, A DISTANCE OF 9.00 FEET;
4) S 74°52'14" W, A DISTANCE OF 41.00 FEET;
5) N 15°07'46" W, A DISTANCE OF 21.83 FEET TO THE POINT OF BEGINNING.

NG 895 SQUARE FEET, MORE OR LESS.

D LESSEE NON-EXCLUSIVE ACCESS AND UTILITY ROUTE DESCRIPTION:

OOT WIDE NON-EXCLUSIVE ROUTE FOR INGRESS, EGRESS AND UTILITY PURPOSES, LYING 2.50 FEET ON EACH
THE FOLLOWING DESCRIBED CENTERLINE:

IG AT THE HEREINBEFORE DESCRIBED POINT 'A';

1) S 15°07'46" E, A DISTANCE OF 11.50 FEET;
2) S 74°52'14" W, A DISTANCE OF 36.75 FEET TO POINT 'B' AND THE TERMINUS OF THIS DESCRIPTION.

R WITH A 12.00 FOOT WIDE NON-EXCLUSIVE ACCESS ROUTE FOR INGRESS AND EGRESS PURPOSES, LYING 6.00
EACH SIDE OF THE FOLLOWING DESCRIBED CENTERLINE:

IG AT THE HEREINBEFORE DESCRIBED POINT 'B';

1) S 28°24'08" E, A DISTANCE OF 42.92 FEET;
2) S 73°24'08" E, A DISTANCE OF 115.28 FEET;
3) S 28°24'08" E, A DISTANCE OF 14.50 FEET;
4) S 16°35'52" W, A DISTANCE OF 48.67 FEET;
5) S 73°24'08" E, A DISTANCE OF 48.67 FEET;
6) S 28°24'08" E, A DISTANCE OF 23.00 FEET;
7) S 16°35'52" W, A DISTANCE OF 188.21 FEET TO THE SOUTH LINE OF SAID LESSOR'S
PROPERTY AND THE TERMINUS OF THIS DESCRIPTION.

D LESSEE UTILITY ROUTE DESCRIPTION:

OOT WIDE NON-EXCLUSIVE UTILITY ROUTE FOR TELCO AND POWER PURPOSES, LYING 2.50 FEET ON EACH SIDE
FOLLOWING DESCRIBED CENTERLINE:

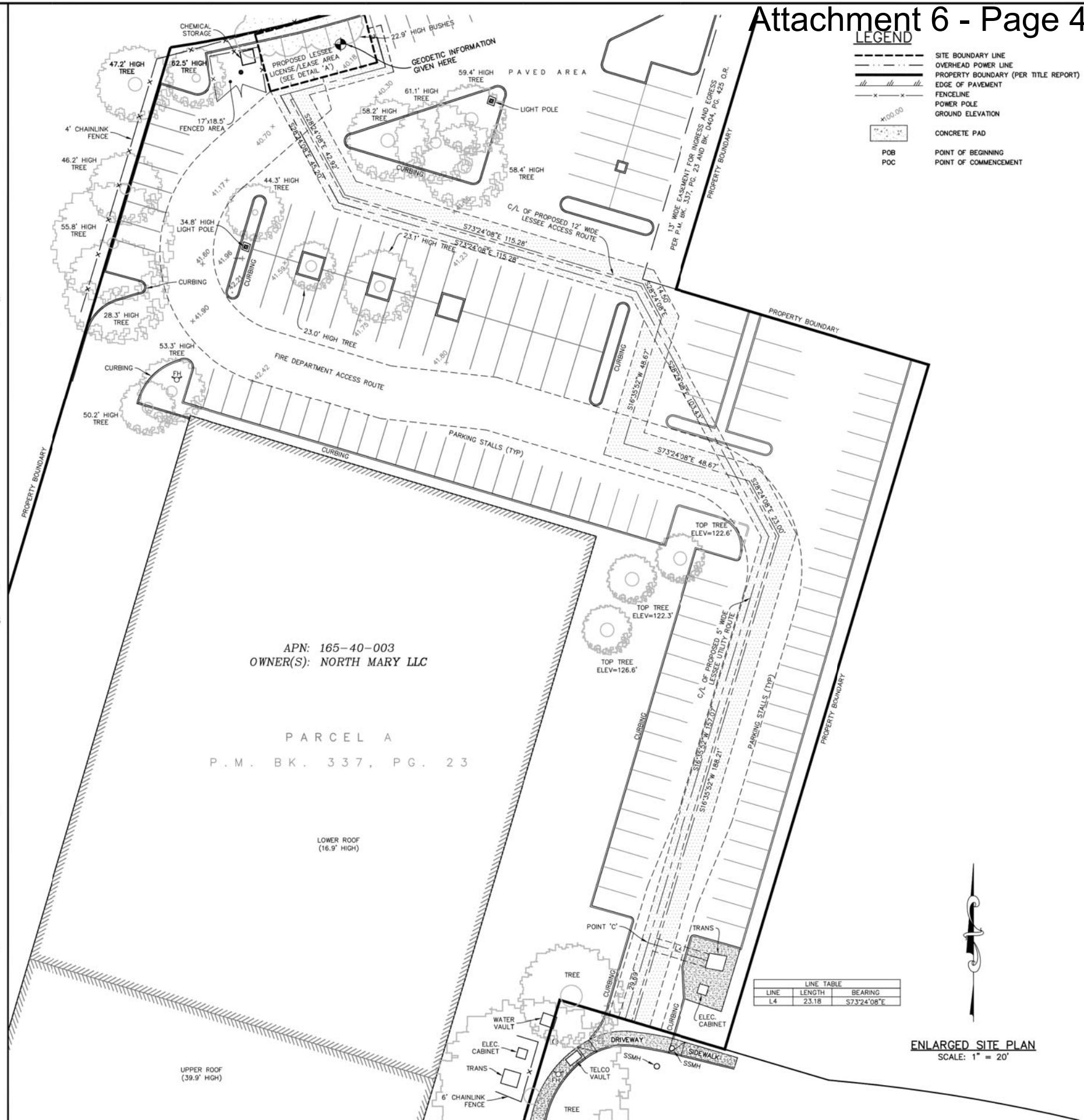
IG AT THE HEREINBEFORE DESCRIBED POINT 'B';

2) S 28°24'08" E, A DISTANCE OF 45.20 FEET;
3) S 73°24'08" E, A DISTANCE OF 115.29 FEET;
4) S 28°24'08" E, A DISTANCE OF 103.43 FEET;
5) S 16°35'52" W, A DISTANCE OF 157.07 FEET TO POINT 'C';
6) CONTINUING S 16°35'52" W, A DISTANCE OF 29.69 FEET TO THE SOUTH LINE OF
SAID LESSOR'S PROPERTY AND THE TERMINUS OF THIS DESCRIPTION.

R WITH A 5.00 WIDE NON-EXCLUSIVE UTILITY ROUTE FOR POWER PURPOSES, LYING 2.50 FEET ON EACH OF
LOWING DESCRIBED CENTERLINE:

IG AT THE HEREINBEFORE DESCRIBED POINT 'C';

1) S 73°24'08" E, A DISTANCE OF 23.18 FEET TO THE TERMINUS OF THIS DESCRIPTION.



NSA Wireless, Inc.
2010 Crow Canyon Place, Ste. 355
San Ramon, CA 94583
Office: 925-244-1890 Fax: 925-355-0672



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DRAWN BY: SL
CHECKED BY: GJS
DATE DRAWN: 05/21/14
SMITHCO JOB #: 88-001

SITE NAME

277666
HWY 101 & 237

SITE ADDRESS


787 NORTH MARY AVENUE
SUNNYDALE, CA 94085
SANTA CLARA COUNTY

SHEET TITLE

SITE SURVEY


FOR EXAMINATION ONLY
SHEET

C-2



DIAMOND ENGINEERING SERVICES

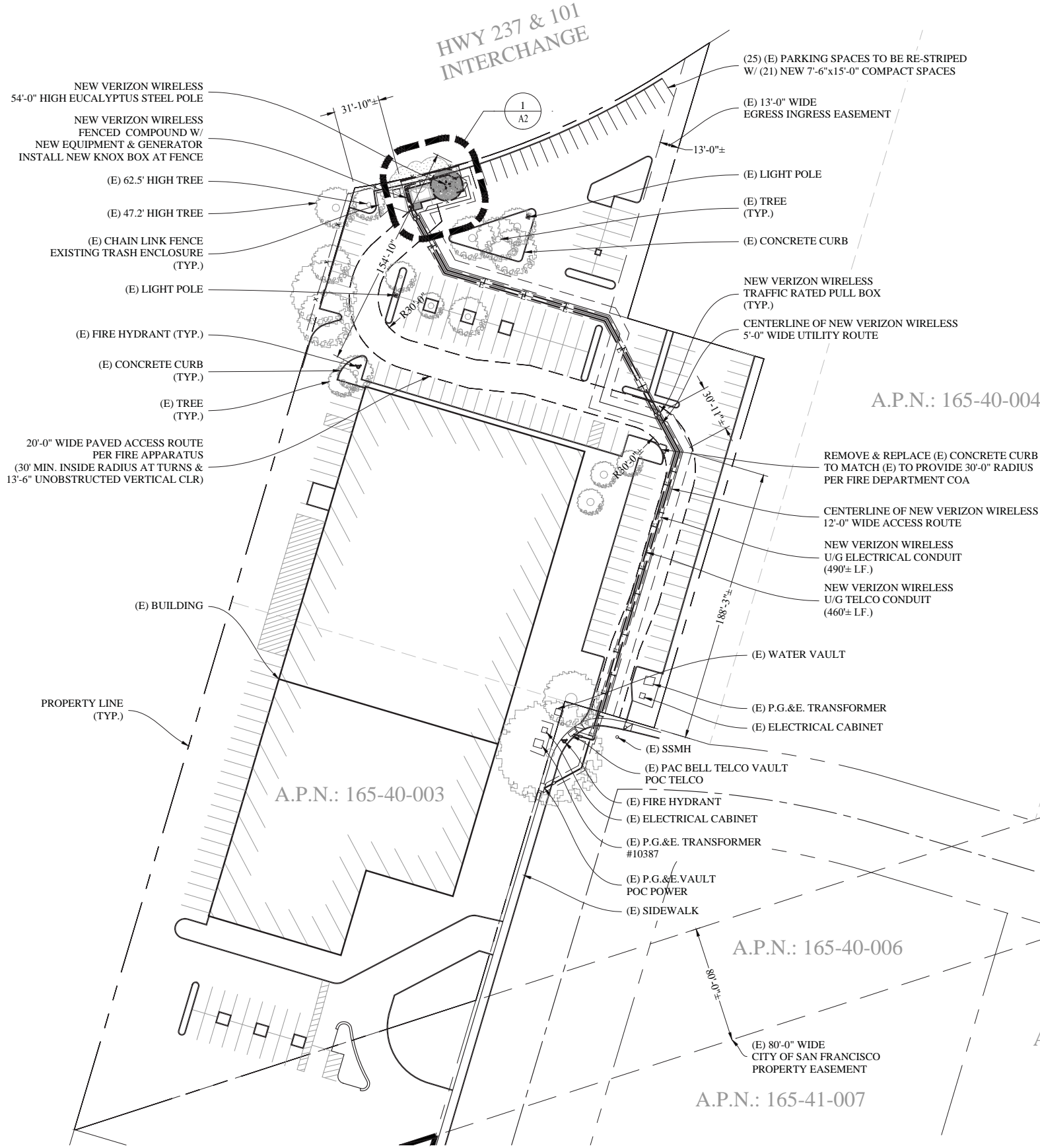
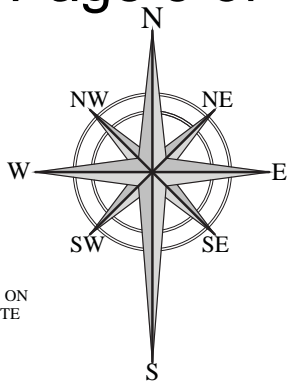
4255 PARK ROAD
BENICIA, CA 94510
TEL: 707-304-3351



verizon

wireless

2785 MITCHELL DRIVE
WALNUT CREEK, CA
94598 TEL: (925) 904-3533
FAX: (925) 904-3513



NOTES:

PRIOR TO ANY COMBUSTIBLE CONSTRUCTION OR MATERIALS ON SITE, PROVIDE FIRE ACCESS DRIVES AND OPERATIONAL ON-SITE FIRE PROTECTION SYSTEMS.

PARKING

EXISTING PARKING SPACES AVAILABLE
PARKING SPACES = 164
ACCESSIBLE SPACES = 4

PROPOSED PARKING SPACES AVAILABLE AFTER RE-STRIPING
PARKING SPACES = 160
ACCESSIBLE SPACES = 4

(5) PARKING SPACES SHALL BE REMOVED TO ACCOMMODATE THE NEW VERIZON WIRELESS LEASE AREA. HOWEVER, THERE WILL BE ONLY A NET LOSS OF (4) SPACES AFTER RE-STRIPING.

1 SITE PLAN

SCALE: 40'-0"=1'-0"


VERIZON WIRELESS		SITE PLAN	
HWY 101 & 237 PSL#277666		787 NORTH MARY AVENUE	
SUNNYVALE, CA 94085			
REVISIONS	DESCRIPTION	DATE	
No.			
D	FOR REVIEW	07-24-2014	
E	FOR REVIEW	08-01-2014	
F	FOR REVIEW	08-04-2014	
G	FOR REVIEW	08-08-2014	
H	PER PLAN COMMENT	02-09-2015	
J	PER PLAN COMMENT	02-17-2015	
Job No.: N14027		Draw/Check By: BLL / EKU	

A1



DIAMOND ENGINEERING SERVICES

4255 PARK ROAD
BENICIA, CA 94510
TEL: 707-304-3351



verizon

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VERIZON WIRELESS
HWY 101 & 237 PSL#277666
787 NORTH MARY AVENUE
SUNNYVALE, CA 94085

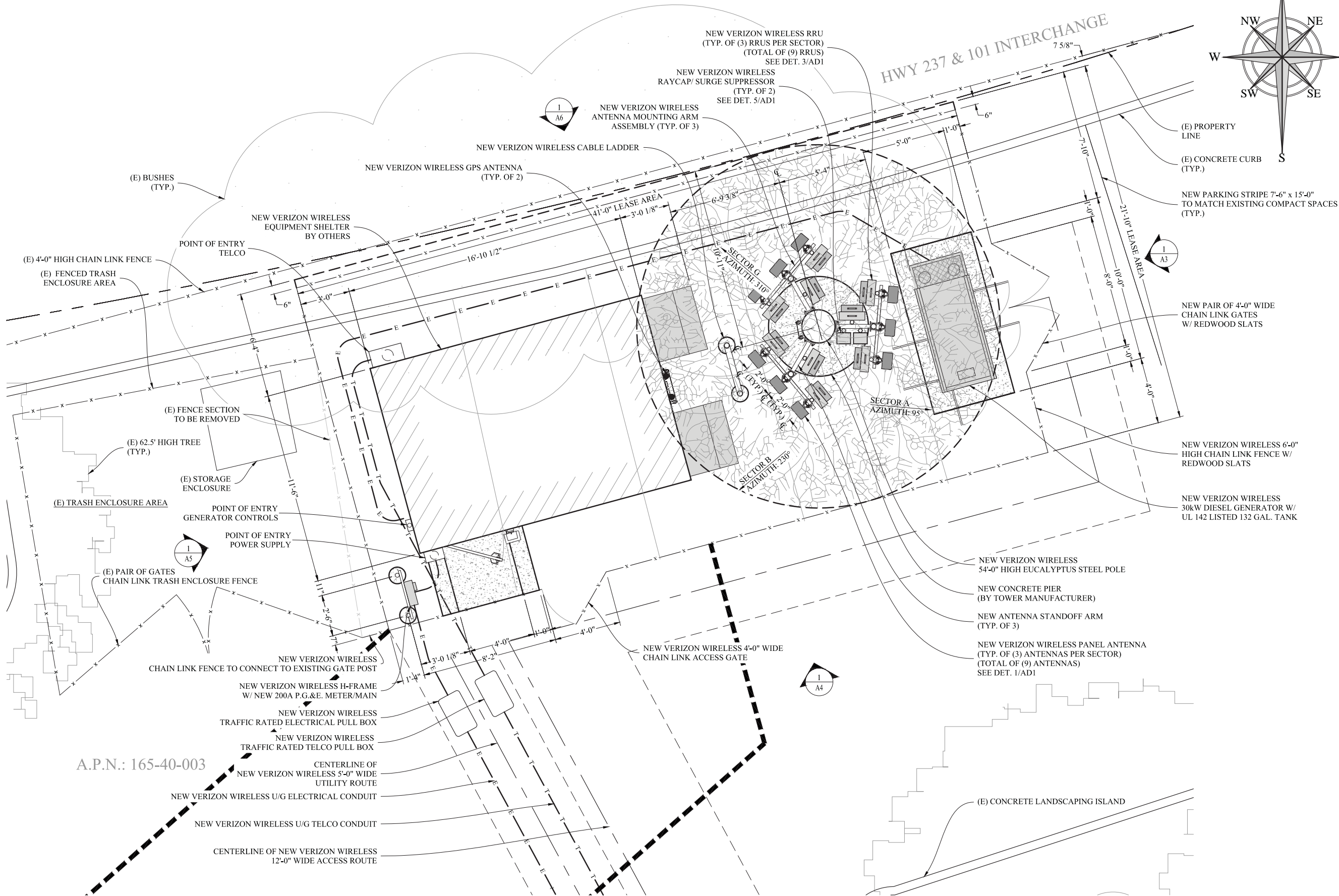
EQUIPMENT PLAN

REVISIONS		DATE
No.	DESCRIPTION	
D	FOR REVIEW	07-24-2014
E	FOR REVIEW	08-01-2014
F	FOR REVIEW	08-04-2014
G	FOR REVIEW	08-08-2014
H	PER PLAN COMMENT	02-09-2015
J	PER PLAN COMMENT	02-17-2015

Job No.:
N14027

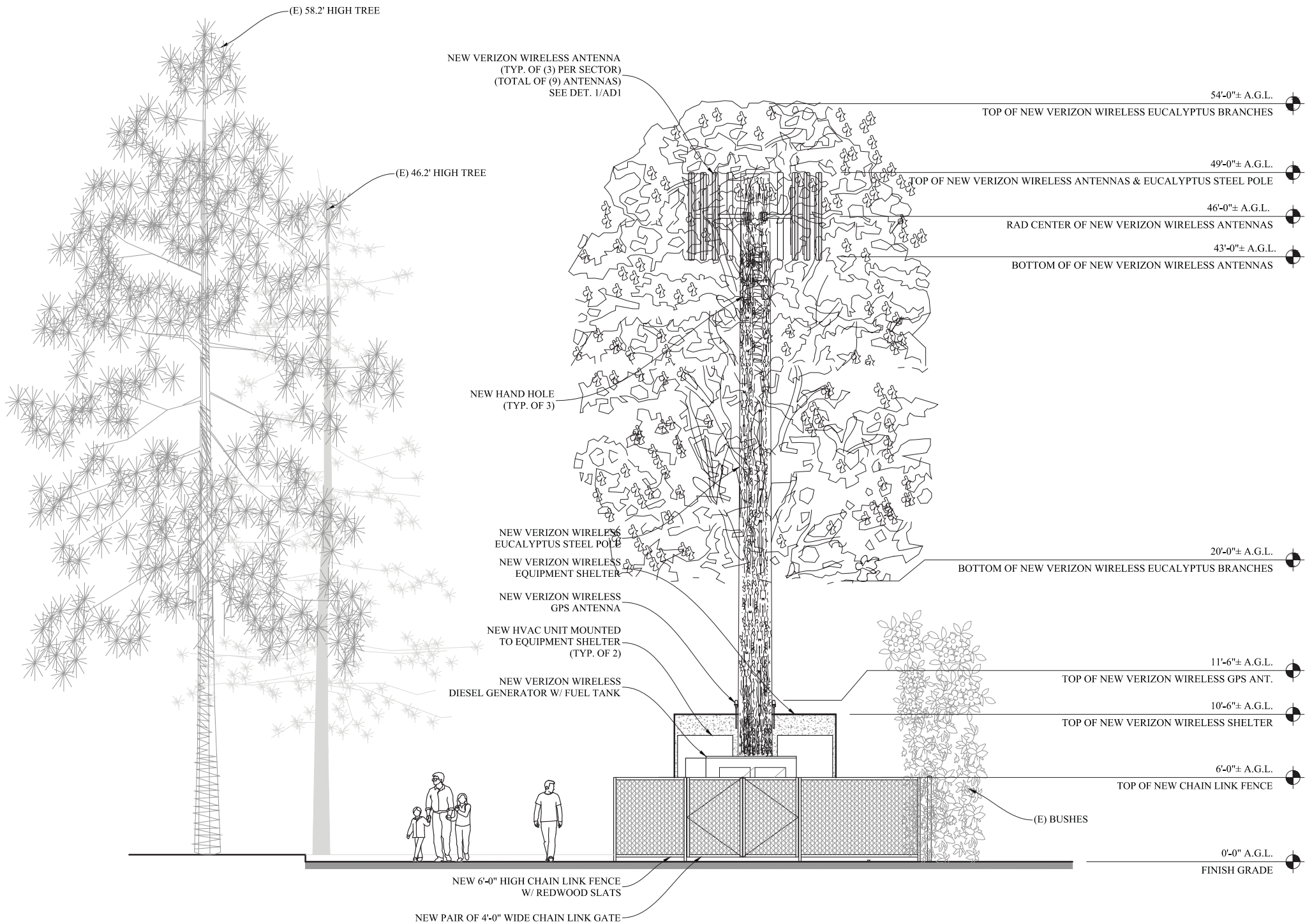
Draw/Check By:
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A2



1 EQUIPMENT PLAN

SCALE: 3/8"=1'-0"



1 NORTH EAST ELEVATION

SCALE: 1/4"=1'-0"



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FAX: (925) 904-3513

VERIZON WIRELESS
HWY 101 & 237 PSL#277666
787 NORTH MARY AVENUE
SUNNYVALE, CA 94085

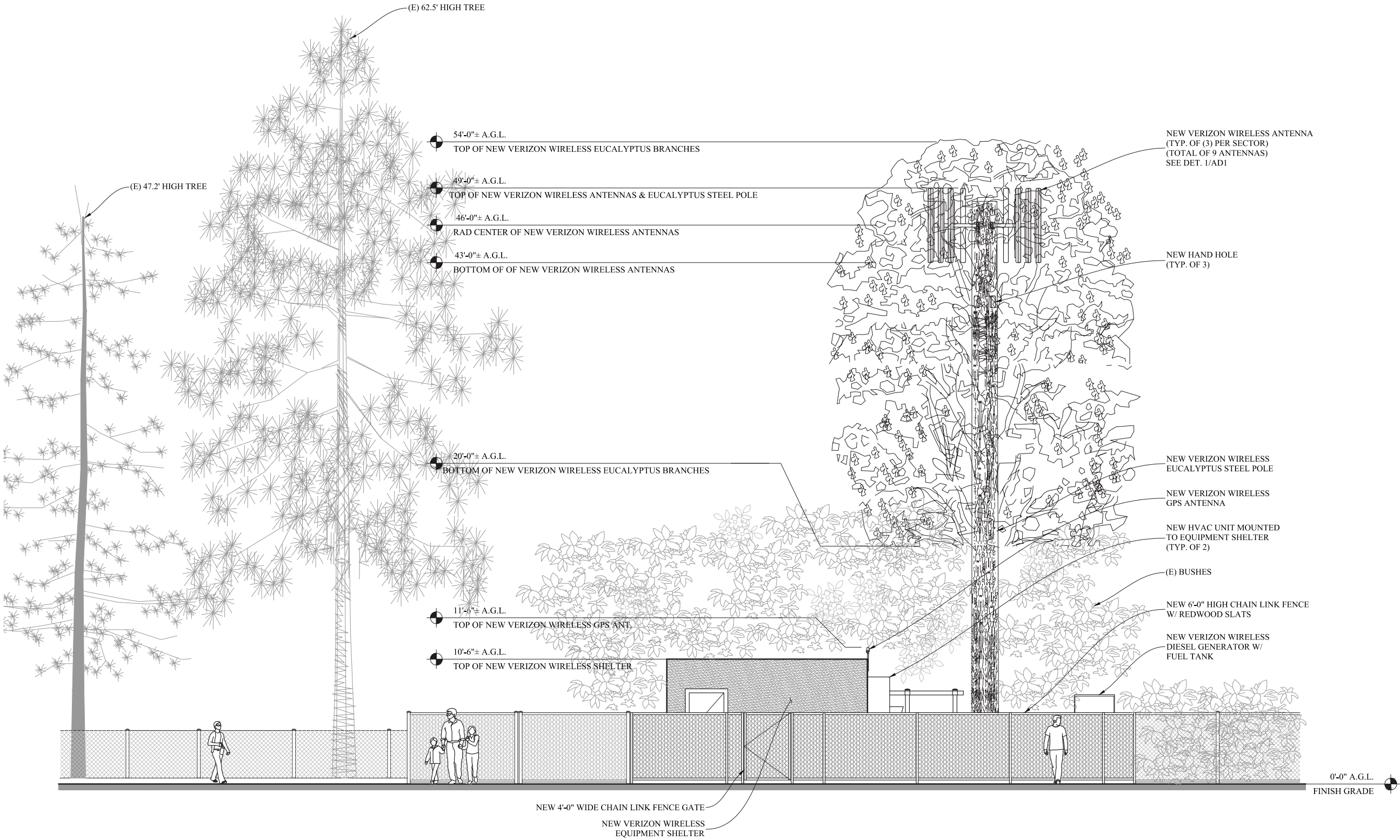
NORTH EAST
ELEVATION

No.	DESCRIPTION	REVISIONS	
		DATE	
D	FOR REVIEW	07-24-2014	
E	FOR REVIEW	08-01-2014	
F	FOR REVIEW	08-04-2014	
G	FOR REVIEW	08-08-2014	
H	PER PLAN COMMENT	02-09-2015	
J	PER PLAN COMMENT	02-17-2015	

Job No.:
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A3




1 SOUTH EAST ELEVATION

SCALE: 1/4"=1'-0"



DIAMOND ENGINEERING SERVICES

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FAX: (925) 904-3513

VERIZON WIRELESS
HWY 101 & 237 PSL#277666
787 NORTH MARY AVENUE
SUNNYVALE, CA 94085

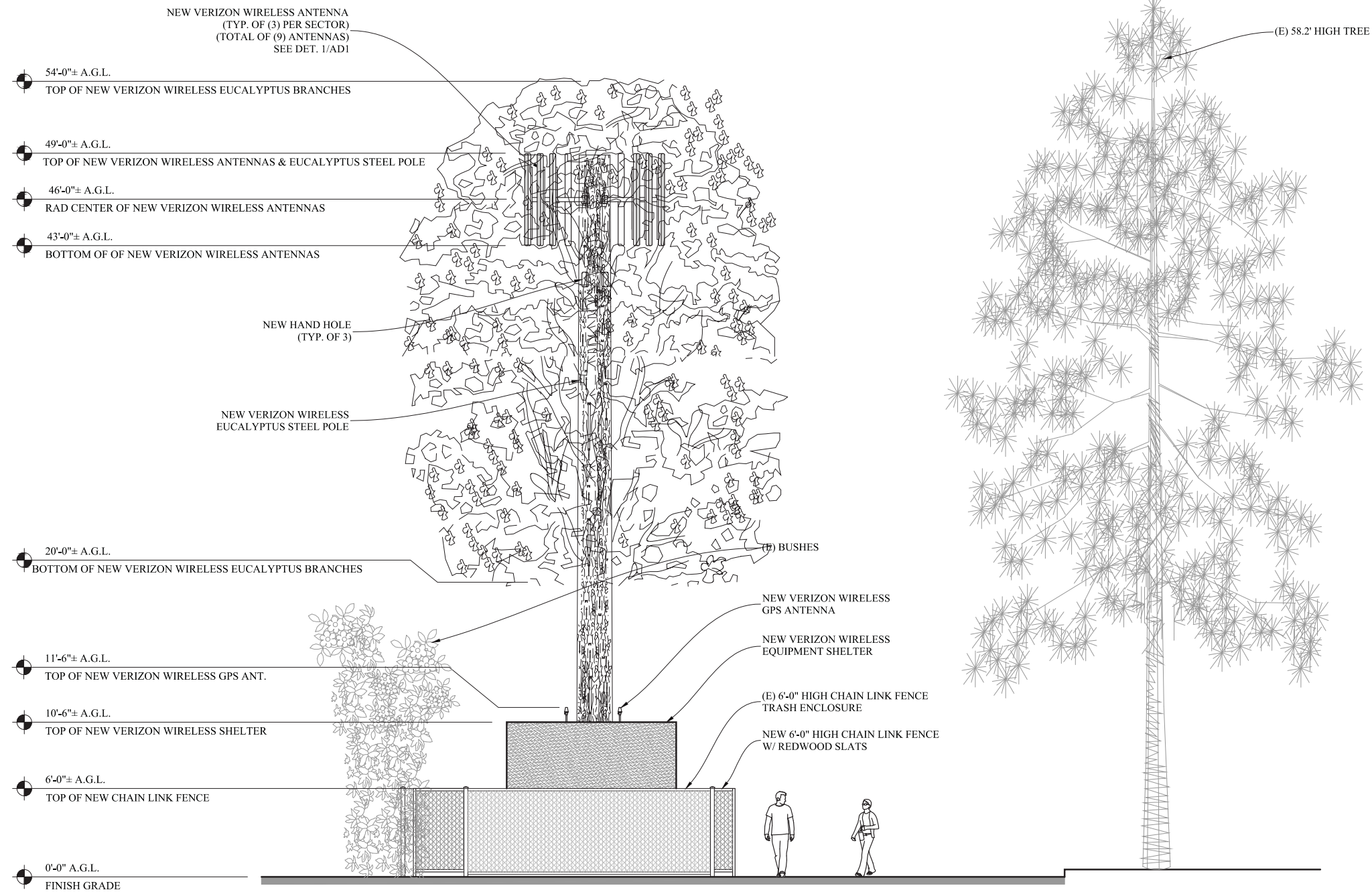
SOUTH EAST
ELEVATION

No.	DESCRIPTION	REVISIONS	
		DATE	
D	FOR REVIEW	07-24-2014	
E	FOR REVIEW	08-01-2014	
F	FOR REVIEW	08-04-2014	
G	FOR REVIEW	08-08-2014	
H	PER PLAN COMMENT	02-09-2015	
J	PER PLAN COMMENT	02-17-2015	

Job No.:
N14027

Draw/Check By:
BLL / EKU

A4



1 SOUTH WEST ELEVATION

SCALE: 1/4"=1'-0"



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VERIZON WIRELESS
HWY 101 & 237 PSL#277666
787 NORTH MARY AVENUE
SUNNYVALE, CA 94085

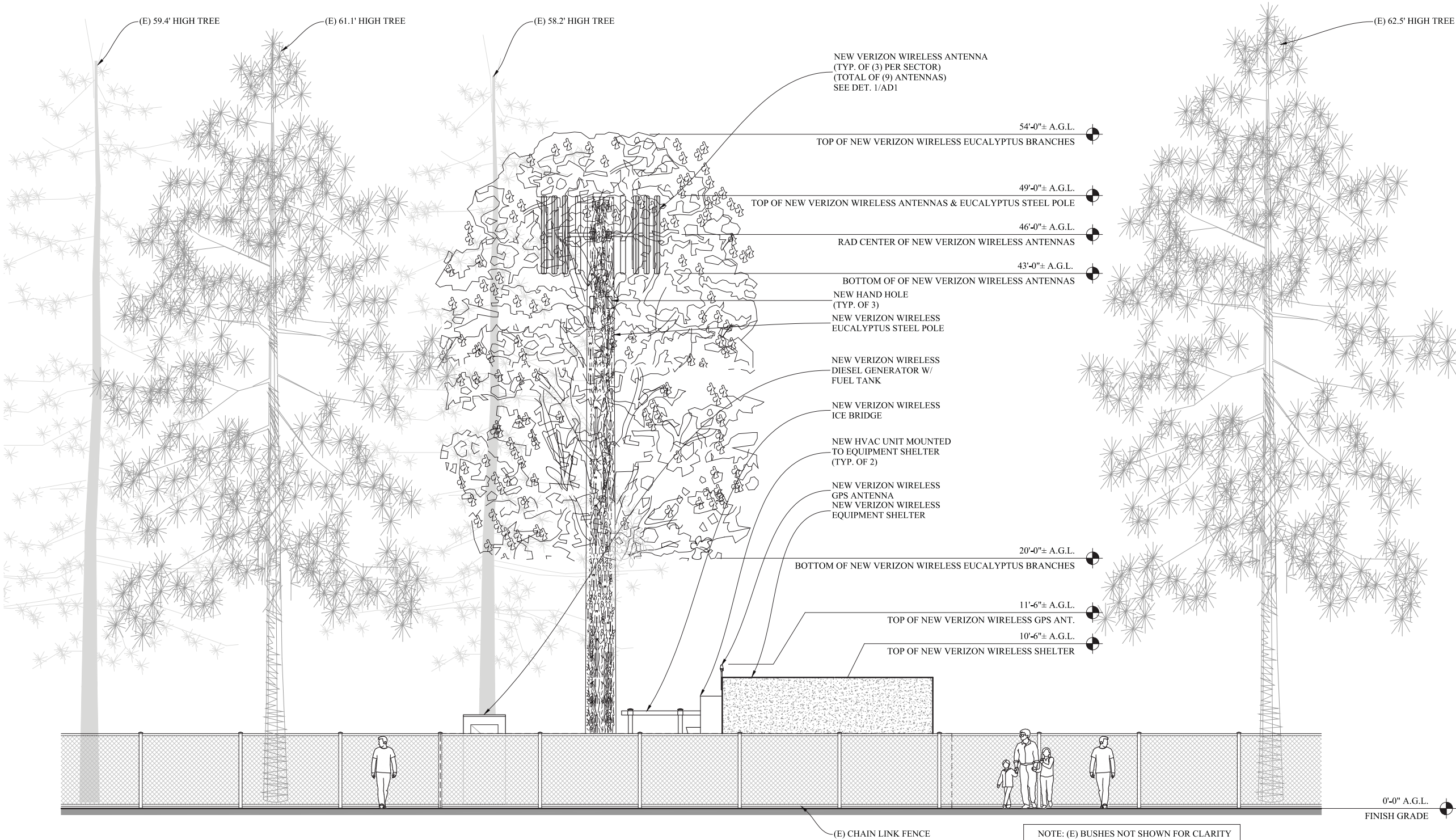
SOUTH WEST
ELEVATION

No.	DESCRIPTION	REVISIONS	
		DATE	
D	FOR REVIEW	07-24-2014	
E	FOR REVIEW	08-01-2014	
F	FOR REVIEW	08-04-2014	
G	FOR REVIEW	08-08-2014	
H	PER PLAN COMMENT	02-09-2015	
J	PER PLAN COMMENT	02-17-2015	

Job No.:
N14027

Draw/Check By:
BLL / ECU

A5



1 NORTH WEST ELEVATION

SCALE: 1/4"=1'-0"

VERIZON WIRELESS
HWY 101 & 237 PSL#277666
787 NORTH MARY AVENUE
SUNNYVALE, CA 94085

NORTH WEST
ELEVATION

No.	DESCRIPTION	REVISIONS	
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H	PER PLAN COMMENT	02-09-2015	
J	PER PLAN COMMENT	02-17-2015	

Job No.: N14027

Draw/Check By: BLL / EKV

LEGEND:

1. WAVE GUIDE ENTRY PORT

2. MASTER GROUND BAR

3. TELCO ENTRANCE PANEL (4' X 8' X 3/4")

4. a)GNB 3-75A27 BATTERY MODULES (6V - 1015 AH)

b)GNB 3-90A27 BATTERY MODULES (6V - 1140 AH)

c)GNB 3-85A29 BATTERY MODULES (6V-1210 AH) MUST BE STACKED

d)POWERSAFE VH-34 BATTERY MODULES (2V - 1040 AH)

e)CAD HD1100 BATTERY MODULES (6V - 1100 AH)

5. AIR CONDITIONING UNIT

6. SECONDARY AC POWER DISTRIBUTION PANEL

7. TRANSFER SWITCH (MANUAL OR AUTOMATIC)

8. MAIN AC POWER DISTRIBUTION PANEL

9. COMMERCIAL POWER DISCONNECT SWITCH

10. FENVAL FIRE ALARM PANEL

11. ALARM TERMINAL CABINET

12. IRRIGATION CONTROLLER

13. HALON GAS STORAGE TANK/FM200 TANK

14. +24V POWER BUSBAR

15. -48V POWER BUSBAR

16. LIGHTNING ARRESTOR

17. ELECTROSTATIC DISCHARGE MAT (ESD MAT)

18. SNAP CONNECTOR FOR ESD MAT

19. TIME CLOCK PANEL

20. AIR CONDITIONING CONTROL PANEL

21. THERMOSTAT

22. EXHAUST FAN CONTROL PANEL

23. SURGE ARRESTOR

24. DEHYDRATOR

25. TRANSFORMER

26. FIRE EXTINGUISHER

27. EXHAUST FAN

28. AIR INTAKE

29. APPLETON PLUG

NOTES:

EFP = EQUIPMENT FLOOR PLAN

PPD = POWER PLANT DETAILS

EQR = EQUIPMENT RACK PROFILES

SID = SITE INTERCONNECT DETAILS

— = FUTURE

— = EQUIPPED

REVISIONS

PROJ. NO: 201

REV: X

DATE: 09/25/15

ENGR

APRV

AMA

PBM +24V MODULAR POWER BAY

PB +24V STANDARD POWER DISTRIBUTION BAYS (E/W LORAIN RL200F25 RECTIFIERS)

PBR LORAIN RL200F25 RECTIFIER BAY

MISC MISCELLANEOUS BAY

AIF AT&T ANTENNA INTERFACE FRAME

RCF AT&T RADIO CHANNEL FRAME

LAF AT&T LINEAR AMPLIFIER FRAME

MW MICROWAVE BAY

CDPD CELLULAR DIGITAL PACKET DATA

△ DENOTES FRONT OF BAY LINEUP

verizon

wireless

ECP: SUNNYVALE

DCS: X

REGION: X AREA: SUNNYVALE

LOCATION CODE: 277666

SWITCH SITE NO: X

SITE NAME: HWY 101 & 237

SHEET NO.

1

2

3

4

EFP ISSUE NO.

1

1

PPD ISSUE NO.

EQR ISSUE NO.

SID ISSUE NO.

3 LEGEND & NOTES

24 x 36 SCALE: NONE

16'-0"

15'-0"

8"

11'-6"

10'-6"

1'-7 1/8"

2'-0"

2'-0"

2'-4"

RAYCAP

RAYCAP

MISC (D)

LTE #2 (E)

LTE #1 (E)

CAD 4800W 24A100P 480AH GE NE-S (-48V) (1)

CAD 680AH 24A100P 480AH GE NE-S (-48V) (1)

TYPICAL PRE-FAB BUILDING EQUIPMENT LAYOUT (16'-0" X 11'-6")

SCALE: 1/4"=1'-0"

PBM = GE NE-S -48V

BATT = CAD 2V

MOD CELL #1 = INDOOR 850 VERSION 4.0B

MOD CELL #2 = INDOOR 1900 VERSION 4.0B

SITE HAS PERM GENERATOR

SYM

WIDTH

DEPTH

HEIGHT

A

2'-1"

1'-8 1/2"

7'-0"

B

2'-1"

1'-3"

7'-0"

C

2'-0"

2'-0"

6'-2"

D

1'-8 1/2"

1'-3"

7'-0"

DI

2'-0 1/2"

1'-3"

7'-0"

E

2'-0"

1'-7"

4'-9"

F

2'-0"

2'-1 1/4"

7'-0"

G

2'-0"

2'-0"

7'-0"

H

2'-6"

3'-8"

7'-0"

I

2'-4"

2'-1 1/4"

7'-0"

J

1'-9"

2'-1 1/4"

6'-9"

SHEET 1

ISSUE X

HWY 101 & 237 (277666)

12x16-1 STIRREPI_MCA.08.EMERSON (2-INVAC RIGHT WALL) 010613 REV. 0 AMA013115 REV 1

1 EQUIPMENT LAYOUT

24 x 36 SCALE: 1/4"=1'-0"

16'-0"

15'-0"

8"

11'-6"

10'-6"

1'-7 1/8"

TYPICAL PRE-FAB BUILDING EQUIPMENT LAYOUT (16'-0" X 11'-6")

SCALE: 1/4"=1'-0"

NOTES: UNLESS OTHERWISE SPECIFIED

1. BOTTOM OF CABLE TRAY IS 8'-0" ABOVE FINISHED FLOOR.

SHEET 2

ISSUE X

HWY 101 & 237 (277666)

12x16-1 STIRREPI2_MCA.08.EMERSON (2-INVAC RIGHT WALL) 010613 REV. 0 AMA013115 REV 1

2 CABLE TRAY LAYOUT

24 x 36 SCALE: 1/4"=1'-0"

DESIGN

Diamond Engineering Services

4255 PARK ROAD

BENICIA, CA 94510

TEL: 707-304-3351

verizon

wireless

2785 MITCHELL DRIVE

WALNUT CREEK, CA

94598TEL: (925) 904-3533

FAX: (925) 904-3513

VERIZON WIRELESS

HWY 101 & 237 PSL#277666

787 NORTH MARY AVENUE

SUNNYVALE, CA 94085

EQUIPMENT & CABLE TRAY

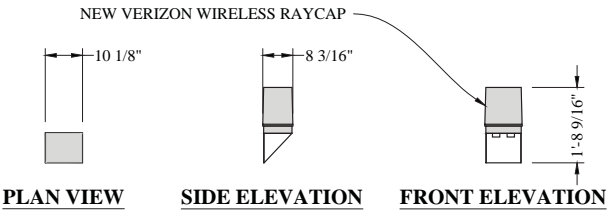
LAYOUTS

No.	DESCRIPTION	REVISIONS	
		DATE	
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H	PER PLAN COMMENT	02-09-2015	
J	PER PLAN COMMENT	02-17-2015	

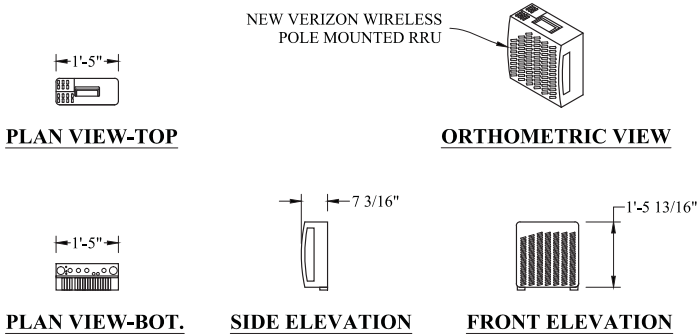
Job No.: N14027

Draw/Check By: BLL / EKU

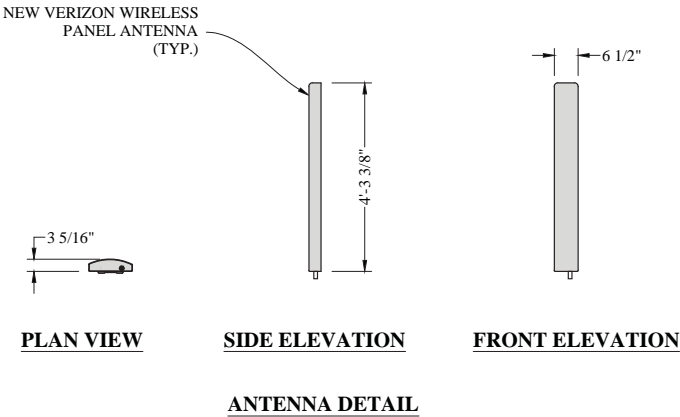
A7



RAYCAP DATA			
DIMENSIONS:	WIDTH	DEPTH	HEIGHT
	10.15"	8.2"	20.58"
TOTAL WEIGHT		14.0 LBS	



RRU DATA			
DIMENSIONS:	WIDTH	DEPTH	HEIGHT
	17"	7.2"	17.8"
POWER CONSUMPTION:		200 WATTS	
TOTAL WEIGHT:		55 LBS	
TEMPERATURE:		-40° TO 55° C	



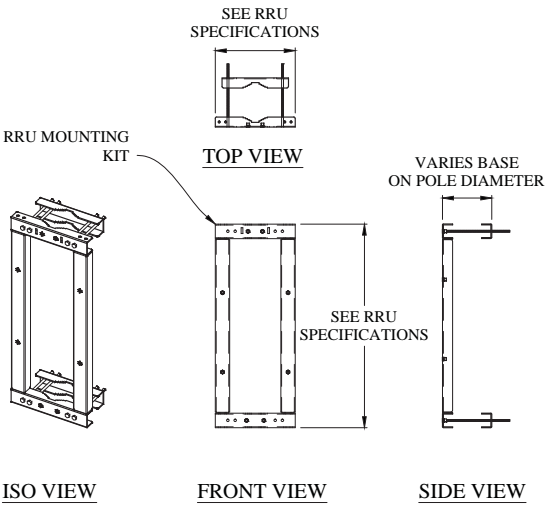
ANTENNA DATA DIMENSIONS			
HBX-6516DS - AIM	Inches	mm	COMMENT
DEPTH	3.3"	83.0	
LENGHT	51.4"	1306.0	
WITDH	6.5"	166.0	
NET WEIGHT	10.4 lbs	4.7 kg	
INCLUDED PARTS:			
DB380-3 PIPE MOUNTING KIT 2.4 - 4.5 in. NOMINAL O.D. ROUND MEMBERS			
DB5083D DOWN TILT MOUNTING KIT 2.4 - 4.5 in. NOMINAL O.D. ROUND MEMBERS			

NOTE: SEE ANTENNA MANUFACTURER DATA SHEETS FOR BILL OF MATERIALS FOR ACCESSORY PARTS

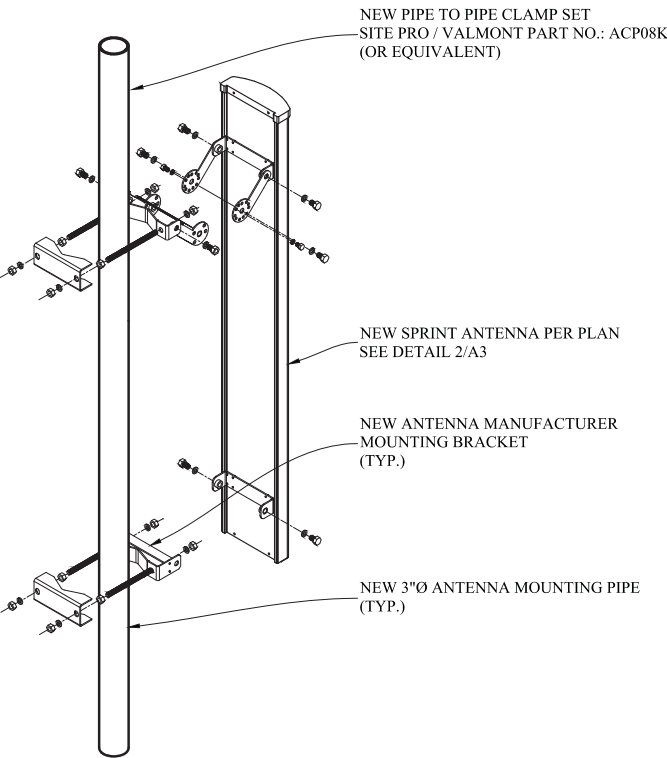
5 RAYCAP DETAIL SCALE: 1/2"=1'-0"

3 RRU DETAIL SCALE: 1/2"=1'-0"

1 ANTENNA DETAIL SCALE: 1/2"=1'-0"



4 RRU MOUNTING FRAME SCALE: 1/2"=1'-0"



2 ANTENNA MOUNTING DETAIL SCALE: 1/2"=1'-0"

REVISIONS		DATE
No.	DESCRIPTION	
D	FOR REVIEW	07-24-2014
E	FOR REVIEW	08-01-2014
F	FOR REVIEW	08-04-2014
G	FOR REVIEW	08-08-2014
H	PER PLAN COMMENT	02-09-2015
J	PER PLAN COMMENT	02-17-2015

Job No.: N14027
Draw/Check By: BLL / EKU

ANTENNA WAVEGUIDE PORT - INSIDE VIEW

123456

789101112

131415161718

192021222324

850PCSLTE

ALPHA SECTOR 1

BETA SECTOR 2

GAMMA SECTOR 3

ANTENNA COLOR CODE SCHEME FOR LUCENT 850 / PCS / LTE MODCELL

PORT NO.	DESCRIPTION	COLOR CODE
19	MICROWAVE #1 (MW-1)	RED / RED / RED
20	MICROWAVE #1 (MW-2)	BLUE / BLUE / BLUE
21	MICROWAVE #1 (MW-3)	GREEN / GREEN / GREEN
22	MICROWAVE #1 (MW-4)	YELLOW / YELLOW / YELLOW
X	MICROWAVE #1 (MW-5)	ORANGE / ORANGE / ORANGE
X	MICROWAVE #1 (MW-6)	PURPLE / PURPLE / PURPLE
X	MICROWAVE #1 (MW-7)	WHITE / WHITE / WHITE
X	MICROWAVE #1 (MW-8)	WHITE / RED / RED
X	MICROWAVE #1 (MW-9)	WHITE / BLUE / BLUE
X	MICROWAVE #1 (MW-10)	WHITE / GREEN / GREEN

ANTENNA COLOR CODE FOR 850_PCS_LTE MODCELL 041312 REV 5A

SHEET

ISSUE

-SID

X

PRIMARY MODCELL ANTENNA PORTS - REAR VIEW

7654321

GPSGAMMA GAMMA BETA BETA ALPHA ALPHA

"B2""B1""B2""B1""B2""B1"

ANTENNA WAVEGUIDE PORT - INSIDE VIEW

123456

789101112

131415161718

192021222324

850PCSLTE

ALPHA SECTOR 1

BETA SECTOR 2

GAMMA SECTOR 3

ANTENNA COLOR CODE SCHEME FOR LUCENT 850 / PCS / LTE MODCELL

PORT NO.	DESCRIPTION	COLOR CODE	COMMENTS
1	ALPHA SECTOR 1 (850)	YELLOW / RED / GREEN	850 SYSTEM
2	ANTENNA TX2RX2	YELLOW / RED / RED / GREEN / GREEN	850 SYSTEM
7	BETA SECTOR 2 (850)	BLUE / RED / GREEN	850 SYSTEM
8	ANTENNA TX4RX4	BLUE / RED / RED / GREEN / GREEN	850 SYSTEM
13	GAMMA SECTOR 3 (850)	ORANGE / RED / GREEN	850 SYSTEM
14	ANTENNA TX6RX6	ORANGE / RED / RED / GREEN / GREEN	850 SYSTEM
3	ALPHA SECTOR 1 (PCS)	PURPLE / YELLOW / RED / GREEN	PCS SYSTEM
4	ANTENNA TX2RX2	PURPLE / YELLOW / RED / RED / GREEN / GREEN	PCS SYSTEM
5	BETA SECTOR 2 (PCS)	PURPLE / BLUE / RED / GREEN	PCS SYSTEM
10	ANTENNA TX4RX4	PURPLE / BLUE / RED / RED / GREEN / GREEN	PCS SYSTEM
15	GAMMA SECTOR 3 (PCS)	PURPLE / ORANGE / RED / GREEN	PCS SYSTEM
16	ANTENNA TX6RX6	PURPLE / ORANGE / RED / RED / GREEN / GREEN	PCS SYSTEM
5	ALPHA SECTOR 1 (LTE)	PURPLE / PURPLE / YELLOW / RED / GREEN	LTE SYSTEM (C - BAND)
6	ANTENNA TX2RX2	PURPLE / PURPLE / YELLOW / RED / RED / GREEN / GREEN	LTE SYSTEM (C - BAND)
11	BETA SECTOR 2 (LTE)	PURPLE / PURPLE / BLUE / RED / GREEN	LTE SYSTEM (C - BAND)
12	ANTENNA TX4RX4	PURPLE / PURPLE / BLUE / RED / RED / GREEN / GREEN	LTE SYSTEM (C - BAND)
17	GAMMA SECTOR 3 (LTE)	PURPLE / PURPLE / ORANGE / RED / GREEN	LTE SYSTEM (C - BAND)
18	ANTENNA TX6RX6	PURPLE / PURPLE / ORANGE / RED / RED / GREEN / GREEN	LTE SYSTEM (C - BAND)

ANTENNA COLOR CODE FOR 850_PCS_LTE MODCELL 041312 REV 5A

GPS COLOR CODE

PORT NO.	DESCRIPTION	COLOR CODE	COMMENTS
23	GPS (850 / PCS)	BLUE	850 / PCS SYSTEM
24	GPS (LTE)	PURPLE / PURPLE / BLUE	LTE SYSTEM

NOTE:
SHARED 850 AND PCS FREQUENCIES THROUGH A DIPLEXED MAINLINE COAX RUN IS TO BE MARKED (1) WITH A DOUBLE BAND OF THE SECTOR COLOR ON THE FIRST LINE, AND (2) WITH A TRIPLE BAND OF THE SECTOR COLOR ON THE SECOND LINE. EX: Y/Y AND Y/Y/Y; B/B AND B/B/B; O/O AND O/O/O

SHEET

ISSUE

-SID

X

3 ANTENNA COLOR CODES (CONT.)

SCALE: NONE

1 ANTENNA COLOR CODES

SCALE: NONE

ANTENNA COLOR CODE SCHEME FOR LTE (A - BAND) AND AWS

PORT NO.	DESCRIPTION	COLOR CODE	COMMENTS
1	ALPHA SECTOR 1 (LTE)	PURPLE / PURPLE / PURPLE / YELLOW / RED / GREEN	LTE SYSTEM (A - BAND)
2	ANTENNA TX2RX2	PURPLE / PURPLE / PURPLE / YELLOW / RED / RED / GREEN / GREEN	LTE SYSTEM (A - BAND)
7	BETA SECTOR 2 (LTE)	PURPLE / PURPLE / PURPLE / BLUE / RED / GREEN	LTE SYSTEM (A - BAND)
8	ANTENNA TX4RX4	PURPLE / PURPLE / PURPLE / BLUE / RED / RED / GREEN / GREEN	LTE SYSTEM (A - BAND)
13	GAMMA SECTOR 3 (LTE)	PURPLE / PURPLE / PURPLE / ORANGE / RED / GREEN	LTE SYSTEM (A - BAND)
14	ANTENNA TX6RX6	PURPLE / PURPLE / PURPLE / ORANGE / RED / RED / GREEN / GREEN	LTE SYSTEM (A - BAND)
3	ALPHA SECTOR 1 (AWS)	PURPLE / YELLOW / RED / GREEN / PURPLE	AWS SYSTEM
4	ANTENNA TX2RX2	PURPLE / YELLOW / RED / RED / GREEN / GREEN / PURPLE	AWS SYSTEM
5	BETA SECTOR 2 (AWS)	PURPLE / BLUE / RED / GREEN / PURPLE	AWS SYSTEM
10	ANTENNA TX4RX4	PURPLE / BLUE / RED / RED / GREEN / GREEN / PURPLE	AWS SYSTEM
15	GAMMA SECTOR 3 (AWS)	PURPLE / ORANGE / RED / GREEN / PURPLE	AWS SYSTEM
16	ANTENNA TX6RX6	PURPLE / ORANGE / RED / RED / GREEN / GREEN / PURPLE	AWS SYSTEM

ANTENNA COLOR CODE FOR 850_PCS_LTE MODCELL 041312 REV 5B

NOTE:
SHARED 850 AND PCS FREQUENCIES THROUGH A DIPLEXED MAINLINE COAX RUN IS TO BE MARKED (1) WITH A DOUBLE BAND OF THE SECTOR COLOR ON THE FIRST LINE, AND (2) WITH A TRIPLE BAND OF THE SECTOR COLOR ON THE SECOND LINE. EX: Y/Y AND Y/Y/Y; B/B AND B/B/B; O/O AND O/O/O

SHEET

ISSUE

-SID

X

2 ANTENNA COLOR CODES (CONT.)

SCALE: NONE

REVISIONS

No.	DESCRIPTION	DATE
D	FOR REVIEW	07-24-2014
E	FOR REVIEW	08-01-2014
F	FOR REVIEW	08-04-2014
G	FOR REVIEW	08-08-2014
H	PER PLAN COMMENT	02-09-2015
J	PER PLAN COMMENT	02-17-2015

Job No.:

N14027

Draw/Check By:

BLL / EKU

VERIZON WIRELESS

HWY 101 & 237 PSL#277666

787 NORTH MARY AVENUE

SUNNYVALE, CA 94085

ANTENNA COLOR CODES

REVISIONS

No.	DESCRIPTION	DATE
D	FOR REVIEW	07-24-2014
E	FOR REVIEW	08-01-2014
F	FOR REVIEW	08-04-2014
G	FOR REVIEW	08-08-2014
H	PER PLAN COMMENT	02-09-2015
J	PER PLAN COMMENT	02-17-2015

Job No.:

N14027

Draw/Check By:

BLL / EKU

verizon

wireless

2785 MITCHELL DRIVE

WALNUT CREEK, CA

94598TEL: (925) 904-3533

FAX: (925) 904-3513

4255 PARK ROAD

BENICIA, CA 94510

TEL: 707-304-3351

Project Title	Use Permit to allow a new 54-foot tall wireless telecommunications facility (mono-eucalyptus) in the parking lot of 787 N. Mary Ave.
Lead Agency Name and Address	City of Sunnyvale 456 W. Olive Ave. P. O. Box 3707 Sunnyvale, CA 94088-3707
Contact Person	Timothy Maier, Assistant Planner
Phone Number	408-730-7257
Project Location	787 N. Mary Ave. Sunnyvale, CA 94085
Applicant's Name	Verizon Wireless/North Mary, LLC
Project Address	787 N. Mary Ave. Sunnyvale, CA 94085
Zoning	MS
General Plan	Industrial
Other Public Agencies whose approval is required	None

DESCRIPTION OF THE PROJECT:

The proposed project is a Use Permit to allow installation of a new 54-foot tall wireless telecommunications facility (mono-eucalyptus) in the parking lot of an industrial site located adjacent to the US-101/CA-237 interchange. The mono-eucalyptus would be sited within a line of existing trees, located in the northwesterly portion of the property adjacent to the US-101 south on-ramp. An equipment shelter would be located at the base of the antenna and contain ancillary equipment related to operation of the wireless telecommunications facility.

DETAILED PROJECT DESCRIPTION:

Background:

The mono-eucalyptus would utilize a design with a pole 24 inches in diameter, topped with an antenna assembly 9 feet, 6 inches in diameter, in which nine panel antennas and nine remote radio units (RRUs) would be located. The panel antennas would be mounted approximately 2 feet, 6 inches from the pole and would be comprised of three groups of three antennas each, arranged radially around the pole. The bottom of the antenna assembly would stand at a height of 43 feet above ground level, with the top of the antennas positioned 49 feet above ground level.

The applicant proposes to locate an equipment shelter of approximately 194 square feet, dedicated to housing ancillary wireless equipment, at the base of the tower. The equipment shelter would contain a new 30 kW Diesel generator with 132-gallon storage tank, GPS antenna, new HVAC unit, two new raycaps/surge suppressors, and associated equipment. The top of the equipment shelter would stand 10 feet, 6 inches above ground level and be enclosed by a 6 foot-tall chain-link fence.

The proposed mono-eucalyptus features simulated branches and foliage, which would partially conceal the antenna configuration. The wireless facility would be surrounded by trees of varying species and of height and foliage arrangement similar to that proposed.

On-Site Development: On-site improvements will include demolition of a portion of an existing fence and removal of four parking spaces associated with the lot. Parking capacity would remain compliant with City Municipal Code.

Off-Site Improvements: No off-site improvements are proposed.

Construction Activities and Schedule: Construction of the proposed project is expected to occur over a period of 45-60 days, based on information provided by the applicant. Construction is anticipated to commence in late July or early August 2015. Construction of the project will not involve pile driving or other extremely high noise-generating activities or significant vibration.

Surrounding Uses and Setting: The site is located near the US-101 /CA-237 interchange, immediately south of the CA-237 east ramp to US-101 south. The proposed location of the wireless facility would lie in the northwest portion of the subject property, in an existing parking lot near a landscaping island. A dense stand of tall trees is located in the area immediately surrounding the proposed project location. Surrounding land uses include the freeway interchange to the north and industrial land uses to the east, and south. Sunnyvale Golf Course lies adjacent to the project site, to the west.

EVALUATION OF ENVIRONMENTAL IMPACTS:

1. A brief explanation is required for all answers except "No Impact" answers that are adequately supported by the information sources a lead agency cites in the parentheses following each question. A "No Impact" answer is adequately supported if the referenced information sources show that the impact simply does not apply to projects like the one involved (e.g. the project falls outside a fault rupture zone). A "No Impact" answer should be explained where it is based on project-specific factors as well as general standards (e.g. the project will not expose sensitive receptors to pollutants, based on a project-specific screening analysis).
2. All answers must take account of the whole action involved, including off-site as well as on-site, cumulative as well as project-level, indirect as well as direct, and construction as well as operational impacts.
3. Once the lead agency has determined that a particular physical impact may occur, and then the checklist answers must indicate whether the impact is potentially significant, less than significant with mitigation, or less than significant. "Potentially Significant Impact" is appropriate if there is substantial evidence that an effect may be significant. If there are one or more "Potentially Significant Impact" entries when the determination is made, an EIR is required.
4. "Negative Declaration: Potentially Significant Unless Mitigation Incorporated" applies where the incorporation of mitigation measures has reduced an effect from "Potentially Significant Impact" to a "Less Significant Impact." The lead agency must describe the mitigation measures, and briefly explain how they reduce the effect to a less than significant level (mitigation measures from Section 17, "Earlier Analysis," may be cross-referenced).
5. Earlier analysis may be used where, pursuant to the tiering, program EIR, or other CEQA process, an effect has been adequately analyzed in an earlier EIR or negative declaration. Section 15063 (c) (3) (d). In this case, a brief discussion should identify the following:
6. Earlier Analysis Used. Identify and state where they are available for review.
7. Impacts Adequately Addressed. Identify which effects from the above checklist were within the scope of and adequately analyzed in an earlier document pursuant to applicable legal standards, and state whether such effects were addressed by mitigation measures based on the earlier analysis.
8. Mitigation Measures. For effects that are "Less than Significant with Mitigation Measures Incorporated," describe the mitigation measures which were incorporated or refined from the earlier document and the extent to which they address site-specific conditions for the project
9. Lead agencies are encouraged to incorporate into the checklist references to information sources for potential impacts (e.g. general plans, zoning ordinances). Reference to a previously prepared or outside document should, where appropriate, include a reference to the page or pages where the statement is substantiated.

ENVIRONMENTAL FACTORS POTENTIALLY AFFECTED:

The environmental factors checked below would be potentially affected by this project, involving at least one impact that is a "Potentially Significant Impact" as indicated by the checklist on the following pages.

- | | | |
|---|--|---|
| <input type="checkbox"/> Aesthetics | <input type="checkbox"/> Hazards & Hazardous Materials | <input type="checkbox"/> Public Services |
| <input type="checkbox"/> Agricultural Resources | <input type="checkbox"/> Hydrology/Water Quality | <input type="checkbox"/> Recreation |
| <input type="checkbox"/> Air Quality | <input type="checkbox"/> Land Use/Planning | <input type="checkbox"/> Transportation/Traffic |
| <input type="checkbox"/> Biological Resources | <input type="checkbox"/> Mineral Resources | <input type="checkbox"/> Utilities/Service Systems |
| <input type="checkbox"/> Cultural Resources | <input type="checkbox"/> Noise | <input type="checkbox"/> Mandatory Findings of Significance |
| <input type="checkbox"/> Geology/Soils | <input type="checkbox"/> Population/Housing | |

MANDATORY FINDINGS OF SIGNIFICANCE (see checklist for further information):

Does the project have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal, or eliminate important examples of the major periods of California history or prehistory?

☐ Yes
☒ No

Mandatory Findings of Significance? Does the project have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of the past projects, the effects of other current projects, and the effects of probable future projects)?

☐ Yes
☒ No

Mandatory Findings of Significance? Does the project have environmental effects which will cause substantial adverse effects on human beings, either directly or indirectly?

☐ Yes
☒ No

DETERMINATION:

On the basis of this initial evaluation:

I find that the proposed project COULD NOT have a significant effect on the environment, and a NEGATIVE DECLARATION will be prepared. ☒

I find that although the proposed project could have a significant effect on the environment, there will not be a significant effect in this case because revisions in the project have been made by or agreed to by the project proponent. A MITIGATED NEGATIVE DECLARATION will be prepared. ☐

I find that the proposed project MAY have a significant effect on the environment, and an ENVIRONMENTAL IMPACT REPORT is required. ☐

I find that the proposed project MAY have a "potential significant impact" or "potentially significant unless mitigated" impact on the environment, but at least one effect (1) has been adequately analyzed in an earlier document pursuant to applicable legal standards, and (2) has been addressed by mitigation measures based on the earlier analysis as described on attached sheets. An ENVIRONMENTAL IMPACT REPORT is required, but it must analyze only the effects that remain to be addressed. ☐

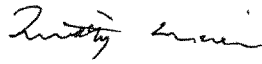
I find that although the proposed project could have a significant effect on the environment, because all potentially significant effects (a) have been analyzed in an earlier EIR or NEGATIVE DECLARATION pursuant to applicable standards and (b) have been avoided or mitigated pursuant to that earlier EIR or NEGATIVE DECLARATION, including revisions or mitigation measures that are imposed upon the proposed project, nothing further is required. ☐

Checklist Planner Name: Timothy Maier

Date: 3/19/15

Title: Assistant Planner

City of Sunnyvale

Signature: 

Planning	Potentially Significant Impact	Less than Sig. With Mitigation	Less Than Significant	No Impact	Source Other Than Project Description and Plans
1. Aesthetics -Substantially damage scenic resources, including, but not limited to trees, historic buildings?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> Sunnyvale General Plan Map www.sunnyvaleplanning.com Community Character and Land Use and Transportation Chapters of the Sunnyvale General Plan generalplan.InSunnyvale.com Sunnyvale Heritage Resources Inventory SMC 19.90 Tree Preservation Ordinance Sunnyvale Inventory of Heritage Trees
2. Aesthetics -Substantially degrade the existing visual character or quality of the site and its surroundings, including significant adverse visual changes to neighborhood character?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<ul style="list-style-type: none"> Sunnyvale General Plan Map Community Character and Land Use Chapters of the Sunnyvale General Plan SMC 19.54 Wireless Telecommunications Ordinance http://qcode.us/codes/sunnyvale/view.php?topic=19-4-19_54&showAll=1&frames=off City-Wide Design Guidelines
3. Aesthetics -Create a new source of substantial light or glare which would adversely affect day or nighttime views in the area?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> Sunnyvale General Plan Map Community Character and Land Use Chapters of the Sunnyvale General Plan SMC 19.54 Wireless Telecommunications Ordinance http://qcode.us/codes/sunnyvale/view.php?topic=19-4-19_54&showAll=1&frames=off City-Wide Design Guidelines
4. Population and Housing - Induce substantial population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of roads or other infrastructure) in a way that is inconsistent with the Sunnyvale General Plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> Land Use and Transportation Chapter of the Sunnyvale General Plan General Plan Map www.sunnyvaleplanning.com City of Sunnyvale General Plan Housing and Community Revitalization Sub-element
5. Population and Housing -Displace substantial numbers of existing housing, necessitating the construction of replacement housing elsewhere?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> Housing Chapter, Land Use and Transportation Chapter of the Sunnyvale General Plan General Plan Map www.sunnyvaleplanning.com
6. Population and Housing -Displace substantial numbers of people, necessitating the construction of	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	City of Sunnyvale General Plan, Housing Chapter www.sunnyvaleplanning.com

Planning	Potentially Significant Impact	Less than Sig. With Mitigation	Less Than Significant	No Impact	Source Other Than Project Description and Plans
replacement housing elsewhere?					
7. Land Use Planning - Physically divide an established community?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Sunnyvale General Plan Map www.sunnyvaleplanning.com
8. Land Use Planning - Conflict with the Sunnyvale General Plan, Zoning Ordinance, San Francisco Bay Conservation and Development Commission (BCDC) area or related specific plan adopted for the purpose of avoiding or mitigating an environmental effect?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> Land Use and Transportation Chapter of the Sunnyvale General Plan Title 19 (Zoning) of the Sunnyvale Municipal Code http://qcode.us/codes/sunnyvale/view.php?topic=19&frames=off
9. Transportation and Traffic - Result in inadequate parking capacity?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> Parking Requirements (Section 19.46) in the Sunnyvale Municipal Code http://qcode.us/codes/sunnyvale/view.php?topic=19-4-19_46&frames=off
10. For a project located the Moffett Field AICUZ or an airport land use plan, or where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard for people residing or working in the project area?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> Moffett Field Air Installations Compatible Use Zones (AICUZ) Sunnyvale Zoning Map Sunnyvale General Plan Map www.sunnyvaleplanning.com
11. For a project within the vicinity of a private airstrip, would the project result in a safety hazard for people residing or working in the project area?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> There are no private airstrips in or in the vicinity of Sunnyvale
12. For a project within the vicinity of Moffett Federal Airfield, would the project result in a safety hazard for people residing or working in the project area?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> Moffett Field Air Installations Compatible Use Zones (AICUZ) Study Map
13. Agricultural Resources - Conflict with existing zoning for agricultural use, or a Williamson Act contract?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> Sunnyvale Zoning Map www.sunnyvaleplanning.com
14. Noise - Exposure of persons to or generation of noise levels in excess of standards established in the Noise Sub-Element, Noise limits in the Sunnyvale Municipal Code, or applicable standards of the California Building Code?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<ul style="list-style-type: none"> Safety and Noise Chapter of the Sunnyvale General Plan www.sunnyvaleplanning.com SMC 19.54 Wireless Telecommunications Ordinance http://qcode.us/codes/sunnyvale/view.php?topic=19-4-19_54&showAll=1&frames=off SMC 19.42 Noise Ordinance

Planning	Potentially Significant Impact	Less than Sig. With Mitigation	Less Than Significant	No Impact	Source Other Than Project Description and Plans
					http://qcode.us/codes/sunnyvale/view.php?topic=19&frames=off
15. Noise -Exposure of persons to or generation of excessive_groundborne vibration?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> Safety and Noise Chapter of the Sunnyvale General Plan www.sunnyvaleplanning.com SMC 19.42 Noise Ordinance http://qcode.us/codes/sunnyvale/view.php?topic=19&frames=off
16. Noise - A substantial permanent or periodic increase in ambient noise levels in the project vicinity above levels existing without the project?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<ul style="list-style-type: none"> Safety and Noise Chapter of the Sunnyvale General Plan www.sunnyvaleplanning.com SMC 19.54 Wireless Telecommunications Ordinance http://qcode.us/codes/sunnyvale/view.php?topic=19-4-19_54&showAll=1&frames=off SMC 19.42 Noise Ordinance http://qcode.us/codes/sunnyvale/view.php?topic=19&frames=off
17. Biological Resources - Have a substantially adverse impact on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations, or by the California Department of Fish and Game or U.S Wildlife Service?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> General Plan Map Project Description
18. Biological Resources -Have a substantial adverse effect on federally protected wetlands as defined by Section 404 of the Clean Water Act (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> General Plan Map Project Description
19. Biological Resources -Interfere substantially with the movement of any resident or migratory fish or wildlife species or with established native resident migratory wildlife corridors, or impede the use of native wildlife nursery sites?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> General Plan Map Project Description
20. Biological Resources -Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<ul style="list-style-type: none"> SMC 19.90 Tree Preservation Ordinance Sunnyvale Inventory of Heritage Trees
21. Biological Resources -Conflict with the provisions of an adopted Habitat	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> General Plan Map

Planning	Potentially Significant Impact	Less than Sig. With Mitigation	Less Than Significant	No Impact	Source Other Than Project Description and Plans
Conservation Plan, Natural Conservation Community Plan, other approved local, regional, or state habitat conservation plan?					<ul style="list-style-type: none"> Project Description
22. Historic and Cultural Resources - Cause a substantial adverse change in the significance of a historical resource or a substantial adverse change in an archeological resource?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> Community Character Chapter of the Sunnyvale General Plan, Sunnyvale Inventory of Heritage Resources The United States Secretary of the Interior's "Guidelines for Rehabilitation" Criteria of the National Register of Historic Places
23. Historic and Cultural Resources - Disturb any human remains, including those interred outside of formal cemeteries?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> Project description
24. Public Services - Would the project result in substantial adverse physical impacts associated with the provision of new or expanded public schools, the construction of which could cause significant environmental impacts, in order to maintain acceptable performance objectives?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> The following public school districts are located in the City of Sunnyvale: Fremont Union High School District, Sunnyvale Elementary School District, Cupertino Union School District and Santa Clara Unified School District.
25. Air Quality - Conflict with or obstruct implementation of the <u>BAAQMD</u> air quality plan? How close is the use to a major road, hwy. or freeway?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> BAAQMD CEQA Guidelines Sunnyvale General Plan Map www.sunnyvaleplanning.com BAAQMD Screening Analysis Tables -Advanced Screening for Mobile Hazardous Sources within 1,000 feet of Project Site
26. Air Quality - Would the project generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> BAAQMD CEQA Guidelines
27. Air Quality -Would the project conflict with any applicable plan, policy or regulation of any agency adopted for the purpose of reducing the emissions of greenhouse gases?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> BAAQMD CEQA Guidelines AB 32
28. Air Quality -Violate any air quality standard or contribute substantially to	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> BAAQMD CEQA Guidelines CalEEMod Results

Planning	Potentially Significant Impact	Less than Sig. With Mitigation	Less Than Significant	No Impact	Source Other Than Project Description and Plans
an existing or projected air quality violation.					
29. Air Quality -Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard (including releasing emissions which exceed quantitative thresholds for ozone precursors)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> BAAQMD CEQA Guidelines
30. Air Quality -Expose sensitive receptors to substantial pollutant concentrations?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> BAAQMD CEQA Guidelines
31. Seismic Safety -Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> Safety and Noise Chapter of the Sunnyvale General Plan www.sunnyvaleplanning.com California Geological Survey – Alquist-Priolo Earthquake Fault Zones Map
32. Seismic Safety - Inundation by seiche, tsunami, or mudflow?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> Safety and Noise Chapter of the Sunnyvale General Plan www.sunnyvaleplanning.com
33. Seismic Safety-Strong seismic ground shaking?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> Safety and Noise Chapter of the Sunnyvale General Plan www.sunnyvaleplanning.com California Building Code
34. Seismic Safety-Seismic-related ground failure, including liquefaction?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> Safety and Noise Chapter of the Sunnyvale General Plan www.sunnyvaleplanning.com 2001 ABAG Liquefaction Hazard Map www.abag.ca.gov California Building Code

Further Discussion if "Less than Significant" with or without mitigation:

2. Aesthetics (Less than Significant): The project site is an industrial property. Project photosimulations demonstrate that the proposed mono-eucalyptus would be visible from adjacent freeway ramps and from within the property. The facility would be visible from the adjacent interstate and state highway, and, likely, from surrounding properties, including the Sunnyvale Golf Course; however, per the applicant's photosimulation, the facility would blend in with surrounding vegetation. The proposed aesthetic treatment of the facility minimizes visual impacts, as does the project location, which would be sited within a line of existing trees similar in height to the pole. The project is not expected to have significant visual impacts on the site or surrounding neighborhood, nor would the facility substantially affect neighborhood character.

14. and 16. Noise (Less than Significant with No Mitigation Required): The propose project would locate new ancillary ground equipment in an equipment shelter near the base of the proposed monopole. Equipment includes two raycaps/surge suppressors, and a new 30kW diesel-powered generator, in addition to other equipment associated with

operation of the wireless facility. The proposed generator would be utilized only for emergency purposes or in the event of extended power outage. The proposed location of the project site is at the outer extent of an industrial parking lot, near a freeway ramp. Per information provided by the applicant, a noise level of 72.2 dBA was detected at the property line to the north of the project site during operation of the emergency generator and HVAC equipment. However, operation of the generator would occur only on a temporary basis and for emergency purposes, and is not expected to act as a source of ongoing elevated noise levels; therefore, noise produced is a less than significant impact. The project is subject to compliance with noise requirements per Municipal Code and is not expected to result in substantial permanent increase in noise levels in the surrounding area.

Construction activity associated with the proposed project is estimated to take place over a 45-to 60-day period, and would result in a temporary increase in noise levels on the site and in the surrounding neighborhood. Title 16 of the City of Sunnyvale Municipal Code limits construction hours for all projects to the period between 7:00 AM and 6:00 PM Monday through Friday, and between 8:00 AM and 5:00 PM Saturday. Sunday and holiday construction is prohibited. Higher noise-generating activities such as jackhammering are expected to be limited in duration. Based on the short duration of construction and the hours imposed by Municipal Code, construction of the project is not expected to have a significant noise impact on surrounding properties.

20. Biological Resources (Less than Significant with No Mitigation Required): The project proposes removal of vegetation (trees, 22.9' in height per plans) to allow for installation of the proposed monopole and equipment shelter. Based on the indicated type and size of vegetation, the trees would not be "protected trees" per City of Sunnyvale Tree Protection Ordinance (SMC 19.94). Removal of the trees would not result in a significant impact to biological resources.

Responsible Division: Planning Division

Completed by: Timothy Maier

Date: 3/19/15

Transportation	Potentially Significant Impact	Less than Sig. With Mitigation	Less Than Significant	No Impact	Source Other Than Project Description and Plans
35. Exceeds the capacity of the existing circulation system, based on an applicable measure of effectiveness (as designated in a general plan policy, ordinance, etc.), taking into account all modes of transportation including nonmotorized travel and all relevant components of the circulation system, including but not limited to intersections, streets, highways and freeways, pedestrian walkways, bicycle paths, and mass transit?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> Project description Land Use and Transportation Chapter of the Sunnyvale General Plan www.sunnyvaleplanning.com
36. Conflict with an applicable congestion management program, including, but not limited to level of service standards and travel demand measurements, or other standards established by the county congestion management agency for designated roads or highways?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> Project description Land Use and Transportation Chapter of the Sunnyvale General Plan www.sunnyvaleplanning.com 2009 VTA Congestion Management Program
37. Results in a change in air traffic patterns, including either an increase in air traffic levels or a change in flight patterns or location that results in substantial safety risks to vehicles, bicycles, or pedestrians?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> Project description Land Use and Transportation Chapter of the Sunnyvale General Plan www.sunnyvaleplanning.com
38. Substantially increase hazards to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g. farm equipment)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> Project description Land Use and Transportation Chapter of the Sunnyvale General Plan www.sunnyvaleplanning.com
39. Conflict with adopted policies, plans, or programs regarding public transit or nonmotorized transportation?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> Project description Land Use and Transportation Chapter of the Sunnyvale General Plan www.sunnyvaleplanning.com
40. Affect the multi-modal performance of the highway and/or street and/or rail and/or off road nonmotorized trail transportation facilities, in terms of structural, operational, or perception-based measures of effectiveness (e.g. quality of service for nonmotorized and transit modes)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> Project description Land Use and Transportation Chapter of the Sunnyvale General Plan www.sunnyvaleplanning.com
41. Reduce, sever, or eliminate pedestrian or bicycle circulation or access, or preclude future planned and approved	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> Project description Land Use and Transportation Chapter of the Sunnyvale

Transportation	Potentially Significant Impact	Less than Sig. With Mitigation	Less Than Significant	No Impact	Source Other Than Project Description and Plans
bicycle or pedestrian circulation?					General Plan www.sunnyvaleplanning.com
42. Cause a degradation of the performance or availability of all transit including buses, light or heavy rail for people or goods movement?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> • Project description • 2009 VTA Congestion Management Program

Further Discussion if "Less than Significant" with or without mitigation: None required.

Responsible Division: Planning Division

Completed by: Timothy Maier

Date: 3/19/15

Building	Potentially Significant Impact	Less than Sig. With Mitigation	Less Than Significant	No Impact	Source Other Than Project Description and Plans
43. Hydrology and Water Quality - Place housing within a 100-year floodplain, as mapped on a federal Flood Hazard Boundary or Flood Insurance Rate Map or other flood hazard delineation map?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> FEMA Flood Insurance Rate Map No. 06085C0045H Effective 5/18/09 California Building Code Title 16 (Building) of the Sunnyvale Municipal Code
44. Hydrology and Water Quality - Place within a 100-year flood hazard area structures which would impede or redirect flood flows?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> FEMA Flood Insurance Rate Map No. 06085C0045H Effective 5/18/09 California Building Code Title 16 (Building) of the Sunnyvale Municipal Code
45. Hydrology and Water Quality - Expose people or structures to a significant risk of loss, injury or death involving flooding, including flooding as a result of the failure of a levee or dam?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> 1995 ABAG Dam Inundation Map www.abag.ca.gov California Building Code Title 16 (Building) of the Sunnyvale Municipal Code
46. Geology and Soils -Result in substantial soil erosion or the loss of topsoil?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> Sunnyvale Municipal Code 12.60 Storm Water Quality Best Sunnyvale Management Practices Guideline Manual
47. Geology and Soils -Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction or collapse?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<ul style="list-style-type: none"> Safety and Noise Chapter of the Sunnyvale General Plan, www.sunnyvaleplanning.com California Building Code California Plumbing, Mechanical, and Electrical Codes Title 16 (Building) of the Sunnyvale Municipal Code
48. Geology and Soils -Be located on expansive soil, as defined by the current building code, creating substantial risks to life or property?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<ul style="list-style-type: none"> California Building Code California Plumbing, Mechanical, and Electrical Codes Title 16 (Building) of the Sunnyvale Municipal Code

Further Discussion if "Less than Significant" with or without mitigation:

Discussion: California Building Code contains a series of requirements to address safety issues regarding seismic shaking, flooding, and potential hazards associated with various soil types. In addition, Title 16.62 of the City of Sunnyvale Municipal Code requires a series of measures for provisions to reduce flood-related hazards to buildings. These standards are suggested by the Federal Emergency Management Agency and required by Code by the City of Sunnyvale. These standards must be met for a Building permit to be issued.

Engineering	Potentially Significant Impact	Less than Sig. With Mitigation	Less Than Significant	No Impact	Source Other Than Project Description and Plans
49. Utilities and Service Systems: Exceed wastewater treatment requirements of the applicable Regional Water Quality Control Board?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> Environmental Management Chapter of the Sunnyvale General Plan
50. Utilities and Service Systems: Require or result in construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> Project Description Environmental Management Chapter of the Sunnyvale General Plan
51. Utilities and Service Systems: Require or result in the construction of new storm water drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> Project Description Environmental Management Chapter of the Sunnyvale General Plan
52. Utilities and Service Systems: Have sufficient water supplies available to serve the project from existing entitlements and resources, or are new or expanded entitlements needed?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> Project Description Environmental Management Chapter of the Sunnyvale General Plan
53. Utilities and Service Systems: Result in a determination by the wastewater treatment provider which services or may serve the project determined that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> Project Description Environmental Management Chapter of the Sunnyvale General Plan
54. Utilities and Service Systems: Be served by a landfill with sufficient permitted capacity to accommodate the project's solid waste disposal needs?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> Environmental Management Chapter of the Sunnyvale General Plan
55. Hydrology and Water Quality - Violate any water quality standards or waste discharge requirements?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> Project description Regional Water Quality Control Board (RWQCB) Region 2 Municipal Regional Permit

Engineering	Potentially Significant Impact	Less than Sig. With Mitigation	Less Than Significant	No Impact	Source Other Than Project Description and Plans
56. Hydrology and Water Quality - Substantially degrade groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or a lowering of the local groundwater table level (e.g., the production rate of pre-existing nearby wells would drop to a level which would not support existing land uses or planned uses for which permits have been granted)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> Santa Clara Valley Water District Groundwater Protection Ordinance www.valleywater.org
57. Hydrology and Water Quality - Otherwise substantially degrade water quality?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> Project description Environmental Management Chapter of the Sunnyvale General Plan
58. Hydrology and Water Quality - Create or contribute runoff which would exceed the capacity of existing or planned storm water drainage systems in a manner which could create flooding or provide substantial additional sources of polluted runoff?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> RWQCB, Region 2 Municipal Regional Permit, Storm water Quality BMP Guidance Manual for New and Redevelopment Projects www.sunnyvaleplanning.com
59. Hydrology and Water Quality - Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> Santa Clara Valley Water District (SCVWD) Guidelines and Standards for Land Use Near Streams www.valleywater.org City of Sunnyvale Storm water Quality Best Management Practices (BMP) Guidance Manual for New and Redevelopment Projects
60. Utilities and Service Systems: Comply with federal, state, and local statutes and regulations related to solid waste?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Environmental Management Chapter of the Sunnyvale General Plan
61. Public Services Infrastructure? Would the project result in substantial adverse physical impacts associated with the provision of new or physically altered government facilities, need for new or physically altered government facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	

Public Safety	Potentially Significant Impact	Less than Sig. With Mitigation	Less Than Significant	No Impact	Source Other Than Project Description and Plans
62. Public Services Police and Fire protection - Would the project result in substantial adverse physical impacts associated with the provision of new or physically altered government facilities, need for new or physically altered government facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Safety and Noise Chapter of the Sunnyvale General Plan www.sunnyvaleplanning.com
63. Public Services Police and Fire protection - Would the project result in inadequate emergency access?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	California Building Code SMC Section 16.52 Fire Code

Further Discussion if "Less Than Significant" with or without mitigation: None required.

Responsible Division: Planning Division

Completed by: Timothy Maier

Date: 3/19/15

Public Safety – Hazardous Materials	Potentially Significant Impact	Less than Sig. With Mitigation	Less Than Significant	No Impact	Source Other Than Project Description and Plans
64. Hazards and Hazardous Materials - Create a significant hazard to the public or the environment through the routine transport, use or disposal of hazardous materials?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<ul style="list-style-type: none"> Project description Project Radio Frequency (RF) Emissions Analysis, prepared by Hammett and Edison, Inc., dated October 10, 2014
65. Hazards and Hazardous Materials - Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the likely release of hazardous materials into the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> Project description Sunnyvale Project Environmental Information Form
66. Hazards and Hazardous Materials - Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> Project description Sunnyvale Zoning Map General Plan Map Community Vision Chapter of the City of Sunnyvale General Plan
67. Hazards and Hazardous Materials - Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result would it create a significant hazard to the public or the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> Envirostar Hazardous Waste and Substances Site List (State of California) SWRCB GeoTracker Map Sunnyvale Project Environmental Information Form
68. Hazards and Hazardous Materials - Impair implementation of, or physically interfere with an adopted emergency response plan or emergency evacuation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> Safety and Noise Chapter of the City of Sunnyvale General Plan www.sunnyvaleplanning.com

Further Discussion if "Less than Significant" with or without mitigation:

64. Hazards and Hazardous Materials (Less than Significant with Mitigation):

The proposed wireless telecommunications facility is expected to generate Radio Frequency (RF) emissions. The facility is subject to the standards and limits set by the Federal Communications Commission (FCC) for human exposure to radiofrequency emissions. The project RF Emissions Analysis (prepared by Hammett and Edison, dated 10/10/14) demonstrates that the emissions generated by the proposed project would be substantially below the maximum exposure limit established by the FCC. As a result, the proposed project is not expected to have significant adverse effects related to hazardous materials or emissions.

Responsible Division: Planning Division

Completed by: Timothy Maier

Date: 3/19/15

Community Services	Potentially Significant Impact	Less than Sig. With Mitigation	Less Than Significant	No Impact	Source Other Than Project Description and Plans
69. Public Services Parks-Would the project result in substantial adverse physical impacts associated with the provision of new or physically altered government facilities, need for new or physically altered government facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> Land Use and Transportation Chapter of the Sunnyvale General Plan, Community Character Chapter of the Sunnyvale General Plan www.sunnyvaleplanning.com
70. Recreation - Would the project increase the use of existing neighborhood or regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<ul style="list-style-type: none"> Land Use and Transportation Chapter of the Sunnyvale General Plan, Community Character Chapter of the Sunnyvale General Plan www.sunnyvaleplanning.com
71. Recreation - Does the project include recreational facilities or require the construction or expansion of recreational facilities which might have an adverse physical effect on the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> Land Use and Transportation Chapter of the Sunnyvale General Plan, Community Character Chapter of the Sunnyvale General Plan www.sunnyvaleplanning.com

Further Discussion if "Less Than Significant" with or without mitigation: None required.

Responsible Division: Planning Division

Completed by: Timothy Maier

Date: 3/19/15

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City of Sunnyvale General Plan:

Sunnyvale General Plan Consolidated in (2011)
generalplan.lnsunnyvale.com

- Community Vision
- Land Use and Transportation
- Community Character
- Housing
- Safety and Noise
- Environmental Management
- Appendix A: Implementation Plans

City of Sunnyvale Municipal Code:

- Title 8 Health and Sanitation
- Title 9 Public Peace, Safety or Welfare
- Title 10 Vehicles and Traffic
- Title 12 Water and Sewers
- Chapter 12.60 Storm Water Management
- Title 13 Streets and Sidewalks
- Title 16 Buildings and Construction
 - Chapter 16.52 Fire Code
 - Chapter 16.54 Building Standards for Buildings Exceeding Seventy-Five Feet in Height
- Title 18 Subdivisions
- Title 19 Zoning
 - Chapter 19.28 Downtown Specific Plan District
 - Chapter 19.29 Moffett Park Specific plan District
 - Chapter 19.39 Green Building Regulations
 - Chapter 19.42 Operating Standards
 - Chapter 19.54 Wireless Telecommunication Facilities
 - Chapter 19.81 Streamside Development Review
 - Chapter 19.96 Heritage Preservation
- Title 20 Hazardous Materials

Specific Plans:

- Downtown Specific Plan
- El Camino Real Precise Plan
- Lockheed Site Master Use Permit
- Moffett Park Specific Plan
- 101 & Lawrence Site Specific Plan
- Southern Pacific Corridor Plan
- Lakeside Specific Plan
- Arques Campus Specific Plan

Environmental Impact Reports:

- Futures Study Environmental Impact Report
- Lockheed Site Master Use Permit Environmental Impact Report

- Tasman Corridor LRT Environmental Impact Study (supplemental)
- Kaiser Permanente Medical Center Replacement Center Environmental Impact Report (City of Santa Clara)
- Downtown Development Program Environmental Impact Report
- Caribbean-Moffett Park Environmental Impact Report
- Southern Pacific Corridor Plan Environmental Impact Report
- East Sunnyvale ITR General Plan Amendment EIR
- Palo Alto Medical Foundation Medical Clinic Project EIR
- Luminaire (Lawrence Station Road/Hwy 237 residential) EIR
- NASA Ames Development Plan Programmatic EIS
- Mary Avenue Overpass EIR
- Mathilda Avenue Bridge EIR

Maps:

- General Plan Map
- Zoning Map
- City of Sunnyvale Aerial Maps
- Flood Insurance Rate Maps (FEMA)
- Santa Clara County Assessor's Parcel
- Utility Maps
- Air Installations Compatible Use Zones (AICUZ) Study Map
- 2010 Noise Conditions Map

Legislation / Acts / Bills / Resource Agency Codes and Permits:

- Subdivision Map Act
- San Francisco Bay Region
- Municipal Regional Stormwater NPDES Permit
- Santa Clara County Valley Water District Groundwater Protection Ordinance
- Section 404 of Clean Water Act

Lists / Inventories:

- Sunnyvale Cultural Resources Inventory List
- Heritage Landmark Designation List
- Santa Clara County Heritage Resource Inventory
- Hazardous Waste & Substances Sites List (State of California)
- List of Known Contaminants in Sunnyvale
- USFWS / CA Dept. F&G Endangered and Threatened Animals of California
<http://www.dfg.ca.gov/biogeodata/cnddb/pdfs/TEAnimals.pdf>

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- The Leaking Underground Petroleum Storage Tank List www.geotracker.waterboards.ca.gov
- The Federal EPA Superfund List www.epa.gov/region9/cleanup/california.html
- The Hazardous Waste and Substance Site List www.dtsc.ca.gov/SiteCleanup/Cortese_List.cfm

Guidelines and Best Management Practices

- Storm Water Quality Best Management Practices Guidelines Manual 2007
- Sunnyvale Citywide Design Guidelines
- Sunnyvale Industrial Guidelines
- Sunnyvale Single-Family Design Techniques
- Sunnyvale Eichler Guidelines
- Blueprint for a Clean Bay
- Santa Clara Valley Water District (SCVWD) Guidelines and Standards for Land Use Near Streams
- The United States Secretary of the Interior's Guidelines for Rehabilitation
- Criteria of the National Register of Historic Places

Transportation:

- California Department of Transportation Highway Design Manual
- California Department of Transportation Traffic Manual
- California Department of Transportation Standard Plans & Standard Specifications
- Highway Capacity Manual
- Institute of Transportation Engineers - Trip Generation Manual & Trip Generation Handbook
- Institute of Transportation Engineers - Traffic Engineering Handbook
- Institute of Transportation Engineers - Manual of Traffic Engineering Studies
- Institute of Transportation Engineers - Transportation Planning Handbook
- Institute of Transportation Engineers - Manual of Traffic Signal Design
- Institute of Transportation Engineers - Transportation and Land Development
- U.S. Dept. of Transportation Federal Highway Administration Manual on Uniform Traffic Control Devices for Street and Highways & CA Supplements
- California Vehicle Code
- Santa Clara County Congestion Management Program and Technical Guidelines
- Santa Clara County Transportation Agency Short Range Transit Plan
- Santa Clara County Transportation Plan

- Traffic Volume Studies, City of Sunnyvale Public Works Department of Traffic Engineering Division
- Statewide Integrated Traffic Records System
- Sunnyvale Zoning Ordinance – including Titles 10 & 13
- City of Sunnyvale General Plan – land Use and Transportation Element
- City of Sunnyvale Bicycle Plan
- City of Sunnyvale Neighborhood Traffic Calming Program
- Valley Transportation Authority Bicycle Technical Guidelines
- Valley Transportation Authority Community Design & Transportation – Manual of Best Practices for Integrating Transportation and Land Use
- Santa Clara County Sub-Regional Deficiency Plan
- City of Sunnyvale Deficiency Plan
- AASHTO: A Policy on Geometric Design of Highways and Streets

Public Works:

- Standard Specifications and Details of the Department of Public Works
- Storm Drain Master Plan
- Sanitary Sewer Master Plan
- Water Master Plan
- Solid Waste Management Plan of Santa Clara County
- Geotechnical Investigation Reports
- Engineering Division Project Files
- Subdivision and Parcel Map Files

Miscellaneous Agency Plans:

- ABAG Projections 2010
- Bay Area Clean Air Plan
- BAAQMD CEQA Guidelines

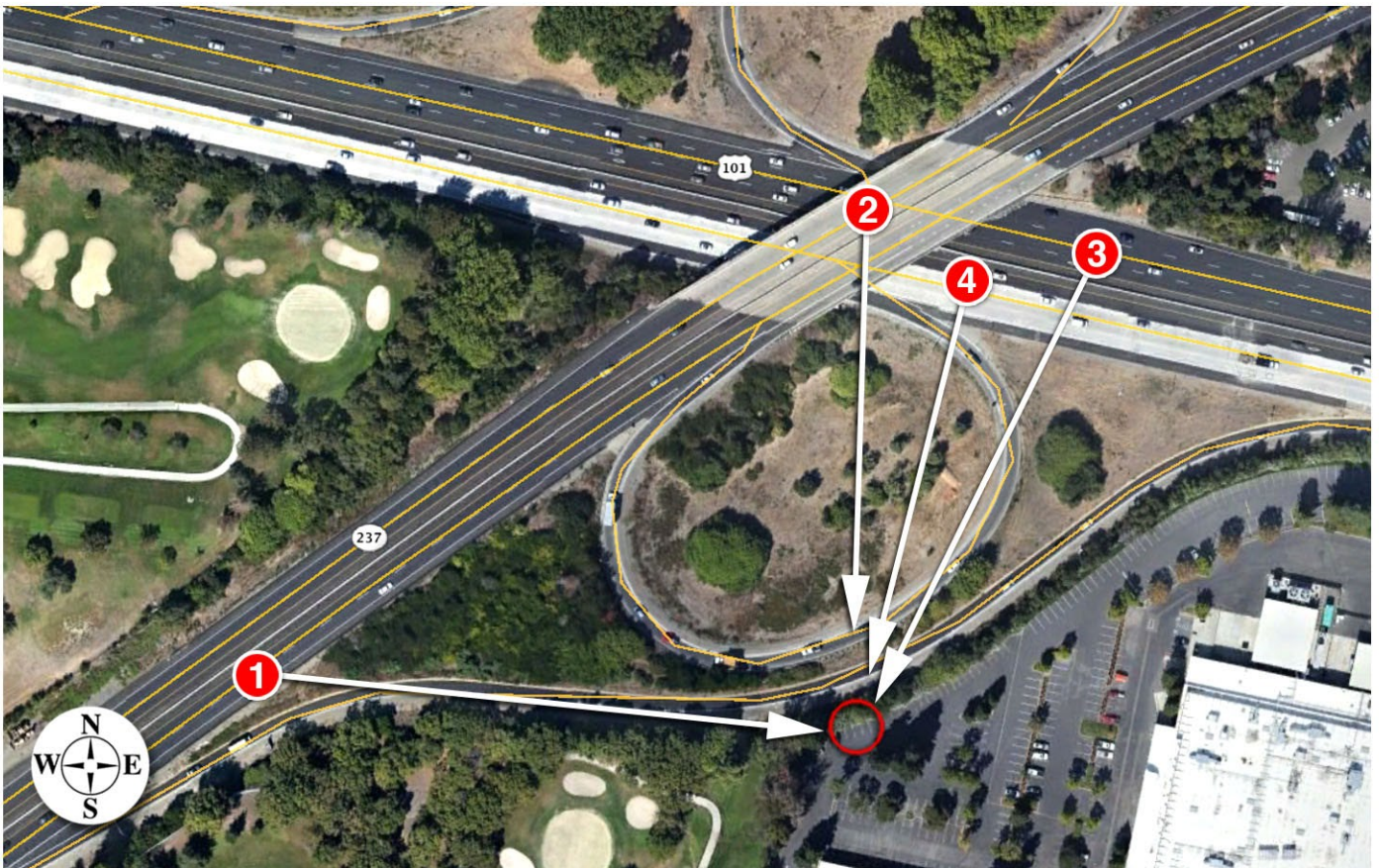
Building Safety:

- California Building Code,
- California Energy Code
- California Plumbing Code,
- California Mechanical Code,
- California Electrical Code
- California Fire Code
- Title 16.52 Sunnyvale Municipal Code
- Title 16.53 Sunnyvale Municipal Code
- Title 16.54 Sunnyvale Municipal Code
- Title 19 California Code of Regulations
- National Fire Protection Association (NFPA) standards

OTHER:

Project Specific Information

- Project Description
- Sunnyvale Project Environmental Information Form dated 11/21/14
- Project Development Plans, dated February 17, 2015
- Project acoustical analysis prepared by Hammett and Edison, Inc., dated 12/12/14
- Project Radio Frequency (RF) Emissions Analysis, prepared by Hammett and Edison, Inc., dated 10/10/14
- Project photosimulations dated 9/15/14











**Verizon Wireless • Proposed Base Station (Site No. 277666 “Highway 101 & 237”)
787 North Mary Avenue • Sunnyvale, California**

Statement of Hammett & Edison, Inc., Consulting Engineers

The firm of Hammett & Edison, Inc., Consulting Engineers, has been retained on behalf of Verizon Wireless, a personal wireless telecommunications carrier, to evaluate the base station (Site No. 277666 “Highway 101 & 237”) proposed to be located at 787 North Mary Avenue in Sunnyvale, California, for compliance with appropriate guidelines limiting human exposure to radio frequency (“RF”) electromagnetic fields.

Executive Summary

Verizon proposes to install directional panel antennas on a tall pole to be sited behind the building located at 787 North Mary Avenue in Sunnyvale. The proposed operation will comply with the FCC guidelines limiting public exposure to RF energy.

Prevailing Exposure Standards

The U.S. Congress requires that the Federal Communications Commission (“FCC”) evaluate its actions for possible significant impact on the environment. A summary of the FCC’s exposure limits is shown in Figure 1. These limits apply for continuous exposures and are intended to provide a prudent margin of safety for all persons, regardless of age, gender, size, or health. The most restrictive FCC limit for exposures of unlimited duration to radio frequency energy for several personal wireless services are as follows:

Wireless Service	Frequency Band	Occupational Limit	Public Limit
Microwave (Point-to-Point)	5,000–80,000 MHz	5.00 mW/cm ²	1.00 mW/cm ²
BRS (Broadband Radio)	2,600	5.00	1.00
WCS (Wireless Communication)	2,300	5.00	1.00
AWS (Advanced Wireless)	2,100	5.00	1.00
PCS (Personal Communication)	1,950	5.00	1.00
Cellular	870	2.90	0.58
SMR (Specialized Mobile Radio)	855	2.85	0.57
700 MHz	700	2.40	0.48
[most restrictive frequency range]	30–300	1.00	0.20

General Facility Requirements

Base stations typically consist of two distinct parts: the electronic transceivers (also called “radios” or “channels”) that are connected to the traditional wired telephone lines, and the passive antennas that send the wireless signals created by the radios out to be received by individual subscriber units. The transceivers are often located at ground level and are connected to the antennas by coaxial cables. A small antenna for reception of GPS signals is also required, mounted with a clear view of the sky. Because of the short wavelength of the frequencies assigned by the FCC for wireless services, the



**Verizon Wireless • Proposed Base Station (Site No. 277666 “Highway 101 & 237”)
787 North Mary Avenue • Sunnyvale, California**

antennas require line-of-sight paths for their signals to propagate well and so are installed at some height above ground. The antennas are designed to concentrate their energy toward the horizon, with very little energy wasted toward the sky or the ground. This means that it is generally not possible for exposure conditions to approach the maximum permissible exposure limits without being physically very near the antennas.

Computer Modeling Method

The FCC provides direction for determining compliance in its Office of Engineering and Technology Bulletin No. 65, “Evaluating Compliance with FCC-Specified Guidelines for Human Exposure to Radio Frequency Radiation,” dated August 1997. Figure 2 attached describes the calculation methodologies, reflecting the facts that a directional antenna’s radiation pattern is not fully formed at locations very close by (the “near-field” effect) and that at greater distances the power level from an energy source decreases with the square of the distance from it (the “inverse square law”). The conservative nature of this method for evaluating exposure conditions has been verified by numerous field tests.

Site and Facility Description

Based upon information provided by Verizon, including zoning drawings by Diamond Engineering Services, Inc., dated August 4, 2014, it is proposed to install nine Andrew Model SBNHH-1D65B directional panel antennas on a new 54-foot pole, configured to resemble a eucalyptus tree, to be sited in the parking lot behind the building located at 787 North Mary Avenue in Sunnyvale. The antennas would be mounted at an effective height of about 51 feet above ground and would be oriented in groups of three toward 95°T, 230°T, and 310°T. The maximum effective radiated power in any direction would be 9,700 watts, representing simultaneous operation at 3,970 watts for AWS, 3,910 watts for PCS, and 1,820 watts for 700 MHz service. There are reported no other wireless telecommunications base stations at the site or nearby.

Study Results

For a person anywhere at ground, the maximum RF exposure level due to the proposed Verizon operation is calculated to be 0.030 mW/cm², which is 3.1% of the applicable public exposure limit. The maximum calculated level at the top-floor elevation of any nearby building* is 2.0% of the public exposure limit. It should be noted that these results include several “worst-case” assumptions and therefore are expected to overstate actual power density levels from the proposed operation.

* Located at least 145 feet away, based on photographs from Google Maps.

**Verizon Wireless • Proposed Base Station (Site No. 277666 “Highway 101 & 237”)
787 North Mary Avenue • Sunnyvale, California**

No Recommended Mitigation Measures

Due to their mounting locations, the Verizon antennas would not be accessible to the general public, and so no mitigation measures are necessary to comply with the FCC public exposure guidelines. It is presumed that Verizon will, as an FCC licensee, take adequate steps to ensure that its employees or contractors receive appropriate training and comply with FCC occupational exposure guidelines whenever work is required near the antennas themselves.

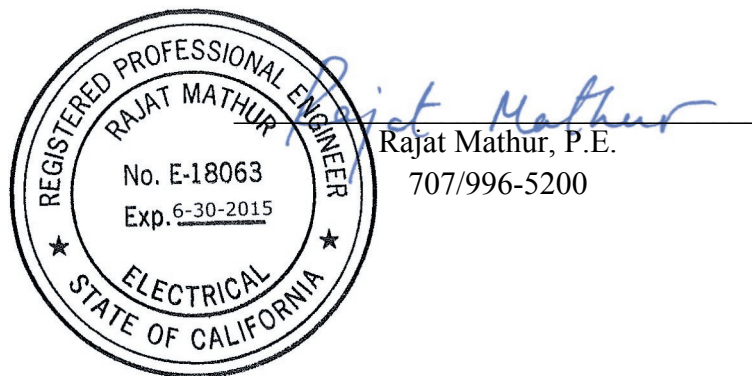
Conclusion

Based on the information and analysis above, it is the undersigned’s professional opinion that operation of the base station proposed by Verizon Wireless at 787 North Mary Avenue in Sunnyvale, California, will comply with the prevailing standards for limiting public exposure to radio frequency energy and, therefore, will not for this reason cause a significant impact on the environment. The highest calculated level in publicly accessible areas is much less than the prevailing standards allow for exposures of unlimited duration. This finding is consistent with measurements of actual exposure conditions taken at other operating base stations.

Authorship

The undersigned author of this statement is a qualified Professional Engineer, holding California Registration No. E-18063, which expires on June 30, 2015. This work has been carried out under his direction, and all statements are true and correct of his own knowledge except, where noted, when data has been supplied by others, which data he believes to be correct.

October 10, 2014

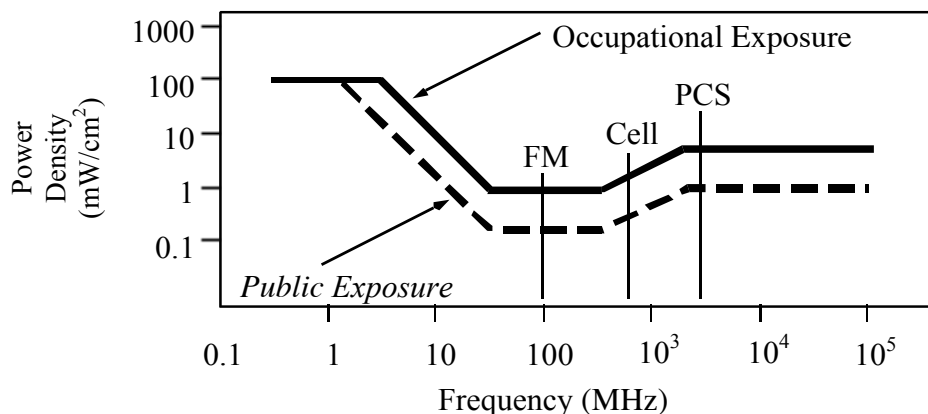


FCC Radio Frequency Protection Guide

The U.S. Congress required (1996 Telecom Act) the Federal Communications Commission (“FCC”) to adopt a nationwide human exposure standard to ensure that its licensees do not, cumulatively, have a significant impact on the environment. The FCC adopted the limits from Report No. 86, “Biological Effects and Exposure Criteria for Radiofrequency Electromagnetic Fields,” published in 1986 by the Congressionally chartered National Council on Radiation Protection and Measurements (“NCRP”). Separate limits apply for occupational and public exposure conditions, with the latter limits generally five times more restrictive. The more recent standard, developed by the Institute of Electrical and Electronics Engineers and approved as American National Standard ANSI/IEEE C95.1-2006, “Safety Levels with Respect to Human Exposure to Radio Frequency Electromagnetic Fields, 3 kHz to 300 GHz,” includes similar limits. These limits apply for continuous exposures from all sources and are intended to provide a prudent margin of safety for all persons, regardless of age, gender, size, or health.

As shown in the table and chart below, separate limits apply for occupational and public exposure conditions, with the latter limits (in *italics* and/or dashed) up to five times more restrictive:

Frequency Applicable Range (MHz)	Electromagnetic Fields (f is frequency of emission in MHz)					
	Electric Field Strength (V/m)		Magnetic Field Strength (A/m)		Equivalent Far-Field Power Density (mW/cm ²)	
0.3 – 1.34	614	<i>614</i>	1.63	<i>1.63</i>	100	<i>100</i>
1.34 – 3.0	614	<i>823.8/f</i>	1.63	<i>2.19/f</i>	100	<i>180/f²</i>
3.0 – 30	1842/f	<i>823.8/f</i>	4.89/f	<i>2.19/f</i>	900/f ²	<i>180/f²</i>
30 – 300	61.4	<i>27.5</i>	0.163	<i>0.0729</i>	1.0	<i>0.2</i>
300 – 1,500	3.54√f	<i>1.59√f</i>	√f/106	<i>√f/238</i>	f/300	<i>f/1500</i>
1,500 – 100,000	137	<i>61.4</i>	0.364	<i>0.163</i>	5.0	<i>1.0</i>



Higher levels are allowed for short periods of time, such that total exposure levels averaged over six or thirty minutes, for occupational or public settings, respectively, do not exceed the limits, and higher levels also are allowed for exposures to small areas, such that the spatially averaged levels do not exceed the limits. However, neither of these allowances is incorporated in the conservative calculation formulas in the FCC Office of Engineering and Technology Bulletin No. 65 (August 1997) for projecting field levels. Hammett & Edison has built those formulas into a proprietary program that calculates, at each location on an arbitrary rectangular grid, the total expected power density from any number of individual radio sources. The program allows for the description of buildings and uneven terrain, if required to obtain more accurate projections.



RFR.CALC™ Calculation Methodology

Assessment by Calculation of Compliance with FCC Exposure Guidelines

The U.S. Congress required (1996 Telecom Act) the Federal Communications Commission (“FCC”) to adopt a nationwide human exposure standard to ensure that its licensees do not, cumulatively, have a significant impact on the environment. The maximum permissible exposure limits adopted by the FCC (see Figure 1) apply for continuous exposures from all sources and are intended to provide a prudent margin of safety for all persons, regardless of age, gender, size, or health. Higher levels are allowed for short periods of time, such that total exposure levels averaged over six or thirty minutes, for occupational or public settings, respectively, do not exceed the limits.

Near Field.

Prediction methods have been developed for the near field zone of panel (directional) and whip (omnidirectional) antennas, typical at wireless telecommunications base stations, as well as dish (aperture) antennas, typically used for microwave links. The antenna patterns are not fully formed in the near field at these antennas, and the FCC Office of Engineering and Technology Bulletin No. 65 (August 1997) gives suitable formulas for calculating power density within such zones.

For a panel or whip antenna, power density $S = \frac{180}{\theta_{BW}} \times \frac{0.1 \times P_{net}}{\pi \times D \times h}$, in mW/cm²,

and for an aperture antenna, maximum power density $S_{max} = \frac{0.1 \times 16 \times \eta \times P_{net}}{\pi \times h^2}$, in mW/cm²,

where θ_{BW} = half-power beamwidth of the antenna, in degrees, and

P_{net} = net power input to the antenna, in watts,

D = distance from antenna, in meters,

h = aperture height of the antenna, in meters, and

η = aperture efficiency (unitless, typically 0.5-0.8).

The factor of 0.1 in the numerators converts to the desired units of power density.

Far Field.

OET-65 gives this formula for calculating power density in the far field of an individual RF source:

power density $S = \frac{2.56 \times 1.64 \times 100 \times RFF^2 \times ERP}{4 \times \pi \times D^2}$, in mW/cm²,

where ERP = total ERP (all polarizations), in kilowatts,

RFF = relative field factor at the direction to the actual point of calculation, and

D = distance from the center of radiation to the point of calculation, in meters.

The factor of 2.56 accounts for the increase in power density due to ground reflection, assuming a reflection coefficient of 1.6 ($1.6 \times 1.6 = 2.56$). The factor of 1.64 is the gain of a half-wave dipole relative to an isotropic radiator. The factor of 100 in the numerator converts to the desired units of power density. This formula has been built into a proprietary program that calculates, at each location on an arbitrary rectangular grid, the total expected power density from any number of individual radiation sources. The program also allows for the description of uneven terrain in the vicinity, to obtain more accurate projections.





Mail Processing Center
Federal Aviation Administration
Southwest Regional Office
Obstruction Evaluation Group
2601 Meacham Boulevard
Fort Worth, TX 76193

Aeronautical Study No.
2014-AWP-7768-OE

Issued Date: 02/17/2015

Candice Koenig
GTE Mobilnet of California Limited Partnership
1120 Sanctuary Pkwy, #150 GASA5REG
Alpharetta, GA 30009

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Hwy 101 and 237
Location:	Sunnyvale, CA
Latitude:	37-23-57.35N NAD 83
Longitude:	122-02-08.67W
Heights:	41 feet site elevation (SE)
	54 feet above ground level (AGL)
	95 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

☐ At least 10 days prior to start of construction (7460-2, Part 1)
☒ Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

See attachment for additional condition(s) or information.

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed and maintained in accordance with FAA Advisory circular 70/7460-1 K Change 2.

Any height exceeding 54 feet above ground level (95 feet above mean sea level), will result in a substantial adverse effect and would warrant a Determination of Hazard to Air Navigation.

This determination expires on 08/17/2016 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.

- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates , heights, frequency(ies) and power . Any changes in coordinates , heights, and frequencies or use of greater power will void this determination. Any future construction or alteration , including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

Any failure or malfunction that lasts more than thirty (30) minutes and affects a top light or flashing obstruction light, regardless of its position, should be reported immediately to (877) 487-6867 so a Notice to Airmen (NOTAM) can be issued. As soon as the normal operation is restored, notify the same number.

A copy of this determination will be forwarded to the Federal Communications Commission (FCC) because the structure is subject to their licensing authority.

If we can be of further assistance, please contact our office at (310) 725-6557. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2014-AWP-7768-OE.

Signature Control No: 231662256-243500861

(DNE)

Karen McDonald
Specialist

Attachment(s)
Additional Information
Case Description
Frequency Data
Map(s)

cc: FCC

Additional information for ASN 2014-AWP-7768-OE

SPONSOR VOLUNTARILY LOWERED HEIGHT FROM 60 TO NEH 54 FEET AGL.

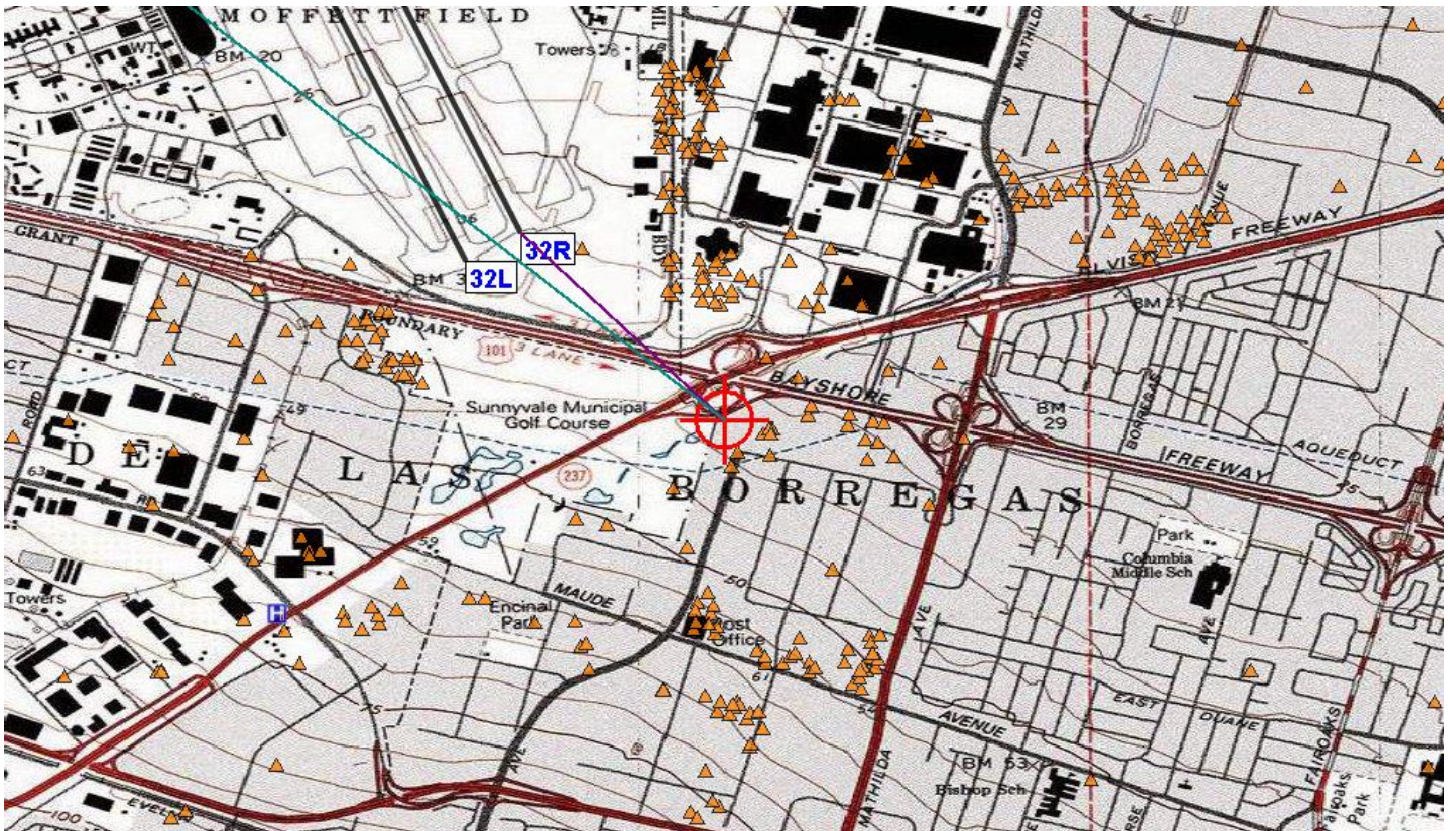
Case Description for ASN 2014-AWP-7768-OE

Proposed 60' stealth structure (eucalyptus tree).

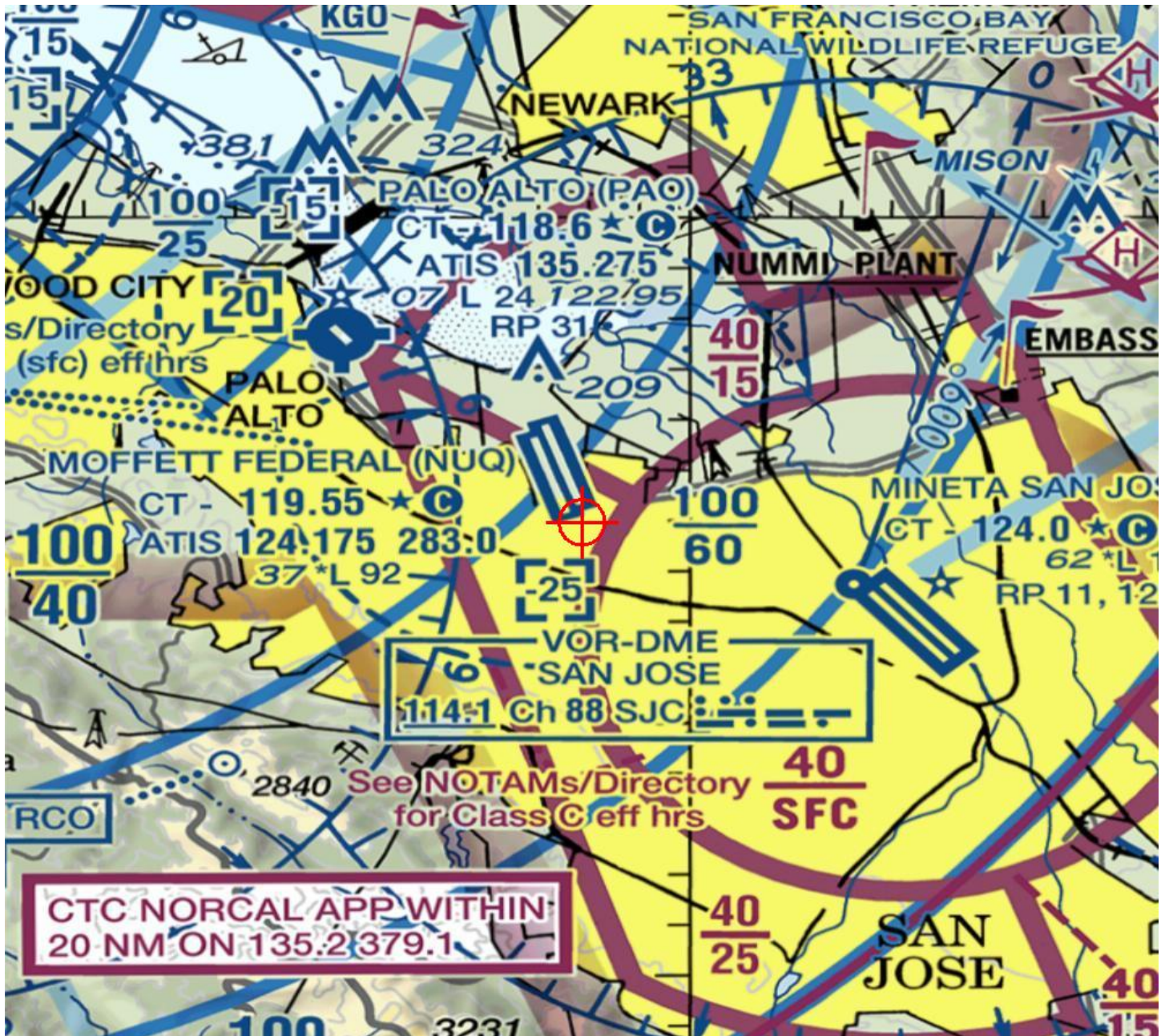
Frequency Data for ASN 2014-AWP-7768-OE

LOW FREQUENCY	HIGH FREQUENCY	FREQUENCY UNIT	ERP	ERP UNIT
698	806	MHz	1000	W
806	824	MHz	500	W
824	849	MHz	500	W
851	866	MHz	500	W
869	894	MHz	500	W
896	901	MHz	500	W
901	902	MHz	7	W
930	931	MHz	3500	W
931	932	MHz	3500	W
932	932.5	MHz	17	dBW
935	940	MHz	1000	W
940	941	MHz	3500	W
1850	1910	MHz	1640	W
1930	1990	MHz	1640	W
2305	2310	MHz	2000	W
2345	2360	MHz	2000	W

Verified Map for ASN 2014-AWP-7768-OE



Sectional Map for ASN 2014-AWP-7768-OE





City of Sunnyvale

Agenda Item

15-0167

Agenda Date: 4/13/2015

REPORT TO THE PLANNING COMMISSION

SUBJECT

Peery Park Specific Plan - Draft Project Description, Draft Vision, Guiding Principles, Goals and Policies and Conceptual Policy Framework Diagrams and Maps

Staff Contact: Amber El-Hajj, (408) 730-2723, ael-hajj@sunnyvale.ca.gov

REPORT IN BRIEF

The goal of the Peery Park Specific Plan (PPSP) is to physically reshape Peery Park over time through a program of public improvements and private investments in redevelopment to create a thriving workplace district that also provides benefits for the surrounding area.

The next step in the process is to obtain direction from the City Council on the project description and conceptual policy framework so that a draft Specific Plan and draft Environmental Impact Report (EIR) can be developed for further public review.

Staff is recommending that the City Council direct staff to move forward with the draft Specific Plan and draft EIR based on the draft project description (Attachment 2), draft vision, guiding principles, goals and policies (Attachment 3) and conceptual policy framework diagrams and maps (Attachment 4). Once Council gives staff direction, the draft Specific Plan and draft EIR will be developed. It is expected that these items would be available for public review in summer 2015 and would return to the Council in fall 2015.

The City Council is scheduled to consider this item on April 28, 2015.

BACKGROUND

Preparation of the PPSP has been a longstanding uncompleted study issue; the attached study issue paper (Attachment 1) discusses the original rationale for the specific plan. The study issue was highly ranked by the City Council in 2009, but was deferred in subsequent years when the budgeted funds were transferred to update the Land Use and Transportation Element (LUTE) and to prepare the Climate Action Plan (CAP).

With the economic downturn in 2008 and the lack of significant development activity in Peery Park, it was not urgent to begin work on the specific plan at that time. However, this situation has changed considerably in the past three years. Interest in higher density Class 'A' office buildings has intensified throughout the City and particularly for properties in Peery Park. The proposed (and recently approved) projects indicate a high interest in Class 'A' office space with a higher floor area ratio (FAR) than what is currently allowed with the base zoning in the industrial zoning district. At the same time, Peery Park is still an important location for research and development/industrial buildings that occupy the existing Class 'B' and 'C' buildings that were mostly built in the 1960s and 1970s.

The City Council approved the budget for the PPSP on October 16, 2012 and Freedman, Tung & Sasaki were chosen as the PPSP consultants in early 2013. Preliminary preparation of the PPSP began shortly after the selection of the consultant and included community outreach, traffic analysis and stakeholder discussions.

EXISTING POLICY

General Plan

Land Use and Transportation Element

Goal LT-4 Quality Neighborhoods and Districts - Preserve and enhance the quality character of Sunnyvale's industrial, commercial and residential neighborhoods by promoting land use patterns and related transportation opportunities that are supportive of the neighborhood concept.

Goal LT-6 Supportive Economic Development Environment - An economic development environment that is supportive of a wide variety of businesses and promotes a strong economy within existing environmental, social, fiscal and land use constraints.

ENVIRONMENTAL REVIEW

Preparation of the PPSP includes preparation of an Environmental Impact Report (EIR). It is anticipated that the Draft EIR (DEIR) and draft Specific Plan will be released for public review in summer 2015.

Obtaining feedback on the preliminary policy framework and project description from the Planning Commission and City Council does not require CEQA review. Rather, the preliminary recommendations and outcome from these public hearings will be further developed in the Specific Plan and analyzed in the EIR. Action on the Specific Plan would only occur after an EIR has been circulated and certified.

DISCUSSION

The purpose of the PPSP is to guide future development of the existing industrial business park, addressing the type, location, intensity, and design of industrial and commercial buildings, as well as transportation and infrastructure improvements. The Plan would include land use changes to allow for redevelopment of under-utilized or under-developed industrial properties to provide new commercial, industrial, and research and development space for technology-based business development, as well as supporting uses within defined activity centers, such as mixed commercial and residential uses along San Aleso on the east side of Mathilda. The Specific Plan would include development policies, land use regulations, design standards, a capital improvement program and a financing program concisely within a single document to define and guide development within the Project area over the next 10-20 years. The Specific Plan may also include amendments to the Sunnyvale General Plan and the Sunnyvale Municipal Code.

The goal of the PPSP is to physically reshape Peery Park over time through a program of public improvements and private investments in redevelopment to create a thriving workplace district. A mixture of complementary innovation companies will be encouraged that includes both knowledge and production uses. Additionally, a well-integrated district is envisioned that accommodates both larger scale campuses and sites for smaller companies. The regulations would allow redevelopment of Peery Park properties to replace or modify existing one- and two-story structures with four- to six-story Class 'A' office and technology-based industrial buildings. New

development would include functional open space to serve employee recreation demands and parking structures, including potentially subterranean structures.

Based on community feedback, stakeholder workshops and internal staff work, a vision, guiding principles, goals and conceptual policy framework have been created for the PPSP. It is important at this point in the process to confirm that the Council conceptually supports the draft policy framework and project description before staff and consultants expend budgeted funds to prepare the draft PPSP and draft EIR and prior to conducting further public outreach.

Overview of the PPSP Area

The Peery Park District is approximately 407 net acres and is made up of more than 180 properties, consisting of over 77 percent industrial uses, 12 percent commercial and less than 1 percent residential. The Project area has approximately 6.6 million sq. ft. of existing development with remaining buildout potential (under current zoning) of approximately 2.2 million sq. ft. (0.9 million sq. ft. is already under construction). The current predominate zoning for Peery Park is MS (Industrial and Service) with a maximum 35 percent floor area ratio (FAR). Combining Districts occur on select parcels in the Peery Park District and provide for potential increases in FAR (up to 100 percent on some parcels). FARs higher than that “maximum” may be approved through a Use Permit, or with the City Green Building density bonus. Currently, an average FAR of 34 percent exists throughout Peery Park’s industrial properties. The typical industrial lot is a one to two-story concrete tilt-up structure with surface parking and ornamental landscaping, accessed primarily from commercial/industrial collector streets and internal networks of driveways and drive aisles.

Project Development Capacity, Environmental Project Description and Project Alternatives

The planned development capacity is based first on a market analysis projecting demand for 645,000 sq. ft. of office, 553,000 sq. ft. of industrial and 137,000 sq. ft. of retail within Peery Park over 10 years. This is the project area’s capture (based on historic trends) of anticipated regional growth. Then, given various economic trends, the current increased level of development activity, and a new vision and regulations that could permit increase development capacity, the Plan anticipates that Peery Park will capture a larger share of that growth than projected in the market analysis. The planned development capacity for the Peery Park Specific Plan (that will be evaluated in the DEIR) was based on the following:

	Workplace (msf)	Housing Units	Workplace FAR	Housing du/ac
Existing (2013)	6.6	3	0.34	0
Under Construction	0.9	0	0.4 - 0.96	0
Net Growth	2.2	215	0.4 - 1.0	20-30
Planned Development Capacity	9.7	215 - 218	0.5	30

More development beyond the planned development capacity is theoretically possible under the project’s proposed land use and development regulations. However, the intent of the Plan was to

project a reasonable scenario within a reasonable time horizon for Specific Plans. This allows the project to identify impacts due to development, identify realistic mitigations for those impacts, and if necessary, set reasonable caps on development capacity based on the magnitude of the impacts and the timing of mitigations. It also anticipates monitoring Plan implementation over time and adjusting the development capacity or mitigation measures as necessary in response to actual impacts. Exceeding the development capacity would require an amendment to the Specific Plan and further environmental analysis. Ultimately, development capacity is a balance between developer/property owner interest in redevelopment and the City/community's concerns about traffic and other environmental impacts. Additional information on the development potential for Peery Park can be found in Attachment 5.

The environmental consultants for the PPSP, AMEC Environmental, have prepared a draft project description for the PPSP project (Attachment 2) which will be used in the DEIR. In addition to the project description, AMEC has also suggested project alternatives to the Peery Park Specific Plan Project (Attachment 6) for consideration by the Planning Commission and City Council. These project alternatives or variations thereof will be evaluated in the DEIR along with the project described in the project description. Environmental analysis and impacts of all of the alternatives will be clearly defined in the DEIR.

Visioning, Goals and Guiding Principles

The following vision statement, goals and guiding principles were created through the community workshops, stakeholder discussions and internal dialog between City Departments. Detailed information on each of the following guiding principles and further policies for each of the goals can be found in Attachment 3, staff will continue to develop these concepts further as the plan moves forward.

PPSP Vision Statement

A cutting edge workplace district that has been physically re-shaped to align with 21st century workplace trends and the innovation economy

PPSP Guiding Principles

1. Innovation
2. Connectivity
3. Transportation Demand Management (TDM)
4. Environmental Sustainability and Resilience
5. Public Spaces
6. Complementary Uses, Diverse Job Opportunities and Businesses
7. Economic Vitality
8. Respect Nearby Neighborhoods
9. Quality Design
10. Healthy Lifestyles
11. Community Benefits

Goals of the PPSP

1. Align both public and private interests with workplace and market trends.
2. Make Peery Park a center of knowledge and innovation.
3. Allow innovative businesses and workers to thrive.
4. Foster a dynamic mix of buildings and uses.

5. Provide settings that bring people together.
6. Provide new district amenities and uses.
7. Contribute to community sustainability.
8. Protect adjacent neighborhoods.
9. Place priority on TDM and alternative transportation.
10. Enable feasible development and provide clear direction for investors.

Key Implementation Concepts

1. Transportation Demand Management
2. Transportation Management Association
3. Parking Ratios
4. Community Benefits/Specific Plan Fees

Conceptual Policy Framework Maps and Diagrams

The following diagrams and maps can be found in Attachment 4 to illustrate the conceptual policy framework for the PPSP:

- Preliminary District Regulations Map
- Connectivity/Street Improvements Map
- Retail/Activity Uses Map
- Signature Space - Pastoria Avenue
- Streetscape Design Diagram
- District Streetscape and Public Space Concepts

Next Steps

Once the Council provides direction to staff on the draft project description and conceptual policy framework, work will begin on the draft Specific Plan and DEIR. It is expected that these documents will be available for public review in summer 2015 through additional community and stakeholder meetings. Planning Commission and City Council public hearings on the Peery Park Specific Plan and Final EIR would be held in fall of 2015.

It is important to note that all of the concepts discussed in the project description and framework will be evaluated in the DEIR. If significant impacts are found, the project may change to mitigate potential impacts. It is also important to note that Council direction on the policy framework and project description for the PPSP does not bind the Council to approve the Specific Plan.

FISCAL IMPACT

There is no fiscal impact from obtaining preliminary feedback on the policy direction of the PPSP. Detailed fiscal analysis and market information will be included with the draft Specific Plan when it is released for public review and comment.

PUBLIC CONTACT

Public contact on the PPSP consisted of:

1. Posting the Planning Commission agenda on the City's official-notice bulletin board outside City Hall and by making the agenda and report available at the Sunnyvale Public Library and on the City's website;
2. Publication in the *Sun* newspaper, at least 10 days prior to the hearing;

3. Mailed notices to property owner's, at least 10 days prior to the hearing;
4. E-mail notification of the hearing dates sent to all interested parties and Sunnyvale neighborhood associations;
5. Three community workshops held in October 2013, December 2014 and January 2015; and
6. Two stakeholder workshops held in January and February 2015.

Community Workshops

Three community workshops were held at the Washington Park Community Room to discuss the PPSP and present the project information. All three community workshops included a Q&A segment and a group exercise.

Community Workshop #1: October 16, 2013

The first community workshop included discussion of existing district issues and opportunities, economics and workplace trends. This workshop included an interactive exercise where community members commented on the preliminary framework maps and existing conditions diagram. Meeting notes from this workshop can be found in Attachment 7.

Community Workshop #2: December 3, 2014

The second community workshop included discussion of anticipated development types, public spaces and amenities, including an overview of the preliminary land use concepts. The workshop also included a group exercise to receive ideas and gain knowledge about the community's priorities, the concepts discussed in the meeting, traffic concerns and transportation demand management. Meeting notes from this workshop can be found in Attachment 8.

Community Workshop #3: January 21, 2015

The third community workshop included discussion and presentation of the preliminary traffic impacts, transportation demand management and the preliminary framework for the specific plan. This workshop also included two group exercises; one to help develop the guiding principles for the specific plan and the second to allow participants to comment on the conceptual policy framework diagrams and maps. Meeting notes from this workshop can be found in Attachment 9.

Summary of Community Workshops

The most prominent concerns raised at the community workshop by residents were traffic impacts and the compatibility of the industrial development with the adjacent and nearby residential neighborhoods. As shown in the conceptual framework, the plan proposes to lower maximum heights in the neighborhood transition, grand boulevard, and innovation edge sub-districts that are nearest to the single-family neighborhoods and Mathilda Avenue. Traffic impacts will be fully evaluated in the EIR; however, strong transportation demand management strategies have already been discussed with the community and Peery Park property owners.

Discussions with Peery Park Property Owners

City staff conducted interviews with major Peery Park property owners in late 2013 to understand the demand for development in the Peery Park area. The property owners confirmed that trends are a demand for higher FAR, need for a mix of industrial and office space and the desire for district-wide improvements. These trends were incorporated into the workshops and visioning for the PPSP. In January and February 2015, two workshops were held with property owners that focused on the comments from the earlier interviews. At the second workshop staff shared a handout of discussion points (Attachment 10) that highlighted the need for a strong TDM program and introduced the

concepts of a Transportation Management Association and a shuttle bus system. A letter (Attachment 11) has been submitted by Irvine Company, the major property owner in Peery Park that provides comments on the handout distributed at the stakeholder workshop.

Details of the Transportation Demand Management (TDM) program and other transportation/transit improvements will continue to be a focus of the draft plan and EIR and staff will continue to work with the stakeholders on this topic.

Study Session with the City Council and Planning Commission

On February 24, 2015 a joint study session with the Planning Commission and City Council was held to introduce the conceptual policy framework. The meeting summary for that study session can be found in Attachment 12. Many of the topics that the Council and Commission were most concerned about will be further vetted in the DEIR, draft plan and fiscal analysis; however, staff has already developed additional information on a few of the topics:

- Potential bike connection from the Peery Park area to the Moffett Park area - shown in Attachment 4;
- Additional buildout information - discussed above and in Attachment 5; and
- Pedestrian Connection from SNAIL neighborhood to San Aleso - shown in Attachment 4.

ALTERNATIVES

Recommend that Council:

1. Direct staff to prepare the draft Specific Plan and draft EIR based on the draft project description (Attachment 2) and conceptual policy framework (Attachments 3 and 4).
2. Direct staff to prepare the draft Specific Plan and draft EIR with specific modifications to the draft project description and conceptual policy framework.

STAFF RECOMMENDATION

Alternative 1: Recommend that Council direct staff to prepare the draft Specific Plan and draft EIR based on the draft project description (Attachment 2) and conceptual policy framework (Attachments 3 and 4).

Considerable time has been spent discussing the concepts and framework with the community, stakeholders and internally among City Departments. The concerns and issues that have come out of these meetings have been used to create the policy framework and preliminary ideas for the PPSP. Further environmental analysis will help refine the policies, goals, development standards, and planned development capacity for Peery Park, and will identify the significant impacts associated with the PPSP and feasible mitigation measures.

The high demand levels for new development in the Peery Park area emphasize the need for the completion of the Specific Plan for the area. The PPSP will help facilitate new development while also keeping the communities' desires and concerns in mind. While many details are pending further analysis and discussion, preliminary direction from the Planning Commission and City Council is important at this stage in the planning process prior to preparing a draft plan for public review.

Prepared by: Amber El-Hajj, Senior Planner

Reviewed by: Trudi Ryan, Planning Officer

Reviewed by: Hanson Hom, Director, Community Development

Reviewed by: Robert A. Walker, Assistant City Manager

Approved by: Deanna J. Santana, City Manager

ATTACHMENTS

1. Study Issue Paper
2. Draft Project Description
3. Draft Vision, Guiding Principles, Goals and Policies
4. Conceptual Policy Framework Diagrams and Maps
5. Specific Plan Buildout Information
6. Environmental Project Alternatives Memorandum from AMEC
7. Meeting Notes from the October 16, 2013 Community Workshop
8. Meeting Notes from the December 3, 2014 Community Workshop
9. Meeting Notes from the January 21, 2015 Community Workshop
10. Discussion Points Handout from the February 11, 2015 Property Owners Workshop
11. Public Comment from Irvine Company, dated March 3, 2015
12. Meeting Summary from the February 24, 2015 Joint Planning Commission/City Council Study Session
13. Link to the Peery Park Specific Plan Webpage: PeeryPark.InSunnyvale.com

Proposed New Council Study Issue

Number	CDD-11
Status	Pending
Calendar Year	2008
New or Previous	Previous
Title	Preparation of Peery Park Specific Plan
Lead Department	Community Development
Element or SubElement	Land Use and Transportation

1. What are the key elements of the issue? What precipitated it?

Peery Park is one of Sunnyvale's older industrial neighborhoods and is located in a prime location that is served by two major freeways as well as Central Expressway. In addition, it is located near the proposed new NASA Ames Research Center (which hopes to focus on the convergence of bio-technology, information technology and nano-technology) and the cluster of research and development facilities in the adjacent area of Mountain View. As a result of the advantages mentioned above, it could be beneficial to the City to focus reinvestment in the area which could result in increased revenues through increased property tax as well as sales and use taxes.

For the City of Sunnyvale to be competitive in the Silicon Valley economy, we must continue to develop new Class A office buildings. The reinvestment to Class A type office buildings is dependent on allowing a higher FAR. Class A office buildings are most likely to be built when allowable Floor Area Ratios (FAR) are at least 50%.

The Peery Park study would be in two phases. The first step would be to evaluate the infrastructure (transportation, water, sewer, etc.) and determine what types of improvements may be needed for various levels of development. This information would be presented to the Planning Commission and City Council. The Council could determine that further study is not needed, or direct staff to proceed with the second phase of preparation of appropriate environmental review and documentation and a Specific Plan. This study would look at the type of industrial development in Peery Park (primarily Class B and C) and examine the opportunity to recycle and upgrade the older buildings to Class A structures. Techniques such as higher FARs (such as in the Futures Industrial Sites) and a Development Reserve (as in the Moffett Park Specific Plan) would be evaluated. Other techniques that may encourage reinvestment in the Peery Park area will also be explored. Brokers and property owners who work in the Peery Park area have indicated that in-place zoning and streamlined project reviews are key items for facilitating reinvestment.

This item fell below the line and ranked 18 for 2007.

2. How does this relate to the General Plan or existing City Policy?

LAND USE AND TRANSPORTATION ELEMENT

GOAL N1: Preserve and enhance the quality character of Sunnyvale's industrial, commercial and residential neighborhoods by promoting land use patterns and related transportation opportunities that are supportive of neighborhood concept.

Policy N1.2.1: Integrate new development and redevelopment into existing neighborhoods.

Policy N1.2.3: Develop specific area plans to guide change in neighborhoods that need special attention.

Policy N1.8: Cluster high intensity industrial uses in areas with easy access to transportation corridors.

Action Statement N1.8.1: Require high quality site, landscaping, and building design for higher intensity industrial development.

3. Origin of issue

Council Member(s) Spitaleri, Swegles
General Plan
City Staff
Public
Board or Commission none

4. Multiple Year Project? Yes Planned Completion Year

5. Expected participation involved in the study issue process?

Does Council need to approve a work plan? No
Does this issue require review by a Board/Commission? Yes
If so, which?
Planning Commission
Is a Council Study Session anticipated? Yes
What is the public participation process?
Outreach to property owners, businesses and developers as well as notification of public meetings and public hearings.

6. Cost of Study

Operating Budget Program covering costs
242 Land Use Planning; 247 Economic Prosperity
Project Budget covering costs
Budget modification \$ amount needed for study
\$300,000
Explain below what the additional funding will be used for
Staff estimates approximately \$80,000 for a traffic consultant to run several land use scenarios and assist in capacity analysis, and approximately \$220,000 for other infrastructure analysis and preparation of an EIR.

7. Potential fiscal impact to implement recommendations in the Study approved by Council

Capital expenditure range None
Operating expenditure range None
New revenues/savings range \$51K - \$100K
Explain impact briefly
The City has not imposed a fee for recovery of costs for EIR and Specific Plan preparation in

prior Specific Plan efforts, however, these costs can be passed along to the property owners through project review fees during implementation of a Specific Plan.

8. Staff Recommendation

Staff Recommendation None

If 'For Study' or 'Against Study', explain

9. Estimated consultant hours for completion of the study issue

Managers

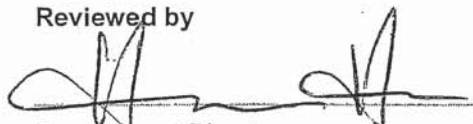
Role	Manager	Hours			
Lead	Ryan, Trudi	Mgr CY1:	25	Mgr CY2:	0
		Staff CY1:	260	Staff CY2:	0
Support	Mc Queen, Brice	Mgr CY1:	30	Mgr CY2:	0
		Staff CY1:	0	Staff CY2:	0
Interdep	Berry, Kathryn	Mgr CY1:	20	Mgr CY2:	0
		Staff CY1:	0	Staff CY2:	0
Interdep	Rogge, Mark	Mgr CY1:	20	Mgr CY2:	0
		Staff CY1:	0	Staff CY2:	0
Interdep	Witthaus, Jack	Mgr CY1:	20	Mgr CY2:	0
		Staff CY1:	0	Staff CY2:	0

Total Hours CY1: 375

Total Hours CY2: 0

Note: If staff's recommendation is 'For Study' or 'Against Study', the Director should note the relative importance of this Study to other major projects that the Department is currently working on or that are soon to begin, and the impact on existing services/priorities.


Reviewed by


Department Director

Date

11/7/07

Approved by


City Manager

Date

11-14-07

Addendum

A. Board / Commission Recommendation

☐ Issue Created Too Late for B/C Ranking

Board or Commission	Rank	Rank 1 year ago	Rank 2 years ago
Arts Commission			
Bicycle and Pedestrian Advisory Committee			
Board of Building Code Appeals			
Board of Library Trustees			
Child Care Advisory Board			
Heritage Preservation Commission			
Housing and Human Services Commission			
Parks and Recreation Commission			
Personnel Board			
Planning Commission			

1 of 7

Board or Commission ranking comments

This study was identified after the Planning Commission considered potential study issues for ranking.

B. Council

Council Rank (no rank yet)
 Work Plan Review Date (blank)
 Study Session Date (blank)
 RTC Date (blank)
 Actual Complete Date (blank)
 Staff Contact

**PEERY PARK SPECIFIC PLAN
DRAFT PROJECT DESCRIPTION FOR
DRAFT ENVIRONMENTAL IMPACT REPORT**

PROJECT DESCRIPTION

The purpose of the Project is to guide future development of the existing industrial business park, addressing the type, location, intensity, and design of industrial and commercial buildings, as well as transportation and infrastructure improvements for the 407 net acre area identified as Peery Park. The Project would include land use changes to allow for redevelopment of under-utilized or under-developed industrial properties to provide new commercial space for technology-based business development, as well as supporting uses within defined activity centers, such as mixed commercial and residential uses along San Aleso on the east side of Mathilda. The Project would include development policies, land use regulations, design standards, a capital improvement program, and a financing program concisely within a single document to define and guide development within the Project area over 10 to 20 years. The project will also include amendments to the Sunnyvale General Plan and the Sunnyvale Municipal Code.

The Project area has approximately 6.6 million square feet (sf) of existing development with 0.9 million sf under construction and a remaining buildout potential, under current zoning, of approximately 1.2 million sf. Recent interest in higher density Class 'A' office and tech-based industrial buildings has intensified throughout the City and particularly for properties in Peery Park. This interest is focused primarily on existing Class 'C' buildings as attractive locations for redevelopment of aging structures that do not meet the needs of current and future Silicon Valley businesses.

The current predominate zoning for Peery Park is MS (Industrial and Service) with a maximum 35% floor area ratio (FAR)¹. FARs greater than 35% are permitted in conjunction with an approved Use Permit or through a green building incentive (additional 10% FAR). Combining Districts occur on select parcels in the Mathilda Avenue corridor (north of Maude Avenue) that provide for potential increases in FARs, as much as 100% on select parcels. Currently, an average of 34% FAR is realized throughout Peery Park's industrial properties, where the typical industrial lot is a one to two-story concrete tilt-up structure with surface parking and ornamental landscaping, accessed primarily from commercial/industrial collector streets and internal driveways and drive aisles.

The goal of the Project is to physically reshape Peery Park over time through a program of public improvements and private investments in redevelopment to create a thriving workplace district. The Project would allow redevelopment of Peery Park properties to replace or modify existing one- and two-story structures with maximum four- to six-story (75 to 115 feet including roof screens, mechanical equipment, etc.) Class 'A' office and

¹ Floor area ratio = (total covered area on all floors of all buildings on a certain plot, Gross Floor Area) / (area of the plot of land upon which it is built)

technology-based industrial buildings. New development would include functional open space to meet employee recreational needs. On-site surface and structured parking would be provided for projects, including potentially subterranean structures and shared parking facilities. The Project would also involve modifications to zoning standards, including modifications to building height limits (both increases and decreases from the existing maximum of 75 feet) and to the allowable FAR up to 100% (which permits heights up to 100 feet, including roof screens and equipment).

Table 1: Comparison of Existing and Potential Standards for Peery Park Specific Plan

	<i>Existing General Plan/ Zoning Regulations</i>	<i>Proposed Peery Park Land Use Regulations</i>
FAR	35% to 100%	N/A
Maximum Building Height	75 to 100 feet 8 floors	75 to 115 feet 4 to 6 floors
General Land Uses	One to two-story industrial and manufacturing structures	Four to six-story office and technology-based light industrial structures

The proposed Project's primary objective is to accommodate new Class 'A' development served by activity centers strategically located within walking and biking distance of new development. Based on the quality, age, and utilization of existing development within Peery Park, the Project specifies the following land use strategies to encourage a range of desired development types:

- **Activity Center/Core:** Development of core locations for employee commercial services and amenities, including retail, restaurant, public open space and gathering locations, and entertainment uses. Development of the Activity Center/Core areas would consist of up to 6-story tech-based commercial buildings with ground floor retail shops to serve the needs of Peery Park employees and adjacent neighborhoods. The Activity Centers would be located strategically to provide these services within walking and biking distance to all key development locations within Peery Park. Specifically, the Activity Centers may include:
 - *Primary Activity Center - Mary Avenue at Central Expressway:* This activity center encompasses approximately 23 acres centered on a common open space area to serve the eastern reaches of the Project area, including the Innovation Edges areas along Mary Avenue, and would connect Encinal Park with centrally located Production Core and Mixed Workplace Transition areas between Mary and Mathilda Avenues, south of Maude Avenue.
 - *Potential Activity Center - Almanor Avenue at N. Pastoria Avenue:* This activity center encompasses approximately 25 acres centered on a common open space area to serve the northern reaches of the Project area, including the Production Core area along Pastoria Avenue from Del Rey Avenue to Almanor Avenue, and would connect the Innovation Edge development along Mary Avenue and Mathilda Avenue.

- *Local Activity Centers*: The Project proposes small, local activity centers as locations for small-scale, employee serving retail, restaurant, or other commercial services. The local activity centers would provide daytime services and would not likely cater to evening hours activities (e.g., bars).
- **Innovation Edges**: Development of Class A office and tech-based manufacturing buildings designed to support and attract high profile firms. Building heights would be allowed up to six stories, but would be limited to four stories within 300 feet of Mathilda Avenue. Innovation Edge development would be designed to “step down” to adjacent residential neighborhoods and include deep landscaped setbacks to provide a buffer along the residential edges of the Project area and transition the employment center to neighborhood uses. Interconnected campuses would support onsite recreational amenities and services for employees as part of facilities for office and research and development (R&D) space. Innovation Edge land uses would be encouraged along Mathilda Avenue, Maude Avenue, and Mary Avenue, as well as for a designated transit-oriented workplace adjacent to the Sunnyvale Station at Mathilda Avenue and Evelyn Avenue. District Edges would comprise approximately 200 acres of the Project area.
- **Production Core**: Development of smaller-scale production and R&D space to support a mixture of work activity. These land uses would be lower intensity with limited building heights of four stories, lower density structures with smaller footprints, shallower setbacks to encourage active street frontage, an interconnected by a system of pathways and green spaces, and shared parking strategies. The Production Core areas would center on Pastoria Avenue and connect to employee amenities associated with the Innovation Edges and the Activity Center/Core areas. Production Core areas would comprise approximately 95 acres of the Project area.
- **Mixed Workplace Transition**: Development of smaller-scale workplace space to support a mixture of work activity and uses, including offices and R&D space. These land uses would be lower intensity with limited building heights of four stories, lower density structures with smaller footprints, and more permissive ground floor use and design requirements. The Mixed Workplace Transition areas would apply along Vaqueros Avenue, Benicia Avenue, and Potrero Avenue and connect to employee amenities associated with the Innovation Edges and the Production Core areas. Mixed Workplace Transition areas would comprise approximately 70 acres of the Project area.
- **Grand Boulevard**: At the northerly portion of Mathilda Avenue, development of office, large-scale commercial, hotel, and limited retail uses would be allowed on the west side of Mathilda Avenue to serve as a gateway to Peery Park and the City of Sunnyvale. Deep landscape setbacks are also proposed to improve the visual character at this gateway. On the east side of Mathilda Avenue, commercial development would also be allowed, but building heights would be limited to three stories. Grand Boulevard areas would comprise approximately 25 acres of the Project area.

- **Neighborhood Transition:** Development of mixed use residential uses (attached/stacked residential and office) with a maximum of three stories (two stories within 75 feet of the single-family neighborhood behind San Aleso). The Neighborhood Transition area would buffer the existing neighborhoods east of the Project area from the Grand Boulevard and Innovation Edge development along Mathilda Avenue. The Neighborhood Transition area would comprise approximately 12 acres of the Project area.

Given Peery Park's favorable location for high-tech and other emerging companies in Silicon Valley, proximate access to two freeways and Central Expressway, and underutilized access to regional transit, including VTA light rail/bus service and Caltrain rail service, the City of Sunnyvale intends the Specific Plan to accommodate projected growth in a manner that fosters development of a cutting edge workplace district and provides high wage jobs. The Project would program redevelopment of older structures with more Class 'A' offices within the District Edges while also addressing the continued need for Class 'B' and 'C' space within the Mixed R&D Core areas. Peery Park's planned development capacity over the life of the plan (estimated at 20 years) are derived based on presumed maximized building footprints FARs and heights allowed under the proposed Project for opportunity sites within the Project area (Table 2).

Table 2: Proposed Net Increase in Building Space/Residential Units in Peery Park²

Commercial	2,200,000sf
Retail	200,000 sf
Office/R&D/Industrial	2,000,000 sf
Residential	215 units

The Project would include improvements to transit accessibility and interconnectivity to support the non-vehicle commutes, including potential expansion of shuttle services within Peery Park to connect employees with regional rail transit provided by VTA and Caltrain. The Project proposes improvements to Mathilda Avenue to support increase transit and multi-modal access to the Caltrain Sunnyvale Station located 0.2 miles from the southern edge of the Project area on Evelyn Avenue. The Project also identifies options for providing multi-modal access to the Moffett Park Station located 0.5 miles from the northern edge of the Project area on West Moffett Park Drive. Additionally, existing local roadways would be improved where needed to serve multi-modal needs of employees and visitors, including completing sidewalk and pathway connections, bike lane striping, and streetscape and signage improvements. Where block lengths are long, new connector streets or public bicycle/pedestrian pathways are proposed to divide the block and improve internal circulation. The Project would include intersection improvements where needed to improve multi-modal access, including intersections along Mathilda Avenue. The Project proposes 35,216 linear feet of streetscape

² Preliminary land use calculations are preliminary estimates and subject to change through the draft EIR and proposed Project.

improvements along existing roadways including the following range of improvement types:

- **High Amenity Pedestrian:** Pastoria Avenue would provide a 22-foot wide landscaped pedestrian paseo in addition to vehicle facilities and parallel parking. This streetscape improvement would provide a pedestrian-friendly connection between the northern and southern areas of the Project area along a central multi-modal access.
- **Major Arterial:** Maude Avenue, Mary Avenue, and Mathilda Avenue would provide a multi-modal roadway with bicycle lanes and/or cycle tracks, landscaped medians, and complete sidewalks. Mathilda would be improved with either an express bus lane or a combined express bus lane and bicycle lane.
- **Local Streets:** Two-lane roads serving local mobility within the Project area would provide complete sidewalks, bicycle connections to the Major Arterials, and streetscaping.

The project will propose both public and private open space throughout the district and on individual properties. Minimum required project space will be based on the gross square feet of the individual project and will range between 20 and 35% of the total project site. Public open space will be desired and may be provided near cafes/restaurants, exercise facilities or provided as plazas or courtyards.

Draft Vision, Guiding Principles, Goals, Policies and Key Implementation Concepts for the Peery Park Specific Plan

Vision Statement:

A cutting edge workplace district that has been physically re-shaped to align with 21st century workplace trends and the innovation economy.

Guiding Principles:

1. Innovation: New development and capital improvements will provide the district with the mix of uses, building types, and public spaces that businesses and workers need to be innovative and successful in the 21st Century economy.
2. Connectivity: New and improved vehicular, pedestrian, and bicycle connections into, out of, and within Peery Park will improve the experience of district businesses/employees, nearby residents, and reduce traffic impacts. New development will not be isolated and cut-off from the surrounding district or adjacent neighborhoods.
3. Transportation Demand Management (TDM): The transportation impacts of new development will be limited by focusing on pedestrians, bicyclists, transit, and alternative transportation improvements. Emphasis will be on improvements that reduce vehicle trips instead of street network changes that simply accommodate more cars. A Transportation Management Association will be one of the tools used to manage district-wide transportation demand.
4. Environmental sustainability and resilience: Increased open space, improved landscaping, and green architecture will improve water quality, improve air quality, and reduce energy within the district and contribute to City-wide sustainability goals.
5. Public spaces: Creating spaces for people to meet, interact, recreate, and relax will support innovation in the district and provide amenities for residents of nearby neighborhoods.
6. Complementary uses, diverse job opportunities and businesses: A broad range of mutually supportive land uses will be encouraged to create a strong workplace ecosystem and provide diverse jobs.
7. Economic viability: District transformation will build first off existing strengths by retaining existing firms and targeting existing industry clusters. A mix of building types for businesses of different sizes and flexible workspaces will allow the district to adapt to a changing economy by accommodating a range of uses and tenants over time.
8. Respect nearby neighborhoods: Residents will have convenient retail services within walking distance. Height limits, land use transitions, and landscape buffers will preserve neighborhood character.
9. Quality design: New development will contribute to an improved district image with architecture and landscaping that reflects the Sunnyvale community's standards for quality development.

10. Healthy lifestyles: The district will include a mix of uses, a variety of public spaces, and a bike/pedestrian network connecting it all that will encourage and enable healthy lifestyles.
11. Community Benefits: New development within Peery Park will provide benefits serving the whole community. Projects will include public improvements such as parks and public space, pedestrian and bicycle improvements, shared parking, and impact fees or assessments to fund items such as transportation improvements and affordable housing.

Goals & Policies:

1. Align both public and private interests with workplace and market trends.
 - a. Balance demand for new development with neighborhood preservation.
 - b. Permit market feasible development types which meet the needs of current and future Silicon Valley businesses.
 - c. Plan streetscape/transportation improvements, restrict development, and require landscaping in ways that will buffer neighborhoods from potential traffic, noise, visual, and safety impacts caused by Peery Park development and uses.
 - d. Make the plan consistent with county density & height guidelines to maintain safety in airport protection areas.
2. Make Peery Park a center of knowledge and innovation.
 - a. Physically transform the district to create the type of environment that attracts innovative businesses and employees.
 - b. Plan a network of signature streets and public spaces that establish the district's identity and mark it as a premier Silicon valley workplace destination.
 - c. Avoid isolated developments that are cut-off from the surrounding district. Instead, require public space, streetscape improvements, and workplace-oriented retail to give the district as a whole a campus-like feel.
 - d. Work with property owners, developers, and institutions to attract and integrate innovation anchors in to the district (such as start-up schools, incubators/accelerators, co-working spaces, college/university branches, business development services, etc.).
 - e. Consider creating an online idea sharing portal or directory that promotes the district and creates a platform to facilitate connections, idea sharing, collaboration, networking, and discussion between businesses and workers.
3. Allow innovative businesses and workers to thrive.
 - a. Prioritize actions, development, and district improvements that encourage collaboration, interaction, and activity.
 - b. Create public spaces and encourage designs that bring people together.
 - c. Encourage and accommodate innovative, 21st century infrastructure including fiber-optic cable and district wifi.
4. Foster a dynamic mix of buildings and uses.
 - a. Ensure a healthy business ecosystem by

- i. Accommodating large established firms, small firms, and start-ups through a wide range of building types and work space sizes and classes. Use a variety of regulatory/policy tools to ensure that space for small scale tenants are maintained throughout the district.
 - ii. Permit a range of land uses that align with innovation economy and market trends. Use a variety of regulatory/policy tools to maintain a variety of research, design, engineering, and manufacturing activities within the district.
 - b. Regulate development capacity district-wide in order to allow development flexibility on individual properties while limiting environmental impacts.
 - c. Adopt economic development policies to proactively attract a variety of uses.
5. Provide settings that bring people together.
- a. Require on and off-site open space in a variety of configurations that accommodate a variety of activities.
 - b. Plan a connected network of pedestrian/bike-friendly streets and paths to connect workers/businesses with district activity and public spaces.
 - c. Require architecture and building disposition that makes work visible and brings activity to sidewalks and public spaces.
6. Provide new district amenities and uses.
- a. Plan and strategically locate one or two publicly accessible activity centers in addition to small clusters of lunch time activity evenly distributed throughout the district.
 - b. Encourage personal and business services within activity centers to serve district employees and nearby residents.
 - c. Plan a network of public open spaces that can accommodate recreation, physical activity, and encourage healthy lifestyles.
 - d. Plan seating, shelters, kiosks, turn-outs, bulb-outs, and other amenities/improvements to support transit use.
 - e. Improve pedestrian comfort with improved street lighting, sidewalks, street trees, and other landscaping.
7. Contribute to community sustainability.
- a. Establish a variety of standards and guidelines to ensure the district is part of an environmentally sustainable Sunnyvale. Include regulations addressing:
 - i. Green buildings
 - ii. Pedestrian, bike, and vehicular connectivity to improve air quality though decreased VMT and reduced congestion.
 - iii. Climate Action Plan strategies to decrease energy use, water consumption, solid waste and greenhouse gas emissions.
 - iv. Stormwater best management practices to improve water quality
 - v. Green Streets and preservation of existing mature trees to improve air quality, provide species habitat, minimize urban heat island effect, reduce stormwater run-off, and improve the pedestrian environment.

8. Protect adjacent neighborhoods.
 - a. Limit the height of development where it is visible from adjacent residential neighborhoods, especially along Mathilda Ave.
 - b. Manage transportation to reduce traffic impacts.
 - c. Manage parking to limit spillover in to adjacent neighborhoods and encourage alternative modes of transportation.
 - d. Buffer existing residential neighborhoods from new development with increased landscaping.
9. Place priority on TDM and alternative transportation.
 - a. Work with VTA to identify and implement changes or additions to bus routes in order to better serve the district and increase headways.
 - b. Maintain, improve, and increase connections throughout the district to distribute traffic and create more direct and more bike/pedestrian-friendly routes.
 - i. Add or improve Bike lanes/paths and make connections with the existing bike network.
 - ii. Add sidewalks where they do not exist, increase landscaping and pedestrian amenities throughout the district, and create new routes to create a more connected pedestrian network.
 - c. Encourage the provision of convenient services within the district to reduce vehicular trips into/out of the district throughout the day and especially mid-day trips.
 - d. Require each development application to include TDM plan with clear trip reduction and management goals.
 - e. Require a transportation management association to coordinate TDM programs, monitor and report on traffic performance, and guide placemaking improvements.
 - f. Outline a plan to implement a Private/Public district shuttle including early phase pilot program, feasibility study, and potential funding/implementation strategies.
 - g. Seek grants to assist with financing and implementing TDM programs and tools.
10. Enable feasible development and provide clear direction for investors.
 - a. Prepare regulations which permit market feasible development types.
 - b. Present a clear entitlement process that ensures straight-forward approval of projects which meet all of the Plan's requirements and are consistent with the Plan's vision.
 - c. Prepare simple to administer development regulations that simplify application review and make requirements and built outcomes clear to developers and the community.
 - d. Establish a clear set of fees and required infrastructure/placemaking improvements to ensure an implementable vision, support intensification, and minimizing impacts without limiting feasible development.

Key Implementation Concepts:**Transportation Demand Management (TDM)**

- The City may require TDM target goals at a sliding scale of 20-35% for each project based on project gross square footage and changes in occupancy. Preliminarily this scale may look similar to the following table:

Project (gross sq. ft.)	TDM Trip Reduction Goal*
Over 750,000	35%
300,001 to 750,000	30%
100,001 to 300,000	25%
Up to 100,000 and change in occupancy that intensifies prior use	20%

*Trip reduction goal based on Institute of Transportation Engineers (ITE) AM/PM peak hour rates for each development.

- A reduced TDM goal could be considered for a project if district-wide transportation improvements are proposed that would increase mobility (e.g. local street, bicycle, or pedestrian connections), or substantial onsite or offsite facilities/amenities are proposed that could reduce local vehicle trips for employees and visitors.
- Annual vehicle driveway counts will be performed by the City that will be paid for by the property owners/tenants. Penalties will be assessed annually if TDM goals are not met based on the percentage or number of trips exceeding the required TDM goal for each project.
- For phased projects, the TDM goal would increase as the cumulative amount of constructed building square footage increases.

Transportation Management Association (TMA)

- Property owners will be required to participate in a TMA that will have certain responsibilities and is privately funded.
 - Responsibilities of TMA are flexible, will be defined by governing board and can be adjusted over time, but may include:
 - Transportation Coordinator/district informational website;
 - Transportation program with employer/employee incentives;
 - Carshare, rideshare, carpooling and bikeshare programs;
 - Transit passes;
 - Coordination on TDM monitoring and reporting;
 - Feasibility study and shuttle bus coordination or operation;
 - Installation and maintenance of pedestrian, bicycle, transit, recreation and sense of place amenities (bike shop?);
 - Regular reporting of updates to the City; and Work with the City to obtain TDM grants and with VTA to implement bus transit improvements.
- TMA or property owners shall prepare a feasibility study to determine the feasibility of operating a privately funded pilot shuttle bus program to serve the

district and possibly a larger area. Individual property owners/tenants can include a shuttle bus in their TDM program and opt out of the feasibility study.

- The timing for creating a TMA is still under discussion with Peery Park stakeholders. The City will consider offering a one-time matching grant of up to \$100,000 to help form a TMA or implement a shuttle bus program following adoption of the PPSP.

Parking

- The City proposes to set maximums for overall parking and for surface parking in a sliding scale that may look like the following:

Project (gross sq. ft.)	Parking Maximums* (spaces/1,000 sq. ft.)
Over 750,000	3.0
300,001 to 750,000	3.2
100,001 to 300,000	3.4
Up to 100,000 and change in occupancy that intensifies prior use	3.6

*Additional parking allowed up to 3.6/1,000 with structured parking or project enhancements; count structured parking spaces as a fractional amount (e.g. 0.75 per space) for complying with the parking maximums.

- Allow additional parking greater than the listed maximum may be allowed with incentives or benefits to the community and district, such as additional open space, shared parking, unbundled parking or more aggressive TDM measures.
- Allow reduced or no parking for small retail uses and cafes.
- Allow reduced parking for mixed use, shared parking or unbundled parking.

Open Space

- On-site open space requirements will vary based on the size of the proposed project and can include paved plazas and courtyards:

Project (gross sq. ft.)	Minimum Open Space (percentage of total project site)
Over 750,000	40%
501,000 to 750,000	35%
300,001 to 500,000	30%
100,001 to 300,000	25%
Up to 100,000	20%

- A reduction in minimum open space can be considered when:
 - Other on-site or district amenities are provided, such as cafes/restaurants or exercise facilities that are open to the public; or
 - On-site open space, plazas or courtyards are accessible to the general public to enjoy.

- Exceptions to development standards or zoning regulations can be considered with provision of increased open space.

Community Benefits:**Potential Infrastructure Concepts/Fees**

- Each property will be responsible for frontage improvements such as sidewalks, street trees, utilities, etc.
- The following fees or assessments would be determined following further analysis to define projects, estimated cost and fair share formula.
 - Specific Plan Fee – prepare and manage Peery Park Specific Plan, with credit for pre-paid contribution
 - Sense of Place Fee – fund bicycle, pedestrian and area-wide amenities
 - Supplemental Transportation Impact Fee (TIF) - fund district-related transportation improvements to mitigate traffic impacts
 - Infrastructure Improvement Fee – fee or assessment for improvements to accommodate planned development capacity
 - Park Dedication In Lieu fee – acquire/improve parkland and other recreational facilities, with credit for on-site usable open space, plazas and courtyards that are designed for employee or public use
 - Other community benefit fees or incentives that may be proposed with future study.
- In addition, projects will be subject to paying housing linkage fees for affordable housing based on net increase in floor area.

Preliminary District Regulations Map



Activity Center/Core

ATTACHMENT 4

- 6 Floors - Potentially Taller Conditional
- Ground Floor Retail Shopfronts
- Minimum Intensity

Innovation Edge

- 6 Floors (4 fl w/in 300 ft of Mathilda)
- Grand Entrances
- Deeper Landscaped Setbacks

Production Core

- 4 Floors max
- Fine Grained Ground Floor Space
- Shallower Setbacks to Activate Sidewalks & Make Work Visible
- Limited/Shared Parking

Mixed Workplace Transition

- 4 Floors max
- Less Strict Ground Floor Requirements

Grand Boulevard

- 3 Floors max
- Deeper Landscaped Setbacks
- Office, Large Scale Commercial, Hotel, Limited "Retail"

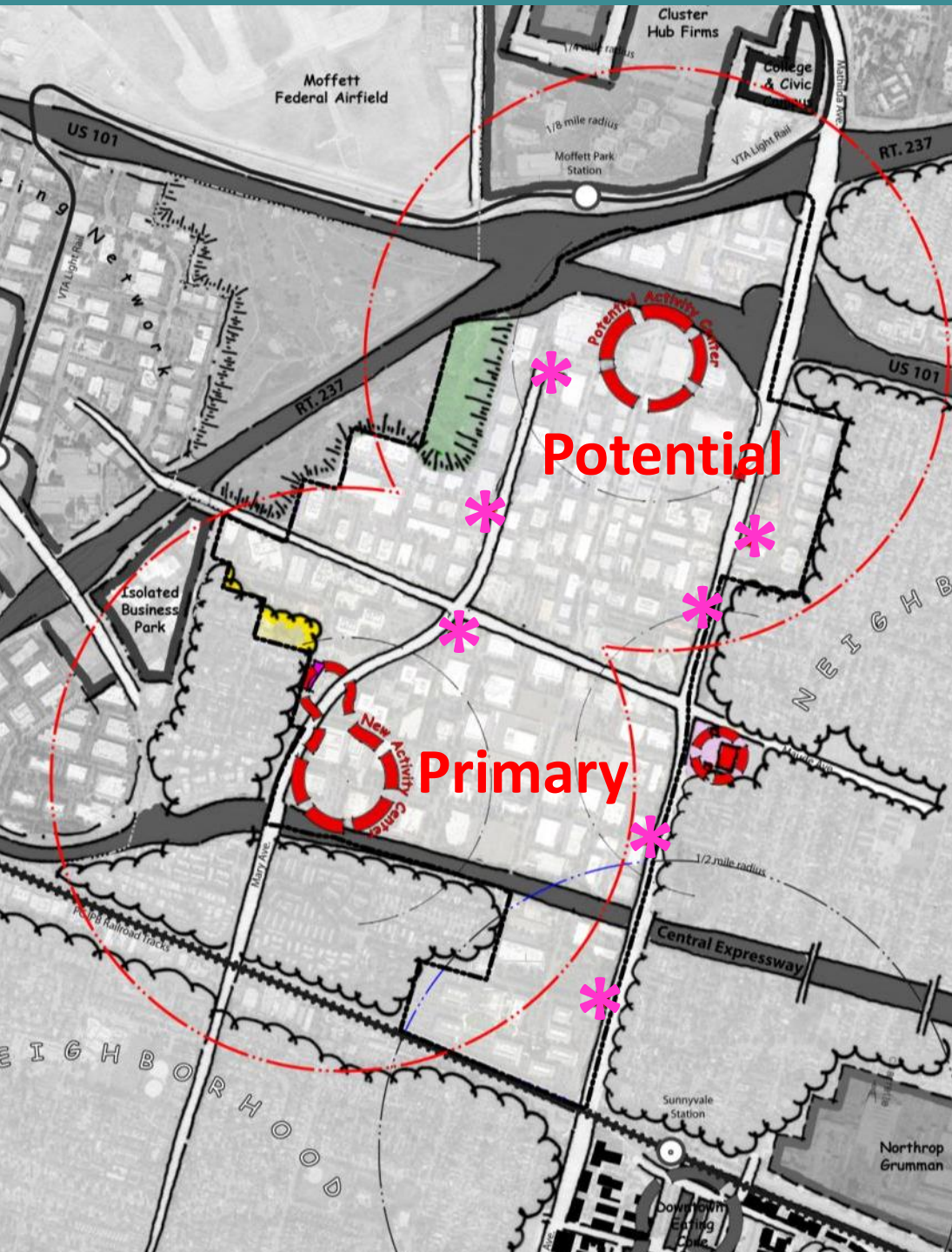
Neighborhood transition

- 3 Floor Attached/Stacked Residential, Office
- Height/Setbacks/Buffering Adjacent to Homes

Potential Street Improvements



- Landscaped Medians
- Street Lighting
- New & Improved Sidewalks
- New & Improved Bike Lanes
- New Street & Pedestrian Connections



Retail/ Activity Uses

Quick Breaks

Immediately outside
or within building

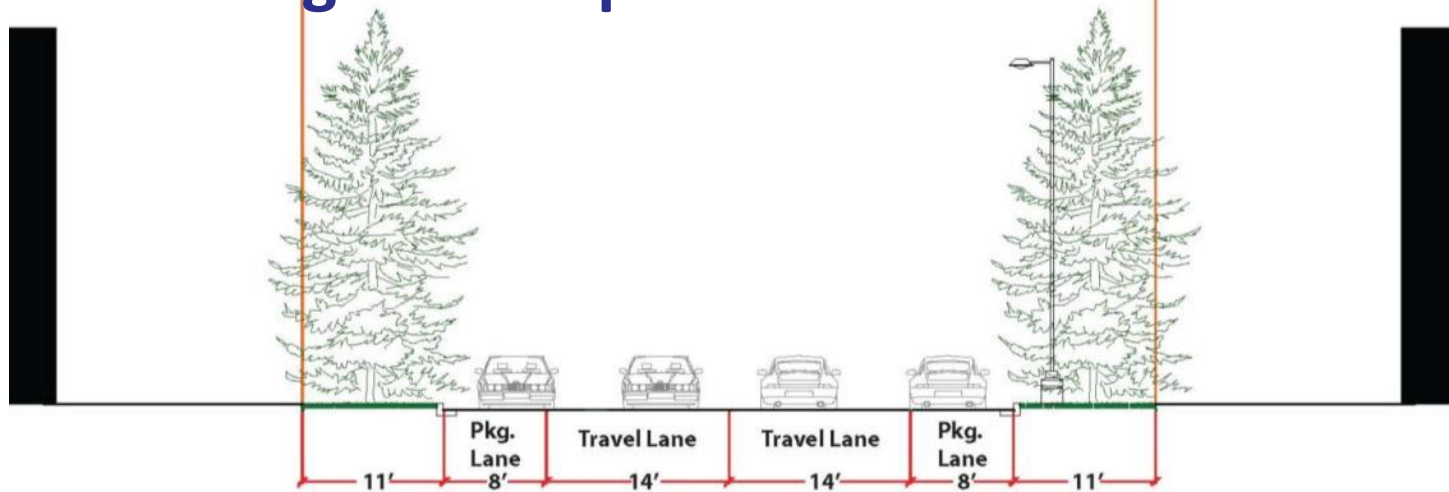
* Lunchtime Activity

Evenly distributed
(within a 3 min. walk)

After Work Activity

Centrally located
(within 10-15 min.
walk, bike, drive, or
transit)

Signature Space - Pastoria Ave.



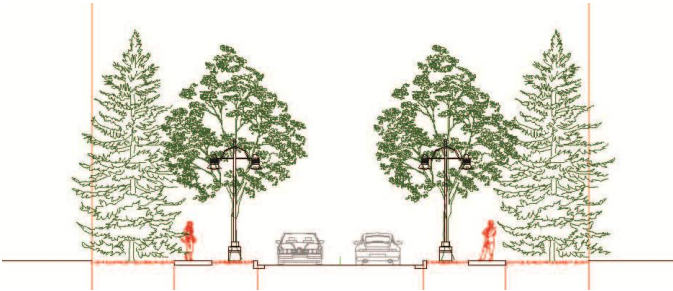
Existing Cross-Section



Proposed Design

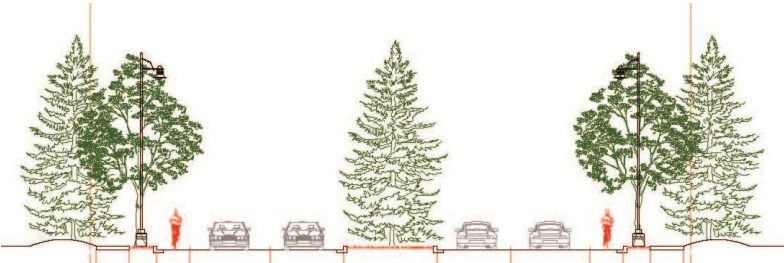
Local Streets:

Convert Parking Lane
to Sidewalks & Bike Lanes



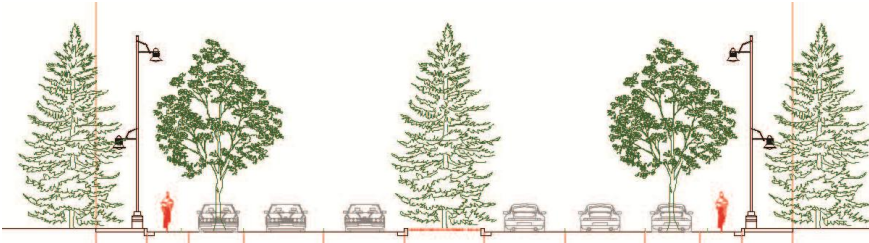
Maude Ave:

Convert Center Turn Lane
to Landscaped Median



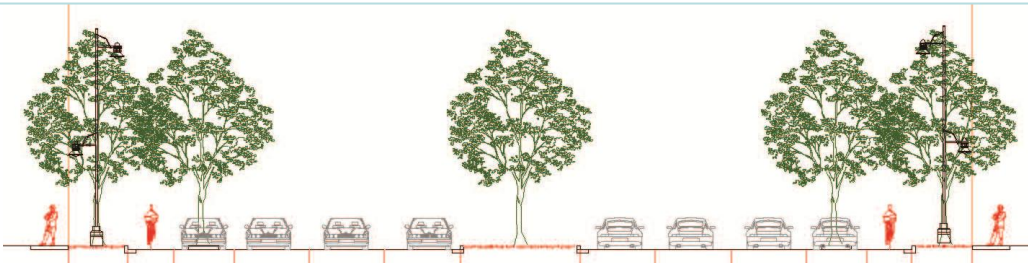
Mary Ave:

Convert Center Turn Lane
to Landscaped
Median & Create
Protected Bike Lane

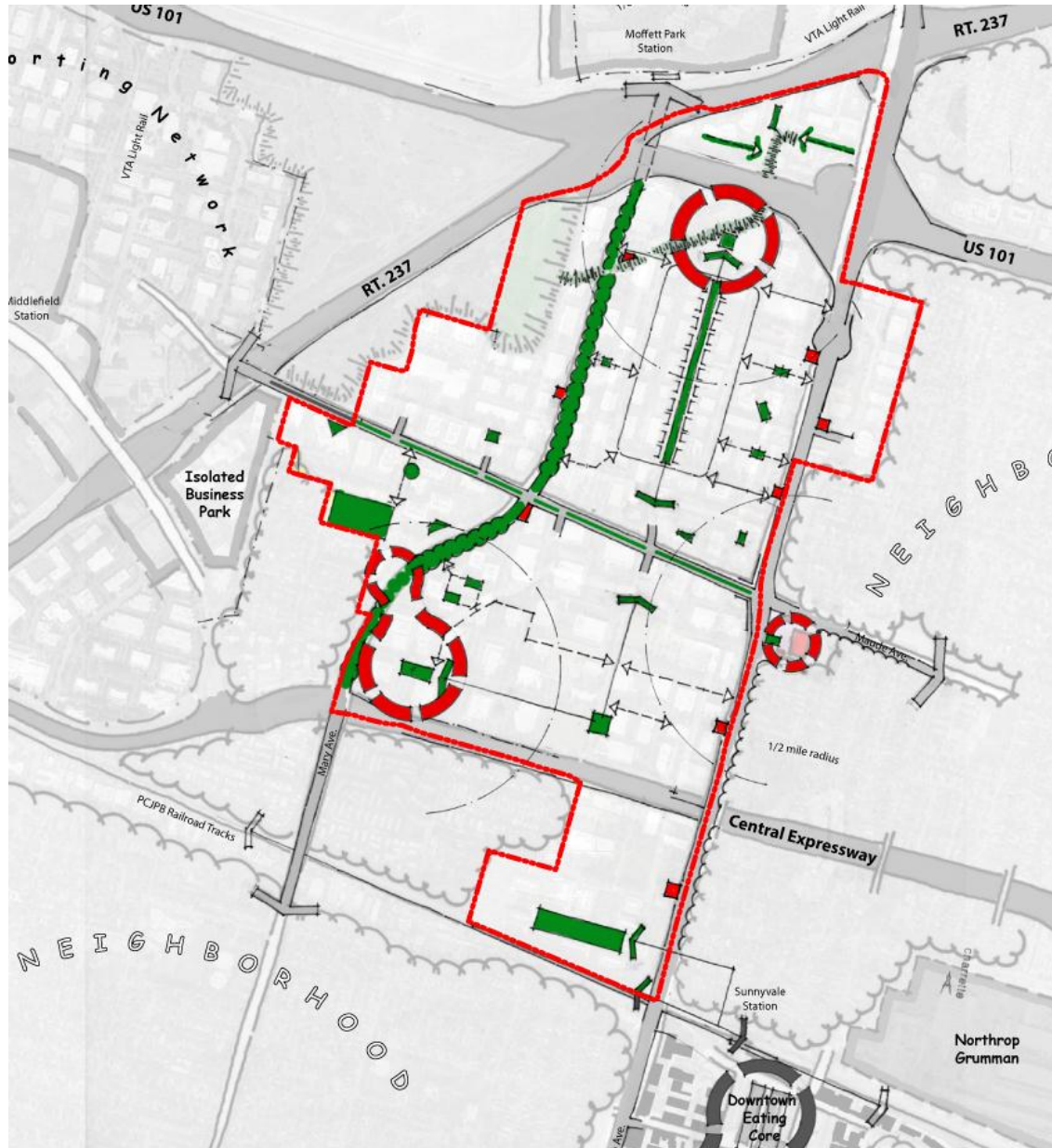


Mathilda Ave:

Add Protected
Bike Lane



District Streetscape & Public Space Concept



Interaction & Activity



District Identity



Lunch & Short Breaks

Peery Park Specific Plan Buildout Memo from Freedman, Tung & Sasaki

March 16, 2015 - Peery Park Specific Plan & EIR: Buildout Development Process

A strong economy, combined with Peery Park's excellent location within Silicon Valley, has resulted in increased development activity in the Project Area. Multiple applications have recently been submitted or approved requesting increased development capacity. Prior to kicking off the Peery Park Specific Plan project, the City had to review these applications on a case-by-case basis. The City and the community have been concerned about traffic, density, and height impacts of significant district redevelopment and intensification but it has been difficult to get a comprehensive understanding of the implications of wide-spread change in Peery Park when reviewing the projects in this way.

In response to this situation, the City initiated a Specific Plan process in order to prepare a coordinated and comprehensive vision, regulations, and capital improvements plan for the future of Peery Park. As part of this process, a likely buildout was prepared to be used as the basis for an analysis of traffic, infrastructure, fiscal, and environmental impacts as well as to inform the Plan's financing/implementation strategy.

The buildout is based first on a market analysis projecting demand for 645,000 sq. ft. of office, 553,000 sq. ft. of industrial and 137,000 sq. ft. of retail within Peery Park over 10 years. This is the project area's capture (based on historic trends) of anticipated regional growth. Then, given various economic trends, the current increased level of development activity, and a new vision and regulations that could permit increase development capacity, the Plan anticipates that Peery Park will capture a larger share of that growth than projected in the market analysis.

To estimate how much actual development may occur over the timeframe of the Plan, the project analyzed perceived opportunity sites, patterns of ownership, and actual development activity/interest. Then feasible development types were projected on potential opportunity sites (these were identified in the market analysis as comparable to ongoing development throughout Sunnyvale and the surrounding region). Projects that were under construction in the Project Area at the time the buildout was prepared were considered existing development. Projects in the pipeline at the time the buildout was prepared were included in the projection of new development. The cumulative square footage of existing buildings, under construction projects, pipeline projects, and projected development is the basis of the buildout.

More development beyond the projected buildout is theoretically possible under the project's proposed land use and development regulations. However, the intent of the

Plan was to project a reasonable scenario within a reasonable time horizon for Specific Plans. This allows the project to identify impacts due to development, identify realistic mitigations for those impacts, and if necessary, set reasonable caps on development capacity based on the magnitude of the impacts and the timing of mitigations. It also anticipates monitoring Plan implementation over time and adjusting the development cap or mitigation measures as necessary in response to actual impacts. Ultimately the buildout is a balance between actual developer/property owner interest in large scale re-development on one hand and the City/community's concerns about traffic and other environmental impacts on the other hand.



MEMORANDUM

DATE: March 17, 2015

TO: Erik Calloway

FROM: Dan Gira, Erika Leachman

SUBJECT: Potential Alternatives to Peery park Specific Plan for EIR

This memorandum identifies initial potential alternatives to be analyzed in the Peery Park Specific Plan (Project) Environmental Impact Report (EIR).

Purpose of Project Alternatives

The state Guidelines for implementation of the California Environmental Quality Act (CEQA) require that EIRs identify evaluate a reasonable range of alternatives that would avoid or reduce the significant environmental impacts of a proposed project, while still attaining most of the basic project objectives.

Alternatives to the proposed Project are identified, screened, and recommended to either be retained for further analysis or eliminated as described below. The Alternatives screening process consisted of the following steps:

Step 1: Define the alternatives to allow comparative evaluation.

Step 2: Evaluate each alternative in the context of the following criteria:

- The extent to which the alternative would accomplish most of the basic goals and objectives of the Project;
- The potential feasibility of the alternative, taking into account site suitability, economic viability, availability of infrastructure, General Plan consistency, and consistency with other applicable plans and regulatory limitations;
- The extent to which the alternative would avoid or lessen one or more of the identified significant environmental effects of the Project; and
- The requirement of the state CEQA Guidelines to consider a “no project” alternative and to identify, under specific criteria, an “environmentally superior” alternative. For example, pursuant to State CEQA Guidelines section 15126.6, subdivision (e), “if the environmentally superior alternative is the ‘no project’ alternative, the EIR shall also identify an environmentally superior alternative among the other alternatives.”

Step 3: Determine the suitability of the proposed alternatives for full analysis in the EIR based on Steps 1 and 2 above. Alternatives considered to be unsuitable, were eliminated, with appropriate justification, from further consideration.

MEMORANDUM

Preliminary Discussion of Potential Project Alternatives for the Peery Park Specific Plan EIR

Potential Project Alternatives

Project Alternatives	Key Features	Key Outcomes
No Project (Status Quo)	<ul style="list-style-type: none">• No change to existing Industrial land use and zoning (M-S and C-1)• Continued piecemeal development of individual properties• No new Development Standards• No uniform public improvement plan	<ul style="list-style-type: none">• Incrementally fewer impacts (e.g., transportation and utilities)• No community benefits or coordinated District improvements - streetscape, activity centers, transportation, etc.• Reduced tech based employment opportunities.
Proposed Project	<ul style="list-style-type: none">• Moderate intensification of Industrial and Commercial land use• Changes to zoning development standards to allow targeted mixed use activity centers and revitalized business subdistricts	<ul style="list-style-type: none">• Provision of community benefits and coordinated District improvements - streetscape, activity centers, transportation, etc.• Increased tech-based employment opportunities
Reduced Project	<ul style="list-style-type: none">• Reduced intensification of Industrial and Commercial land use• More restrictive zoning development standards to reduce the Project's proposed intensity of development within activity centers and business subdistricts	<ul style="list-style-type: none">• Incrementally fewer impacts (e.g., transportation and utilities)• Reduced community benefits or District coordinated improvements - streetscape, activity centers, transportation, etc.• Reduced tech based employment opportunities
Intensified Tech-based Buildout	<ul style="list-style-type: none">• Increased intensification of Industrial and Commercial land use.• More permissive zoning development standards to increase the Project's proposed intensity of development within activity centers and business subdistricts.	<ul style="list-style-type: none">• Potentially increases in impacts - transportation and utilities• Increased community benefits or District improvements - streetscape, activity centers, transportation, etc.• Substantial increase in tech-based employment

MEMORANDUM

Preliminary Discussion of Potential Project Alternatives for the Peery Park Specific Plan EIR

		opportunities
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Descriptions of Potential Alternatives

- 1) **No Project Alternative** – Under the required No Project Alternative, the Project would not be adopted and piecemeal development and redevelopment would occur in accordance with land use designations and provisions of the 2011 General Plan, existing Zoning Ordinance for M-S and C-1 zone districts, and the City's Industrial Design Guidelines. Over the long-term, the No Project Alternative would substantially reduce overall development of the Project area when compared to the Project. Specifically, the amount of Class A office space and mixed use commercial that could be developed under the draft Project would be substantially reduced. Instead, this alternative would favor development of more R&D type uses under the current Industrial–Service (M-S) zoning with associated reductions in potential future employment. This alternative would incrementally reduce some potential impacts of the proposed Specific Plan, such as traffic congestion, utilities, and jobs-housing balance. However, this alternative would also not provide the community benefits of the Project, including streetscape improvements, activity centers, and employee amenities.
- 2) **Reduced Project Alternative** – The goal of this alternative would be to reduce potential future development to reduce environmental impacts, such as traffic congestion and air quality. Under this alternative, the Project would include use of development standards to limit the height and or Floor to Area ratio (FAR) of potential development across the Project area. For example, existing industrial areas may be limited to 2-3 stories with moderate FARs to promote R&D uses and limit overall area-wide development potential. Activity centers and those areas targeted for high-tech mixed use Class A office space may also be restricted using height limits and FAR. Buildings in such areas may be reduced to 3-5 stories when compared to the proposed Project. This Alternative would limit development, while retaining some of the areas of the proposed Project designated to support and attract high profile firms, as well as the proposed new activity centers. This alternative would incrementally reduce potential impacts, but would not as effectively attract high-profile firms and increase employment within the Project area. The incremental reduction in impacts would also be associated with the loss of employment opportunities and community benefits associated with the proposed Project.
- 3) **Intensified Tech Based Buildout Alternative** – The goal of this alternative would be to intensify and concentrate development within the proposed activity centers and edges of the Project Area to increase the employment and economic viability of Peery Park beyond the proposed Project. Under this alternative, the Project would include use of development standards to increase the height and or FAR of potential development across the Project area. For example, activity centers and those areas targeted for high-tech

MEMORANDUM

Preliminary Discussion of Potential Project Alternatives for the Peery Park Specific Plan EIR

mixed use Class A office space may involve increased height limits and FAR to increase the overall development capacity of the Project area for tech-based development. Buildings in such areas may be increased to 5-8 stories when compared to the proposed Project. This alternative would focus development on areas of the proposed Project designated to support and attract high profile firms, as well as the proposed new activity centers. This alternative may incrementally increase some potential impacts, but would more effectively attract high-profile firms and increase employment within the Project area. The incremental increase in impacts would also be associated with the increase of employment opportunities and community benefits associated with the proposed Project.

Alternatives Considered and Discarded.

- 1) Increased Housing and Tech-Based Development Alternative** – This alternative would include both increased housing and greater amounts of new mixed use office/ tech-based development to balance housing and employment opportunities. Balancing job opportunities and housing within the Project area would likely reduce overall traffic impacts while increasing the amount of allowable tech-based development and would minimize potential decreases in future employment associated with constructing housing rather than industrial uses. This alternative was discarded as it is inconsistent with the existing General Plan framework for the area and initial City Council direction regarding the goals and objectives for Peery Park.
- 2) Moffett Federal Airfield CLUP Consistency Alternative** –This alternative would ensure that the Project is completely consistent with the guidelines for Moffett Federal Airfield Comprehensive Land Use Plan (CLUP), thereby avoiding or reducing potential impacts to land use and aircraft hazards. This alternative would be the same as the proposed Project, except that maximum allowable building heights would be reduced and land uses would be restricted in all areas subject to CLUP restrictions. Because the employment density within the CLUP Safety Zones would be less, this would further reduce the number of employees subject to aircraft safety hazards as well as reducing traffic generation and other impacts. This alternative was discarded because it did not meet the Project objectives or initial City Council direction regarding the goals and objectives for Peery Park.

Peery Park Specific Plan Community Workshop #1

City of Sunnyvale

October 16, 2013

Washington Park Community Room
840 W. Washington Avenue

Meeting Notes

City Councilmembers Present:

Tara Martin-Milius

City Staff Present:

Hanson Hom, Director of Community Development

Kent Steffens, Director of Public Works

Connie Verceles, Economic Development Manager

Amber El-Hajj, Senior Planner/Project Planner for the Peery Park Specific Plan

Carla Ochoa, Traffic Engineer

Terilyn Anderson, Project Specialist/Notetaker

Consultant Team Present:

Erik Calloway, FTS (Freeman Tung + Saski)

Tim Cornwell, The Concord Group

Open House

The meeting began with an informal open house where photos and timelines addressing the development of industry and technology in Sunnyvale could be viewed in large poster format throughout the room.

Introduction

Hanson Hom, Community Development Director, introduced Councilmember Martin-Milius, the consultant team and staff. The purpose of this first workshop is to bring together residents, property owners, businesses and other members of the public to provide input to help shape a vision for future development in Peery Park. This vision will be incorporated into a Specific Plan which is like a “mini” General Plan, in that it addresses the district comprehensively, rather than on a project by project basis.

An online survey will be posted on the Peery Park website (*PeeryPark.inSunnyvale.com*) in the near future to solicit feedback and suggestions, and periodic project e-mail updates will be sent to interested parties, including those that listed their email address on the meeting's signup sheet. Mr. Hom emphasized the importance of receiving community input and said that staff is available to attend neighborhood, business and other community meetings. A second community workshop will occur in the near future which will look at traffic and streetscape improvements.

Presentation

Erik Calloway, FTS, gave a PowerPoint presentation addressing existing district issues, opportunities, economics and workplace trends. A key point of his presentation was that Peery Park was largely built out between 1960 and 1990 to serve the industrial model that existed before the advent of the internet and smart phones. With digitization,

business functions are now highly decentralized and require less labor to make and transport products. The new “innovation” model involves individuals with specialized expertise synergistically collaborating to develop new business ideas and opportunities. This calls for working spaces that provide ample opportunity for social mixing both in and out of the office, including during leisure activities, at restaurants and in people friendly open spaces. Mr. Calloway noted that the presentation will be posted online at *PeeryPark.inSunnyvale.com*.

Tim Cornwell, The Concord Group, gave a PowerPoint presentation examining market demand and development feasibility at Peery Park.

Next, Mr. Calloway facilitated a Q and A discussion.

Q and A Summary:

Several community members asked questions and offered comments. Erik Calloway and Hanson Hom responded to the questions.

Q. What kinds of jobs are anticipated for retail and lunch spots?

A. The more jobs in the district the more need for restaurants, health/exercise opportunities and other retail shops.

Q. How long is FTS’s contract?

A. It is anticipated that the specific plan will be adopted in the fall of 2014.

Q. What happens to districts similar to Peery Park that do not get planning guidance?

A. An extreme worst case is Detroit. Without a common vision defined in a specific plan, and regulations to implement the vision, nothing happens. People will not want to work there, and in cases where there is market demand the community may not accept it.

Q. It was recommended to integrate retail shops and restaurants into residential neighborhoods, with easy walking distances and access to transit.

At what point does this big picture planning process hand-off to reality, when buildings are actually built?

A. The Specific Plan comes down to zoning and policies. Policies are written to guide development, such as what characteristics a building must have.

Q. How many people are interested in developing Peery Park?

A. There is quite a bit of interest. There are several projects in the pipeline and a lot of support for developing the district.

Q. Increased retail is positive, but it brings a lot of traffic, so we may want to build transit on Mathilda.

A. Currently there is one bus that runs along Mathilda. Transit availability increases when there are more people to serve. Transit agencies want to see plans before they can justify expanding service. Increased retail is more oriented to serving the district.

Q. How does the City establish its policies?

A. Zoning criteria addresses the amount of open space, which is a ratio related to the size of development, or it can be district-wide, where everyone in the district contributes to common open space.

Q. What does the brown versus purple area represent on the display map?

A. It demonstrates the innovative model where more “thinking” firms are needed, and shows more office type facilities along Maude, Mary and Mathilda. These

offices in the “brown” areas tend to be located in smaller buildings. The next need is to figure out how the buildings should look. Industrial buildings are cheaper to rent than offices devoted to “thinking” activities. The Specific Plan should provide direction to investors.

Q. With the desire to attract knowledge workers, can we discourage taller buildings (like LinkedIn) which are close too close to the street and don’t have sufficient green space to separate them from the road?

A. There are lots of ways to do this, such as building setbacks, design, location of parking areas, etc.

Q. If the City wanted to build a hospital, would it need to develop regulations to do this?

A. The specific plan does not address any City development. During the planning phase for development of the Onizuka Air Force Station site, staff contacted hospitals and medical facilities to see if they were interested in that site. They are very strategic about where they locate and have a regional, rather than city-specific orientation to their market and service areas.

Q. Is the preparation of the Specific Plan driven by the City or by developers?

A. The plan is strictly City-driven.

Comment Exercise

Meeting attendees were invited to place sticky notes with their ideas and suggestions for development of Peery Park on large district maps posted on the wall. The two maps showed existing conditions and conceptual future conditions in the Peery Park District. A PDF of the maps can be found on the project website (PeeryPark.inSunnyvale.com) for reference. The comments received in the sticky note exercise are shown in the tables below and grouped into like categories. Participants were told to add a check mark to the sticky note if they agreed with the statement (number of check marks are shown in the tables along with the color of sticky note (red or green)).

Sticky Note Color	# of Checks (next to statement)	Comment on Sticky Note
General Land Use/Zoning		
Green	5	(Posted in the area between Mathilda and Pastoria pertaining to the map colors) The buildings in the “purple” area (adjacent to Mathilda) should be lower than the buildings in the inner “grey-green” area for aesthetics.
Green	4	This area (posted in the brown/grey area off Mathilda (north end)) should be good for larger and taller buildings.
Green	2	Streetscape/setback layers are important along Mathilda.
Green	2	Live up to Goal N1 on 2007 proposed new Council Study Issue; to enhance character of residential neighborhoods.
Green	1	Lower zoning heights along Mathilda corridor.
Green	0	Height along Mathilda/flight path restrictions in interior of PPSP area.
Green	0	Limit height along Mathilda and include more retail.
Green	0	All parking should be unbundled, not specific # of parking spaces per site.
Green	2	(Posted on Mathilda/Central) This would be a good location for a hospital.
Green	0	Lower zoning on Mathilda corridor.
Green	1	(Posted on the east side of Mathilda within the project area) Transition area is not wide enough. Should go out to Mathilda.

Green	0	Pleasanton type zoning with retail near residential.
Red	3	LinkedIn looks like a “great wall”. It is a mistake. Let’s not have more great walls along Mathilda, set the tall buildings way back.
Red	3	Keep buildings facing residential areas low or very set back.
Red	2	No more tall buildings visible from SNAIL neighborhood.
Red	3	Add trees along Mathilda (as was done on Mary Ave.).
Red	1	The exterior glass on the new LinkedIn building is blinding in the morning. There needs to be tall trees between new buildings and streets.
Red	1	(Posted at Mathilda & Maude) This would be a good location for Sunnyvale’s second public high school.
Red	0	Need to avoid a corridor that discourages interaction between neighborhoods and industrial zones.
Walkability/Bikeability of Peery Park		
Green	5	Wider sidewalks so the nearby residents can walk the area of Mathilda and Maude.
Green	3	Put sidewalks and bike lanes on all streets.
Green	2	Pedestrian bridges over main roads and freeways.
Green	2	Bike paths should not share with cars. Bike paths should be next to sidewalks instead.
Green	1	Easy access to the area for nearby residents so they can also benefit from the improvements of Peery Park.
Green	0	Make the area more pedestrian friendly.
Green	0	More green access between Moffett and Peery business parks.
Green	1	Green belt connection across Mathilda.
Alternative Transportation/VTA Light Rail/Caltrain		
Green	5	Encourage non-car transit.
Green	4	Bring light rail down Mary or get it over the 101 before moving ahead.
Green	1	Walk/Bicycle access from VTA light rail (to the north) to the PPSP area.
Red	1	In regards to VTA station north of 237: Need light rail extension down Mary to Central Expressway.
Red	1	Posted near downtown Caltrain: Disappointed development isn’t focused here to connect Sunnyvale Caltrain Station.
Red	0	Posted near downtown Caltrain: Need really good connections to Caltrain for people and bikes.
Red	2	Whole area should have 0 net increase in cars, Must have TMA and shuttles to Caltrain.
Retail/Activity Centers		
Green	0	More usable retail along Mathilda to encourage use by residential so that it can be used at night/weekends also.
Green	3	Locating retail near major thoroughfares will allow access from local tenants as well as the public. Retail “buried” or surrounded by commercial will die after lunch hours.
Green	1	Encouragement of residential serving retail along residential edges.
Green	0	Upper activity center: Retail should be moved to downtown and/or closer to residential.
Green	1	Posted on north activity center: Second retail area should go on Maude, Almanor is too isolated.
Green	0	Residents and employees from Mtn. View may want to use retail in Peery Park.
Green	3	Prefer mom and pop retail outlets rather than chains.

Red	2	Activity Center should go on Maude or in the middle, not Almanor.
Red	2	Restaurants need to be close to each other, not spread out as shown by red.
Red	0	More retail is good; however, if main customers are the workers in PPSP they'll be closed during the weekend and no use for nearby residents.
Red	2	Posted on the north activity center: This site is too noisy and has too much pollution to be a good activity center/public space.
Sustainability/Green Building		
Green	5	Solar panels over all parking lots and on the roofs of buildings.
Green	4	Encourage green sustainable "futuristic" buildings and outdoor public areas.
Green	3	Solar panels on all roofs mandatory.
Green	2	All buildings should be zero net energy.
Green	1	Solar panels over all parking.
Green	0	All sites need to capture and use 50% of average rain fall on-site.
Green	0	All buildings should have dual plumbing and whole area should have access to reclaimed water.
Parks/Open Space/Food Trucks		
Green	2	Need nice parks and a place for food trucks.
Green	1	Food truck area near a park area with trees, seats and tables.
Green	0	A park in Peery Park where people can go to hang out.
Green	0	Open areas should be included into designs (business lunch areas/recreation for employees).
Roads/Streets/Traffic		
Green	0	All future intersections should be roundabouts. They are safer and quicker than intersections.
Green	0	Please leave room for Mary Ave. to remain a boulevard (a la Mathilda) as demand on Mary grows. Please don't let Mary become another Lawrence (no trees).
Green	0	Posted on Rt. 237: Consider road and intersection improvements.
Green	0	Improvements to traffic flow along rt. 237. Ex: Where Ross St. crosses Mathilda, a right turn lane separating the orchard gardens from commute traffic would significantly improve both.
Red	1	Stop cars from cutting through neighborhoods to avoid traffic.
Red	4	Shouldn't Mary Ave. connect to 101 here (posted on the north end of Mary)? What about the Mary Ave. overpass?
Red	3	Current traffic in the morning going northbound on Mary is often bumper to bumper between Washington and Maude. This will only worsen. How will safety be ensured? Bicyclists and pedestrians are already at risk.
Red	0	Fix traffic onto Maude.
Residential		
Red	1	Sunnyvale is short thousands of homes; some of them should be in Peery Park (near retail).
Red	0	Without new housing development will occur along Mathilda and where freeway exits are located and along transportation.
Other Comments/Questions		
Red	1	What happens when Moffett Field is sold and height restrictions are removed?
Red	0	What safeguards will be put into the plan to keep the manufacturing from becoming a superfund site?
Red	0	Please don't be dismissive of public input. Our input might not fall in line with what you would like to permit but is still valid.

Red	4	Change the planning process, the current process belongs in the 20 th century (300 ft. noticing, no neighborhood outreach).
Red	0	The purple and brown color code (on posters) is confusing. Elaborate and be clearer next time.

Next Steps

The next Community Workshop will be posted on the project website, notices will be mailed and e-mails will be sent out to everyone on the interest list. The next community meeting will address traffic and streetscape improvements and how the comments and ideas will formulate the Specific Plan. If you wish to be added to the project interest e-mail list send an e-mail to the project planner, Amber El-Hajj, at ael-hajj@sunnyvale.ca.gov.

Website

PeeryPark.inSunnyvale.com

Peery Park Specific Plan Community Workshop #2
City of Sunnyvale
December 3, 2014

Washington Park Community Room
840 W. Washington Avenue

Meeting Notes

City Councilmembers Present:

Mayor Jim Griffith
Glenn Hendricks
Gustav Larsson
Tara Martin-Milius
David Whittum

City Staff Present:

Deanna Santana, City Manager
Kent Steffens, Assistant City Manager
Hanson Hom, Director of Community Development
Manuel Pineda, Director of Public Works
Trudi Ryan, Planning Officer
Connie Verceles, Economic Development Manager
Amber El-Hajj, Senior Planner/Project Planner for the Peery Park Specific Plan
Rosemarie Zulueta, Associate Planner
Carla Ochoa, Traffic Engineer
Carol Shariat, Principal Transportation Engineer/Planner
Terilyn Anderson, Project Specialist/Notetaker

Consultant Present:

Erik Calloway, FTS (Freeman Tung + Sasaki)

Planning Commissioners Present

Larry Klein

Bicycle and Pedestrian Advisory Commissioners Present

Richard Kolber

Introduction

Mayor Griffith opened the community meeting by thanking everyone for coming. The purpose of the meeting is to solicit as much public input as possible before the Peery Park Specific Plan is developed. He noted that the content of the plan will be similar to what was included in the Moffett Park Specific Plan.

The City envisions Peery Park as a vibrant workplace that meets the needs of modern workplace functions. Community feedback is needed on the major features of the plan, such as how the District's buildings and architecture will be designed to integrate with

surrounding neighborhoods and how traffic will be managed. Mayor Griffith then introduced Hanson Hom, Director of Community Development.

Mr. Hom thanked everyone for attending and said that this was the second community planning workshop for Peery Park. The first workshop, held on October 16, 2013, addressed existing conditions at the District, workplace trends, market analysis, and a broad brush strategic framework.

The purpose of this workshop is to gain public input on an envisioned future for Peery Park, a preliminary policy regulatory framework and priorities for the District. During the first half of the meeting a presentation will be given on the District's history and how workplace trends have changed in the digital age. This background information will be helpful for the second half of the meeting when everyone will break into groups to identify the highest and lowest priorities for the District and discuss what would improve the character of the adjacent neighborhood and what strategies would be most effective at relieving traffic. Each group will report their findings to the larger group after they complete the exercise.

The third community workshop is scheduled for January 21, 2015 and will address mobility analysis, streetscape improvements, traffic concerns and transportation demand management.

Mr. Hom then reported the results of an on-line survey that was conducted in Fall 2013 which invited public input on the existing conditions, future conditions and community concerns for Peery Park. He added that the results of the on-line survey showed similar results as the comment exercise conducted at the first community workshop on October 16, 2013:

- 89% of respondents were residents.
- What works?
 - Vehicular access
 - Landscaping
- What Needs Improvement?
 - Walkability, bikeability, & transit
 - Food and services
 - Useable open space
 - Mathilda streetscape
- Additional Feedback
 - Clarify the envisioned development
 - Push for green buildings/technology
 - Parking
- Primary Concerns
 - Traffic
 - Height / impact on adjacent neighborhoods
 - Questions about the interactivity of the process

Presentation

Mr. Erik Calloway, FTS, briefly recapped the PowerPoint Presentation he gave at the first community workshop which examined how industrial era workplace trends influenced the development of Peery Park between 1960 and 1990 when the District was large built out. Digitization has replaced this old industrial model with a new “innovation economy model” in which business functions are highly decentralized and require less labor to make and transport products (see Workshop 1 *PeeryPark.inSunnyvale.com*).

Under this new innovation model “thinkers” with specialized expertise need a variety of venues and settings that bring them together to exchange ideas and develop new business opportunities. Since interaction is essential to innovation it is important to consider strategies that foster activity. This includes open co-work spaces, attractive outdoor areas for a break or lunch, work cafes, and after work amenities such as health clubs, recreation facilities and restaurants.

District-wide features should include prominent public spaces, attractive streetscapes and landscaping, inviting building entrances, building setbacks, convenient bike paths and pedestrian walkways. Architecture should be contemporary with a mix of small and medium scale spaces that allow for a dynamic range of uses among synergistic industrial clusters. Strategies to limit building height impacts on adjacent neighborhoods should be employed such as height limits, setbacks and buffering. Mr. Calloway concluded his remarks by noting that his presentation will be posted online at *PeeryPark.in Sunnyvale.com*.

Group Exercise – District Priorities

Next the workshop participants were organized into small groups for the group exercise. Each participant filled out an individual worksheet identifying their top five and lowest five priorities for the District. Each group then engaged in a discussion to identify the group’s top three and lowest three priorities. The groups then discussed two major questions: 1) what items should be included in the specific plan than would improve, enhance or preserve the character of the adjacent neighborhoods, and 2) what traffic improvements or transportation demand management (TDM) measures would be the most effective at relieving traffic near Peery Park. A representative of each group then reported the results of their discussion to the full workshop. The consolidated results of the group exercise are available online at *PeeryPark.in Sunnyvale.com*.

Open House

The evening concluded with an Open House where community members could view and discuss large wall posters which outlined the concepts discussed at the workshop.

Community Workshop #3

The next Community Workshop will be Thursday, January 21, 2015 from 6:30 to 9:00 pm at the Washington Park Community Room, 840 W. Washington Avenue in Sunnyvale.

The topics for the third workshop will be streetscape concepts, traffic conditions, transportation demand management (TDM) strategies and the conceptual framework for the specific plan.

Peery Park Specific Plan Community Workshop #3
City of Sunnyvale
January 21, 2015

Washington Park Community Room
840 W. Washington Avenue

Meeting Notes

City Councilmembers Present:

Vice Mayor Tara Martin-Milius
David Whittum
Glenn Hendricks
Gustav Larsson

City Staff Present:

Deanna Santana, City Manager
Kent Steffens, Assistant City Manager
Hanson Hom, Director of Community Development
Manuel Pineda, Director of Public Works
Trudi Ryan, Planning Officer
Connie Verceles, Economic Development Manager
Amber El-Hajj, Senior Planner/Project Planner for the Peery Park Specific Plan
Carla Ochoa, Traffic Engineer
Carol Shariat, Principal Transportation Engineer/Planner
Terilyn Anderson, Project Specialist/Notetaker

Consultants Present:

Erik Calloway, FTS (Freeman Tung + Sasaki)
Jill Hough, Hexagon Transportation Consultants, Inc.

Planning Commissioners Present:

Sue Harrison

Bicycle and Pedestrian Advisory Commissioners Present:

Richard Kolber

Introduction

Hanson Hom, Community Development Director, thanked everyone for coming and mentioned how important public input is in developing the Peery Park Specific Plan. Hanson mentioned that tonight's workshop was the third Peery Park Community Workshop. The first workshop was held on October 16, 2014, and covered existing conditions, workplace trends, market analysis and a broad brush approach to a strategic framework. The second workshop, on December 3, 2014, focused on the envisioned future, conceptual regulatory framework and priorities for the Peery Park District. The second workshop also included an interactive exercise where participants identified and

ranked the community's priorities, gave valuable feedback on the concepts discussed in the meeting and provided feedback on the preliminary traffic analysis.

Mr. Hom said that the input received at the workshops, on-line surveys and stakeholder interviews, plus additional research, culminated in the preliminary draft policy framework that will be used to guide development of the Peery Park District. Mr. Hom went over the topics for tonight's meeting: traffic conditions, mobility analysis, transportation demand management strategies and streetscape concepts. He also mentioned that two feedback exercises would be conducted during the last half of the meeting.

Mr. Hom went over the upcoming project schedule:

- February 24, 2015: Joint study session with the City Council and Planning Commission to discuss draft plan concepts.
- April 13, 2015: Planning Commission Public Hearing on the draft project description and conceptual policy framework.
- April 28, 2015: City Council Public Hearing on the draft project description and conceptual policy framework.

Mr. Hom said that public feedback has been extremely important in each step of the planning process. The results of the second community workshop reinforced and identified previous public feedback through the identification of three top priorities:

- 1) Traffic with a focus on Transportation Demand Management (TDM) and multimodal improvements over accommodating cars;
- 2) Relationships with adjacent neighborhoods (focused on visible height and privacy); and
- 3) Creating a mix of retail and service uses to generate activity, reduce traffic and provide amenities for residents and employees in the Peery Park area.

Presentation

Mr. Erik Calloway, FTS, briefly recapped the presentations he gave at the first and second community workshops held on October 16, 2013 and December 3, 2014. Mr. Calloway mentioned that the results of the last workshop show that the community and stakeholders' top three concerns were traffic impacts, the plans compatibility with adjacent neighborhoods and the need for a greater mix of uses and amenities. Meeting summaries of the previous two workshops and additional information from all of the workshops are available online at *PeeryPark.inSunnyvale.com*.

Mr. Calloway explained the twentieth century work model and how this has changed in the digital age. In the twentieth century, work was based on a 9:00 a.m. to 5:00 p.m. schedule where most people worked the same hours. The pattern of land use and development separated employment hubs from residential areas so employees commuted to work, often from suburban areas. Sunnyvale's pollution grew almost 500% between 1950 and 1970 and, between 1950 and 2001, vehicle growth exceeded population growth rate. In 1950 people drove an average of 10 miles per day whereas, by the year 2000, the average increased to 40 miles per day. Early in the twenty-first century the digital revolution replaced the traditional 9:00 a.m. to 5:00 p.m. work model

with a “non-synchronized” model enabling people to work “off-site” at any hour of the day.

This change in work pattern has resulted in an increasing use of public transportation and a decreasing number of miles driven. Small localized employment sub-regions have also emerged. For example, half of the employees at Peery Park live within ten miles of the District.

Next Mr. Calloway introduced Jill Hough, Hexagon Transportation Consultants, Inc. Ms. Hough explained how traffic impacts are measured through the traffic model. The model takes into account regional growth, City-wide growth, Peery Park growth, it calculates trips generated by different land uses for morning and evening peaks, takes into account mode choice and measures the level of service impact on the intersection(s) being studied. Level of service refers to how long a car waits at a traffic signal and is rated A through F. For example, Level A is ≤ 10 seconds and Level F is ≥ 80 seconds.

Two versions of the model were run for the year 2030: The first version was based on the current General Plan and existing conditions and the second version includes the Peery Park project, the Lawrence Station project and the update to the Land Use and Transportation Element. A total of 60 intersections and seven freeway segments were studied with this model run and the results from the two versions are compared to measure the level of service with and without the project. The results of the model runs show that approximately 17 intersections would be impacted with the cumulative project model run mentioned above.

Mr. Calloway then explained that there are two ways to reduce traffic impact: lower the overall travel demand and/or reduce the peak traffic time when people come and go to work. Mr. Calloway mentioned that studies show that thirty percent of people will use transit if they live within a half mile of a station and currently, there are two buses and a CalTrain shuttle that serve the Peery Park area. He mentioned that the City will continue discussions with VTA that may result in improved public transportation methods to the specific plan area. Another strategy is to provide opportunities to take care of mid-day and after-work errands without need of a car by providing nearby amenities, such as restaurants, retail shops, service uses and recreational opportunities.

Mr. Calloway outlined the elements that are typically included in a Transportation Demand Management (TDM) Plan and discussed strategies to transition away from auto dependency, including trip reduction targets. Mr. Calloway also discussed the number (2.2 million net new square feet) used in the model run and mentioned that that number may be used as the development cap for the Peery Park Specific Plan.

Next, he discussed several streetscape designs and potential connectivity improvements such as landscaped medians, street lighting, new and improved bike lanes, sidewalks and pedestrian connections.

Mr. Calloway concluded his remarks by reviewing the Peery Park vision statement and goals and noted that his PowerPoint presentation will be posted online at *PeeryPark.in Sunnyvale.com*.

Q and A Summary

Next, there were a few questions from community members. Erik Calloway, Jill Hough and City staff members responded.

How does the replacement of turning lanes with landscaped medians help traffic conditions?

- A. Turning movements can be sources of vehicle conflicts. Landscaped center medians prevent traffic turns and therefore increase traffic flow. They also have a traffic calming effect because they narrow the travel path and cause people to slow down. The goal isn't to reduce capacity, but, rather is to make visual improvements that maintain capacity. The primary focus of a transportation plan is to reduce traffic impacts as development occurs through strategies such as trip reduction targets and a development cap.

How much will traffic increase on Mathilda, 237 and Lawrence Expressway and what impact will this have on the SNAIL residential neighborhood?

- A. The traffic model studied 60 intersections, including Maude and Mary, Maude and Mathilda, and Maude and Fair Oaks. The model showed that there would be additional traffic on Maude Avenue as a result of the cumulative projects but that the preliminary information showed that the projects would not create a LOS E or F along Maude or in direct proximity to the SNAIL neighborhood.

How does the Mary Avenue Extension fit into the study?

- A. The Mary Avenue extension project that would connect the Peery Park District with Moffett Park (north across 237) is a proposed long-term project that would reduce congestion and is included in the model run.

How is the "average" level of service calculated?

- A. The average a.m. and p.m. peak traffic level for is recorded along with the movements at various intersections and then the results are averaged. For example, an intersection with four approaches has 12 turning movements. Each of these 12 movements has a delay. The model software calculates these turning movements and determines the average delay.

Is it possible to adjust traffic signals based on the time of day?

- A. Yes.

Even if the Peery Park Project doesn't go forward, there are many properties under current zoning that are slated for development. What impact would those projects have if the Peery Park Specific Plan is not completed?

- A. The City is updating its Transportation Demand Management (TDM) program and individual projects have TDM requirements. The difference between the level of

review for the specific plan vs. individual projects is that it isn't as easy to evaluate the cumulative impacts when projects come in individually and therefore you might not get the same results without a full evaluation of the entire area.

Group Exercise

Amber El-Hajj, Senior Planner, introduced two group exercises. In the first exercise participants broke into small groups and developed guiding principles from the vision statement and goals that had already been developed for the specific plan. Each group's guiding principles were then posted on the wall in the community room. In the second exercise participants moved throughout the room and posted their comments on large posters at each of five subject stations: 1) Connectivity, 2) Neighborhood Compatibility, 3) Traffic and Transportation Demand Management, 4) Land Use Character, and 5), Vision Statement, Goals and Guiding Principles (developed in the first exercise by each small group).

Website

PeeryPark.inSunnyvale.com

Updated Version of Property Owners Workshop #2 Discussion Points

Transportation Demand Management (TDM)

- The City may require TDM target goals at a sliding scale of 20-35% for each project based on project gross square footage and some changes in occupancy. The TDM goal will be based on the ITE rate for each development. Preliminarily this scale may look similar to the following table:

Project (gross sq. ft.)	TDM Trip Reduction Goal*
Over 600,000	35%
300,001 to 600,000	30%
100,001 to 300,000	25%
Up to 100,000 and change in occupancy that intensifies prior use	20%

*Trip reduction goal based on Institute of Transportation Engineers (ITE) rates for each development.

- There will be specific monetary penalties if a property owner (or tenant) do not meet their target TDM goal or does not actively participate in the TMA.
- The City may consider providing incentives if project proposes to meet a stricter TDM target (i.e. an additional 5% reduction over what's required). Incentives may include deviations to the Specific Plan or zoning standards.
- The City may consider reduced TDM requirement on a case-by-case basis in exchange for community benefits such as public restaurant/mixed use (generate activity or support walking trips within the district), public open space/pathways, bicycle/transit/street improvements, etc.

Transportation Management Association (TMA)

- Property owners will be required to participate in a TMA that will have certain responsibilities and is privately funded. Prior to a specific build-out threshold (e.g. 500,000 of net new square feet of development) or for the first project over a certain size (e.g. 250,000 gross square feet), whichever occurs first, a mandatory TMA must be established prior to issuance of a building permit.
 - Responsibilities of TMA are flexible, will be defined by governing board and can be adjusted over time, but may include:
 - Transportation Coordinator/district informational website;
 - Transportation program with employer/employee incentives;
 - Carshare, rideshare, carpooling and bikeshare programs;
 - Transit passes;
 - Coordination on TDM monitoring and reporting;
 - Feasibility study and shuttle bus coordination or operation;
 - Installation and maintenance of pedestrian, bicycle, transit, recreation and sense of place amenities (bike shop?); and
 - Regular reporting of updates to the City.

Shuttle Bus Program

- Prior to a specific build-out threshold (e.g. 500,000 of net new square feet of development) or for the first project over a certain size (e.g. 250,000 gross square feet), whichever occurs first, a feasibility study shall be prepared to implement a privately funded pilot shuttle bus program to serve the district and possibly larger area (can be a task of the TMA or an individual developer).
- The costs/benefits of the shuttle bus program would be evaluated for success in contributing to TDM goals after an initial period of operation.
- Alternative, a developer can commit to providing a private shuttle for its own project.

Parking

- The City proposes to set maximums for overall parking and for surface parking in a sliding scale that may look like the following:

Project (gross sq. ft.)	Parking Maximums* (spaces/1,000 sq. ft.)
Over 600,000	3.0
300,001 to 600,000	3.2
100,001 to 300,000	3.4
Up to 100,000 and change in occupancy that intensifies prior use	3.6

*Additional parking allowed up to 3.6/1,000 with structured parking or project enhancements.

- Property owners may have the ability to add additional parking (to a maximum of 3.6 spaces/1,000 sq. ft.) when structured parking is proposed in-lieu of surface parking.
- Allow additional parking greater than the listed maximum to be negotiated with incentives to the community and district.
- Allow reduced parking for mixed-use/shared use sites, potentially requiring no off-street parking for certain types of retail/restaurants.
- Allow reduced parking if additional open space, shared parking, unbundled parking or more aggressive TDM measures are provided.
- Alternative: if higher parking standards than above are desired, unbundle parking with requirement to charge for parking or offer a cash-out program for employees that use transit, carpool or alternative modes.

Potential Infrastructure Concepts/Fees

- Each property responsible for frontage improvements such as sidewalks, street trees, etc.
- The following fees would be determined following further analysis to define projects, estimated cost and fair share formula.
 - Sense of place fee.
 - Recycled water fee.

- Supplemental transportation impact fee (TIF) for district-related transportation improvements that can mitigate traffic impacts.
- Consider reduced infrastructure fee in exchange for energy efficient architecture, additional open space, or water efficient architecture and site design that would reduce infrastructure demand.

Land Use

- A maximum overall development capacity for the district based on net increase over existing building space will be defined in the Peery Park Specific Plan. Development exceeding this capacity will require a plan amendment and additional environmental analysis.
- A height limit of four to six stories with a maximum building height will be defined (except three stories with transition on east side of Mathilda). No FAR standards are currently anticipated for individual properties, but other development standards (e.g. parking, setbacks, and open space/landscaping) and possible incentives will influence the development capacity of each parcel.
- No maximum development site or parcel will be defined, but development standards will influence site planning.
- Consider allowing height above four stories or a maximum height along west side of Mathilda in locations where existing development on the east side of Mathilda is taller and/or blocks visibility from SNAIL neighborhood (context study would be required) and the project provides benefits to the community or district. Additional height could possibly be approved through use permit process.
- Individual projects will be referred to the FAA for review; this may result in reduced height allowances for certain properties within the most critical safety zone. Projects will also be referred to Airport Land Use Commission staff for comments.

Open Space/Landscaping/Outdoor Space

- Define minimum open space/landscaping standards (20-40% based on project and/or property size) with reduction available for public dedication/easements or enhanced pedestrian, recreation or place-making amenities both on or off site.
- Establish a park dedication in-lieu fee for public park and trail improvements. Allow credit for usable outdoor space and facilities exceeding minimum standards; partial credit if private, full credit if public.

Retail Uses

- Primary retail center at Mary and Central Expressway to serve district employees, nearby residents and regional traffic (minimum size TBD).
- Optional secondary retail center at north end of study area (subject to further market analysis).
- Allow smaller convenience retail/cafes throughout district with incentives for providing uses open to the public (e.g. exceptions to certain zoning standards such as open space).



March 3, 2015

Hanson Hom
Community Development/Planning Division
City of Sunnyvale
456 W Olive Ave
Sunnyvale, CA 94086

RE: Irvine Company Positions on Peery Park Stakeholder Workshop #2 Presentation

Dear Mr. Hom:

It was a pleasure participating in the Peery Park Stakeholder Workshop #2 on February 11, 2015. We appreciate continued efforts by the City to discuss Peery Park planning issues with stakeholders. As the largest land owner in the Peery Park Specific Plan area, we would like to take the opportunity to share a few observations regarding the items discussed at the latest meeting.

Transportation Demand Management

1. The City has proposed a TDM sliding scale of 20-35% for each project. The Company agrees that a TDM Program is essential for incorporation into the Peery Park Specific Plan. However, we believe that the City's TDM sliding scale goals are aggressive and will be difficult to obtain, particularly for multi-tenant buildings. These high bar goals might actually be counter-productive because it could encourage owners/tenants to pay the penalty for not reaching these goals rather than expending significant funds towards a TDM program in which they do not believe will result in reaching the required peak hour traffic reduction. Therefore, we recommend the following revisions to the City's sliding scale goals":

<u>Project</u>	<u>TDM Goal</u>
750,000 gross sq. ft or more	30%
250,000-749,999 gross sq. ft.	25%
100,000-249,999 gross sq. ft.	20%
Up to 100,000 gross sq. ft.	15%

2. We have recommended the above revisions to the TDM Goal based on your response at the 2/11 meeting that no credit will be given for any existing development on our site. However, we could support your recommended TDM percentage Goals if the TDM Program also included two provisions. The first provision would state that no monetary penalties would be imposed on a property owner if the peak hour traffic counts conducted to monitor the effectiveness of the implemented TDM Program are less than the peak hour traffic counts generated by the existing development prior to the re-development of the site. The second provision would establish that the TDM trip reduction calculation would be based on the existing trips on the site plus the TDM goal percentage on the amount of square footage above the existing square footage on the site.

3. Achievement of the required TDM goal during the monitoring of the TDM's effectiveness should recognize that projects are often phased. As such, the required TDM goal should be on a sliding scale that reflects the project's phasing.
4. It is encouraged that the City develop its proposed monetary penalties as soon as possible to allow the development community to comment on such penalties. The City should also identify how any monetary penalty funds that are received by the City will be used. We suggest that use of the funds be limited to programs or projects that will facilitate peak hour trip reduction within the Peery Park Specific Plan area only.
5. It is our understanding from the 2/11 meeting that the TDM goals are for peak hour traffic and not average daily trips. It was also stated the trip rates to be used in determining the TDM goals would be different than the trip rates used in the Peery Park Specific Plan EIR. Please provide further explanation as to why different trip rates would be used since the TDM Program is a mitigation measure to address cumulative impacts identified in the EIR. For consistency, we believe that the same rates should be utilized for both the EIR and the TDM Program.

Transportation Management Association

1. The City's proposed TDM program would require a mandatory Transportation Management Association (TMA). It would also place the burden of establishing this TMA on the first project exceeding certain thresholds. While we firmly agree that a TMA could ultimately provide many benefits towards a successful and cost effective TDM program, we recommend that the TMA be a voluntary element of the TDM. Creation of a TMA will only make sense when there is a critical mass of employers who find that combining TDM efforts as a part of a TMA is more cost effective than managing individual TDM programs at each employment site. We recommend that you allow the private sector to determine when and if a TMA is feasible.

Shuttle Bus Program

1. Since the City's proposal requires achievement of a specific TDM Goal trip reduction, we see no reason to also require a shuttle bus feasibility study regarding implementation of a privately funded shuttle bus program to serve the district at some build-out threshold. If an owner proposes a TDM program that will obtain the required trip reduction with or without a private shuttle for only its own project, it seems unreasonable to require this owner to prepare a feasibility study for district wide private shuttle bus. Similar to the TMA discussion above, there may be a point in time where various owners/tenants determine that it makes economic sense for them to jointly fund a district wide private shuttle service in lieu of project only private shuttle service as a means of satisfying their TDM trip reduction requirement.

Parking

1. The Irvine Company has concerns over limiting parking below market demand levels. On the other hand the flexibility provided by the bulleted items in the Stakeholder Workshop #2 memo provide sufficient ability to increase parking based on specific project characteristics.
2. The ability to increase parking to 3.5 based on provision of structured parking needs to be clarified as many projects will have a combination of surface and structured parking. Perhaps a quota of at

least 50% of parking to be structured could be considered in order to qualify for the higher 3.5/1000 provision.

3. The allowance of a higher parking provision in exchange for additional public benefit should be quantified so clarity can be gained in the initial master planning and project feasibility process.

Potential Infrastructure Concepts/Fees

1. We concur that each project should be responsible for on-site frontage improvements. A more detailed concept landscape plans need to be developed for the Peery Park street network in order to better evaluate the proposed level of improvement and impact of costs.
2. The “Sense of Place Fee” does not carry sufficient clarity to allow a full assessment. We would rather see that individual projects provide sufficient place-making on-site to avoid an overall special assessment that would add to costs without concrete expectations regarding value-added to the district and/or land owners.
3. We question the nexus between reducing infrastructure fees in exchange for a higher level of green building. Infrastructure assessment fees should be based on a formal set of proposed improvements that require specific funding. Promoting underfunding of infrastructure (street improvements to mitigate traffic impacts, recycled water, incremental streetscape/sidewalks) in exchange for abstract benefits of green buildings does not on the surface seem a good representation of fiduciary responsibility.

Land Use

1. The stakeholders have asked several times for the basis on which the 2.2 MM SF of additional space was calculated. This information would be helpful to understand the basis of the increment.
2. For IC parcels in the north/central portion of Peery Park, the primary restriction to development is the Moffett Field over-flight criteria. Rather than referring projects for review on an incremental/individual basis, could not the specific planning process negotiate a standard set of rules and restrictions to help clarify for proponents over-flight restriction at project inception rather than part of a formal approval process? This would allow clarity at the conceptual feasibility level rather than much later after the plans have been well cast.

Open Space/Landscaping/Outdoor Space

1. The city should be careful in mandating high open space criteria if it forces either structured parking or taller building heights, either which could push a project beyond market-driven financial feasibility. The cost of structured parking (caused by disproportionate open space requirements) for smaller project could very well cause delay in desired revitalization velocity.
2. For most workplace environments the highest valued open space is often ‘urban’ in scale, that is plazas and courtyards where workers can have lunch and enjoy small social or work meetings. Large park expanses should be at the discretion of the developer and not forced in commercial areas as a base requirement.

3. Official "public park" has historically and logically been linked to residential development rather than commercial. To do both constitutes double counting especially since most park use is associated with weekend and evening family-oriented discretionary leisure time.

Retail Uses

1. The Irvine Company supports the provision of local workplace services at the Mary and Central Expressway intersection. The scale of the provision needs to be market-driven and scaled to a size that can be realistically supported by an office, daytime-driven service population.
2. It must also be understood that the primary goal of potential redevelopment of the Central Research Park (South Campus) is to provide a large, single user campus. If successful in this venture, the user may have requirements that limit the extent of public accessible retail vs. single company services and facilities. Flexibility needs to be maintained regarding the nature of the retail/services complex if the larger goal of capturing 'best of class' companies is to be realized.
3. In light of the above two points, an alternative network of smaller food service and local service retail centers should be encouraged. Such a network organized around major intersections may provide the most sustainable retail service over the long run.

The Irvine Company is highly supportive of the interactive Specific Plan process. Due to our history we are cognizant of the human and economic benefits of well-planned working and living environments. We look forward to discussing the points outlined above and make ourselves available for a meeting at your convenience. We look forward to continued participation in the Peery Park Specific Plan preparation.

Sincerely,



Carlene Matchniff
Vice President, Entitlements & Public Affairs
The Irvine Company

cc: Amber El-Hajj, Senior Planner
Manuel Pineda, Director Public Works
Trudi Ryan, Planning Officer
Deanna Santana, City Manager
Carol Shariat, Principal Transportation Engineer
Connie Verceles, Economic Development Manager



Amber El-Hajj <ael-hajj@sunnyvale.ca.gov>

PPSP Stakeholder Workshop #2 Comments

Peter Larko

Thu, Feb 12, 2015 at 1:33 PM

To: Amber El-Hajj <ael-hajj@sunnyvale.ca.gov>Cc: Hanson Hom <hhom@sunnyvale.ca.gov>, Trudi Ryan <trian@sunnyvale.ca.gov>, Connie Verceles<cverceles@sunnyvale.ca.gov>, John DiNapoli, Eire Stewart, Pat Castillo

Amber-

We missed you at yesterday's Stakeholder Workshop #2. The session was very informative and there was a lot of good discussion. We'd like to provide you with some thoughts and comments for staff's consideration at this time as noted below:

Transportation Demand Management (TDM)

We appreciate staff's suggestion that the percent of peak trip reduction goal be graduated by project size. In order to realistically achieve reductions of 30-35%, the size of projects is critical. In fact, to reach a 35% goal, our traffic consultant has indicated that a private company shuttle would likely be needed in addition to many other tools, even if an area-wide shuttle were implemented. Accordingly, we recommend that the range for 25% reduction goal be set at 150,000 to 399,999 square feet, the range for 30% be set at 400,000 to 749,999 square feet, and the 35% goal apply to projects of 750,000 square feet or more.

We agree with staff's suggestions regarding incentives and case-by-case reduction considerations with community benefits.

Transportation Management Association (TMA)

The concept of a TMA for Peery Park is generally a good one, but how to initiate one is difficult. We agree with the comments made yesterday as to what a potentially unachievable burden this could place on the first project and, therefore, why this should be a voluntary option among the property owners. If staff feels that a mandatory trigger is necessary, we recommend at a minimum that the starting points be an aggregate of at least 1,000,000 square feet and a single project of at least 750,000 square feet.

Generally, the potential responsibilities of a TMA suggested by staff look to be a good starting point.

Shuttle Bus Program

Again, the concept is a good one, but initiation and implementation could be very difficult. First, the idea of analyses to determine whether a shuttle program would be cost effective at the time of development proposals is sound. We suggest that such a study first be required once an aggregate of 1,000,000 square feet is reached or for any single project of at least 750,000 square feet. We will gladly work with City staff and other property owners to engage with VTA to explore whether they could operate or participate in such a program.

Parking

We agree with staff recommendations on parking.

Potential Infrastructure Concepts/Fees

We generally support staff's suggestions here but want to understand and have staff be sensitive to the cost of such potential fees in the aggregate.

Land Use

We support staff's suggestions and recommendations with the following exception:

Building heights along the west side of Mathilda, north of San Aleso should be capped at six floors, not four floors. As we have explained previously, the additional setback from the neighborhood to the east would allow six story development with little or no impact on residents and is consistent with the City's long-standing gateway vision for this area. We appreciate staff's suggestion that visual analyses be required here to demonstrate the view from throughout the neighborhood, but don't believe that a use permit requirement should be imposed.

We also respectfully request that the District Regulation Map reflect the dark purple Innovation Edge on the parcels from Mathilda west to Vaqueros, and that the six-story setback be eliminated. We also suggest that the City be flexible on building setbacks along the frontage on Mathilda since VTA points out that buildings closer to the street encourage transit ridership. We make these suggestions with the caveat that visual analyses be required and neighborhood protection be afforded.

Open Space/Landscaping/Outdoor Space

We support the concept of a visually pleasing district with ample open/outdoor space, but would like to see more detail on what may be proposed as specific standards. Additionally, another in-lieu fee is suggested here and we ask that any potential fees be looked at in the aggregate.

Retail Uses

We generally agree with concepts here, however, we question the viability of the proposed secondary retail center based on its location.

Building Height

It was discussed whether building height should be controlled by the number of floors or by specific building height since the height of floors can vary by building type and design. We support the staff recommendation of using number of floors for ample flexibility in the regulations. If there is a desire to use specific heights instead, we suggest that the heights for four-story and six-story buildings be set at up to 66 and 96 feet to the roof, respectively.

If parapets were to be included, an additional 6 feet should be allowed to account for slopes. It is recommended that roof screens and mechanical equipment be excluded from the measurements since these are setback significantly from the edge of the roof and it is difficult to determine the precise height of equipment early in the design process.

Thank you for the opportunity to participate in the specific plan process and we look forward to reviewing staff's next iteration of the PPSP and development standards.

Peter Larko, AICP
J.P. DiNapoli Companies, Inc.
99 Almaden Boulevard, Suite 565
San Jose, CA 95113
O (408) 535-2224
C (408) 203-6212



City of Sunnyvale

Agenda Item

15-0311
Agenda Date: 3/17/2015

Study Session Summary of February 24, 2015 - Peery Park Specific Plan

Call to Order

Mayor Jim Griffith called the Study Session to order at 6:10 p.m.

City Councilmembers Present:

Mayor Jim Griffith
 Vice Mayor Jim Davis
 Councilmember David Whittum
 Councilmember Tara Martin-Milius
 Councilmember Glenn Hendricks
 Councilmember Gustav Larsson

City Councilmembers Recused from Item:

Councilmember Pat Meyering

City Councilmembers Absent:

None

Planning Commissioners Present:

Chairman Melton
 Commissioner Ralph Durham
 Commissioner Sue Harrison
 Commissioner Larry Klein
 Commissioner Ken Rheaume

Planning Commissioners Absent:

Commissioner Olevson
 Commissioner Simons

Public Comment:

John Cordes - SNAIL Neighborhood Association

Study Session Summary:

Director of Community Development, Hanson Hom, gave a presentation regarding project background, community participation, transportation demand management, conceptual plan framework and upcoming project milestones. Director of Public Works, Manuel Pineda, presented traffic background and findings of recent traffic analyses.

Issues raised by City Council:

15-0311Agenda Date: 3/17/2015

- Potential height impacts on adjacent neighborhoods
- The need for more commercial activity adjacent to Mathilda
- Potential bike connection from the Peery Park area to the Moffett Park area
- Ways to maintain a diversity of uses in the district
- Transit contributions from property owners
- Golf course enhancements
- Whether the 2010 traffic numbers are “current”
- Understanding the development potential (2.2 million square feet)
- How the district will deal with traffic impacts
- Understanding how Pastoria will function as a signature space
- Jobs vs. Housing
- Methods to ensure we are not completely dependent on the market
- Pedestrian connection from SNAIL neighborhood to San Aleso
- Transportation Demand Management (TDM) measures and shuttle services for the district
- Understanding the concept of screening 4-story buildings behind 3-story buildings.
- How to manage walled communities separated from each other and surrounding area

Adjournment:

Mayor Griffith adjourned the meeting at 6:55 p.m.