



City of Sunnyvale

Notice and Agenda City Council

Tuesday, October 30, 2018

5:00 PM

Council Chambers and West Conference
Room, City Hall, 456 W. Olive Ave.,
Sunnyvale, CA 94086

**Special Meeting: Closed Session - 5 P.M. | Study Session - 6 P.M. |
Regular Meeting - 7 P.M.**

5:00 PM SPECIAL COUNCIL MEETING (Closed Session)

- 1. Call to Order in the West Conference Room**
- 2. Roll Call**
- 3. Public Comment**

The public may provide comments regarding the Closed Session item(s) just prior to the Council beginning the Closed Session. Closed Sessions are not open to the public.

4. Convene to Closed Session

[18-0904](#)

Closed Session held pursuant to California Government Code
Section 54956.9:

LEGAL COUNSEL-ANTICIPATED LITIGATION

Significant exposure to litigation pursuant to paragraph (2) of
subdivision (d) of Section 54956.9: Related to Notice of
California Voting Rights Act Violation by Samir Kalra

5. Adjourn Special Meeting

6 P.M. SPECIAL COUNCIL MEETING (Study Session)

- 1. Call to Order in the West Conference Room (Open to the Public)**
- 2. Roll Call**
- 3. Public Comment**
- 4. Study Session**

[18-0716](#)

Presentation on Public Safety Bureau of Fire Services
Standards of Cover

5. Adjourn Special Meeting

7 P.M. COUNCIL MEETING

Pursuant to Council Policy, City Council will not begin consideration of any agenda item after 11:30 p.m. without a vote. Any item on the agenda which must be continued due to the late hour shall be continued to a date certain. Information provided herein is subject to change from date of printing of the agenda to the date of the meeting.

CALL TO ORDER

Call to Order in the Council Chambers (Open to the Public)

SALUTE TO THE FLAG

ROLL CALL

CLOSED SESSION REPORT

SPECIAL ORDER OF THE DAY

[18-0611](#)

Ceremonial Oath of Office for Commission Members

PRESENTATION

[18-0950](#)

Annual Report Presentation from Silicon Valley Clean Energy

ORAL COMMUNICATIONS

This category provides an opportunity for members of the public to address the City Council on items not listed on the agenda and is limited to 15 minutes (may be extended or continued after the public hearings/general business section of the agenda at the discretion of the Mayor) with a maximum of up to three minutes per speaker. Please note the Brown Act (Open Meeting Law) does not allow the Council to take action on an item not listed on the agenda. If you wish to address the Council, please complete a speaker card and give it to the City Clerk. Individuals are limited to one appearance during this section.

CONSENT CALENDAR

All matters listed on the consent calendar are considered to be routine and will be acted upon by one motion. There will be no separate discussion of these items. If a member of the public would like a consent calendar item pulled and discussed separately, please submit a speaker card to the City Clerk prior to the start of the meeting or before approval of the consent calendar.

- 1.A** [17-1137](#) Approve the List(s) of Claims and Bills Approved for Payment by the City Manager

Recommendation: Approve the list(s) of claims and bills.

- 1.B** [18-0851](#) Award of Bid No. PW19-02 for Pavement Rehabilitation 2018 Project, and Finding of California Environmental Quality Act (CEQA) Categorical Exemption

Recommendation: 1) Make a finding of a California Environmental Quality Act (CEQA) categorical exemption pursuant to CEQA Guidelines Section 15301 for the restoration or rehabilitation of existing highways and streets, sidewalks, gutters, bicycle and pedestrian trails, and similar facilities, involving negligible or no expansion of use beyond that presently existing; 2) Award a contract in substantially the same form as Attachment 2 to the report in the amount of \$928,283 to Interstate Grading & Paving, Inc. for the Pavement Rehabilitation 2018 Project, and authorize the City Manager to execute the contract when all necessary conditions have been met; and 3) approve a 10% construction contingency in the amount of \$92,828.

- 1.C** [18-0880](#) Approve Budget Modification No. 6 to Appropriate \$270,000 in one-time funding to Program 365 - Wastewater Treatment.

Recommendation: Approve Budget Modification No. 6 to appropriate \$270,000 in one-time funding to Program 365 - Wastewater Treatment.

- 1.D** [18-0926](#) Adopt Ordinance No. 3137-18 amending Section 3.80.040 of Chapter 3.80 (Minimum Wage) of Title 3 (Revenue and Finance) of the Sunnyvale Municipal Code related to Minimum Wage Consumer Price Index Increases

Recommendation: Adopt Ordinance No. 3137-18.

PUBLIC HEARINGS/GENERAL BUSINESS

If you wish to speak to a public hearing/general business item, please fill out a speaker card and give it to the City Clerk. You will be recognized at the time the

item is being considered by Council. Each speaker is limited to a maximum of three minutes. For land-use items, applicants are limited to a maximum of 10 minutes for opening comments and 5 minutes for closing comments.

2. [18-0856](#) Adopt a Resolution to Establish 15 Mile Per Hour Speed Zones at 35 Locations Adjacent to 16 Public Schools in Sunnyvale, Find the Action is Categorically Exempt under CEQA and Approve Budget Modification No. 7 for \$107,600 for Installation of Signs

Recommendation: Staff recommends Alternative No. 1: Adopt the attached Resolution to create 15 mph speed zones at 35 locations adjacent to 16 public schools in Sunnyvale, find the action categorically exempt under California Environmental Quality Act Guidelines section 15301(c), and approve Budget Modification No. 7 to appropriate \$107,600 for installation costs.

3. [18-0882](#) File #: 2018-7048
Location: 669 & 673 Old San Francisco Rd. (APNs 209-17-050 & 209-17-051)
Proposed Project: Appeal by a neighbor of a decision by the Planning Commission to conditionally approve related applications on a .34-acre site:
SPECIAL DEVELOPMENT PERMIT and VESTING
TENTATIVE MAP to allow a three-story six-unit townhouse development.
Zoning: R-3/PD
Applicant / Owner: Innovative Concepts / George Nejat
Environmental Review: Mitigated Negative Declaration
Project Planner: Ryan Kuchenig, (408) 730-7431, rkuchenig@sunnyvale.ca.gov

Recommendation: Alternative 1: Deny the appeal and affirm the Planning Commission's determination to adopt the Mitigated Negative Declaration, and affirm the approval of the Special Development Permit and Vesting Tentative Map subject to the recommended conditions of approval in Attachment 4 of the report.

4. [18-0719](#) Introduce an Ordinance Amending Chapter 5.36 (Taxicabs) of the Sunnyvale Municipal Code to Comply with State Law (AB

1069 and AB 939) and to Make Minor Operational Revisions

Recommendation: Alternative 1: Introduce an Ordinance Amending Chapter 5.36 (Taxicabs) of the Sunnyvale Municipal Code to comply with state law (AB1069 and AB 939) and to make minor operational revisions and find that the action is exempt under the California Environmental Quality Act (CEQA) pursuant to CEQA Guidelines Section 15061(b)(3).

5. [18-0853](#) Introduce an Ordinance to Amend Chapter 5.04 of the Sunnyvale Municipal Code - Business License Tax Ordinance to Reflect Changes to the Consumer Price Index Published by the United States Bureau of Labor Statistics

Recommendation: Introduce an ordinance to amend Section 5.04.110 of the Sunnyvale Municipal Code to reflect the changes to the Consumer Price Index adopted by the Bureau of Labor Statistics and find that the action is exempt from the California Environmental Quality Act ("CEQA") pursuant to CEQA Guidelines section 15061 (b) (3).

COUNCILMEMBERS REPORTS ON ACTIVITIES FROM INTERGOVERNMENTAL COMMITTEE ASSIGNMENTS

NON-AGENDA ITEMS & COMMENTS

-Council

-City Manager

INFORMATION ONLY REPORTS/ITEMS

[18-0907](#) Tentative Council Meeting Agenda Calendar

[18-0908](#) Information/Action Items

[18-0909](#) Board/Commission Meeting Minutes

ADJOURNMENT

NOTICE TO THE PUBLIC

The agenda reports to council (RTCs) may be viewed on the City's website at sunnyvale.ca.gov after 7 p.m. on Thursdays or at the Sunnyvale Public Library,

665 W. Olive Ave. as of Fridays prior to Tuesday City Council meetings. Any agenda related writings or documents distributed to members of the City of Sunnyvale City Council regarding any open session item on this agenda will be made available for public inspection in the Office of the City Clerk located at 603 All America Way, Sunnyvale, California during normal business hours and in the Council Chamber on the evening of the Council Meeting, pursuant to Government Code §54957.5. Please contact the Office of the City Clerk at (408) 730-7483 for specific questions regarding the agenda.

PLEASE TAKE NOTICE that if you file a lawsuit challenging any final decision on any public hearing item listed in this agenda, the issues in the lawsuit may be limited to the issues which were raised at the public hearing or presented in writing to the Office of the City Clerk at or before the public hearing. PLEASE TAKE FURTHER NOTICE that Code of Civil Procedure section 1094.6 imposes a 90-day deadline for the filing of any lawsuit challenging final action on an agenda item which is subject to Code of Civil Procedure 1094.5.

Pursuant to the Americans with Disabilities Act, if you need special assistance in this meeting, please contact the Office of the City Clerk at (408) 730-7483. Notification of 48 hours prior to the meeting will enable the City to make reasonable arrangements to ensure accessibility to this meeting. (28 CFR 35.160 (b) (1))

Planning a presentation for a City Council meeting?

To help you prepare and deliver your public comments, please review the "Making Public Comments During City Council or Planning Commission Meetings" available on the City website at sunnyvale.ca.gov.

Planning to provide materials to Council?

If you wish to provide the City Council with copies of your presentation materials, please provide 12 copies of the materials to the City Clerk (located to the left of the Council dais). The City Clerk will distribute your items to the Council.

Upcoming Meetings

Visit <https://sunnyvaleca.legistar.com> for upcoming Council, board and commission meeting information.



City of Sunnyvale

Agenda Item

18-0904

Agenda Date: 10/30/2018

Closed Session held pursuant to California Government Code Section 54956.9:

LEGAL COUNSEL-ANTICIPATED LITIGATION

Significant exposure to litigation pursuant to paragraph (2) of subdivision (d) of Section 54956.9:
Related to Notice of California Voting Rights Act Violation by Samir Kalra



City of Sunnyvale

Agenda Item

18-0716

Agenda Date: 10/30/2018

Presentation on Public Safety Bureau of Fire Services Standards of Cover



City of Sunnyvale

Agenda Item

18-0611

Agenda Date: 10/30/2018

Ceremonial Oath of Office for Commission Members



City of Sunnyvale

Agenda Item

18-0950

Agenda Date: 10/30/2018

Annual Report Presentation from Silicon Valley Clean Energy



City of Sunnyvale

Agenda Item

17-1137

Agenda Date: 10/30/2018

REPORT TO COUNCIL

SUBJECT

Approve the List(s) of Claims and Bills Approved for Payment by the City Manager

BACKGROUND

Pursuant to Sunnyvale Charter Section 802(6), the City Manager has approved for payment claims and bills on the following list(s); and checks have been issued.

List No.	Date	Total Disbursements
941	09-30-18 through 10-06-18	\$8,622,166.41
942	10-07-18 through 10-13-18	\$3,582,969.71

Payments made by the City are controlled in a variety of ways. In general, payments are reviewed by the appropriate City staff for compliance with the goods or services provided. Any discrepancies are resolved and re-submitted for payment. Different levels of dollar amounts for payments require varying levels of approval within the organization. Ultimately payments are reviewed and processed by the Finance Department. Budgetary control is set by Council through the budget adoption resolution.

ENVIRONMENTAL REVIEW

The action being considered does not constitute a "project" with the meaning of the California Environmental Quality Act ("CEQA") pursuant to CEQA Guidelines section 15378(b)(4) in that it is a fiscal activity that does not involve any commitment to any specific project which may result in a potential significant impact on the environment.

PUBLIC CONTACT

Public contact was made by posting the Council agenda on the City's official-notice bulletin board outside City Hall, at the Sunnyvale Senior Center, Community Center and Department of Public Safety; and by making the agenda and report available at the Sunnyvale Public Library, the Office of the City Clerk and on the City's website.

RECOMMENDATION

Approve the list(s) of claims and bills.

Prepared by: Timothy J. Kirby, Director of Finance

Reviewed by: Teri Silva, Assistant City Manager

Approved by: Kent Steffens, City Manager

ATTACHMENTS

1. List(s) of Claims and Bills Approved for Payment

10/12/2018

City of Sunnyvale

LIST # 941

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List of All Claims and Bills Approved for Payment
For Payments Dated 9/30/2018 through 10/6/2018

Sorted by Payment Number

Payment No.	Payment Date	Vendor Name	Invoice No.	Description	Invoice Amount	Discount Taken	Amount Paid	Payment Total
xxx307059	10/2/18	AAA SPEEDY SMOG TEST ONLY STATION	027840	Auto Maint & Repair - Labor	40.00	0.00	40.00	\$80.00
			027849	Auto Maint & Repair - Labor	40.00	0.00	40.00	
xxx307060	10/2/18	AARON TRIEU	CK REQ 19-039	DED Services/Training - Books	16.27	0.00	16.27	\$16.27
xxx307061	10/2/18	ACCESS HARDWARE	5700438-IN	Bldg Maint Matls & Supplies	525.80	0.00	525.80	\$525.80
xxx307062	10/2/18	ADAMSON POLICE PRODUCTS	INV282814	Clothing, Uniforms & Access	8.72	0.00	8.72	\$8.72
xxx307063	10/2/18	AIR COOLED ENGINES INC	81129	Parts, Vehicles & Motor Equip	19.65	0.00	19.65	\$19.65
xxx307064	10/2/18	AIRGAS USA LLC	9078882984	General Supplies	578.62	0.00	578.62	\$1,858.43
			9078980068	General Supplies	518.56	0.00	518.56	
			9079711965	General Supplies	578.62	0.00	578.62	
			9956104389	General Supplies	182.63	0.00	182.63	
xxx307065	10/2/18	ALLSTAR FIRE EQUIPMENT INC	210050	Clothing, Uniforms & Access	365.15	0.00	365.15	\$365.15
xxx307066	10/2/18	ALPINE AWARDS INC	5529843	Customized Products	690.52	0.00	690.52	\$690.52
xxx307067	10/2/18	BAY AREA AIR QUALITY MANAGEMENT DISTRICT	4ED96	Permit Fees	589.00	0.00	589.00	\$589.00
xxx307068	10/2/18	BAY AREA AIR QUALITY MANAGEMENT DISTRICT	4ED98	Permit Fees	589.00	0.00	589.00	\$589.00
xxx307069	10/2/18	CLAP ARTS	1792	Rec Instructors/Officials	5,255.60	0.00	5,255.60	\$5,255.60
xxx307070	10/2/18	CWEA-SCVS	OCT/09/2018	Training and Conferences	1,495.00	0.00	1,495.00	\$1,495.00
xxx307071	10/2/18	CALIFORNIA DEPT OF GENERAL SERVICES	1414771	Utilities - Gas	37,994.86	0.00	37,994.86	\$37,994.86
xxx307072	10/2/18	CARBONIC SERVICE INC	169748	Equipment Rental/Lease	233.52	0.00	233.52	\$233.52
xxx307073	10/2/18	CENTURY GRAPHICS	49756	Clothing, Uniforms & Access	761.66	0.00	761.66	\$1,430.01
			49770	Clothing, Uniforms & Access	213.64	0.00	213.64	
			49863	Clothing, Uniforms & Access	344.15	0.00	344.15	
			49907	Clothing, Uniforms & Access	110.56	0.00	110.56	
xxx307074	10/2/18	CINDI SWEET	09152018	Professional Services	200.00	0.00	200.00	\$200.00
xxx307075	10/2/18	COAST COUNTIES PETERBILT	0162325P	Parts, Vehicles & Motor Equip	124.95	0.00	124.95	\$124.95
xxx307076	10/2/18	CONTRACTOR COMPLIANCE & MONITORING INC	10085	Consultants	2,000.00	0.00	2,000.00	\$6,000.00
			10360	Consultants	2,000.00	0.00	2,000.00	
			9968	Consultants	2,000.00	0.00	2,000.00	

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xxx307077	10/2/18	CORIX WATER PRODUCTS US INC	17813028016	Water Meters	439.37	0.00	439.37	\$439.37
xxx307078	10/2/18	COUNTRY GOURMET RESTAURANT	948579	Employee Recognition Expenses	512.35	0.00	512.35	\$512.35
xxx307079	10/2/18	D & M TRAFFIC SERVICES INC	59478	Materials - Land Improve	337.90	0.00	337.90	\$337.90
xxx307080	10/2/18	DISCOUNT SCHOOL SUPPLY	W31105410101	General Supplies	835.77	0.00	835.77	\$835.77
xxx307081	10/2/18	DOOLEY ENTERPRISES INC	55713	Ammunition	8,796.11	0.00	8,796.11	\$8,796.11
xxx307082	10/2/18	E-Z-GO TEXTRON INC	91567244	Parts, Vehicles & Motor Equip	134.85	0.00	134.85	\$261.43
			91572869	Parts, Vehicles & Motor Equip	126.58	0.00	126.58	
xxx307084	10/2/18	FEDEX	6-307-18841	Mailing & Delivery Services	6.94	0.00	6.94	\$6.94
xxx307086	10/2/18	FITGUARD INC	0000148851	Misc Equip Maint & Repair - Labor	95.00	0.00	95.00	\$95.00
xxx307087	10/2/18	FIX AIR	3033124	Bldg Maint Matls & Supplies	686.24	0.00	686.24	\$686.24
xxx307088	10/2/18	GALE/CENGAGE LEARNING	65072905	Library Acquisitions, Books	352.17	0.00	352.17	\$380.06
			65187190	Library Acquisitions, Books	27.89	0.00	27.89	
xxx307089	10/2/18	GARDENLAND POWER EQUIPMENT	607287	Parts, Vehicles & Motor Equip	52.33	0.00	52.33	\$52.33
xxx307090	10/2/18	GOLDEN GATE MECHANICAL INC	3914	Facilities Maintenance & Repair Labor	1,300.00	0.00	1,300.00	\$1,300.00
xxx307091	10/2/18	GRAINGER	9856963765	General Supplies	1,918.19	0.00	1,918.19	\$1,918.19
xxx307092	10/2/18	H T HARVEY & ASSOC	49559	Miscellaneous Services	1,823.58	0.00	1,823.58	\$1,823.58
xxx307093	10/2/18	HAUTE CUISINE INC	106-2018	Food Products	140.00	0.00	140.00	\$140.00
xxx307094	10/2/18	HI TECH EMERGENCY VEHICLE SERVICE INC	030112REV	Parts, Vehicles & Motor Equip	-52.10	0.00	-52.10	\$34.50
			161445	Parts, Vehicles & Motor Equip	52.10	0.00	52.10	
			161840	Parts, Vehicles & Motor Equip	34.50	0.00	34.50	
xxx307096	10/2/18	ICE MACHINE RENTALS	45820	Miscellaneous Services	150.08	0.00	150.08	\$150.08
xxx307097	10/2/18	INDUSTRIAL SAFETY SUPPLY CORP	4514660	General Supplies	261.60	0.00	261.60	\$261.60
xxx307098	10/2/18	JWC ENVIRONMENTAL	93657	Water/Wastewater Treat Equip	42,363.95	0.00	42,363.95	\$42,363.95
xxx307099	10/2/18	JAVELCO EQUIPMENT SERVICE INC	54313	Parts, Vehicles & Motor Equip	47.84	0.00	47.84	\$47.84
xxx307100	10/2/18	KME FIRE APPARATUS	CA545671	Parts, Vehicles & Motor Equip	295.25	0.00	295.25	\$1,349.04
			CA545797	Parts, Vehicles & Motor Equip	509.86	0.00	509.86	
			CA545859	Parts, Vehicles & Motor Equip	90.13	0.00	90.13	
			CA545933	Parts, Vehicles & Motor Equip	453.80	0.00	453.80	
xxx307101	10/2/18	KOFFLER ELECTRICAL	0090095-IN	Equipment Maintenance & Repair Labor	900.00	0.00	900.00	\$900.00
xxx307102	10/2/18	KOHLWEISS AUTO PARTS INC	01PT7167	Parts, Vehicles & Motor Equip	41.77	0.00	41.77	\$61.19
			01PT7457	Parts, Vehicles & Motor Equip	2.14	0.00	2.14	

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Payment No.	Payment Date	Vendor Name	Invoice No.	Description	Invoice Amount	Discount Taken	Amount Paid	Payment Total
			01PT7609	Parts, Vehicles & Motor Equip	17.28	0.00	17.28	
xxx307103	10/2/18	KONECRANES INC	152152232	Equipment Maintenance & Repair Labor	1,565.00	0.00	1,565.00	\$3,389.84
			152160815	Misc Equip Maint & Repair - Labor	1,040.00	0.00	1,040.00	
			152160815	Misc Equip Maint & Repair - Materials	409.84	0.00	409.84	
			152160815	Equipment Rental/Lease	375.00	0.00	375.00	
xxx307104	10/2/18	L N CURTIS & SONS INC	429094	General Supplies	205,233.92	0.00	205,233.92	\$208,780.56
			INV215613	Misc Equip Maint & Repair - Labor	220.00	0.00	220.00	
			INV215613	Misc Equip Maint & Repair - Materials	26.16	0.00	26.16	
			INV215644	Clothing, Uniforms & Access	1,962.00	0.00	1,962.00	
			INV217448	Supplies, Fire Protection	112.23	0.00	112.23	
			INV217687	Clothing, Uniforms & Access	1,226.25	0.00	1,226.25	
xxx307105	10/2/18	LANDCARE USA LLC	176391	Services Maintain Land Improv	485.00	0.00	485.00	\$485.00
xxx307106	10/2/18	LIFETIME TENNIS INC	092018-01	Utilities - Electric	60.00	0.00	60.00	\$120.00
			092018-02	Utilities - Electric	60.00	0.00	60.00	
xxx307107	10/2/18	LOZANO SUNNYVALE CAR WASH	052	Auto Maint & Repair - Labor	1,942.50	0.00	1,942.50	\$1,942.50
xxx307108	10/2/18	MSI FUEL MANAGEMENT INC	4534	Auto Maint & Repair - Labor	995.00	0.00	995.00	\$1,565.00
			4544	Auto Maint & Repair - Labor	570.00	0.00	570.00	
xxx307109	10/2/18	MARSHA HOVEY, LLC	SV-001	Consultants	3,187.50	0.00	3,187.50	\$3,187.50
xxx307110	10/2/18	MCMaster CARR SUPPLY CO	73997046	General Supplies	19.05	0.00	19.05	\$19.05
xxx307111	10/2/18	MIDPEN HOUSING CORPORATION	1718-832040 #7	Outside Group Funding	10,345.00	0.00	10,345.00	\$10,345.00
xxx307112	10/2/18	MIDWEST TAPE	96413759	Library Acquis, Audio/Visual	533.16	0.00	533.16	\$5,381.34
			96413800	Library Acquis, Audio/Visual	467.77	0.00	467.77	
			96413832	Library Acquis, Audio/Visual	219.77	0.00	219.77	
			96432503	Library Acquis, Audio/Visual	1,324.27	0.00	1,324.27	
			96433160	Library Acquis, Audio/Visual	2,563.13	0.00	2,563.13	
			96433161	Library Acquis, Audio/Visual	273.24	0.00	273.24	
xxx307113	10/2/18	MOFFATT & NICHOL	736376	Consultants	7,324.14	0.00	7,324.14	\$7,324.14
xxx307114	10/2/18	MOUNTAIN VIEW GARDEN CENTER	95914	Materials - Land Improve	177.67	0.00	177.67	\$1,222.03
			95990	Materials - Land Improve	129.66	0.00	129.66	
			96006	Materials - Land Improve	113.31	0.00	113.31	
			96028	Materials - Land Improve	129.66	0.00	129.66	

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			96083	Materials - Land Improve	177.67	0.00	177.67	
			96108	Materials - Land Improve	177.67	0.00	177.67	
			96124	Materials - Land Improve	129.66	0.00	129.66	
			96181	Materials - Land Improve	186.73	0.00	186.73	
xxx307115	10/2/18	MUNICIPAL MAINTENANCE EQUIPMENT INC	0131274-IN	Parts, Vehicles & Motor Equip	117.53	0.00	117.53	\$117.53
xxx307116	10/2/18	MUSIC FOR FAMILIES INC	SVU18	Rec Instructors/Officials	9,073.24	0.00	9,073.24	\$9,073.24
xxx307117	10/2/18	NAPA AUTO PARTS	5983-424177	Parts, Vehicles & Motor Equip	20.57	0.00	20.57	\$290.21
			5983-424192	Parts, Vehicles & Motor Equip	21.35	0.00	21.35	
			5983-424221	Parts, Vehicles & Motor Equip	14.89	0.00	14.89	
			5983-424482	Parts, Vehicles & Motor Equip	24.12	0.00	24.12	
			5983-424497	Parts, Vehicles & Motor Equip	171.73	0.00	171.73	
			5983-424508	Parts, Vehicles & Motor Equip	37.55	0.00	37.55	
xxx307118	10/2/18	NORTHERN CALIFORNIA NURSING ACADEMY LLC	0026	DED Services/Training - Training	600.00	0.00	600.00	\$1,200.00
			025	DED Services/Training - Training	600.00	0.00	600.00	
xxx307119	10/2/18	OLDCASTLE STORMWATER SOLUTIONS	500022167	Construction Services	85.00	0.00	85.00	\$85.00
xxx307120	10/2/18	OMEGA ENGRAVING	803	General Supplies	70.50	0.00	70.50	\$70.50
xxx307121	10/2/18	OPTONY INC	183706	Consultants	545.00	0.00	545.00	\$545.00
xxx307122	10/2/18	ORLANDI TRAILER INC	174363	Parts, Vehicles & Motor Equip	107.55	0.00	107.55	\$107.55
xxx307123	10/2/18	PACIFIC ECO-RISK	14844	Water Lab Services	2,955.15	0.00	2,955.15	\$2,955.15
xxx307124	10/2/18	PACIFIC WEST SECURITY INC	10060	Alarm Services	79.00	0.00	79.00	\$172.00
			10331	Alarm Services	93.00	0.00	93.00	
xxx307125	10/2/18	PENINSULA BATTERY INC	126813	Inventory Purchase	427.72	0.00	427.72	\$427.72
xxx307126	10/2/18	QOVO SOLUTIONS INC	26-2208	Audio Visual Products	595.72	0.00	595.72	\$1,260.00
			26-2208	Computer Software	169.91	0.00	169.91	
			26-2208	Mailing & Delivery Services	12.13	0.00	12.13	
			26-2208	Professional Services	482.24	0.00	482.24	
xxx307127	10/2/18	QUALITY GLASS AND TINT	15473	Auto Maint & Repair - Labor	225.00	0.00	225.00	\$225.00
xxx307128	10/2/18	R E P NUT N BOLT GUY	29912	Inventory Purchase	150.69	0.00	150.69	\$246.96
			29913	Inventory Purchase	96.27	0.00	96.27	
xxx307129	10/2/18	R J GORDON CONSTRUCTION INC	LNDFLLEROSN #03	Construction Services	60,094.15	0.00	60,094.15	\$60,094.15

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xxx307130	10/2/18	RFSIGNALMAN	966	Contracts/Service Agreements	7,625.00	0.00	7,625.00	\$41,250.00
			981	Contracts/Service Agreements	33,625.00	0.00	33,625.00	
xxx307131	10/2/18	READYREFRESH BY NESTLE	18I0023249071	General Supplies	68.62	0.00	68.62	\$283.21
			18I0023360647	General Supplies	3.26	0.00	3.26	
			18I0028805083	General Supplies	34.17	0.00	34.17	
			18I5736476002	General Supplies	3.26	0.00	3.26	
			18I5740153001	General Supplies	112.05	0.00	112.05	
			18I5740154009	General Supplies	21.19	0.00	21.19	
			18I5740156004	General Supplies	40.66	0.00	40.66	
xxx307132	10/2/18	SAFETY KLEEN SYSTEMS INC	77923500	Equipment Maintenance & Repair Labor	45.00	0.00	45.00	\$45.00
xxx307133	10/2/18	SAFEWAY INC	431575-092618	Food Products	64.68	0.00	64.68	\$252.52
			721937-092718	Food Products	13.06	0.00	13.06	
			802107-091018	Food Products	17.97	0.00	17.97	
			804080-092118	Food Products	29.34	0.00	29.34	
			806270-092518	Food Products	26.42	0.00	26.42	
			806709-092618	Food Products	61.06	0.00	61.06	
			807851-0927201	Food Products	39.99	0.00	39.99	
xxx307134	10/2/18	SAN JOSE BMW	4295361	Auto Maint & Repair - Labor	832.50	0.00	832.50	\$1,091.00
			4295361	Auto Maint & Repair - Materials	258.50	0.00	258.50	
xxx307135	10/2/18	SANTA CLARA COUNTY ROADS & AIRPORTS	36266	Permit Fees	5,509.11	0.00	5,509.11	\$5,509.11
xxx307136	10/2/18	SANTA CLARA VALLEY HEALTH & HOSPITAL SYS	H6590437400	Medical Services	373.00	0.00	373.00	\$373.00
xxx307137	10/2/18	SHIMADZU SCIENTIFIC INSTRUMENTS	P4037970	General Supplies	2,533.00	0.00	2,533.00	\$2,533.00
xxx307138	10/2/18	SILICON VALLEY POLYTECHNIC INSTITUTE	09192018-587	DED Services/Training - Training	300.00	0.00	300.00	\$300.00
xxx307139	10/2/18	SMART & FINAL INC	048575-092018	Food Products	40.16	0.00	40.16	\$40.16
xxx307140	10/2/18	SONSRAY MACHINERY LLC	P09342-12	Parts, Vehicles & Motor Equip	1,717.10	0.00	1,717.10	\$2,402.33
			P09348-12	Parts, Vehicles & Motor Equip	193.02	0.00	193.02	
			P09442-12	Parts, Vehicles & Motor Equip	434.51	0.00	434.51	
			P09610-12	Parts, Vehicles & Motor Equip	57.70	0.00	57.70	
xxx307141	10/2/18	STEPHANIE TSAO	09152018	Professional Services	200.00	0.00	200.00	\$200.00

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xxx307142	10/2/18	STEVENS CREEK CHRYSLER JEEP DODGE	356882	Parts, Vehicles & Motor Equip	47.60	0.00	47.60	\$47.60
xxx307143	10/2/18	SUBURBAN PROPANE	2386486	Fuel, Oil & Lubricants	26.43	0.00	26.43	\$50.50
			2386571	Fuel, Oil & Lubricants	24.07	0.00	24.07	
xxx307144	10/2/18	SUNNYVALE TOWING INC	305205	Vehicle Towing Services	45.00	0.00	45.00	\$90.00
			314797	Vehicle Towing Services	45.00	0.00	45.00	
xxx307145	10/2/18	TJKM	0047660	Consultants	995.00	0.00	995.00	\$995.00
xxx307146	10/2/18	TOGOS SUNNYVALE	219	Food Products	178.00	0.00	178.00	\$476.00
			220	Food Products	120.00	0.00	120.00	
			221	Food Products	178.00	0.00	178.00	
xxx307147	10/2/18	TRIPAC FASTENERS	5734403	Construction Services	1,752.11	0.00	1,752.11	\$1,752.11
xxx307148	10/2/18	TURF & INDUSTRIAL EQUIPMENT CO	IV27771	Parts, Vehicles & Motor Equip	68.14	0.00	68.14	\$68.14
xxx307149	10/2/18	TURF STAR INC	7027008-00	Parts, Vehicles & Motor Equip	401.59	0.00	401.59	\$3,065.54
			7028968-00	Parts, Vehicles & Motor Equip	52.05	0.00	52.05	
			7030433-00	Parts, Vehicles & Motor Equip	86.83	0.00	86.83	
			7031584-00	Parts, Vehicles & Motor Equip	224.66	0.00	224.66	
			7031586-00	Parts, Vehicles & Motor Equip	2,191.17	0.00	2,191.17	
			7031586-01	Parts, Vehicles & Motor Equip	109.24	0.00	109.24	
xxx307150	10/2/18	UNITED RENTALS	159746520-003	Equipment Rental/Lease	127.65	0.00	127.65	\$127.65
xxx307151	10/2/18	UNIVAR USA INC	SJ902683	Chemicals	3,034.24	0.00	3,034.24	\$9,590.89
			SJ904187	Chemicals	3,098.71	0.00	3,098.71	
			SJ904476	Chemicals	3,457.94	0.00	3,457.94	
xxx307152	10/2/18	UNIVERSITY OF CALIFORNIA SANTA CRUZ	57923	DED Services/Training - Training	426.00	0.00	426.00	\$606.00
			58050	DED Services/Training - Training	180.00	0.00	180.00	
xxx307153	10/2/18	VWR INTERNATIONAL LLC	8083692803	General Supplies	96.27	0.00	96.27	\$1,091.09
			8083699457	General Supplies	145.49	0.00	145.49	
			8083757251	General Supplies	662.97	0.00	662.97	
			8083761626	General Supplies	122.08	0.00	122.08	
			8083761628	General Supplies	23.66	0.00	23.66	
			8083785287	General Supplies	40.62	0.00	40.62	
xxx307154	10/2/18	VALLEY OIL CO	43297	Fuel, Oil & Lubricants	143.88	0.00	143.88	\$143.88

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xxx307155	10/2/18	WAYPOINT ANALYTICAL CALIFORNIA INC	073851	Water Lab Services	252.00	0.00	252.00	\$252.00
xxx307156	10/2/18	WINSUPPLY OF SILICON VALLEY	693195 01	Miscellaneous Equipment Parts & Supplies	522.53	0.00	522.53	\$522.53
xxx307157	10/2/18	YI FENG	146890-5665014	DED Services/Training - Books	8.71	0.00	8.71	\$57.27
			970749-2809808	DED Services/Training - Books	48.56	0.00	48.56	
xxx307158	10/2/18	ZALCO LABORATORIES	1809096	Miscellaneous Services	390.00	0.00	390.00	\$390.00
xxx307159	10/2/18	WAITER.COM INC	I0925475434	Food Products	180.94	0.00	180.94	\$180.94
xxx307160	10/2/18	ALAMEDA COUNTY	29013109185647	Training and Conferences	600.00	0.00	600.00	\$600.00
xxx307161	10/2/18	DEPT OF FORESTRY & FIRE PROTECTION	154331	Training and Conferences	168.00	0.00	168.00	\$168.00
xxx307162	10/2/18	KIRBY CANYON RECYCLING & DISPOSAL FAC	AUG2018	Landill Fees to be Allocated	911,510.32	0.00	911,510.32	\$911,510.32
xxx307163	10/2/18	MEDINAS CATERING	10082018	Food Products	1,465.82	0.00	1,465.82	\$1,465.82
xxx307164	10/2/18	NADENE RUSSELL	100518-101018	Excursions	750.00	0.00	750.00	\$750.00
xxx307165	10/2/18	OFFICE DEPOT INC	192659120003	Supplies, Office 1	106.51	0.00	106.51	\$9,448.27
			194984967001	Supplies, Office 1	61.03	0.00	61.03	
			195625291002	Supplies, Office 1	29.12	0.00	29.12	
			197040615001	Supplies, Office 1	33.44	0.00	33.44	
			197173728001	Supplies, Office 1	184.21	0.00	184.21	
			198101740001	Supplies, Office 1	71.32	0.00	71.32	
			198108123001	Supplies, Office 1	107.71	0.00	107.71	
			198120891001	Supplies, Office 1	78.39	0.00	78.39	
			198132471001	Supplies, Office 1	272.48	0.00	272.48	
			198132472001	Supplies, Office 1	42.05	0.00	42.05	
			198205233001	Supplies, Office 1	16.74	0.00	16.74	
			198218633001	Supplies, Office 1	86.14	0.00	86.14	
			198218633002	Supplies, Office 1	10.90	0.00	10.90	
			198219641001	Supplies, Office 1	17.30	0.00	17.30	
			198250460001	Supplies, Office 1	50.02	0.00	50.02	
			198502286001	Supplies, Office 1	21.52	0.00	21.52	
			198504441001	Supplies, Office 1	119.89	0.00	119.89	
			198643745001	Supplies, Office 1	349.84	0.00	349.84	
			198645335001	Supplies, Office 1	10.46	0.00	10.46	

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			198816734001	Supplies, Office 1	43.58	0.00	43.58	
			198820390001	Supplies, Office 1	126.66	0.00	126.66	
			198936778001	Supplies, Office 1	99.50	0.00	99.50	
			198947493001	Supplies, Office 1	1,229.66	0.00	1,229.66	
			199030851001	Supplies, Office 1	85.22	0.00	85.22	
			199174161001	Supplies, Office 1	292.63	0.00	292.63	
			199213704001	Supplies, Office 1	5.45	0.00	5.45	
			199250716001	Supplies, Office 1	169.04	0.00	169.04	
			199250716002	Supplies, Office 1	14.92	0.00	14.92	
			199250716003	Supplies, Office 1	7.95	0.00	7.95	
			199252224001	Supplies, Office 1	10.45	0.00	10.45	
			199262092001	Supplies, Office 1	12.00	0.00	12.00	
			199502399001	Supplies, Office 1	33.79	0.00	33.79	
			199521982001	Supplies, Office 1	104.30	0.00	104.30	
			199525632001	Supplies, Office 1	68.04	0.00	68.04	
			199577783001	Supplies, Office 1	1,009.22	0.00	1,009.22	
			199587207001	Supplies, Office 1	95.03	0.00	95.03	
			199592989001	Supplies, Office 1	9.44	0.00	9.44	
			199659926001	Supplies, Office 1	171.68	0.00	171.68	
			199816050001	Supplies, Office 1	59.50	0.00	59.50	
			199842747001	Supplies, Office 1	9.81	0.00	9.81	
			199985305001	Supplies, Office 1	108.40	0.00	108.40	
			200345965001	Supplies, Office 1	121.40	0.00	121.40	
			200347176001	Supplies, Office 1	98.44	0.00	98.44	
			200528327001	Supplies, Office 1	65.64	0.00	65.64	
			201800389001	Supplies, Office 1	98.74	0.00	98.74	
			201990336001	Supplies, Office 1	41.62	0.00	41.62	
			201994348001	Supplies, Office 1	63.88	0.00	63.88	
			202005548001	Supplies, Office 1	38.67	0.00	38.67	
			202011399001	Supplies, Office 1	101.56	0.00	101.56	
			202013371001	Supplies, Office 1	1.49	0.00	1.49	

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			202015480001	Supplies, Office 1	248.27	0.00	248.27	
			202020177001	Supplies, Office 1	61.38	0.00	61.38	
			202029160001	Supplies, Office 1	148.00	0.00	148.00	
			202146527001	Supplies, Office 1	31.60	0.00	31.60	
			202146587001	Supplies, Office 1	7.51	0.00	7.51	
			202146588001	Supplies, Office 1	5.42	0.00	5.42	
			202155836001	Supplies, Office 1	21.23	0.00	21.23	
			202251825001	Supplies, Office 1	31.29	0.00	31.29	
			202350423001	Supplies, Office 1	90.56	0.00	90.56	
			202369173001	Supplies, Office 1	117.93	0.00	117.93	
			202392848001	Supplies, Office 1	271.48	0.00	271.48	
			202396708001	Supplies, Office 1	44.45	0.00	44.45	
			202439805001	Supplies, Office 1	4.63	0.00	4.63	
			202450240001	Supplies, Office 1	320.48	0.00	320.48	
			202733195001	Supplies, Office 1	38.80	0.00	38.80	
			202823256001	Supplies, Office 1	186.65	0.00	186.65	
			202824256001	Supplies, Office 1	91.29	0.00	91.29	
			202942821001	Supplies, Office 1	23.00	0.00	23.00	
			203137896001	Supplies, Office 1	68.99	0.00	68.99	
			203175662001	Supplies, Office 1	17.97	0.00	17.97	
			203253585001	Supplies, Office 1	1,036.24	0.00	1,036.24	
			203282043001	Supplies, Office 1	38.78	0.00	38.78	
			203602038001	Supplies, Office 1	211.84	0.00	211.84	
			203692884001	Supplies, Office 1	12.82	0.00	12.82	
			203694927001	Supplies, Office 1	5.55	0.00	5.55	
			203893461001	Supplies, Office 1	220.75	0.00	220.75	
			203896116001	Supplies, Office 1	24.58	0.00	24.58	
xxx307171	10/2/18	AMADOR EDUWIGES MONSERRAT	403406	Refund Recreation Fees	1,000.00	0.00	1,000.00	\$1,000.00
xxx307172	10/2/18	CAMP & ASSOCIATES INC	2018-7718	Administrative Request Fees	471.00	0.00	471.00	\$471.00
xxx307173	10/2/18	DNYANESHWARI CHANDARANA	402288	Refund Recreation Fees	350.00	0.00	350.00	\$350.00
xxx307174	10/2/18	INFINITY MUSICALS	402484	Refund Recreation Fees	500.00	0.00	500.00	\$500.00

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xxx307175	10/2/18	OMNIS RISK INSURANCE SOLUTIONS INC	2018-7681	Major Permit Application Fees - Other	334.00	0.00	334.00	\$2,045.00
			2018-7681	Minor Permit Application Fees - Other	1,711.00	0.00	1,711.00	
xxx307176	10/2/18	REDGWICK CONSTRUCTION CO	SNYSRTGAPED #01	Construction Services	21,964.00	0.00	21,964.00	\$21,964.00
xxx307177	10/4/18	AARON'S INDUSTRIAL PUMPING	7/3/2018	Facilities Maintenance & Repair Labor	170.00	0.00	170.00	\$170.00
xxx307178	10/4/18	ACCESS HARDWARE	5700362-IN	Bldg Maint Matls & Supplies	364.07	0.00	364.07	\$364.07
xxx307179	10/4/18	ACME BOILER & WATER HEATING CO	3970	Facilities Maint & Repair - Labor	1,400.00	0.00	1,400.00	\$1,528.35
			3970	Facilities Maint & Repair - Materials	128.35	0.00	128.35	
xxx307180	10/4/18	ACTERRA	SUNNYVALE4	Consultants	5,000.00	0.00	5,000.00	\$5,000.00
xxx307181	10/4/18	AIRGAS-NCN	9080176729	Equipment Rental/Lease	238.80	0.00	238.80	\$238.80
xxx307182	10/4/18	ALL CITY MANAGEMENT SERVICES INC	56231	Contracts/Service Agreements	26,717.20	0.00	26,717.20	\$26,717.20
xxx307183	10/4/18	ALLSTAR FIRE EQUIPMENT INC	205980	Clothing, Uniforms & Access	365.15	0.00	365.15	\$365.15
xxx307184	10/4/18	ARGO CHEMICAL INC	1807144	Chemicals	3,667.30	0.00	3,667.30	\$3,667.30
xxx307185	10/4/18	AXON ENTERPRISE INC	SI-1545907	Software Licensing & Support	292,293.00	0.00	292,293.00	\$292,293.00
xxx307186	10/4/18	BAY AREA WATER SUPPLY & CONSERVATION ACY	3035	Membership Fees	50,192.00	0.00	50,192.00	\$50,251.95
			6932	Membership Fees	59.95	0.00	59.95	
xxx307187	10/4/18	BAY-VALLEY PEST CONTROL INC	0245808	Facilities Maintenance & Repair Labor	42.00	0.00	42.00	\$2,126.00
			0246360	Facilities Maintenance & Repair Labor	43.00	0.00	43.00	
			0246361	Facilities Maintenance & Repair Labor	43.00	0.00	43.00	
			0246362	Facilities Maintenance & Repair Labor	43.00	0.00	43.00	
			0246363	Facilities Maintenance & Repair Labor	63.00	0.00	63.00	
			0246364	Facilities Maintenance & Repair Labor	43.00	0.00	43.00	
			0246365	Facilities Maintenance & Repair Labor	43.00	0.00	43.00	
			0246366	Facilities Maintenance & Repair Labor	59.00	0.00	59.00	
			0246367	Facilities Maintenance & Repair Labor	59.00	0.00	59.00	
			0246368	Facilities Maintenance & Repair Labor	59.00	0.00	59.00	
			0246369	Facilities Maintenance & Repair Labor	59.00	0.00	59.00	
			0246370	Facilities Maintenance & Repair Labor	72.00	0.00	72.00	
			0246371	Facilities Maintenance & Repair Labor	56.00	0.00	56.00	
			0246373	Facilities Maintenance & Repair Labor	32.00	0.00	32.00	
			0246374	Facilities Maintenance & Repair Labor	56.00	0.00	56.00	

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			0246376	Facilities Maintenance & Repair Labor	42.00	0.00	42.00	
			0246377	Facilities Maintenance & Repair Labor	86.00	0.00	86.00	
			0246378	Facilities Maintenance & Repair Labor	42.00	0.00	42.00	
			0246379	Facilities Maintenance & Repair Labor	86.00	0.00	86.00	
			0246380	Facilities Maintenance & Repair Labor	42.00	0.00	42.00	
			0246381	Facilities Maintenance & Repair Labor	86.00	0.00	86.00	
			0246382	Facilities Maintenance & Repair Labor	42.00	0.00	42.00	
			0246383	Facilities Maintenance & Repair Labor	86.00	0.00	86.00	
			0246384	Facilities Maintenance & Repair Labor	42.00	0.00	42.00	
			0246385	Facilities Maintenance & Repair Labor	86.00	0.00	86.00	
			0246386	Facilities Maintenance & Repair Labor	42.00	0.00	42.00	
			0246387	Facilities Maintenance & Repair Labor	86.00	0.00	86.00	
			0246388	Facilities Maintenance & Repair Labor	86.00	0.00	86.00	
			0246408	Services Maintain Land Improv	62.00	0.00	62.00	
			0246411	Services Maintain Land Improv	58.00	0.00	58.00	
			0246420	Services Maintain Land Improv	62.00	0.00	62.00	
xxx307190	10/4/18	BELKORP AG LLC	0246753	Facilities Maintenance & Repair Labor	318.00	0.00	318.00	
			485192	Parts, Vehicles & Motor Equip	173.97	0.00	173.97	\$360.73
			487505	Parts, Vehicles & Motor Equip	186.76	0.00	186.76	
xxx307191	10/4/18	BURTONS FIRE INC	S40996	Parts, Vehicles & Motor Equip	175.03	0.00	175.03	\$839.38
			S41885	Parts, Vehicles & Motor Equip	507.34	0.00	507.34	
			S42174	Parts, Vehicles & Motor Equip	157.01	0.00	157.01	
xxx307192	10/4/18	C OVERAA & CO	PRMRYTRTMT 2#13	Construction Services	5,355,032.20	0.00	5,355,032.20	\$5,355,032.20
xxx307193	10/4/18	CSAC EXCESS INSURANCE AUTHORITY	19401099	Insurances - Employee Assistance Program	13,307.58	0.00	13,307.58	\$13,307.58
xxx307194	10/4/18	CSAC EXCESS INSURANCE AUTHORITY	5063	Insurances - Life/AD&D Insurance	17,058.95	0.00	17,058.95	\$42,523.54
			5063	Insurances - Long Term Disability	25,464.59	0.00	25,464.59	
xxx307195	10/4/18	CALCON SYSTEMS INC	42984	Contracts/Service Agreements	3,558.31	0.00	3,558.31	\$3,558.31
xxx307196	10/4/18	CALTEST ANALYTICAL LABORATORY	590012	Water Lab Services	57.83	0.00	57.83	\$308.75
			590125	Water Lab Services	250.92	0.00	250.92	
xxx307197	10/4/18	CORIX WATER PRODUCTS US INC	17813025525	Construction Services	269.14	0.00	269.14	\$19,325.70

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Payment No.	Payment Date	Vendor Name	Invoice No.	Description	Invoice Amount	Discount Taken	Amount Paid	Payment Total
			17813025764	Water Meters	7,152.99	0.00	7,152.99	
			17813026248	Water Meters	56.70	0.00	56.70	
			17813026280	Water Meters	5,350.21	0.00	5,350.21	
			17813029846	Inventory Purchase	6,073.72	55.72	6,018.00	
			17813030684	Bldg Maint Matls & Supplies	478.66	0.00	478.66	
xxx307198	10/4/18	CYBERSOURCE CORP	235958000757	Software As a Service	75.00	0.00	75.00	\$75.00
xxx307199	10/4/18	DE ANZA APPLIANCE	18-15317836071	Facilities Maint & Repair - Labor	245.00	0.00	245.00	\$616.58
			18-15317836071	Facilities Maint & Repair - Materials	371.58	0.00	371.58	
xxx307200	10/4/18	DELL MARKETING LP	10269437796	Computer Hardware	212.52	0.00	212.52	\$702.99
			10269852901	Computer Hardware	490.47	0.00	490.47	
xxx307201	10/4/18	DELTA DENTAL INSURANCE CO	BE003050141	Insurances - Dental	1,674.83	0.00	1,674.83	\$1,674.83
xxx307202	10/4/18	E-Z-GO TEXTRON INC	91553286	Parts, Vehicles & Motor Equip	442.29	0.00	442.29	\$1,136.58
			91559208	Parts, Vehicles & Motor Equip	58.59	0.00	58.59	
			91563278	Parts, Vehicles & Motor Equip	217.43	0.00	217.43	
			91565104	Parts, Vehicles & Motor Equip	204.46	0.00	204.46	
			91569232	Parts, Vehicles & Motor Equip	45.05	0.00	45.05	
			91570256	Parts, Vehicles & Motor Equip	61.47	0.00	61.47	
			91571512	Parts, Vehicles & Motor Equip	67.70	0.00	67.70	
			91583555	Parts, Vehicles & Motor Equip	39.59	0.00	39.59	
xxx307203	10/4/18	ENVIRONMENTAL RESOURCE ASSOC	877383	General Supplies	332.23	0.00	332.23	\$728.31
			877384	General Supplies	396.08	0.00	396.08	
xxx307204	10/4/18	FEDEX	6-306-50074	Miscellaneous Services	9.18	0.00	9.18	\$9.18
xxx307205	10/4/18	FISHER SCIENTIFIC CO LLC	2644038	General Supplies	1,563.97	0.00	1,563.97	\$1,563.97
xxx307206	10/4/18	FOSTER BROS SECURITY SYSTEMS INC	302572	Bldg Maint Matls & Supplies	79.35	0.00	79.35	\$391.14
			303165	Bldg Maint Matls & Supplies	311.79	0.00	311.79	
xxx307207	10/4/18	GRM INFORMATION MANAGEMENT SERVICES	0094325	Records Related Services	2,280.77	0.00	2,280.77	\$2,280.77
xxx307208	10/4/18	GABRIEL PEREZ	CMS-B1-GP	Rec Instructors/Officials	1,000.00	0.00	1,000.00	\$1,000.00
xxx307209	10/4/18	GRANITEROCK CO	1116743	Materials - Land Improve	1,462.37	0.00	1,462.37	\$4,459.82
			1118791	Materials - Land Improve	159.07	0.00	159.07	
			1119628	Materials - Land Improve	2,838.38	0.00	2,838.38	
xxx307210	10/4/18	GREENSIDE SUPPLY & SERVICE						\$94.18

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			034215A	Inventory Purchase	94.18	0.00	94.18	
xxx307211	10/4/18	HACH CO INC	11048304	General Supplies	357.18	0.00	357.18	\$807.67
			11141923	General Supplies	450.49	0.00	450.49	
xxx307212	10/4/18	HI TECH EMERGENCY VEHICLE SERVICE INC	161708	Auto Maint & Repair - Labor	1,127.00	0.00	1,127.00	\$2,100.91
			161708	Auto Maint & Repair - Materials	973.91	0.00	973.91	
xxx307213	10/4/18	HINDERLITER DE LLAMAS & ASSOC	0028793-IN	Sales And Use Tax	19,601.33	0.00	19,601.33	\$22,151.33
			0028793-IN	Financial Services	2,550.00	0.00	2,550.00	
xxx307214	10/4/18	IDEXX DISTRIBUTION INC	3036612528	General Supplies	943.52	0.00	943.52	\$943.52
xxx307215	10/4/18	IPS GROUP INC	36246	Credit Card Fees	587.54	0.00	587.54	\$587.54
xxx307217	10/4/18	INSIGHT GLOBAL LLC	71	Professional Services	3,410.00	0.00	3,410.00	\$3,410.00
xxx307218	10/4/18	JAMES K SHARP	CMS-B1-JS	Rec Instructors/Officials	1,000.00	0.00	1,000.00	\$1,000.00
xxx307219	10/4/18	JAVELCO EQUIPMENT SERVICE INC	54043	Misc Equip Maint & Repair - Labor	110.00	0.00	110.00	\$244.91
			54043	Misc Equip Maint & Repair - Materials	134.91	0.00	134.91	
xxx307220	10/4/18	JOHNSON ROBERTS & ASSOC INC	136747	Investigation Expense	60.00	0.00	60.00	\$120.00
			136792	Investigation Expense	60.00	0.00	60.00	
xxx307221	10/4/18	KELLY MOORE PAINT CO INC	820-362703	Bldg Maint Matls & Supplies	-74.38	0.00	-74.38	\$254.72
			820-365547	Materials - Land Improve	329.10	0.00	329.10	
xxx307222	10/4/18	KEYSER MARSTON ASSOC INC	0032651	Developer Passthroughs-Downtown Projects	1,765.00	0.00	1,765.00	\$1,765.00
xxx307223	10/4/18	KOHLWEISS AUTO PARTS INC	01PT8323	Inventory Purchase	80.39	1.61	78.78	\$78.78
xxx307224	10/4/18	KRONOS INC	11360000	Computer Software	1,425.00	0.00	1,425.00	\$1,425.00
xxx307225	10/4/18	LANDCARE USA LLC	149476	Services Maintain Land Improv	24,999.00	0.00	24,999.00	\$24,999.00
xxx307226	10/4/18	LANGUAGE SELECT LLC	68597	Miscellaneous Services	526.61	0.00	526.61	\$526.61
xxx307227	10/4/18	MM COMMUNICATIONS	INV-0235	Miscellaneous Services	500.00	0.00	500.00	\$500.00
xxx307228	10/4/18	MALLORY SAFETY & SUPPLY LLC	4522337	Inventory Purchase	113.53	0.00	113.53	\$113.53
xxx307229	10/4/18	MAX DEHART	SEPT/15/2018	Professional Services	200.00	0.00	200.00	\$200.00
xxx307230	10/4/18	METROPOLITAN PLANNING GROUP	1000535	Professional Services	19,493.50	0.00	19,493.50	\$19,493.50
xxx307231	10/4/18	MIALI MARIE REYNOSO	CMS-B1-MR	Rec Instructors/Officials	1,000.00	0.00	1,000.00	\$1,000.00
xxx307232	10/4/18	MIDWEST TAPE	96453774	Library Acquis, Audio/Visual	679.44	0.00	679.44	\$679.44
xxx307233	10/4/18	MISSION LINEN SERVICE	508138833	Laundry & Cleaning Services	61.35	0.00	61.35	\$1,522.86
			508166269	Laundry & Cleaning Services	47.57	0.00	47.57	

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			508166270	Laundry & Cleaning Services	80.54	0.00	80.54	
			508166271	Laundry & Cleaning Services	80.54	0.00	80.54	
			508166275	Laundry & Cleaning Services	64.96	0.00	64.96	
			508190734	Laundry & Cleaning Services	61.35	0.00	61.35	
			508190738	Laundry & Cleaning Services	59.18	0.00	59.18	
			508200386	Laundry & Cleaning Services	47.57	0.00	47.57	
			508200387	Laundry & Cleaning Services	80.54	0.00	80.54	
			508200388	Laundry & Cleaning Services	80.54	0.00	80.54	
			508200392	Laundry & Cleaning Services	64.96	0.00	64.96	
			508231475	Laundry & Cleaning Services	66.83	0.00	66.83	
			508231479	Laundry & Cleaning Services	59.18	0.00	59.18	
			508252001	Laundry & Cleaning Services	47.57	0.00	47.57	
			508252002	Laundry & Cleaning Services	80.54	0.00	80.54	
			508252003	Laundry & Cleaning Services	80.54	0.00	80.54	
			508252007	Laundry & Cleaning Services	64.96	0.00	64.96	
			508288471	Laundry & Cleaning Services	61.35	0.00	61.35	
			508288475	Laundry & Cleaning Services	59.18	0.00	59.18	
			508306841	Laundry & Cleaning Services	47.57	0.00	47.57	
			508306842	Laundry & Cleaning Services	80.54	0.00	80.54	
			508306843	Laundry & Cleaning Services	80.54	0.00	80.54	
			508306847	Laundry & Cleaning Services	64.96	0.00	64.96	
xxx307235	10/4/18	MUNICIPAL MAINTENANCE EQUIPMENT INC	0131437-IN	Miscellaneous Equipment	555.29	0.00	555.29	\$555.29
xxx307236	10/4/18	NEWCOMB MECHANICAL INC	11522	Bldg Maint Matls & Supplies	1,275.30	0.00	1,275.30	\$1,275.30
xxx307237	10/4/18	NORTHERN CALIFORNIA NURSING ACADEMY LLC	0025	DED Services/Training - Training	600.00	0.00	600.00	\$5,400.00
			0027	DED Services/Training - Training	5,400.00	0.00	5,400.00	
			025REV	DED Services/Training - Training	-600.00	0.00	-600.00	
xxx307238	10/4/18	OUTFIT YOUR LOGO	187252	Advertising Services	924.20	0.00	924.20	\$3,205.49
			187253	Advertising Services	2,281.29	0.00	2,281.29	
xxx307239	10/4/18	PDM STEEL SERVICE CENTERS INC	802746-01	Parts, Vehicles & Motor Equip	125.26	0.00	125.26	\$125.26
xxx307240	10/4/18	R. E. BORMANN'S STEEL CO	13736	Bldg Maint Matls & Supplies	376.00	0.00	376.00	\$376.00
xxx307241	10/4/18	REED & GRAHAM INC						\$61,652.43

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			925233	Materials - Land Improve	5,797.31	0.00	5,797.31	
			925409	Materials - Land Improve	4,257.31	0.00	4,257.31	
			925577	Materials - Land Improve	9,017.35	0.00	9,017.35	
			925731	Materials - Land Improve	399.27	0.00	399.27	
			925851	Materials - Land Improve	700.51	0.00	700.51	
			926005	Materials - Land Improve	575.86	0.00	575.86	
			926314	Materials - Land Improve	15,431.18	0.00	15,431.18	
			926435	Materials - Land Improve	429.79	0.00	429.79	
			926436	Materials - Land Improve	3,024.97	0.00	3,024.97	
			929090	Materials - Land Improve	2,993.28	0.00	2,993.28	
			929225	Materials - Land Improve	7,138.97	0.00	7,138.97	
			929359	Materials - Land Improve	3,921.92	0.00	3,921.92	
			929504	Materials - Land Improve	3,514.51	0.00	3,514.51	
			929663	Materials - Land Improve	3,781.09	0.00	3,781.09	
			929818	Materials - Land Improve	669.11	0.00	669.11	
			959504	Materials - Land Improve	0.00	0.00	0.00	
xxx307244	10/4/18	RHYS GRAY	89264401	DED Services/Training - Books	18.42	0.00	18.42	\$52.59
			90547473	DED Services/Training - Books	34.17	0.00	34.17	
xxx307245	10/4/18	SAFEWAY INC	720733-092618	General Supplies	11.98	0.00	11.98	\$11.98
xxx307246	10/4/18	SAN JOSE BMW	4298887	Liability Claims Paid	5,296.73	0.00	5,296.73	\$5,296.73
xxx307247	10/4/18	SANTA CLARA VLY TRANSPORTATION AUTHORITY	0000019084	DED Services/Training - Transportation	80.00	0.00	80.00	\$80.00
xxx307248	10/4/18	SAVIANO CO INC	7594-01	Services Maintain Land Improv	22,500.00	0.00	22,500.00	\$22,500.00
xxx307249	10/4/18	SECURITY ALERT SYSTEMS OF CALIFORNIA INC	071150	Facilities Maintenance & Repair Labor	225.00	0.00	225.00	\$450.00
			071197	Facilities Maintenance & Repair Labor	225.00	0.00	225.00	
xxx307250	10/4/18	SHUMS CODA ASSOC	3641R	Consultants	229,393.92	0.00	229,393.92	\$229,393.92
xxx307251	10/4/18	SIERRA PACIFIC TURF SUPPLY INC	0534676-IN	Facilities Maint & Repair - Materials	1,645.77	0.00	1,645.77	\$1,645.77
xxx307252	10/4/18	SILICON VALLEY POLYTECHNIC INSTITUTE	09192018-588	DED Services/Training - Training	300.00	0.00	300.00	\$600.00
			09192018-589	DED Services/Training - Training	300.00	0.00	300.00	
xxx307253	10/4/18	SITEONE LANDSCAPE SUPPLY LLC	87039833-001	Inventory Purchase	1,395.56	0.00	1,395.56	\$1,395.56
xxx307254	10/4/18	SLAKEY BROTHERS INC	21487350-00	Bldg Maint Matls & Supplies	14.41	0.00	14.41	\$14.41
xxx307255	10/4/18	SRIXON GOLF						\$323.85

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Payment No.	Payment Date	Vendor Name	Invoice No.	Description	Invoice Amount	Discount Taken	Amount Paid	Payment Total
			5458209 SO	Inventory Purchase	323.85	0.00	323.85	
xxx307256	10/4/18	STATCOMM INC	130688	Facilities Maintenance & Repair Labor	6,140.00	0.00	6,140.00	\$6,140.00
xxx307257	10/4/18	TAYLORMADE GOLF CO	33385360	Inventory Purchase	-328.70	0.00	-328.70	\$2,080.43
			33387792	Inventory Purchase	378.69	7.37	371.32	
			33395400	Inventory Purchase	378.78	7.37	371.41	
			33398800	Inventory Purchase	1,699.67	33.27	1,666.40	
xxx307258	10/4/18	TREASA HOVORKA	325337-2243427	DED Services/Training - Books	45.99	0.00	45.99	\$256.25
			398139-6900264	DED Services/Training - Books	210.26	0.00	210.26	
xxx307259	10/4/18	TROY T OBRERO	CMS-B1-TO	Rec Instructors/Officials	1,000.00	0.00	1,000.00	\$1,000.00
xxx307260	10/4/18	TURF & INDUSTRIAL EQUIPMENT CO	IV27901	Inventory Purchase	497.04	0.00	497.04	\$497.04
xxx307261	10/4/18	UC REGENTS	1035473-184	DED Services/Training - Training	4,423.50	0.00	4,423.50	\$4,423.50
xxx307262	10/4/18	UCP INC	62061	Software As a Service	1,710.00	0.00	1,710.00	\$1,710.00
xxx307263	10/4/18	UNITED SITE SERVICES INC	114-7271604	Facilities Maintenance & Repair Labor	290.05	0.00	290.05	\$290.05
xxx307264	10/4/18	UNIVERSITY OF CALIFORNIA SANTA CRUZ	57956	DED Services/Training - Training	548.00	0.00	548.00	\$23,134.00
			58065	DED Services/Training - Training	366.50	0.00	366.50	
			58125	DED Services/Training - Training	300.00	0.00	300.00	
			58277	DED Services/Training - Training	4,041.00	0.00	4,041.00	
			58302	DED Services/Training - Training	3,924.00	0.00	3,924.00	
			58304	DED Services/Training - Training	4,954.50	0.00	4,954.50	
			58308	DED Services/Training - Training	3,600.00	0.00	3,600.00	
			58310	DED Services/Training - Training	5,400.00	0.00	5,400.00	
xxx307265	10/4/18	WEST LITE SUPPLY CO INC	72675H	Bldg Maint Matls & Supplies	48.51	0.00	48.51	\$48.51
xxx307266	10/4/18	WILSON SPORTING GOODS CO	4526048531	General Supplies	2,268.00	0.00	2,268.00	\$2,268.00
xxx307267	10/4/18	WINSUPPLY OF SILICON VALLEY	692271 00	Bldg Maint Matls & Supplies	19.00	0.00	19.00	\$19.00
xxx307268	10/4/18	WITMER TYSON IMPORTS INC	T12706	Canine Program Expenditures	650.00	0.00	650.00	\$650.00
xxx307269	10/4/18	YAMAHA GOLF CARS OF CALIFORNIA INC	L31458	Misc Equip Maint & Repair - Labor	120.00	0.00	120.00	\$1,460.06
			L31458	Facilities Maint & Repair - Materials	655.73	0.00	655.73	
			L31459	Misc Equip Maint & Repair - Labor	400.00	0.00	400.00	
			L31459	Facilities Maint & Repair - Materials	284.33	0.00	284.33	
xxx307270	10/4/18	SHARON RASMUSSEN	174299-20040	Refund Utility Account Credit	329.06	0.00	329.06	\$329.06
xxx002731	10/2/18	INTERNAL REVENUE SERVICE						\$746,355.15

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			950002731	Retirement Benefits - Deferred Comp - City Portion	13,405.49	0.00	13,405.49	
			950002731	Retirement Benefits - Misc Tier 1 & 2 Employer Required Cont.	176,730.78	0.00	176,730.78	
			950002731	Retirement Benefits - Misc Tier 1&2 Employer Paid Member Cont.	70,802.37	0.00	70,802.37	
			950002731	Retirement Benefits - Misc PEPRA Employer Required Cont.	77,971.41	0.00	77,971.41	
			950002731	Retirement Benefits - Safety Tier 1&2 Employer Required Cont.	210,177.13	0.00	210,177.13	
			950002731	Retirement Benefits - Safety Tier 1&2 Emplyr Paid Member Cont	92,391.97	0.00	92,391.97	
			950002731	Retirement Benefits - Safety PEPRA Employer Required Cont.	42,910.87	0.00	42,910.87	
			950002731	Retirement Benefits - PARS	1,539.79	0.00	1,539.79	
			950002731	Employer Taxes - FICA - Total	346.19	0.00	346.19	
			950002731	Employer Taxes - Medicare - Total	60,079.15	0.00	60,079.15	
xxx906446	10/5/18	ACCLAMATION INSURANCE MANAGEMENT		Workers' Compensation - Claims	115,594.30	0.00	115,594.30	\$115,594.30
Grand Total Payment Amount								<u>\$8,622,166.41</u>

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City of Sunnyvale

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xxx307271	10/9/18	3SOUTH LLC	0218SDPSR440G N	Miscellaneous Equipment	29,990.00	0.00	29,990.00	\$29,990.00
xxx307272	10/9/18	ACE FIRE EQUIPMENT & SERVICE CO INC	10244	Inventory Purchase	803.64	0.00	803.64	\$803.64
xxx307273	10/9/18	ACOM SOLUTIONS INC	0310971-IN	Printing & Related Services	364.15	0.00	364.15	\$364.15
xxx307274	10/9/18	AIRGAS USA LLC	9080176728	Inventory Purchase	272.27	0.00	272.27	\$785.36
			9956764117	Equipment Rental/Lease	513.09	0.00	513.09	
xxx307275	10/9/18	ALPINE TECHNICAL SERVICES	24501	Equipment Rental/Lease	2,398.00	0.00	2,398.00	\$2,398.00
xxx307276	10/9/18	APPLEONE EMPLOYMENT SERVICES	01-4986504	Salaries - Contract Personnel	7,471.43	0.00	7,471.43	\$7,496.01
			01-4986504	Travel Related Services	11.00	0.00	11.00	
			01-4986504	Travel Expenses - Mileage	13.58	0.00	13.58	
xxx307277	10/9/18	B & A FRICTION MATERIALS INC	598665	Parts, Vehicles & Motor Equip	27.95	0.00	27.95	\$27.95
xxx307278	10/9/18	BAY AREA NEWS GROUP DIGITAL FIRST MEDIA	0006182323	Advertising Services	84.00	0.00	84.00	\$1,024.72
			0006182537	Advertising Services	531.72	0.00	531.72	
			0006191865	Advertising Services	84.00	0.00	84.00	
			0006202151	Advertising Services	77.00	0.00	77.00	
			0006206856	Advertising Services	82.00	0.00	82.00	
			0006207684	Advertising Services	64.00	0.00	64.00	
			0006207693	Advertising Services	102.00	0.00	102.00	
xxx307279	10/9/18	CALIFORNIA TRENCHLESS INC	SNTRYSWRMN #01	Construction Services	464,483.25	0.00	464,483.25	\$464,483.25
xxx307280	10/9/18	CONTRACTOR COMPLIANCE & MONITORING INC	10492	Consultants	2,000.00	0.00	2,000.00	\$2,000.00
xxx307281	10/9/18	CORIX WATER PRODUCTS US INC	17813030119	Construction Services	2,681.81	0.00	2,681.81	\$6,689.77
			17813031457	Construction Services	4,007.96	0.00	4,007.96	
xxx307282	10/9/18	D & M TRAFFIC SERVICES INC	60301	Inventory Purchase	1,303.64	0.00	1,303.64	\$1,303.64
xxx307283	10/9/18	FERGUSON ENTERPRISES INC	1386512	Water Meter Boxes, Vaults, and Lids	33,264.00	0.00	33,264.00	\$60,847.92
			1390429	Water Meters	13,824.00	0.00	13,824.00	
			1403943	Construction Services	13,759.92	0.00	13,759.92	
xxx307284	10/9/18	GEORGE R GUINDON	CMS-B1-66	Rec Instructors/Officials	1,000.00	0.00	1,000.00	\$1,000.00
xxx307285	10/9/18	GREENSIDE SUPPLY & SERVICE	034849	Inventory Purchase	581.13	0.00	581.13	\$581.13

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xxx307286	10/9/18	IMPERIAL MAINTENANCE SERVICES INC	4-RE	Professional Services	59,794.00	0.00	59,794.00	\$62,009.40
			SES#0001-RE	Professional Services	667.80	0.00	667.80	
			SES#0002-RE	Professional Services	1,547.60	0.00	1,547.60	
xxx307287	10/9/18	INGRAM LIBRARY SERVICES INC	35970831	Library Acquisitions, Books	-7.61	0.00	-7.61	\$40,293.79
			35970831	Library Materials Preprocessing	-10.14	0.00	-10.14	
			36257680	Library Acquisitions, Books	270.02	0.00	270.02	
			36257680	Library Materials Preprocessing	17.99	0.00	17.99	
			36257880REV	Library Acquisitions, Books	-270.02	0.00	-270.02	
			36257880REV	Library Materials Preprocessing	-17.99	0.00	-17.99	
			36719531	Library Acquisitions, Books	309.67	0.00	309.67	
			36719531	Library Materials Preprocessing	19.11	0.00	19.11	
			36719535	Library Acquisitions, Books	965.33	0.00	965.33	
			36719535	Library Materials Preprocessing	979.27	0.00	979.27	
			36719536	Library Acquisitions, Books	3,917.24	0.00	3,917.24	
			36719536	Library Materials Preprocessing	1,300.55	0.00	1,300.55	
			36719537	Library Acquisitions, Books	80.95	0.00	80.95	
			36719538	Library Acquisitions, Books	6,572.62	0.00	6,572.62	
			36719538	Library Materials Preprocessing	369.92	0.00	369.92	
			36719539	Library Acquisitions, Books	5,389.24	0.00	5,389.24	
			36719539	Library Periodicals/Databases	297.23	0.00	297.23	
			36719539	Library Materials Preprocessing	352.88	0.00	352.88	
			36719540	Library Acquisitions, Books	7,015.47	0.00	7,015.47	
			36719540	Library Materials Preprocessing	1,271.02	0.00	1,271.02	
			36719541	Library Acquisitions, Books	10,022.59	0.00	10,022.59	
			36719541	Library Materials Preprocessing	1,448.45	0.00	1,448.45	
xxx307289	10/9/18	JOSHUA LEPISI	CMS-B1-JL	Rec Instructors/Officials	1,000.00	0.00	1,000.00	\$1,000.00
xxx307290	10/9/18	KMVT COMMUNITY TELEVISION	7317	Engineering Services	45.00	0.00	45.00	\$45.00
xxx307291	10/9/18	KOHLWEISS AUTO PARTS INC	01PT9875	Inventory Purchase	830.53	16.61	813.92	\$813.92
xxx307292	10/9/18	L N CURTIS & SONS INC	429102	General Supplies	802.24	0.00	802.24	\$933.04
			INV218545	General Supplies	130.80	0.00	130.80	
xxx307293	10/9/18	LA OFERTA	13249	Advertising Services	5,080.00	0.00	5,080.00	\$7,722.00

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			13250	Advertising Services	2,642.00	0.00	2,642.00	
xxx307294	10/9/18	LANDCARE USA LLC	163310	Construction Services	458.32	0.00	458.32	\$916.64
			170793	Construction Services	458.32	0.00	458.32	
xxx307295	10/9/18	LAVANYA DUGGIRALA	592618-9256267	DED Services/Training - Books	31.13	0.00	31.13	\$49.92
			741529-9366605	DED Services/Training - Books	18.79	0.00	18.79	
xxx307296	10/9/18	LENIS MAINTENANCE SERVICES INC	6036	Laundry & Cleaning Services	2,150.00	0.00	2,150.00	\$2,150.00
xxx307297	10/9/18	MAHAN AND SONS INC	1629	Services Maintain Land Improv	1,000.00	0.00	1,000.00	\$1,000.00
xxx307298	10/9/18	MOTT MACDONALD LLC	304781-50	Engineering Services	912.30	0.00	912.30	\$912.30
xxx307299	10/9/18	NAPA AUTO PARTS	5983-424589	Parts, Vehicles & Motor Equip	142.67	0.00	142.67	\$200.70
			5983-425028	Parts, Vehicles & Motor Equip	58.03	0.00	58.03	
xxx307300	10/9/18	NATIONAL CONSTRUCTION RENTALS INC	5156061	Equipment Rental/Lease	235.40	0.00	235.40	\$235.40
xxx307301	10/9/18	NIELSEN MERKSAMER PARRINELLO GROSS &	181836	Legal Services	8,636.00	0.00	8,636.00	\$8,636.00
xxx307302	10/9/18	NUTRIEN AG SOLUTIONS, INC	37475079	Facilities Maint & Repair - Materials	1,117.25	0.00	1,117.25	\$1,117.25
xxx307303	10/9/18	OTIS ELEVATOR COMPANY	SJ18471001	Facilities Maintenance & Repair Labor	749.94	0.00	749.94	\$749.94
xxx307304	10/9/18	P&R PAPER SUPPLY CO INC	30216042-00	Inventory Purchase	1,318.46	0.00	1,318.46	\$1,318.46
xxx307305	10/9/18	PAUL THAYNE	CK REQ 19-042	DED Services/Training - Books	302.00	0.00	302.00	\$302.00
xxx307306	10/9/18	PETERSON POWER SYSTEMS INC	R3224407	Equipment Rental/Lease	8,569.14	0.00	8,569.14	\$17,138.28
			R3224408	Equipment Rental/Lease	8,569.14	0.00	8,569.14	
xxx307307	10/9/18	REED & GRAHAM INC	928091	Materials - Land Improve	877.17	0.00	877.17	\$1,265.60
			929503	Materials - Land Improve	388.43	0.00	388.43	
xxx307308	10/9/18	REEDS INDOOR RANGE	556399	Real Property Rental/Lease	70.00	0.00	70.00	\$70.00
xxx307309	10/9/18	RENNE PUBLIC LAW GROUP	1309	Legal Services	180.00	0.00	180.00	\$180.00
xxx307310	10/9/18	ROSE LAW A PROFESSIONAL CORPORATION	20180127	Investigation Expense	5,606.33	0.00	5,606.33	\$5,606.33
xxx307311	10/9/18	ROSS RECREATION EQUIPMENT CO INC	I13662	Materials - Land Improve	1,076.45	0.00	1,076.45	\$1,076.45
xxx307312	10/9/18	SAFEWAY INC	723510-091318	Special Events	65.32	0.00	65.32	\$207.17
			802108-091818	Special Events	20.26	0.00	20.26	
			808085-092018	Employee Recognition Expenses	121.59	0.00	121.59	
xxx307313	10/9/18	SANTA CLARA VALLEY WATER DISTRICT	GM101098	Taxes & Licenses - Misc	12,657.98	0.00	12,657.98	\$12,657.98

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xxx307314	10/9/18	SECURITY ALERT SYSTEMS OF CALIFORNIA INC	071225	Facilities Maintenance & Repair Labor	250.00	0.00	250.00	\$430.00
			071229	Facilities Maintenance & Repair Labor	180.00	0.00	180.00	
xxx307315	10/9/18	SIERRA TRAFFIC MARKINGS INC	GRNPVMTMR KG#03	Construction Services	19,950.00	0.00	19,950.00	\$19,950.00
xxx307316	10/9/18	SILICON VALLEY SECURITY & PATROL INC	2038290	Professional Services	375.00	0.00	375.00	\$375.00
xxx307317	10/9/18	STATCOMM INC	132417	Facilities Maintenance & Repair Labor	101.25	0.00	101.25	\$101.25
xxx307318	10/9/18	STEELHEAD ENGINEERS INC	1818104	Engineering Services	225.00	0.00	225.00	\$225.00
xxx307319	10/9/18	STRATEGIC ENERGY INNOVATIONS	2847	Professional Services	74,350.00	0.00	74,350.00	\$74,350.00
xxx307320	10/9/18	STUDIO EM GRAPHIC DESIGN	17280	Graphics Services	81.75	0.00	81.75	\$327.00
			17282	Graphics Services	245.25	0.00	245.25	
xxx307321	10/9/18	SUPPLYWORKS	457434702	Inventory Purchase	169.39	1.55	167.84	\$515.60
			457434710	Inventory Purchase	205.23	1.88	203.35	
			457434728	Inventory Purchase	145.75	1.34	144.41	
xxx307322	10/9/18	THOMSON REUTERS WEST	838979065	Books & Publications	1,772.62	0.00	1,772.62	\$1,772.62
xxx307323	10/9/18	UNIQUE MANAGEMENT SERVICES INC	468386	Financial Services	349.05	0.00	349.05	\$349.05
xxx307324	10/9/18	UNITED SITE SERVICES INC	114-7359613	Equipment Rental/Lease	245.00	0.00	245.00	\$245.00
xxx307325	10/9/18	VWR INTERNATIONAL LLC	8083514935	General Supplies	88.95	0.00	88.95	\$3,060.98
			8083555540	Chemicals	67.67	0.00	67.67	
			8083564093	General Supplies	97.48	0.00	97.48	
			8083573533	General Supplies	890.75	0.00	890.75	
			8083580093	Chemicals	44.15	0.00	44.15	
			8083673321	General Supplies	193.06	0.00	193.06	
			8083686013	General Supplies	942.10	0.00	942.10	
			8083844524	General Supplies	33.31	0.00	33.31	
			8083864084	General Supplies	142.03	0.00	142.03	
			8083866666	General Supplies	82.64	0.00	82.64	
			8083866667	General Supplies	478.84	0.00	478.84	
xxx307326	10/9/18	VALLEY OIL CO	941887	Inventory Purchase	16,143.26	0.00	16,143.26	\$16,143.26
xxx307327	10/9/18	VERIZON WIRELESS	9814297447	Utilities - Mobile Phones - City Mobile Phones	50.34	0.00	50.34	\$50.34
xxx307328	10/9/18	WHCI PLUMBING SUPPLY	S2343494.001	Bldg Maint Matls & Supplies	-846.29	0.00	-846.29	\$505.70

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			S2351443.001	Bldg Maint Matls & Supplies	1,351.99	0.00	1,351.99	
xxx307329	10/9/18	WOWZY CREATION CORP	90985	Customized Products	120.31	0.00	120.31	\$120.31
xxx307330	10/9/18	WALTS CYCLE	389-2018	Clothing, Uniforms & Access	168.91	0.00	168.91	\$168.91
xxx307331	10/9/18	WENDY CASTANEDA ESCOBAR	17787208463-RE	DED Services/Training - Books	155.00	0.00	155.00	\$155.00
xxx307332	10/9/18	WEST LITE SUPPLY CO INC	60509C	Electrical Parts & Supplies	337.36	0.00	337.36	\$337.36
xxx307333	10/9/18	YORKE ENGINEERING LLC	16988	Professional Services	964.50	0.00	964.50	\$964.50
xxx307334	10/9/18	WAITER.COM INC	10919461558	Food Products	79.26	0.00	79.26	\$79.26
xxx307335	10/9/18	DOUGLAS MCCONNELL	11-07-SV	Excursions	1,000.00	0.00	1,000.00	\$1,000.00
xxx307336	10/9/18	PACIFIC GAS & ELECTRIC CO	03142830050818	Utilities - Electric	19,008.18	0.00	19,008.18	\$34,187.12
			03153947310818	Utilities - Electric	10,824.16	0.00	10,824.16	
			03955461530918	Utilities - Electric	1,783.80	0.00	1,783.80	
			03958470700918	Utilities - Electric	2,530.27	0.00	2,530.27	
			53350770050918	Fuel, Oil & Lubricants	40.71	0.00	40.71	
xxx307337	10/9/18	ART CONSTRUCTION	2018-7575	Legislative Actions	1,071.75	0.00	1,071.75	\$1,071.75
xxx307338	10/9/18	BACKFLOW PREVENTION SPECIALISTS INC	MTR16153213	Cross-Connection Control - Field Inspection	328.00	0.00	328.00	\$328.00
xxx307339	10/9/18	CCI EVELYN LP	2013-9466	Deposits Payable - Miscellaneous	4,120.00	0.00	4,120.00	\$4,120.00
xxx307340	10/9/18	KEREN S BOIMANN	851444	Lib - Lost & Damaged Circulation	10.00	0.00	10.00	\$10.00
xxx307341	10/9/18	VALLEY HEATING COOLING & ELECTRICAL	2018-4143	Permit - Building	393.42	0.00	393.42	\$393.42
xxx307342	10/9/18	CORIX WATER PRODUCTS US INC	17813030576	Inventory Purchase	7,784.21	71.41	7,712.80	\$15,489.76
			17813031321	Inventory Purchase	7,848.97	72.01	7,776.96	
xxx307343	10/10/18	UNITED STATES POSTAL SERVICE	2018ELECTION	Mailing & Delivery Services	11,589.00	0.00	11,589.00	\$11,589.00
xxx307344	10/11/18	AAA SPEEDY SMOG TEST ONLY STATION	027864	Auto Maint & Repair - Labor	40.00	0.00	40.00	\$40.00
xxx307346	10/11/18	AT&T	00001183467	Utilities - Telephone	308.00	0.00	308.00	\$308.00
xxx307347	10/11/18	ACCESS HARDWARE	5701750-IN	Bldg Maint Matls & Supplies	278.56	0.00	278.56	\$278.56
xxx307348	10/11/18	ACOM SOLUTIONS INC	0309142-IN	Printing & Related Services	1,010.77	0.00	1,010.77	\$1,010.77
xxx307349	10/11/18	AD CLUB	287748	Advertising Services	980.00	0.00	980.00	\$1,625.60
			287842	Advertising Services	645.60	0.00	645.60	
xxx307350	10/11/18	AIR COOLED ENGINES INC	81166	Parts, Vehicles & Motor Equip	12.29	0.00	12.29	\$12.29
xxx307351	10/11/18	AIR EXCHANGE INC	43073	Facilities Maint & Repair - Labor	125.00	0.00	125.00	\$615.50
			43073	Facilities Maint & Repair - Materials	490.50	0.00	490.50	

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xxx307352	10/11/18	AIRGAS USA LLC	9080668275	Equipment Rental/Lease	342.16	0.00	342.16	\$342.16
xxx307353	10/11/18	ALAMEDA COUNTY	112-1808057	Software As a Service	2,030.26	0.00	2,030.26	\$2,030.26
xxx307354	10/11/18	ALL CITY MANAGEMENT SERVICES INC	56051	Contracts/Service Agreements	23,905.11	0.00	23,905.11	\$23,905.11
xxx307355	10/11/18	APPLEONE EMPLOYMENT SERVICES	01-4997935	Salaries - Contract Personnel	7,813.49	0.00	7,813.49	\$7,817.82
			01-4997935	Travel Expenses - Mileage	4.33	0.00	4.33	
xxx307357	10/11/18	ASSETWORKS	604-3639	Training and Conferences	7,077.37	0.00	7,077.37	\$7,077.37
xxx307358	10/11/18	ASSOCIATED INFRASTRUCTURE MGMT SERVICES	2018-013	Consultants	5,503.95	0.00	5,503.95	\$5,503.95
xxx307359	10/11/18	AXON ENTERPRISE INC	SI-1545907A	Clothing, Uniforms & Access	1,438.80	0.00	1,438.80	\$1,438.80
xxx307360	10/11/18	BSK ASSOCIATES	A826670	Water Lab Services	675.00	0.00	675.00	\$675.00
xxx307361	10/11/18	BAY AREA POLYGRAPH	843	Investigation Expense	1,300.00	0.00	1,300.00	\$1,300.00
xxx307362	10/11/18	BAY COUNTIES WASTE SERVICES	026520	Recycling Services	62,915.33	0.00	62,915.33	\$62,915.33
xxx307363	10/11/18	BAY-VALLEY PEST CONTROL INC	0241580	Facilities Maintenance & Repair Labor	64.00	0.00	64.00	\$1,170.00
			02416580-REV	Facilities Maintenance & Repair Labor	-64.00	0.00	-64.00	
			0243115	Facilities Maintenance & Repair Labor	43.00	0.00	43.00	
			0243116	Facilities Maintenance & Repair Labor	43.00	0.00	43.00	
			0243117	Facilities Maintenance & Repair Labor	43.00	0.00	43.00	
			0243118	Facilities Maintenance & Repair Labor	63.00	0.00	63.00	
			0243119	Facilities Maintenance & Repair Labor	43.00	0.00	43.00	
			0243120	Facilities Maintenance & Repair Labor	43.00	0.00	43.00	
			0243121	Facilities Maintenance & Repair Labor	59.00	0.00	59.00	
			0243122	Facilities Maintenance & Repair Labor	59.00	0.00	59.00	
			0243123	Facilities Maintenance & Repair Labor	59.00	0.00	59.00	
			0243124	Facilities Maintenance & Repair Labor	59.00	0.00	59.00	
			0243125	Facilities Maintenance & Repair Labor	72.00	0.00	72.00	
			0243128	Facilities Maintenance & Repair Labor	32.00	0.00	32.00	
			0243129	Facilities Maintenance & Repair Labor	56.00	0.00	56.00	
			0243131	Facilities Maintenance & Repair Labor	42.00	0.00	42.00	
			0243132	Facilities Maintenance & Repair Labor	42.00	0.00	42.00	
			0243133	Facilities Maintenance & Repair Labor	42.00	0.00	42.00	
			0243134	Facilities Maintenance & Repair Labor	42.00	0.00	42.00	

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			0243135	Facilities Maintenance & Repair Labor	42.00	0.00	42.00	
			0243136	Facilities Maintenance & Repair Labor	42.00	0.00	42.00	
			0243158	Services Maintain Land Improv	62.00	0.00	62.00	
			0243161	Services Maintain Land Improv	58.00	0.00	58.00	
			0243169	Services Maintain Land Improv	62.00	0.00	62.00	
			0243310	Facilities Maintenance & Repair Labor	514.00	0.00	514.00	
			0244310-REV	Facilities Maintenance & Repair Labor	-514.00	0.00	-514.00	
			0246419	Services Maintain Land Improv	62.00	0.00	62.00	
xxx307366	10/11/18	BEE FRIENDLY HONEY BEE MGMT SOLUTIONS	5693	Services Maintain Land Improv	125.00	0.00	125.00	\$125.00
xxx307367	10/11/18	BELLECCI & ASSOC INC	16155-I	Engineering Services	7,834.75	0.00	7,834.75	\$7,834.75
xxx307368	10/11/18	BOUND TREE MEDICAL LLC	82999187	Inventory Purchase	3,989.40	0.00	3,989.40	\$3,989.40
xxx307369	10/11/18	BRUCE BARTON PUMP SERVICE INC	0097928-IN	Misc Equip Maint & Repair - Materials	1,635.91	0.00	1,635.91	\$4,120.84
			0098004-IN	Bldg Maint Matls & Supplies	2,484.93	0.00	2,484.93	
xxx307370	10/11/18	BURKE WILLIAMS & SORENSEN LLP	231506	Legal Services	2,478.70	0.00	2,478.70	\$2,478.70
xxx307371	10/11/18	CALLANDER ASSOC	18054-1	Consultants	16,998.75	0.00	16,998.75	\$16,998.75
xxx307372	10/11/18	CENTURY GRAPHICS	49697	Miscellaneous Services	192.58	0.00	192.58	\$1,993.79
			49862	Clothing, Uniforms & Access	1,745.11	0.00	1,745.11	
			50016	Clothing, Uniforms & Access	56.10	0.00	56.10	
xxx307373	10/11/18	CITY & COUNTY OF SAN FRANCISCO	REGP-01	Contracts/Service Agreements	7,215.98	0.00	7,215.98	\$7,215.98
xxx307374	10/11/18	CITY OF SAN JOSE - WORK2FUTURE	0001	Contracts/Service Agreements	15,935.54	0.00	15,935.54	\$25,155.74
			33	Contracts/Service Agreements	9,220.20	0.00	9,220.20	
xxx307375	10/11/18	CLAY PLANET	220277	General Supplies	318.66	0.00	318.66	\$318.66
xxx307376	10/11/18	CONSOLIDATED PARTS INC	5049976	Electrical Parts & Supplies	1,444.25	0.00	1,444.25	\$1,444.25
xxx307377	10/11/18	CORIX WATER PRODUCTS US INC	17813027539	Materials - Land Improve	324.30	0.00	324.30	\$324.30
xxx307378	10/11/18	D & M TRAFFIC SERVICES INC	60847	Inventory Purchase	227.81	0.00	227.81	\$227.81
xxx307379	10/11/18	DAVID J POWERS & ASSOC INC	22799	Developer Passthroughs-Downtown Projects	24,800.05	0.00	24,800.05	\$24,800.05
xxx307380	10/11/18	DEL GAVIO GROUP	8641	General Supplies	1,248.65	0.00	1,248.65	\$1,248.65
xxx307381	10/11/18	DELL MARKETING LP	10270350772	Computer Hardware	1,858.73	0.00	1,858.73	\$3,261.50
			10270684819	Computer Hardware	121.85	0.00	121.85	
			10270719362	Computer Hardware	196.61	0.00	196.61	

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			10271003030	Computer Hardware	1,084.31	0.00	1,084.31	
xxx307382	10/11/18	DEPARTMENT OF TRANSPORTATION	19001414	Utilities - Electric	18,670.61	0.00	18,670.61	\$18,670.61
xxx307383	10/11/18	EOA INC	SU60-0818	Professional Services	3,406.17	0.00	3,406.17	\$3,406.17
xxx307384	10/11/18	ESBRO	56778	Chemicals	1,462.94	0.00	1,462.94	\$1,462.94
xxx307385	10/11/18	F&M BANK	PRMRYTRTMT	Construction Project Contract Retainage	158,102.30	0.00	158,102.30	\$439,946.10
			2#12					
			PRMRYTRTMT	Construction Project Contract Retainage	281,843.80	0.00	281,843.80	
			2#13					
xxx307386	10/11/18	FEDEX	6-306-83944	Postage	6.51	0.00	6.51	\$6.51
xxx307387	10/11/18	FEHR & PEERS	124412	Developer Passthroughs-Downtown Projects	18,980.56	0.00	18,980.56	\$29,355.62
			124886	Developer Passthroughs-Downtown Projects	10,375.06	0.00	10,375.06	
xxx307388	10/11/18	FILEMAKER INC	5457676	Software Licensing & Support	857.00	0.00	857.00	\$857.00
xxx307389	10/11/18	FIRE & RISK ALLIANCE LLC	132-001-27	Miscellaneous Services	53,095.22	0.00	53,095.22	\$53,095.22
xxx307390	10/11/18	FIRST ALARM SECURITY & PATROL INC	570655	Contracts/Service Agreements	4,897.75	0.00	4,897.75	\$4,897.75
xxx307391	10/11/18	FITGUARD INC	0000149142	Misc Equip Maint & Repair - Labor	125.00	0.00	125.00	\$657.85
			0000149142	Misc Equip Maint & Repair - Materials	397.85	0.00	397.85	
			0000149447	Professional Services	135.00	0.00	135.00	
xxx307392	10/11/18	FOSTER BROS SECURITY SYSTEMS INC	303187	Bldg Maint Matls & Supplies	6.43	0.00	6.43	\$88.93
			303356	Facilities Maintenance & Repair Labor	82.50	0.00	82.50	
xxx307393	10/11/18	GALE/CENGAGE LEARNING	65168112	Library Acquisitions, Books	108.08	0.00	108.08	\$194.37
			65201911	Library Acquisitions, Books	27.89	0.00	27.89	
			65234482	Library Acquisitions, Books	58.40	0.00	58.40	
xxx307394	10/11/18	GARDA	10430883	Financial Services	4,380.32	0.00	4,380.32	\$4,380.32
xxx307395	10/11/18	GARDENLAND POWER EQUIPMENT	607438	Misc Equip Maint & Repair - Labor	89.93	0.00	89.93	\$4,613.37
			607438	Misc Equip Maint & Repair - Materials	124.81	0.00	124.81	
			608570	Materials - Land Improve	163.81	0.00	163.81	
			608576	Misc Equip Maint & Repair - Labor	224.93	0.00	224.93	
			610959	Misc Equip Maint & Repair - Materials	329.30	0.00	329.30	
			610974	Misc Equip Maint & Repair - Materials	110.62	0.00	110.62	
			610990	Misc Equip Maint & Repair - Materials	1,829.07	0.00	1,829.07	

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For Payments Dated 10/7/2018 through 10/13/2018

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Payment No.	Payment Date	Vendor Name	Invoice No.	Description	Invoice Amount	Discount Taken	Amount Paid	Payment Total
			613204	Hand Tools	1,495.27	0.00	1,495.27	
			613207	Hand Tools	53.37	0.00	53.37	
			614411	Misc Equip Maint & Repair - Materials	192.26	0.00	192.26	
xxx307396	10/11/18	GLOBAL ACCESS INC	16572	Software As a Service	236.00	0.00	236.00	\$236.00
xxx307397	10/11/18	GOLDFARB LIPMAN ATTORNEYS	128309	Legal Services	652.05	0.00	652.05	\$799.55
			128310	Legal Services	88.50	0.00	88.50	
			128332	Legal Services	59.00	0.00	59.00	
xxx307398	10/11/18	GRANITEROCK CO	1132620	Materials - Land Improve	750.51	0.00	750.51	\$750.51
xxx307399	10/11/18	HACH CO INC	11155175	General Supplies	476.26	0.00	476.26	\$476.26
xxx307400	10/11/18	HI-TECH OPTICAL INC	767015	Benefits and Incentives - Prescription Safety Glasses	120.00	0.00	120.00	\$940.00
			770190	Benefits and Incentives - Prescription Safety Glasses	120.00	0.00	120.00	
			770191	Benefits and Incentives - Prescription Safety Glasses	120.00	0.00	120.00	
			770192	Benefits and Incentives - Prescription Safety Glasses	120.00	0.00	120.00	
			770193	Benefits and Incentives - Prescription Safety Glasses	120.00	0.00	120.00	
			770199	Benefits and Incentives - Prescription Safety Glasses	120.00	0.00	120.00	
			770200	Benefits and Incentives - Prescription Safety Glasses	120.00	0.00	120.00	
			772540	Benefits and Incentives - Prescription Safety Glasses	100.00	0.00	100.00	
xxx307401	10/11/18	HIGH LINE CORP	21851	Professional Services	600.00	0.00	600.00	\$1,200.00
			21864	Professional Services	600.00	0.00	600.00	
xxx307402	10/11/18	ICE MACHINE RENTALS	46407	Miscellaneous Services	150.08	0.00	150.08	\$150.08
xxx307403	10/11/18	IMPERIAL SPRINKLER SUPPLY	3448538-00	Materials - Land Improve	2,799.12	0.00	2,799.12	\$2,799.12
xxx307404	10/11/18	J N ABBOTT DISTRIBUTOR INC	269809	Fuel, Oil & Lubricants	3,581.83	0.00	3,581.83	\$3,581.83
xxx307405	10/11/18	JMB CONSTRUCTION INC	WLFVLYNH2O #11	Construction Services	51,497.60	0.00	51,497.60	\$51,497.60
xxx307406	10/11/18	JAVELCO EQUIPMENT SERVICE INC	54309	Misc Equip Maint & Repair - Labor	220.00	0.00	220.00	\$771.67

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			54309	Misc Equip Maint & Repair - Materials	163.83	0.00	163.83	
			54344	Misc Equip Maint & Repair - Labor	110.00	0.00	110.00	
			54344	Misc Equip Maint & Repair - Materials	122.51	0.00	122.51	
			54348	Misc Equip Maint & Repair - Materials	155.33	0.00	155.33	
xxx307407	10/11/18	KRONOS INC	11371666	Computer Software	1,425.00	0.00	1,425.00	\$1,425.00
xxx307408	10/11/18	LAWSON PRODUCTS INC	9306172003	Miscellaneous Equipment Parts & Supplies	526.01	0.00	526.01	\$565.02
			9306175722	Miscellaneous Equipment Parts & Supplies	39.01	0.00	39.01	
xxx307409	10/11/18	MAINTENANCE CONNECTION INC	51927	Software Licensing & Support	27,828.68	0.00	27,828.68	\$27,828.68
xxx307410	10/11/18	MALLORY SAFETY & SUPPLY LLC	4500148	Inventory Purchase	27.07	0.00	27.07	\$3,730.74
			4507246	Inventory Purchase	-27.07	0.00	-27.07	
			4511635	Inventory Purchase	27.07	0.00	27.07	
			4527297	Inventory Purchase	2,406.94	0.00	2,406.94	
			4527629	Inventory Purchase	558.10	0.00	558.10	
			4527633	Inventory Purchase	480.69	0.00	480.69	
			4527756	Inventory Purchase	257.94	0.00	257.94	
xxx307411	10/11/18	MCMaster CARR SUPPLY CO	74262127	Miscellaneous Equipment Parts & Supplies	60.26	0.00	60.26	\$4,444.40
			74383671	Miscellaneous Equipment Parts & Supplies	411.83	0.00	411.83	
			74400024	Miscellaneous Equipment Parts & Supplies	108.76	0.00	108.76	
			74442409	Parts, Vehicles & Motor Equip	546.76	0.00	546.76	
			74442410	Electrical Parts & Supplies	1,378.78	0.00	1,378.78	
			74943694	Miscellaneous Equipment Parts & Supplies	89.77	0.00	89.77	
			75063693	Miscellaneous Equipment Parts & Supplies	473.00	0.00	473.00	
			75063694	Misc Equip Maint & Repair - Materials	18.44	0.00	18.44	
			75163423	Miscellaneous Equipment Parts & Supplies	288.07	0.00	288.07	
			75186510	Miscellaneous Equipment Parts & Supplies	137.03	0.00	137.03	
			75486193	Miscellaneous Equipment Parts & Supplies	219.47	0.00	219.47	
			75497271	Miscellaneous Equipment Parts & Supplies	712.23	0.00	712.23	
xxx307412	10/11/18	METHODOLOGIE LABS LLC	1	Miscellaneous Services	2,000.00	0.00	2,000.00	\$2,000.00
xxx307413	10/11/18	MISSION ACADEMY OF MUSIC LLC	MAMSV18-1	Rec Instructors/Officials	2,016.00	0.00	2,016.00	\$2,016.00
xxx307414	10/11/18	MUSIC FOR FAMILIES INC	SVS18	Rec Instructors/Officials	20,149.76	0.00	20,149.76	\$20,149.76
xxx307415	10/11/18	ORACLE AMERICA INC	44062163	Software Licensing & Support	5,584.62	0.00	5,584.62	\$5,584.62

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xxx307416	10/11/18	P&R PAPER SUPPLY CO INC	3021720-001	Inventory Purchase	3,023.22	0.00	3,023.22	\$3,023.22
xxx307417	10/11/18	PAYFLEX SYSTEMS USA INC	000449522	Miscellaneous Payment	815.50	0.00	815.50	\$815.50
xxx307418	10/11/18	PACIFIC PLUMBING & UNDERGROUND	46613SR	Facilities Maintenance & Repair Labor	625.00	0.00	625.00	\$625.00
xxx307419	10/11/18	PUBLIC AGENCY RISK MANAGERS ASSN	ORDER100990	Membership Fees	150.00	0.00	150.00	\$150.00
xxx307420	10/11/18	RAFT RESOURCE AREA FOR TEACHERS	2018-10-3700	Membership Fees	15.00	0.00	15.00	\$15.00
xxx307421	10/11/18	REED & GRAHAM INC	930529	Materials - Land Improve	7,239.16	0.00	7,239.16	\$11,377.11
			930530	Materials - Land Improve	1,461.01	0.00	1,461.01	
			930531	Materials - Land Improve	522.50	0.00	522.50	
			930762	Materials - Land Improve	2,154.44	0.00	2,154.44	
xxx307422	10/11/18	SCC DTAC	5575144-18/19	Taxes & Licenses	1,445.60	0.00	1,445.60	\$1,894.44
			5575161-18/19	Taxes & Licenses	394.10	0.00	394.10	
			5575177-18/19	Taxes & Licenses	54.74	0.00	54.74	
xxx307423	10/11/18	SAFEWAY INC	726532-100518	Advertising Services	223.29	0.00	223.29	\$302.61
			726845-100518	Food Products	17.08	0.00	17.08	
			728084-092118	Food Products	62.24	0.00	62.24	
xxx307424	10/11/18	SMART & FINAL INC	042713-100818	General Supplies	46.27	0.00	46.27	\$284.96
			043822-100218	Food Products	44.41	0.00	44.41	
			053925-091718	General Supplies	194.28	0.00	194.28	
xxx307425	10/11/18	SPECIAL EVENTS	20934-29	Special Events	12,337.25	0.00	12,337.25	\$12,337.25
xxx307426	10/11/18	STEVEN C DOLEZAL PHD INC	SEPT2018	Professional Services	750.00	0.00	750.00	\$750.00
xxx307427	10/11/18	SUNNYVALE PUBLIC SAFETY OFFICERS ASSN	DENTAL1018	Insurances - Dental	29,656.05	0.00	29,656.05	\$29,656.05
xxx307428	10/11/18	SUNNYVALE PUBLIC SAFETY OFFICERS ASSN	DISABILITY1018	Insurances - Long Term Disability	3,876.00	0.00	3,876.00	\$3,876.00
xxx307429	10/11/18	TJKM	0047574	Engineering Services	3,400.00	0.00	3,400.00	\$5,094.00
			0047661	Engineering Services	1,694.00	0.00	1,694.00	
xxx307430	10/11/18	TECHNOLOGY SERVICES AND SOLUTIONS	1800064400	Software As a Service	1,806.70	0.00	1,806.70	\$3,613.40
			1800064670	Software As a Service	1,806.70	0.00	1,806.70	
xxx307431	10/11/18	US HEALTHWORKS MEDICAL GROUP PC	3386026-CA	Pre-Employment Testing	925.00	0.00	925.00	\$2,316.00
			3389556-CA	Pre-Employment Testing	948.00	0.00	948.00	
			3398591-CA	Pre-Employment Testing	443.00	0.00	443.00	
xxx307432	10/11/18	VERMONT SYSTEMS INC	60417	Training and Conferences	300.00	0.00	300.00	\$300.00

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xxx307433	10/11/18	WATEREUSE ASSN	D2019772	Membership Fees	4,761.50	0.00	4,761.50	\$4,761.50
xxx307434	10/11/18	WINSUPPLY OF SILICON VALLEY	693599 00	Miscellaneous Equipment Parts & Supplies	852.90	0.00	852.90	\$852.90
xxx307435	10/11/18	CWEA - SF SECTION	OCT/17/2018	Training and Conferences	100.00	0.00	100.00	\$100.00
xxx307436	10/11/18	OFFICE DEPOT INC	204237416001	Supplies, Office 1	31.27	0.00	31.27	\$8,286.32
			204262427001	Supplies, Office 1	724.69	0.00	724.69	
			204284102001	Supplies, Office 1	27.15	0.00	27.15	
			204284103001	Supplies, Office 1	20.38	0.00	20.38	
			205325017001	Supplies, Office 1	22.96	0.00	22.96	
			205325370001	Supplies, Office 1	70.25	0.00	70.25	
			205609771001	Supplies, Office 1	9.27	0.00	9.27	
			205633237001	Supplies, Office 1	1,035.49	0.00	1,035.49	
			205648069001	Supplies, Office 1	96.38	0.00	96.38	
			205824470001	Supplies, Office 1	603.15	0.00	603.15	
			206015917001	Supplies, Office 1	66.80	0.00	66.80	
			206208117001	Supplies, Office 1	11.43	0.00	11.43	
			206428379001	Supplies, Office 1	7.95	0.00	7.95	
			206454826001	Supplies, Office 1	324.91	0.00	324.91	
			206459935001	Supplies, Office 1	635.61	0.00	635.61	
			206626059001	Supplies, Office 1	80.73	0.00	80.73	
			206702613001	Supplies, Office 1	87.09	0.00	87.09	
			206718814001	Supplies, Office 1	61.13	0.00	61.13	
			206870643001	Supplies, Office 1	58.85	0.00	58.85	
			206870643002	Supplies, Office 1	48.93	0.00	48.93	
			206910294001	Supplies, Office 1	6.21	0.00	6.21	
			206996135001	Supplies, Office 1	65.56	0.00	65.56	
			207012612001	Supplies, Office 1	57.26	0.00	57.26	
			207049423001	Supplies, Office 1	12.61	0.00	12.61	
			207341538001	Supplies, Office 1	39.46	0.00	39.46	
			207422977001	Supplies, Office 1	124.53	0.00	124.53	
			207468189001	Supplies, Office 1	18.97	0.00	18.97	
			207928013001	Supplies, Office 1	48.24	0.00	48.24	

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			207969784001	Supplies, Office 1	41.68	0.00	41.68	
			208052231001	Supplies, Office 1	412.18	0.00	412.18	
			208174069001	Supplies, Office 1	155.97	0.00	155.97	
			208201938001	Supplies, Office 1	39.89	0.00	39.89	
			208818899001	Supplies, Office 1	19.20	0.00	19.20	
			208819166001	Supplies, Office 1	362.31	0.00	362.31	
			208852873001	Supplies, Office 1	87.12	0.00	87.12	
			208884727001	Supplies, Office 1	4.55	0.00	4.55	
			208887210001	Supplies, Office 1	6.53	0.00	6.53	
			208937139001	Supplies, Office 1	85.57	0.00	85.57	
			208942568001	Supplies, Office 1	141.06	0.00	141.06	
			208947910001	Supplies, Office 1	14.12	0.00	14.12	
			209022191001	Supplies, Office 1	347.42	0.00	347.42	
			209034500001	Supplies, Office 1	118.54	0.00	118.54	
			209126001001	Supplies, Office 1	4.08	0.00	4.08	
			209187584001	Supplies, Office 1	72.90	0.00	72.90	
			209708906001	Supplies, Office 1	12.19	0.00	12.19	
			209731699001	Supplies, Office 1	7.86	0.00	7.86	
			209911312001	Supplies, Office 1	14.08	0.00	14.08	
			209948528001	Supplies, Office 1	53.12	0.00	53.12	
			210071388001	Supplies, Office 1	47.73	0.00	47.73	
			210071403001	Supplies, Office 1	168.56	0.00	168.56	
			210192574001	Supplies, Office 1	14.80	0.00	14.80	
			210344257001	Supplies, Office 1	22.04	0.00	22.04	
			210344378001	Supplies, Office 1	103.33	0.00	103.33	
			210347431001	Supplies, Office 1	157.28	0.00	157.28	
			210510742001	Supplies, Office 1	31.13	0.00	31.13	
			210569265001	Supplies, Office 1	106.81	0.00	106.81	
			210571121001	Supplies, Office 1	129.47	0.00	129.47	
			210600205001	Supplies, Office 1	505.35	0.00	505.35	
			210615040001	Supplies, Office 1	179.60	0.00	179.60	

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			210746882001	Supplies, Office 1	209.82	0.00	209.82	
			210748515001	Supplies, Office 1	21.34	0.00	21.34	
			210756007001	Supplies, Office 1	6.08	0.00	6.08	
			210948818001	Supplies, Office 1	130.35	0.00	130.35	
			210972866001	Supplies, Office 1	13.59	0.00	13.59	
			211343003001	Supplies, Office 1	41.41	0.00	41.41	
xxx307441	10/11/18	MARLON CRUZ	405121	Refund Recreation Fees	53.00	0.00	53.00	\$53.00
xxx307442	10/11/18	THE CBR GROUP INC	404033	Refund Recreation Fees	350.00	0.00	350.00	\$350.00
xxx000542	10/9/18	CALIFORNIA PUBLIC EMP RETIREMENT SYSTEM		Insurances - Medical	1,171,508.63	0.00	1,171,508.63	\$1,603,351.94
				Insurances - Retiree Medical - PERS	431,843.31	0.00	431,843.31	
xxx906449	10/9/18	GEORGE HILLS CO INC		Liability Claims Paid	39,780.92	0.00	39,780.92	\$39,780.92
Grand Total Payment Amount								<u>\$3,582,969.71</u>



City of Sunnyvale

Agenda Item

18-0851

Agenda Date: 10/30/2018

REPORT TO COUNCIL

SUBJECT

Award of Bid No. PW19-02 for Pavement Rehabilitation 2018 Project, and Finding of California Environmental Quality Act (CEQA) Categorical Exemption

REPORT IN BRIEF

Approval is requested to award a construction contract in the amount of \$928,283 to Interstate Grading & Paving, Inc. of San Francisco for pavement rehabilitation. Approval is also requested for a 10% construction contingency in the amount of \$92,828.

EXISTING POLICY

Section 1309 of the City Charter requires public works construction contracts to be awarded to the lowest responsive and responsible bidder.

Pursuant to Section 2.09.040 of the Sunnyvale Municipal Code, City Council approval is required for public works contracts exceeding \$100,000 in any one transaction.

ENVIRONMENTAL REVIEW

The California Environment Quality Act (CEQA) determination for the project is a categorical exemption pursuant to CEQA Guidelines Section 15301(c), for the restoring and rehabilitating of existing highways and streets, sidewalks, gutters, bicycle and pedestrian trails, and similar facilities, involving negligible or no expansion of use beyond that presently existing.

BACKGROUND AND DISCUSSION

Capital Project 825290 (Pavement Rehabilitation) provides funding for ongoing roadway infrastructure rehabilitation. The Pavement Rehabilitation 2018 project includes nearly 195,000 square feet of pavement rehabilitation, which is nearly one mile of roadway. This project includes streets that are in poor condition and have medium to severe block cracking and alligator cracking. The roadways have a Pavement Condition Index (PCI) of 56, 54 and 37 respectively.

The Pavement Rehabilitation 2018 Project includes rehabilitation of three roadway segments:

1. Remington Drive, from Bernardo Avenue to Mango Avenue
2. Mary Avenue, from Fremont Avenue to Cascade Drive
3. Old Mountain View-Alviso Road, from Forgewood Drive to Reamwood Drive.

The construction project was advertised for competitive bidding on the City's DemandStar public procurement network and distributed to Bay Area Builder's Exchanges on August 31, 2018. Seven contractors requested bid documents. Sealed bids were opened on September 19, 2018, with five responsive bids received (Bid Summary attached). The lowest responsive and responsible bid was submitted by Interstate Grading & Paving in the amount of \$928,283. The lowest bid is approximately

3% below the engineer's estimate of \$960,000.

FISCAL IMPACT

Project costs include the base bid of \$928,283 and a recommended 10% contingency in the amount of \$92,828, for a total of \$1,021,111. Budgeted funds are available in Capital Project 825290 (Pavement Rehabilitation).

Funding Source

Funding for pavement rehabilitation comes from the General Fund, SB83 VRF Road Improvement Fund, and Gas Tax (regular and SB-1 Gas Tax Funds). The Wastewater Management (Project 831680-Adjust Sewer Utilities in Support of Pavement) and Water Supply and Distribution (Project 831550-Adjust Sewer Utilities in Support of Pavement) funds pay for adjusting manholes and other utility infrastructure affected by the project.

PUBLIC CONTACT

Public contact was made by posting the Council agenda on the City's official-notice bulletin board outside City Hall, at the Sunnyvale Senior Center, Community Center and Department of Public Safety; and by making the agenda and report available at the Sunnyvale Public Library, the Office of the City Clerk and on the City's website.

RECOMMENDATION

1) Make a finding of a California Environmental Quality Act (CEQA) categorical exemption pursuant to CEQA Guidelines Section 15301 for the restoration or rehabilitation of existing highways and streets, sidewalks, gutters, bicycle and pedestrian trails, and similar facilities, involving negligible or no expansion of use beyond that presently existing; 2) Award a contract in substantially the same form as Attachment 2 to the report in the amount of \$928,283 to Interstate Grading & Paving, Inc. for the Pavement Rehabilitation 2018 Project, and authorize the City Manager to execute the contract when all necessary conditions have been met; and 3) approve a 10% construction contingency in the amount of \$92,828.

Prepared by: Gregory S. Card, Purchasing Officer
Reviewed by: Timothy J. Kirby, Director of Finance
Reviewed by: Chip Taylor, Director of Public Works
Reviewed by: Teri Silva, Assistant City Manager
Approved by: Kent Steffens, City Manager

ATTACHMENTS

1. Bid Summary
2. Draft General Construction Contract

Invitation for Bids No. PW19-02 Pavement Rehabilitation 2018				Interstate Grading & Paving Inc. (1)		O'Grady Paving, Inc.		Granite Construction Company			
				128 S. Maple Ave South San Francisco, CA 94080		2513 Wyandotte St Mountain View, CA 94043		715 Comstock St Santa Clara, CA 95054			
				H. Michael Pariani		Craig E. Young		Karim Massoud			
BASE BID				QTY	UOM	Unit Cost	Extended Cost	Unit Cost	Extended Cost	Unit Cost	Extended Cost
1	MOBILIZATION (5% Max)	1	LS			\$ 45,000.00		\$ 26,000.00		\$ 48,000.00	
2	TRAFFIC CONTROL	1	LS			\$ 87,000.00		\$ 85,000.00		\$ 50,400.00	
3	IMPLEMENTATION OF CONSTRUCTION BEST MANAGEMENT PRACTICES	1	LS			\$ 5,500.00		\$ 5,000.00		\$ 4,000.00	
4	CONSTRUCTION AREA SIGNS	12	EA	\$ 250.00	\$ 3,000.00	\$ 500.00	\$ 6,000.00	\$ 160.00	\$ 1,920.00		
5	CHANGEABLE MESSAGE BOARDS	6	EA	\$ 3,500.00	\$ 21,000.00	\$ 1,500.00	\$ 9,000.00	\$ 2,400.00	\$ 14,400.00		
6	SAMPLING AND TESTING	1	LS			\$ 4,500.00		\$ 10,000.00		\$ 7,000.00	
7	CONCRETE IMPROVEMENTS - PCC CURB AND GUTTER REPLACEMENT	83	LF	\$ 137.00	\$ 11,371.00	\$ 200.00	\$ 16,600.00	\$ 150.00	\$ 12,450.00		
8	CONCRETE REMOVAL	208	SF	\$ 8.50	\$ 1,768.00	\$ 16.00	\$ 3,328.00	\$ 30.00	\$ 6,240.00		
9	ASPHALT CONCRETE GRINDING - 2 INCH FULL GRIND	14500	SY	\$ 4.80	\$ 69,600.00	\$ 7.80	\$ 113,100.00	\$ 5.00	\$ 72,500.00		
10	ASPHALT CONCRETE GRINDING - 2.5 INCH FULL GRIND	9400	SY	\$ 6.00	\$ 56,400.00	\$ 10.30	\$ 96,820.00	\$ 6.00	\$ 56,400.00		
11	ADDITIVE COST FOR DISPOSAL OF GRINDINGS WITH FABRIC (REVOCABLE)	9400	SY	\$ 1.20	\$ 11,280.00	\$ 0.10	\$ 940.00	\$ 0.60	\$ 5,640.00		
12	BASE REPAIR (DIGOUTS)	66.1	CY	\$ 480.00	\$ 31,728.00	\$ 560.00	\$ 37,016.00	\$ 550.00	\$ 36,355.00		
13	CRACK SEALING	1	LS			\$ 17,000.00		\$ 18,000.00		\$ 15,300.00	
14	HOT MIX ASPHALT (HMA) PAVING	2940	TON	\$ 120.00	\$ 352,800.00	\$ 112.00	\$ 329,280.00	\$ 146.00	\$ 429,240.00		
15	THERMOPLASTIC PAVEMENT STRIPING - VARIOUS STRIPES	18800	LF	\$ 2.25	\$ 42,300.00	\$ 1.50	\$ 28,200.00	\$ 1.80	\$ 33,840.00		
16	THERMOPLASTIC PAVEMENT STRIPING - 8 INCH STRIPE	351	LF	\$ 4.50	\$ 1,579.50	\$ 2.20	\$ 772.20	\$ 3.00	\$ 1,053.00		
17	THERMOPLASTIC PAVEMENT STRIPING - 12 INCH STRIPE	508	LF	\$ 6.50	\$ 3,302.00	\$ 5.50	\$ 2,794.00	\$ 7.00	\$ 3,556.00		
18	THERMOPLASTIC PAVEMENT MARKINGS	780	SF	\$ 9.00	\$ 7,020.00	\$ 6.00	\$ 4,680.00	\$ 8.00	\$ 6,240.00		
19	THERMOPLASTIC GREEN PAVEMENT MARKINGS	1640	SF	\$ 19.00	\$ 31,160.00	\$ 20.00	\$ 32,800.00	\$ 26.00	\$ 42,640.00		
20	BLUE FIRE HYDRANT PAVEMENT MARKER	8	EA	\$ 26.00	\$ 208.00	\$ 20.00	\$ 160.00	\$ 21.00	\$ 168.00		
21	RETROREFLECTIVE PAVEMENT MARKER	383	EA	\$ 5.00	\$ 1,915.00	\$ 6.00	\$ 2,298.00	\$ 7.00	\$ 2,681.00		
22	ADJUST STORM DRAIN MANHOLE RIM TO GRADE	8	EA	\$ 1,300.00	\$ 10,400.00	\$ 1,000.00	\$ 8,000.00	\$ 1,600.00	\$ 12,800.00		
23	ADJUST SEWER MANHOLE RIM TO GRADE	10	EA	\$ 1,300.00	\$ 13,000.00	\$ 1,000.00	\$ 10,000.00	\$ 1,600.00	\$ 16,000.00		
24	ADJUST WATER VALVE RIM TO GRADE	15	EA	\$ 800.00	\$ 12,000.00	\$ 700.00	\$ 10,500.00	\$ 960.00	\$ 14,400.00		
25	ADJUST GAS VALVE RIM TO GRADE	2	EA	\$ 800.00	\$ 1,600.00	\$ 700.00	\$ 1,400.00	\$ 960.00	\$ 1,920.00		
26	ADJUST MONUMENT RIM TO GRADE	4	EA	\$ 800.00	\$ 3,200.00	\$ 700.00	\$ 2,800.00	\$ 960.00	\$ 3,840.00		
27	ADJUST TELECOMMUNICATIONS MANHOLE RIM TO GRADE	8	EA	\$ 1,700.00	\$ 13,600.00	\$ 1,000.00	\$ 8,000.00	\$ 2,250.00	\$ 18,000.00		
28	LOOP DETECTOR REPLACEMENT	1	LS			\$ 5,500.00		\$ 6,000.00		\$ 5,000.00	
29	DETECTOR HANDHOLE	1	EA	\$ 1,500.00	\$ 1,500.00	\$ 1,600.00	\$ 1,600.00	\$ 1,500.00	\$ 1,500.00		
30	CONCRETE VALLEY GUTTER (REVOCABLE)	1,503	SF	\$ 38.00	\$ 57,114.00	\$ 33.00	\$ 49,599.00	\$ 20.00	\$ 30,060.00		
31	PAVEMENT CONFORM AT VALLEY GUTTER (REVOCABLE)	1,367	SF	\$ 0.32	\$ 437.44	\$ 15.00	\$ 20,505.00	\$ 6.00	\$ 8,202.00		
32	24" FLAT GRATE DROP INLET (REVOCABLE)	1	EA	\$ 4,500.00	\$ 4,500.00	\$ 4,500.00	\$ 4,500.00	\$ 7,000.00	\$ 7,000.00		
BID TOTAL						\$ 928,282.94	\$ 950,692.20		\$ 968,745.00		

Surety	10% bond	10% bond	10% bond
License	A	A, C-12	A, B
Subcontractors			
Crack Sealing	Bond Blacktop	Graham Contractors	Graham Contractors
Striping	Chrisp Company	Sierra Traffic Marking	Sierra Striping
Utility Adjustment	Johnson Construction Company		Johnson Construction Company
Electrical	Columbia Electric, Inc.	Columbia Electric, Inc.	
Concrete	Golden Bay Construction, Inc.	Spenco Construction, Inc.	
Grinder Rental			ABSL Construction

Notes:

(1) Math errors occurred on bid form Line 12. Correct extended cost and bid total reflected on Bid Summary per IFB Instructions to Bidders Item No. 24.

GENERAL CONSTRUCTION CONTRACT

THIS CONTRACT dated _____ is by and between the CITY OF SUNNYVALE, a municipal corporation of the State of California ("Owner") and INTERSTATE GRADING & PAVING, INC., a California corporation ("Contractor").

RECITALS:

The parties to this Contract have mutually covenanted and agreed, as follows:

1. The Contract Documents. The complete Contract consists of the following documents: Notice Inviting Bids; Instructions to Bidders; Performance Bond and Payment Bond; Guaranty; City of Sunnyvale Standard Specifications for Public Works Construction, 2006 Edition; City of Sunnyvale Standard Details for Public Works Construction, 2006 Edition; Plans and Specifications, "Pavement Rehabilitation 2018, Project No. ST-18-02, Invitation for Bids No. PW19-02", including Addenda No. 1 and 2; OSHA, and other standards and codes as outlined in the Specifications. These documents are all incorporated by reference. The documents comprising the complete contract are collectively referred to as the Contract Documents.

Any and all obligations of the Owner and the Contractor are fully set forth and described therein.

All of the above documents are intended to work together so that any work called for in one and not mentioned in the other or vice versa is to be executed the same as if mentioned in all documents.

2. The Work. Contractor agrees to furnish all tools, equipment, apparatus, facilities, labor, transportation, and material necessary to perform and complete the project in a good and workmanlike manner. The work consist(s) of rehabilitating portions of Remington Drive, Mary Avenue and Old Mountain View-Alviso Road, within the limits shown on the plans and include AC grinding, digouts and AC overlay. In addition: minor concrete work, crack sealing and repair, striping, and adjusting utility boxes to finished grade, as called for, and in the manner designated in, and in strict conformity with, the Plans and Specifications prepared by Moffatt & Nichol and adopted by the Owner. These Plans and Specifications are entitled respectively, Pavement Rehabilitation 2018, Project No. ST-18-02.

It is understood and agreed that the work will be performed and completed as required in the Plans and Specifications under the sole direction and control of the Contractor, and subject to inspection and approval of the Owner, or its representatives. The Owner hereby designates as its representative for the purpose of this contract the Senior Civil Engineer for Construction or an employee of the Owner who will be designated in writing by the Director of Public Works.

3. Contract Price. The Owner agrees to pay and the Contractor agrees to accept, in full payment for the work above agreed to be done, the sum of Nine Hundred Twenty Eight Thousand Two Hundred Eight Two and 94/100 Dollars (\$928,282.94) subject to final determination of the work performed and materials furnished at unit prices per "Exhibit A" attached hereto and incorporated by this reference, and subject to additions and deductions in accordance, as provided in the Documents and in accordance with Contract Documents. The sum includes base bid and accepted Additive Alternate(s) No. Number(s). All other Additive Alternate(s) are rejected by Owner, and are not included in this contract.

4. Permits; Compliance with Law. Contractor shall, at its expense, obtain all necessary permits and licenses, easements, etc., for the construction of the project, give all necessary notices, pay all fees required by law, and comply with all laws, ordinances, rules and regulations relating to the work and to the preservation of the public health and safety.

5. Inspection by Owner. Contractor shall at all times maintain proper facilities and provide safe access for inspection by the Owner to all parts of the work, and to the shops wherein the work is in preparation. Where the Specifications require work to be specially tested or approved, it shall not be tested or covered up without timely notice to the Owner of its readiness for inspection and without the approval thereof or consent thereto by the latter. Should any such work be covered up without such notice, approval,

or consent, it must, if required by Owner, be uncovered for examination at the Contractor's expense.

6. Extra or Additional Work and Changes. Should Owner at any time during the progress of the work request any alterations, deviations, additions or omissions from the Specifications or Plans or other Contract Documents it shall be at liberty to do so, and the same shall in no way affect or make void the contract, but will be added to or deducted from the amount of the contract price, as the case may be, by a fair and reasonable valuation, agreed to in writing between the parties hereto. No extra work shall be performed or change be made unless in pursuance of a written order from the Director of Public Works or authorized representative, stating that the extra work or change is authorized and no claim for an addition to the contract sum shall be valid unless so ordered.

7. Time for Completion. All work under this contract shall be completed before the expiration ninety (90) working days from the date specified in the Notice to Proceed.

If Contractor shall be delayed in the work by the acts or neglect of Owner, or its employees or those under it by contract or otherwise, or by changes ordered in the work, or by strikes, lockouts by others, fire, unusual delay in transportation, unavoidable casualties or any causes beyond the Contractor's control, or by delay authorized by the Owner, or by any cause which the Owner shall decide to justify the delay, then the time of completion shall be extended for such reasonable time as the Owner may decide.

This provision does not exclude the recovery of damages for delay by either party under other provisions.

8. Inspection and Testing of Materials. Contractor shall notify Owner a sufficient time in advance of the manufacture or production of materials, to be supplied under this contract, in order that the Owner may arrange for mill or factory inspection and testing of same, if Owner requests such notice from Contractor.

9. Termination. If Contractor should file a bankruptcy petition and/or be judged bankrupt, or if Contractor should make a general assignment for the benefit of creditors, or if a receiver should be appointed on account of insolvency, or if Contractor or any subcontractors should violate any of the provisions of the Contract, Owner may serve written notice upon Contractor and its surety of Owner's intention to terminate the Contract. The notice shall contain the reasons for such intention to terminate the Contract, and, unless within ten days after serving such notice, such violation shall cease and satisfactory arrangements for correction thereof be made, upon the expiration of the ten days, the Contract shall cease and terminate. In the event of any such termination, Owner shall immediately serve written notice thereof upon the surety and the Contractor, and the surety shall have the right to take over and perform the Contract; provided, however that, if the surety within fifteen days after the serving upon it of notice of termination does not give Owner written notice of its intention to take over and perform the Contract or does not commence performance thereof within thirty days from the date of the serving of such notice, Owner may take over the work and prosecute the same to completion by contract or by any other method it may deem advisable, for the account and at the expense of Contractor, and Contractor and its surety shall be liable to Owner for any excess cost occasioned Owner thereby, and in such event Owner may without liability for so doing take possession of and utilize in completing the work, such materials, appliances, plant and other property belonging to Contractor as may be on the site of the work and necessary therefor.

10. Owner's Right to Withhold Certain Amounts and Make Application Thereof. In addition to the amount which Owner may retain under Paragraph 21 until the final completion and acceptance of all work covered by the Contract, Owner may withhold from payment to Contractor such amount or amounts as in its judgment may be necessary to pay just claims against Contractor or any subcontractors for labor and services rendered and materials furnished in and about the work. Owner may apply such withheld amount or amounts to the payment of such claims in its discretion. In so doing Owner shall be deemed the agent of Contractor and any payment so made by Owner shall be considered as a payment made under the Contract by Owner to the Contractor and Owner shall not be liable to Contractor for any such payment made in good faith. Such payment may be made without prior judicial determination of the claim or claims.

11. Notice and Service Thereof. All notices required pursuant to this Contract shall be communicated in writing, and shall be delivered in person, by commercial courier or by first class or priority mail delivered by the United States Postal Service. Nothing in this provision shall be construed to prohibit communication by more expedient means, such as by email or fax, to accomplish timely communication. Each party may change the address by written notice in accordance with this paragraph. Notices delivered personally shall be deemed communicated as of actual receipt; mailed notices shall be deemed communicated as of three business days after mailing. All notices sent pursuant to this Contract shall be addressed as follows:

Owner: City of Sunnyvale
Department of Public Works
Construction Contract Administrator
P. O. Box 3707
Sunnyvale, CA 94088-3707

Contractor: Interstate Grading & Paving, Inc.
Attn: H. Michael Pariani
128 S. Maple Ave
South San Francisco, CA 94080

12. Assignment of Contract. Neither the Contract, nor any part thereof, nor moneys due or to become due thereunder may be assigned by Contractor without the prior written approval of Owner.

13. Compliance with Specifications of Materials. Whenever in the Specifications, any material or process is indicated or specified by patent or proprietary name, or by name of manufacturer, such Specifications must be met by Contractor, unless Owner agrees in writing to some other material, process or article offered by Contractor which is equal in all respects to the one specified.

14. Contract Security. Contractor shall furnish a surety bond in an amount at least equal to 100 percent of the contract price as security for the faithful performance of this Contract. Contractor shall also furnish a separate surety bond in an amount at least equal to 100 percent of the contract price as security for the payment of all persons for furnishing materials, provisions, provender, or other supplies, or teams, used in, upon, for or about the performance of the work contracted to be done, or for performing any work or labor thereon of any kind, and for the payment of amounts due under the Unemployment Insurance Code with respect to such work or labor in connection with this Contract, and for the payment of a reasonable attorney's fee to be fixed by the court in case suit is brought upon the bond. Bonds shall be issued by an admitted surety insurer authorized to operate in the state of California.

15. Insurance. Contractor shall not commence work under this Contract until all insurance required under this paragraph has been obtained and such insurance has been approved by the Owner, nor shall Contractor allow any subcontractor to commence work on a subcontract until all similar insurance required of the subcontractor has been so obtained and approved. Contractor shall furnish the Owner with satisfactory proof of the carriage of insurance required, and there shall be a specific contractual liability endorsement extending the Contractor's coverage to include the contractual liability assumed by the Contractor pursuant to this Contract and particularly Paragraph 16 hereof. Any policy of insurance required of the Contractor under this Contract shall also contain an endorsement providing that thirty (30) days' notice must be given in writing to the Owner of any pending change in the limits of liability or of any cancellation or modification of the policy. Insurance carrier shall be California-admitted.

(a) Compensation Insurance and Employer's Liability Insurance. Contractor shall take out and maintain during the life of this Contract Workers' Compensation Insurance and Employer's Liability Insurance for all of employees employed at the site of the project and, in case any work is sublet, Contractor shall require the subcontractor similarly to provide Workers' Compensation Insurance and Employer's Liability Insurance for all of the latter's employees unless such employees are covered by the protection afforded by Contractor.

In signing this Contract, Contractor makes the following certification, required by Section 1861 of the Labor Code:

"I am aware of the provision of Section 3700 of the Labor Code which requires every employer to be insured against liability for workers' compensation or to undertake self-insurance in accordance with the provisions of that code, and I will comply with such provisions before commencing the performance of the work of this contract."

(b) General and Automobile Liability Insurance. Contractor, at its own cost and expense, shall maintain personal injury liability and property damage insurance for the period covered by the Contract in the amount of Two Million Dollars (\$2,000,000.00) per occurrence and \$4,000,000 annual aggregate combined single limit coverage. Such coverage shall include, but shall not be limited to, protection against claims arising therefrom, and damage to property resulting from activities contemplated under this Contract, use of owned automobiles, products and completed operations, including U, C and X. Such insurance shall be with insurers and under forms of policies satisfactory in all respects to the Owner and shall provide that notice must be given to Owner at least thirty (30) days prior to cancellation or material change. The following endorsements shall be attached to the policy:

Policy shall cover on an "occurrence" basis. Policy must cover personal injuries as well as bodily injuries. Exclusion of contractual liability must be eliminated from personal injury endorsement. Broad form property damage endorsement must be attached. Owner is to be named as an additional insured on any contracts of insurance under this paragraph (b). Coverage shall not extend to any indemnity coverage for the active negligence of the additional insured in any case where an agreement to indemnify the additional insured would be invalid under Subdivision (b) of Section 2782 of the Civil Code. The policies of insurance shall be considered primary insurance before any policies of insurance maintained by Owner.

16. Indemnification and Hold Harmless. Contractor agrees to defend, save, indemnify and hold harmless Owner and all its officers, employees, and agents, against any and all liability, claims, judgments, or demands, including demands arising from injuries or death of persons (Contractor's employees included) and damage to property, arising directly or indirectly out of the obligations herein undertaken or out of the operations conducted by Contractor, save and except claims or litigation arising through the active negligence or willful misconduct of Owner, or of Owner's officials, agents, employees, servants, or independent contractors who are directly responsible to Owner. Contractor shall make good and reimburse Owner for any expenditures, including reasonable attorneys' fees, Owner may make by reason of such claim or litigation, and, if requested by Owner, Contractor shall defend any such suits at the sole cost and expense of Contractor.

17. Hours of Work. Eight hours of labor during any one calendar day and forty hours of labor during any one calendar week shall constitute the maximum hours of service upon all work done hereunder, and it is expressly stipulated that no laborer, worker, or mechanic employed at any time by the Contractor or by any subcontractor or subcontractors under this Contract, upon the work or upon any part of the work contemplated by this Contract, shall be required or permitted to work thereon more than eight hours during any one calendar day and forty hours during any one calendar week, except, as provided by Section 1815 of the Labor Code of the State of California, work performed by employees of contractors in excess of eight hours per day and forty hours during any one week shall be permitted upon public work upon compensation for all hours worked in excess of eight hours per day at not less than one and one-half times the basic rate of pay. It is further expressly stipulated that for each and every violation of Sections 1811-1815, inclusive, of the Labor Code of the State of California, all the provisions whereof are deemed to be incorporated herein, Contractor shall forfeit, as a penalty to Owner, twenty-five dollars (\$25.00) for each laborer, worker, or mechanic employed in the execution of this Contract by Contractor, or by any subcontractor under this Contract, for each calendar day during which the laborer, worker, or mechanic is required or permitted to work more than eight hours in any one calendar day and forty hours in any one calendar week in violation of the provisions of the Sections of the Labor Code.

Contractor, and each subcontractor, shall, in accordance with California Labor Code Section 1776 or as the same may be later amended, keep accurate payroll records showing the name, address, social security number, work classification, straight time and overtime hours worked each day and week, and the actual per diem wages paid to each journeyman, apprentice, worker, or other employee employed by him or her in connection with work under this agreement. Each payroll record shall contain or be verified by a written declaration under penalty of perjury, in accordance with Labor Code Section 1776(a). Such payroll records shall be made available at all reasonable times at the Contractor's principal office to the persons authorized to inspect such records pursuant to Labor Code Section 1776. A certified copy of all payroll records shall be made available for inspection or furnished upon request to a representative of the Division of Labor Standards Enforcement, and the Division of Apprenticeship Standards of the Department of Industrial Relations, as well as to the Owner's representative. In the event the Contractor or a Subcontractor fails to comply in a timely manner within ten days to a written notice requesting the records, such contractor or subcontractor shall forfeit one hundred dollars (\$100.00) for each calendar day, or portion thereof, for each worker, until strict compliance is effectuated, in accordance with Labor Code Section 1776(h).

18. Wage Rates. Pursuant to the Labor Code of the State of California, or any applicable local law, Owner has ascertained the general prevailing rate per diem wages and rates for holidays, and overtime work in the city, for each craft, classification or type of laborer, worker, or mechanic needed to execute this Contract. Owner has adopted, by reference, the general prevailing rate of wages applicable to the work to be done under the Contract, as adopted and published by the Division of Labor Standards Enforcement and Labor Statistics and Research of the State of California, Department of Industrial Relations, to which reference is hereby made for a full and detailed description. A copy of the prevailing wage rates may be reviewed in the office of the Director of Public Works, City of Sunnyvale, 456 West Olive Avenue, Sunnyvale, California. Wage rates can also be obtained through the California Department of Industrial Relations website at: <http://www.dir.ca.gov/OPRL/DPreWageDetermination.htm>

Neither the notice inviting bids nor this Contract shall constitute a representation of fact as to the prevailing wage rates upon which the Contractor or any subcontractor may base any claim against Owner.

It shall be mandatory upon Contractor and upon any subcontractor to pay not less than the specified rates to all laborers, workers, and mechanics employed in the execution of the Contract. It is further expressly stipulated that Contractor shall, as a penalty to Owner, forfeit two hundred dollars (\$200.00) for each calendar day, or portion thereof, for each laborer, worker, or mechanic paid less than the stipulated prevailing rates for any work done under this Contract by Contractor or by any subcontractor; and Contractor agrees to comply with all provisions of Section 1775 of the Labor Code.

In case it becomes necessary for Contractor or any subcontractor to employ on the project under this Contract any person in a trade or occupation (except executives, supervisory, administrative, clerical, or other non-manual workers as such) for which no minimum wage rate is herein specified, Contractor shall immediately notify Owner who will promptly thereafter determine the prevailing rate for such additional trade or occupation and shall furnish Contractor with the minimum rate based thereon. The minimum rate thus furnished shall be applicable as a minimum for such trade or occupation from the time of the initial employment of the person affected and during the continuance of such employment.

19. Accident Prevention. Precaution shall be exercised at all times for the protection of persons (including employees) and property. The safety provisions of applicable laws, building and construction codes shall be observed. Machinery, equipment, and other hazards shall be guarded or eliminated in accordance with the safety provisions of the Construction Safety Orders issued by the Industrial Accident Commission of the State of California.

20. Contractor's Guarantee. Owner shall not, in any way or manner, be answerable or suffer loss, damage, expense or liability for any loss or damage that may happen to the building, work, or equipment or any part thereof, or in, on, or about the same during its construction and before acceptance. Contractor unqualifiedly guarantees the first-class quality of all workmanship and of all materials, apparatus, and equipment used or installed by Contractor or by any subcontractor or supplier in the project which is

the subject of this Contract, unless a lesser quality is expressly authorized in the Plans and Specifications, in which event Contractor unqualifiedly guarantees such lesser quality; and that the work as performed by Contractor will conform with the Plans and Specifications or any written authorized deviations therefrom. In case of any defect in work, materials, apparatus or equipment, whether latent or patent, revealed to Owner within one year of the date of acceptance of completion of this Contract by Owner, Contractor will forthwith remedy such defect or defects without cost to Owner.

21. Liquidated Damages. Time shall be the essence of this Contract. If Contractor fails to complete, within the time fixed for such completion, the entire work mentioned and described and contracted to be done and performed, Contractor shall become liable to Owner for liquidated damages in the sum of one thousand and no/100 (\$1,000.00), for each and every calendar day during which work shall remain uncompleted beyond such time fixed for completion or any lawful extension thereof. The amount specified as liquidated damages is presumed to be the amount of damage sustained by Owner since it would be impracticable or extremely difficult to fix the actual damage; and the amount of liquidated damages may be deducted by Owner from moneys due Contractor hereunder, or its assigns and successors at the time of completion, and Contractor, or its assigns and successors at the time of completion, and its sureties shall be liable to Owner for any excess.

22. Governing Law, Jurisdiction and Venue. This Agreement shall be governed by and construed in accordance with the laws of the State of California, without regard to conflict of law or choice of law principles. Proper venue for legal actions will be exclusively vested in a state court in the County of Santa Clara. The parties agree that subject matter and personal jurisdiction are proper in state court in the County of Santa Clara, and waive all venue objections.

23. Severability Clause. In case any one or more of the provisions contained herein shall, for any reason, be held invalid, illegal or unenforceable in any respect, it shall not affect the validity of the other provisions which shall remain in full force and effect.

24. Entire Agreement; Amendment. This writing constitutes the entire agreement between the parties relating to the services to be performed or materials to be furnished hereunder. No modification of this Agreement shall be effective unless and until such modification is evidenced by writing signed by all parties.

25. Execution and Counterparts. This Agreement may be executed in multiple counterparts and/or with the signatures of the Parties set forth on different signature sheets and all such counterparts, when taken together, shall be deemed one original.

EXHIBIT A**BID SCHEDULE****Base Bid**

Item No.	Revocable Bid Item	Description	QTY	Unit	Lump Sum/Unit Cost
1		MOBILIZATION (5% Max)	1	LS*	\$45,000.00
2		TRAFFIC CONTROL	1	LS*	\$87,000.00
3		IMPLEMENTATION OF CONSTRUCTION BEST MANAGEMENT PRACTICES	1	LS*	\$5,500.00
4		CONSTRUCTION AREA SIGNS	12	EA	\$250.00
5		CHANGEABLE MESSAGE BOARDS	6	EA	\$3,500.00
6		SAMPLING AND TESTING	1	LS*	\$4,500.00
7		CONCRETE IMPROVEMENTS - PCC CURB AND GUTTER REPLACEMENT	83	LF	\$137.00
8		CONCRETE REMOVAL	208	SF	\$8.50
9		ASPHALT CONCRETE GRINDING - 2 INCH FULL GRIND	14500	SY	\$4.80
10		ASPHALT CONCRETE GRINDING - 2.5 INCH FULL GRIND	9400	SY	\$6.00
11	R	ADDITIVE COST FOR DISPOSAL OF GRINDINGS WITH FABRIC	9400	SY	\$1.20
12		BASE REPAIR (DIGOUTS)	66.1	CY	\$480.00
13		CRACK SEALING	1	LS*	\$17,000.00
14		HOT MIX ASPHALT (HMA) PAVING	2940	TON	\$120.00
15		THERMOPLASTIC PAVEMENT STRIPING - VARIOUS STRIPES	18800	LF	\$2.25
16		THERMOPLASTIC PAVEMENT STRIPING - 8 INCH STRIPE	351	LF	\$4.50
17		THERMOPLASTIC PAVEMENT STRIPING - 12 INCH STRIPE	508	LF	\$6.50
18		THERMOPLASTIC PAVEMENT MARKINGS	780	SF	\$9.00
19		THERMOPLASTIC GREEN PAVEMENT MARKINGS	1640	SF	\$19.00
20		BLUE FIRE HYDRANT PAVEMENT MARKER	8	EA	\$26.00
21		RETROREFLECTIVE PAVEMENT MARKER	383	EA	\$5.00

22		ADJUST STORM DRAIN MANHOLE RIM TO GRADE	8	EA	\$1,300.00
23		ADJUST SEWER MANHOLE RIM TO GRADE	10	EA	\$1,300.00
24		ADJUST WATER VALVE RIM TO GRADE	15	EA	\$800.00
25		ADJUST GAS VALVE RIM TO GRADE	2	EA	\$800.00
26		ADJUST MONUMENT RIM TO GRADE	4	EA	\$800.00
27		ADJUST TELECOMMUNICATIONS MANHOLE RIM TO GRADE	8	EA	\$1,700.00
28		LOOP DETECTOR REPLACEMENT	1	LS*	\$5,500.00
29		DETECTOR HANDHOLE	1	EA	\$1,500.00
30	R	CONCRETE VALLEY GUTTER	1,503	SF	\$38.00
31	R	PAVEMENT CONFORM AT VALLEY GUTTER	1,367	SF	\$0.32
32	R	24" FLAT GRATE DROP INLET	1	EA	\$4,500.00

EXHIBIT B

Utilization of Local Workforce in Construction Projects – The Sunnyvale City Council has adopted a policy which encourages utilization of local workforces, including State-certified apprentices, as a means of supporting economic opportunities for all members of the community. Local workforce is defined as workers residing in Santa Clara County. The lowest responsive and responsible bidder must provide a projection of locally-hired workers utilized for this contract.

Contractor	Projected Number of Locally Hired Workers_____ Projected Percent of Locally Hired Workers_____%
Subcontractor(s)	Projected Number of Locally Hired Workers_____ Projected Percent of Locally Hired Workers_____%

PERFORMANCE BOND

Invitation for Bids No. PW19-02 Pavement Rehabilitation 2018 Project No. ST-18-02

KNOW ALL MEN BY THESE PRESENTS:

THAT WHEREAS, the City of Sunnyvale ("City") has awarded to Interstate Grading & Paving, Inc. as principal ("Contractor"), a contract for the public work described as follows:

The project entitled "Pavement Rehabilitation 2018, Project No. ST-18-02" pursuant to the award made to said Principal by the Council of the City of Sunnyvale to do and perform the following work, to wit: furnish all tools, equipment, apparatus, facilities, labor, transportation, and material necessary to perform and complete in a good and workmanlike manner, the work rehabilitating portions of Remington Drive, Mary Avenue and Old Mountain View-Alviso Road, within the limits shown on the plans and include AC grinding, digouts and AC overlay. In addition: minor concrete work, crack sealing and repair, striping, and adjusting utility boxes to finished grade, as called for, and in the manner designated in, and in strict conformity with, the Plans and Specifications (the "work").

It is acknowledged that the contract provides for a one-year warranty period during which time this Bond remains in full force and effect. The contract and all of its terms and conditions are incorporated into this Bond by reference.

AND WHEREAS, the Contractor is required to furnish a bond in connection with the contract guaranteeing its faithful performance.

AND THEREFORE, we the undersigned Contractor as principal and _____ a _____, admitted and duly authorized to transact business under the laws of the State of California as surety, are held and firmly bound unto the City as obligee in the sum of Nine Hundred Twenty Eight Thousand Two Hundred Eight Two and 94/100 Dollars (\$928,282.94) (which amount is not less than 100% of the contract price) to be paid to the City or its successors and assigns; and for which payment, well and truly to be made, we bind ourselves, our heirs, executors and administrators, successors or assigns, jointly and severally, firmly by these presents.

THE CONDITION of the obligation is such:

That if the Contractor, (or the Contractor's heirs, executors, administrators, successors or assigns) shall in all respects abide by, and well and truly keep and perform all of the covenants, conditions and agreements in the contract (and any alteration made as provided in the contract) at the time and in the

manner specified and in all respects according to their true intent and meaning; and if the contractor shall indemnify and save harmless the City, its officers, employees and agents, as stipulated in the contract, then this obligation shall become and be null and void; otherwise this obligation shall be and remain in full force and effect.

As a condition precedent to the satisfactory completion of the contract, the obligation of the Contractor and surety under this Bond shall remain in effect for a period of one (1) year after the completion and acceptance of the work. During that time, if the Contractor (or the Contractor's heirs, executors, administrators, successors or assigns) fails to make full, complete and satisfactory repair and replacement or totally protect the City from any loss or damage made evident during that year which results from or is caused by either defective materials or faulty workmanship in the prosecution of the work, then the obligation shall remain in full force and effect. However, anything in this paragraph to the contrary notwithstanding, the obligation of the Surety shall continue so long as any obligation of the Contractor remains.

No prepayment or delay in payments, and no change, extension, addition or alteration of any provision of the contract or in the specifications agreed to between the Contractor and the City, or any forbearance on the part of the City shall operate to relieve the surety. The surety hereby waives the provisions of Section 2819 of the California Civil Code. The surety waives all rights of subrogation against the City or any person employed by the City. If the contract price increases by the issuance of change orders, the amount specified in this bond shall increase by the same amount.

IN WITNESS WHEREOF, we have hereunto set our hands and seals on this
_____ day of _____, 20____.

SURETY (Name):

(Address of Principal Place of Business)

Telephone No.: _____

Facsimile No.: _____

By: _____

Attorney in Fact

CONTRACTOR (Name):

(Address)

By: _____

(Name: print or type)

Title: _____

By: _____

(Name: print or type)

Title: _____

**(Notice: The signatures of the Surety
and Contractor on this bond must be
acknowledged before a notary.)**

PAYMENT BOND

Invitation for Bids No. PW19-02 Pavement Rehabilitation 2018 Project No. ST-18-02

KNOW ALL MEN BY THESE PRESENTS:

THAT WHEREAS, the City of Sunnyvale ("City") has awarded to Interstate Grading & Paving, Inc. as principal ("Contractor"), a contract for the public work described as follows:

The project entitled "Pavement Rehabilitation 2018, Project No. ST-18-02" pursuant to the award made to said Principal by the Council of the City of Sunnyvale to do and perform the following work, to wit: furnish all tools, equipment, apparatus, facilities, labor, transportation, and material necessary to perform and complete in a good and workmanlike manner, the work rehabilitating portions of Remington Drive, Mary Avenue and Old Mountain View-Alviso Road, within the limits shown on the plans and include AC grinding, digouts and AC overlay. In addition: minor concrete work, crack sealing and repair, striping, and adjusting utility boxes to finished grade, as called for, and in the manner designated in, and in strict conformity with, the Plans and Specifications (the "work").

It is acknowledged that the contract provides for a one year warranty period during which time this Bond remains in full force and effect. The contract and all of its terms and conditions are incorporated into this Bond by reference.

AND WHEREAS, the Contractor is required to furnish a bond in connection with the contract guaranteeing payment of persons who provide labor and material;

AND THEREFORE, we the undersigned Contractor as principal and _____ a _____, admitted and duly authorized to transact business under the laws of the State of California, as surety, are held and firmly bound unto the City or its successors and assigns as obligee in the sum of _____; (which amount is not less than 100% of the contract price) and for which payment, well and truly to be made, we bind ourselves, our heirs, executors and administrators, successors or assigns, jointly and severally, firmly by these presents.

THE CONDITION of the obligation is such:

That if the Contractor, (or the Contractor's subcontractors, heirs, executors, administrators, successors or assigns) fails to pay any of the persons named in Section 3181 of the Civil Code of the State of California, or the amounts due under the Unemployment Insurance Code of the State of California with respect to work or labor performed under the Contract, or for any amounts required to be deducted,

withheld, and paid over to the Employment Development Department from the wages of employees of the contractor and subcontractors pursuant to Section 13020 of the Unemployment Insurance Code of the State of California, with respect to such work and labor, that the surety will pay for the same, in an amount not exceeding the sum specified in this bond, and also, in case suit is brought upon the bond, shall pay reasonable attorney's fees, to be fixed by the Court.

This bond shall inure to the benefit of any and all persons, companies, and corporations named in Section 3181 of the Civil Code of the State of California, so as to give a right of action to them or their assigns in any suit brought upon this bond.

No prepayment or delay in payments, and no change, extension, addition or alteration of any provision of the contract or in the specifications agreed to between the Contractor and the City, or any forbearance on the part of the City shall operate to relieve the surety. The surety hereby waives the provisions of Section 2819 of the California Civil Code. The surety waives all rights of subrogation against the City or any person employed by the City. If the contract price increases by the issuance of change orders, the amount specified in this bond shall increase by the same amount.

IN WITNESS WHEREOF, we have hereunto set our hands and seals on this
_____ day of _____, 20____.

SURETY (Name):

(Address of Principal Place of Business)

Telephone No.: _____

Facsimile No.: _____

By: _____

Attorney in Fact

CONTRACTOR (Name):

(Address)

By: _____

(Name: print or type)

Title: _____

By: _____

(Name: print or type)

Title: _____

**(Notice: The signatures of the Surety
and Contractor on this bond must be
acknowledged before a notary.)**

GUARANTY

TO THE CITY OF SUNNYVALE, for construction of

Pavement Rehabilitation 2018, ST-18-02

The undersigned guarantees all construction performed on this project and also guarantees all material and equipment incorporated therein.

Contractor hereby grants to City for a period of one (1) year following the date of Final Acceptance of the Work, or such longer period specified in the Contract Documents, its unconditional warranty of the quality and adequacy of all of the Work and its compliance with the Contract Documents including, without limitation, all labor, materials and equipment provided by Contractor and its Subcontractors of all tiers in connection with the Work.

Neither final payment nor use or occupancy of the Work performed by the Contractor shall constitute an acceptance of work not done in accordance with this Guaranty or relieve Contractor of liability in respect to any express warranties or responsibilities for faulty materials or workmanship. Contractor shall remedy any defects in the Work and pay for any damage resulting therefrom which shall appear within one (1) year, or longer if specified, from the date of Final Acceptance.

If within one year after the date of Final Acceptance, or such longer period of time as may be prescribed by laws or regulations, or by the terms of Contract Documents, any Work is found to be defective, Contractor shall promptly, without cost to City and in accordance with City's written instructions, correct such defective Work. Contractor shall remove any defective Work rejected by City and replace it with Work that is not defective, and satisfactorily correct or remove and replace any damage to other Work or the work of others resulting therefrom. If Contractor fails to promptly comply with the terms of such instructions, or in an emergency where delay would cause serious risk of loss or damage, City may have the defective Work corrected or the rejected Work removed and replaced. Contractor shall pay for all claims, costs, losses and damages caused by or resulting from such removal and replacement. Where Contractor fails to correct defective work, or defects are discovered outside the correction period, City shall have all rights and remedies granted by law.

Inspection of the work shall not relieve Contractor of any of its obligations under the Contract Documents. Even though equipment, materials, or work required to be provided under the Contract Documents have been inspected, accepted, and estimated for payment, Contractor shall, at its own expense, replace or repair any such equipment, material, or work found to be defective or otherwise not to comply with the requirements of the Contract Documents up to the end of the guaranty period.

All abbreviations and definitions of terms used in this Agreement shall have the meanings set forth in the Contract Documents, including, without means of limitation, Supplemental General Provisions, Definition of Works and Terms.

The foregoing Guaranty is in addition to any other warranties of Contractor contained in the Contract Documents, and not in lieu of, any and all other liability imposed on Contractor under the Contract Documents and at law with respect to Contractor's duties, obligations, and performance under the Contract Documents. In the event of any conflict or inconsistency between the terms of this Guaranty and any warranty or obligation of the Contractor under the Contract Documents or at law, such inconsistency or conflict shall be resolved in favor of the higher level of obligation of the Contractor.

_____(Contractor Name)

_____(Contractor Address)

Contractor Signature

Date



City of Sunnyvale

Agenda Item

18-0880

Agenda Date: 10/30/2018

REPORT TO COUNCIL

SUBJECT

Approve Budget Modification No. 6 to Appropriate \$270,000 in one-time funding to Program 365 - Wastewater Treatment.

BACKGROUND

In March 2018, Sunnyvale Employees' Association (SEA)/IFPTE Local 21 filed a grievance alleging that Water Pollution Control Plant (WPCP) Operators were required to arrive 15 minutes early to conduct shift turnover activities with the outgoing Operator, and were not paid for that time.

ENVIRONMENTAL REVIEW

The action being considered does not constitute a "project" with the meaning of the California Environmental Quality Act ("CEQA") pursuant to CEQA Guidelines section 15378(b)(4) in that it is a fiscal activity that does not involve any commitment to any specific project which may result in a potential significant impact on the environment.

DISCUSSION

On September 25, 2018, the City Council authorized settlement of the grievance in closed session to compensate current and separated employees working in the classifications of Water Pollution Control Plant Operator in Training, Water Pollution Control Plant Operator I/II, Senior Water Pollution Control Plant Operator, and Principal Water Pollution Control Plant Operator between March 29, 2015 to July 28, 2018, calculated based on 15 minutes per each individual's work shift and regular rates of pay during that period. . The Office of the City Attorney is preparing a Settlement Agreement and Release of Claims that impacted individuals will sign in order to be eligible for payment under the settlement.

SEA/Local 21 is in agreement with this resolution of the grievance. The parties also met and conferred and reached agreement regarding how WPCP shift changeovers will be handled going forward, with the new practice of one Senior Operator handling shift change effective August 1, 2018.

FISCAL IMPACT

The anticipated cost of the settlement to all employees is approximately \$270,000. All positions eligible for payment under the settlement are funded by the Wastewater Management Fund. Staff is recommending approval of Budget Modification No. 6 to appropriate \$270,000 in one-time funding to Program 365 - Wastewater Treatment for the payment. Actual amounts paid out will depend on the number of eligible individuals that opt in to the settlement, and any unused funds will be returned to fund balance at the end of the year. As this is a one-time cost, it can be absorbed without affecting wastewater utility rates.

**Budget Modification No. 6
FY 2018/19**

	Current	Increase/ (Decrease)	Revised
Solid Waste Management Fund			
<u>Expenditures</u>			
Program 365 - Wastewater Treatment	\$11,506,995	\$270,000	\$11,776,995
<u>Reserves</u>			
Rate Stabilization Reserve	\$9,121,253	(\$270,000)	\$8,851,253

Funding Source

This cost will be funded by the Wastewater Management Fund

PUBLIC CONTACT

Public contact was made by posting the Council agenda on the City's official-notice bulletin board outside City Hall, at the Sunnyvale Senior Center, Community Center and Department of Public Safety; and by making the agenda and report available at the Sunnyvale Public Library, the Office of the City Clerk and on the City's website.

RECOMMENDATION

Approve Budget Modification No. 6 to appropriate \$270,000 in one-time funding to Program 365 - Wastewater Treatment.

Prepared by: Tina Murphy, Director of Human Resources

Reviewed by: Timothy J. Kirby, Director of Finance

Reviewed by: Teri Silva, Assistant City Manager

Approved by: Kent Steffens, City Manager



City of Sunnyvale

Agenda Item

18-0926

Agenda Date: 10/30/2018

SUBJECT

Adopt Ordinance No. 3137-18 amending Section 3.80.040 of Chapter 3.80 (Minimum Wage) of Title 3 (Revenue and Finance) of the Sunnyvale Municipal Code related to Minimum Wage Consumer Price Index Increases

RECOMMENDATION

Adopt Ordinance No. 3137-18.

ATTACHMENT

1. Ordinance No. 3137-18

ORDINANCE NO. 3137-18

**AN ORDINANCE OF THE CITY COUNCIL OF THE CITY
OF SUNNYVALE TO AMEND SECTION 3.80.040 OF
CHAPTER 3.80 (MINIMUM WAGE) OF TITLE 3
(REVENUE AND FINANCE) OF THE SUNNYVALE
MUNICIPAL CODE RELATED TO MINIMUM WAGE
CONSUMER PRICE INDEX INCREASES**

WHEREAS, on October 28, 2014, the City Council adopted Sunnyvale Municipal Code Chapter 3.80 to implement a local minimum wage for the benefit of workers within the City of Sunnyvale; and

WHEREAS, the City's minimum wage increased to fifteen dollars (\$15.00) per hour on January 1, 2018, with the next increase scheduled for January 1, 2019, based on the Consumer Price Index (Urban Wage Earners and Clerical Workers, San Francisco-Oakland-San Jose, CA for All Items); and

WHEREAS, the Santa Clara County Cities Association made regional minimum wage a priority and many cities adopted a minimum wage that capped the Consumer Price Index ("CPI") increases at a maximum of five percent annually; and

WHEREAS, the Bureau of Labor Statistics modified the name of the Bay Area CPI Index to replace San Jose with Hayward and is now referred to as "San Francisco-Oakland-Hayward"; and

WHEREAS, this ordinance is intended to align the City's local minimum wage rates with other cities within Santa Clara County by imposing a five percent maximum cap for the annual CPI increase as well as update the Bay Area CPI index name.

NOW THEREFORE, THE CITY COUNCIL OF THE CITY OF SUNNYVALE DOES ORDAIN AS FOLLOWS:

SECTION 1. Section 3.80.040 AMENDED. Section 3.80.040 of Chapter 3.80 (Minimum Wage Ordinance) of Title 3 (Revenue and Finance) of the Sunnyvale Municipal Code is hereby amended as follows:

3.80.040. Minimum wage.

- (a) [Text unchanged]
- (b) Effective January 1, 2015, the Minimum Wage shall be an hourly rate of ten dollars and thirty cents (\$10.30). On July 1, 2016, the minimum wage shall be an hourly rate of eleven dollars (\$11.00). On January 1, 2017, the minimum wage shall be an hourly rate of thirteen dollars (\$13.00). On January 1, 2018, the minimum wage shall be an hourly rate of fifteen dollars (\$15.00). To prevent inflation from eroding its value, beginning on January 1, 2019, and each

January 1st thereafter, the Minimum Wage shall increase by an amount corresponding to the prior year's increase, if any, in the cost of living up to a maximum of five (5) percent annually. The prior year's increase in the cost of living shall be measured by the percentage increase, if any, as of August of the immediately preceding year of the Bay Area Consumer Price Index (Urban Wage Earners and Clerical Workers, San Francisco-Oakland-Hayward, CA for All Items) or its successor index as published by the U.S. Department of Labor or its successor agency, with the amount of the Minimum Wage increase rounded to the nearest multiple of five (5) cents. The adjusted Minimum Wage shall be announced by November 1st of each year, or as soon as practicable thereafter if the Consumer Price Index for August has not yet been published, and shall become effective as the new Minimum Wage on January 1st of the following year. If there is no increase in the Bay Area Consumer Price Index, the minimum wage shall remain unchanged for that year.

(c) – (e) [Text unchanged]

SECTION 2. CEQA - EXEMPTION. The City Council finds, pursuant to Title 14 of the California Code of Regulations, Section 15061(b)(3), that this ordinance is exempt from the requirements of the California Environmental Quality Act (CEQA) in that it is not a Project which has the potential for causing a significant effect on the environment.

SECTION 3. CONSTITUTIONALITY; SEVERABILITY. If any section, subsection, sentence, clause or phrase of this ordinance is for any reason held to be invalid, such decision or decisions shall not affect the validity of the remaining portions of this ordinance. The City Council hereby declares that it would have passed this ordinance, and each section, subsection, sentence, clause and phrase thereof irrespective of the fact that any one or more sections, subsections, sentences, clauses or phrases be declared invalid.

SECTION 4. EFFECTIVE DATE. This ordinance shall be in full force and effect thirty (30) days from and after the date of its adoption.

SECTION 5. POSTING AND PUBLICATION. The City Clerk is directed to cause copies of this ordinance to be posted in three (3) prominent places in the City of Sunnyvale and to cause publication once in The Sun, the official publication of legal notices of the City of Sunnyvale, of a notice setting forth the date of adoption, the title of this ordinance, and a list of places where copies of this ordinance are posted, within fifteen (15) days after adoption of this ordinance.

Introduced at a regular meeting of the City Council held on October 16, 2018, and adopted as an ordinance of the City of Sunnyvale at a regular meeting of the City Council held on _____, by the following vote:

AYES:

NOES:

ABSTAIN:

ABSENT:

RECUSAL:

ATTEST:

APPROVED:

City Clerk
Date of Attestation: _____

Mayor

(SEAL)

APPROVED AS TO FORM:

City Attorney



City of Sunnyvale

Agenda Item

18-0856

Agenda Date: 10/30/2018

REPORT TO COUNCIL

SUBJECT

Adopt a Resolution to Establish 15 Mile Per Hour Speed Zones at 35 Locations Adjacent to 16 Public Schools in Sunnyvale, Find the Action is Categorically Exempt under CEQA and Approve Budget Modification No. 7 for \$107,600 for Installation of Signs

BACKGROUND

Staff received a City Council request to consider the possibility of implementing 15 miles per hour (mph) speed limits at 35 locations adjacent to 16 Sunnyvale public schools (Attachment 1). City of Sunnyvale Municipal Code (SMC) Chapter 10.08 allows the City Traffic Engineer to place and maintain official traffic control devices when and as required under the City's traffic ordinances and resolutions or pursuant to the California Vehicle Code. These traffic control devices shall be installed as specified in the traffic control regulations document as per the latest standards and guidelines established by the California Department of Transportation (Caltrans). Standards and guidelines for establishing speed limits on city streets including school zones are given in the California Manual on Uniform Traffic Control Devices (CA MUTCD) and the 2014 California Manual for Setting Speed Limits published by Caltrans. Additionally, California Vehicle Code section 22358.4 allows a local agency to implement 15 MPH school speed zones in certain residential locations.

On February 28, 2012, staff prepared a Report to the Council (RTC No. 12-040, Attachment 2) for authorizing and establishing 15 mph speed zones at 37 locations near public schools in Sunnyvale. The Council did not approve changing the speed limits to 15 mph at these locations, and directed staff to further evaluate lowering the speed limit to 15 mph in school zones as part of a larger study.

In November 2012, the Comprehensive School Traffic Study was completed, which identified improvements on school routes near Sunnyvale public schools. This study did not recommend establishing 15 mph speed zones at schools, however, the study did identify locations requiring further evaluation for establishing 15 mph speed zones. At its December 4, 2012 meeting, the City Council adopted a criterion as part of a Comprehensive School Traffic Study to allow consideration of 15 mph speed zones on those qualifying locations that have documented higher traffic speeds (RTC No. 12-279, Attachment 3). The criterion is for those school area streets that have an 85th percentile speed greater than 25 mph during school commute times, establishment of a 15 mph speed zones would be recommended. The City established criterion is consistent with the Caltrans standards and guidelines for setting speed limits including school zones. Following this criterion so far, staff has implemented 15 mph speed zones at the following locations (RTC No. 14-0092, Attachment 4): Lakechime Drive, Meadowlake Drive, and Silverlake Drive. Of these three locations, Silverlake Drive was not one of the locations listed in the 2012 staff report.

EXISTING POLICY

General Plan, Chapter 3, *Land Use and Transportation Element*:

Goal C: An Effective Multimodal Transportation System - Offer the community a variety of transportation modes for local travel that are also integrated with the regional transportation system and land use pattern. Favor accommodation of alternative modes to the automobile as a means to enhance efficient transit use, bicycling, and walking and corresponding benefits to the environment, person-throughput, and qualitative improvements to the transportation system environment.

ENVIRONMENTAL REVIEW

Approving the Resolution changing speed limits on certain streets in Sunnyvale is a project that is categorically exempt from environmental review pursuant to the California Environmental Quality Act Guidelines section 15301(c) because it involves the operation of an existing street involving negligible or no expansion of use.

DISCUSSION

Notwithstanding the provision of the CA MUTCD and the 2014 California Manual for Setting Speed Limits, the California Vehicle Code (CVC) section 22358.4 (b)(1) allows a local authority, by ordinance or resolution, to determine and declare prima facie speed limits as follows:

A 15 mph prima facie speed limit in a residence district, on a highway with a posted speed limit of 30 mph or slower, when approaching, at a distance of less than 500 feet from, or passing, a school building or the grounds of a school building, contiguous to a highway and posted with a school warning sign that indicates a speed limit of 15 mph, while children are going to or leaving the school, either during school hours or during the noon recess period.

The prima facie speed limit shall also apply when approaching, at a distance of less than 500 feet from, or passing, school grounds that are not separated from the highway by a fence, gate, or other physical barrier while the grounds are in use by children and the highway is posted with a school warning sign that indicates a speed limit of 15 mph.

The prima facie speed limits established under this criterion apply only to the streets that meet all the following conditions:

- a. A residential street with a maximum of two traffic lanes.
- b. A maximum posted 30 mph prima facie speed limit immediately prior to and after the school zone.
- c. The prima facie speed limits established will apply to both directions of travel.
- d. When determining the need to lower the prima facie speed limit, the local authority shall take the provisions of CVC Section 627 described in the subsequent paragraph into consideration.

The CVC Section 627 further requires that when determining the need to lower prima facie speed limit, the local authority shall take the following provisions into consideration:

- a. Prevailing speeds as determined by traffic engineering measurements.
- b. Accident records.
- c. Highway, traffic, and roadside conditions not readily apparent to the driver.
- d. Residential density.
- e. Pedestrian and bicyclist safety.

Staff has reviewed the remaining 35 locations identified in the Comprehensive School Traffic Study. Staff has considered the provisions of CVC Section 627 in general, but has not performed detailed evaluations of factors listed in the CVC Section 627 for the 35 remaining individual locations. A comprehensive evaluation of these factors will require additional data collection. However, based on the proximity to schools and roadway characteristics, staff feels that the remaining 35 locations (Attachment 4) could qualify for consideration of 15 mph school zones.

The Department of Public Safety may receive additional complaints at these locations if drivers do not modify behavior upon the posting of the new 15 mph zones.

FISCAL IMPACT

Staff estimates \$107,600 for installation of speed limit signs at all 35 locations. Another \$75,000 will be needed if council directs staff to perform a detailed analysis of these locations. Staff recommends approval of Budget Modification No. 7 to establish a new project to fund this work.

Budget Modification No. 7 FY 2018/19

	Current	Increase/ (Decrease)	Revised
General Fund			
<u>Expenditures</u>			
New Project - Fifteen MPH Speed Limit Near Public Schools	\$0	\$107,600	\$107,600
<u>Reserves</u>			
Budget Stabilization Reserve	\$32,762,690	(\$107,600)	\$32,655,090

Funding Source

This cost will be funded by the General Fund Budget Stabilization Fund.

PUBLIC CONTACT

Public contact was made by posting the Council agenda on the City's official-notice bulletin board outside City Hall, at the Sunnyvale Senior Center, Community Center and Department of Public Safety; and by making the agenda and report available at the Sunnyvale Public Library, the Office of the City Clerk and on the City's website.

ALTERNATIVES

1. Adopt the attached Resolution to create 15 mph speed zones at 35 locations adjacent to 16 public schools in Sunnyvale, find the action categorically exempt pursuant to CEQA section 15301(c), and approve Budget Modification No. 7 to appropriate \$107,600 for installation costs.
2. Direct Staff to conduct a study based on the City established criteria at 35 locations adjacent to 16 public schools in Sunnyvale (Attachment 4), amend Budget Modification No. 7 to provide additional funding of \$75,000 for the study, and approve the amended Budget Modification No. 7 to appropriate \$182,600 for the study and sign installations.

3. Do not conduct a citywide study and continue to follow the existing city practices for setting speed zones including 15 mph speed zones. This alternative will not require any additional funding.

STAFF RECOMMENDATION

Staff recommends Alternative No. 1: Adopt the attached Resolution to create 15 mph speed zones at 35 locations adjacent to 16 public schools in Sunnyvale, find the action categorically exempt under California Environmental Quality Act Guidelines section 15301(c), and approve Budget Modification No. 7 to appropriate \$107,600 for installation costs.

Prepared by: Ganesh Karkee, Traffic Engineer

Reviewed by: James Boone, Captain, Public Safety

Reviewed by: Shahid Abbas, Transportation and Traffic Manager

Reviewed by: Timothy J. Kirby, Director of Finance

Reviewed by: Chip Taylor, Director, Public Works

Reviewed by: Teri Silva, Assistant City Manager

Approved by: Kent Steffens, City Manager

ATTACHMENTS

1. 35 locations adjacent to 16 public schools in Sunnyvale for establishing 15 mph speed zones
2. RTC No. 12-040
3. RTC No. 12-279 including the Comprehensive School Traffic Study
4. RTC No. 14-0092
5. Resolution to adopt 15 MPH School Speed Zones

CITY OF SUNNYVALE
Candidate Locations for Establishing 15 MPH Speed Zones

No.	School Name	Street Name	Limit 1	Limit 2	Current Speed Limit
1	Fairwood Elementary	Sandia Avenue	500 ft west of school property line	500 ft southeast of school property line	25
2	Fairwood Elementary	Fairwood Avenue	500 ft north of school property line	500 ft south of school property line	25
3	Columbia Middle	Morse Avenue	Ahwanee Avenue	500 ft south of school property line	25
4	Columbia Middle	San Diego Avenue	Hemlock Avenue	500 ft south of school property line	25
5	Columbia Middle	Hemlock Avenue	500 ft west of San Diego Avenue	San Diego Avenue	25
6	San Miguel Elementary	San Miguel Avenue	Alvarado Drive	500 ft south of school property line	25
7	San Miguel Elementary	Alvarado Avenue	San Juan Drive	San Pablo Avenue	25
8	Bishop Elementary	Maude Avenue	500 ft west of school property line	500 ft east of school property line	30
9	Bishop Elementary	Sunnyvale Avenue	Maude Avenue	500 ft south of school property line	25
10	Bishop Elementary	Hazelton Avenue	Sunnyvale Avenue	Hazelton Avenue	25
11	Bishop Elementary	Bayview Avenue	Arbor Drive	500 ft south of school property line	25
12	Vargas Elementary	Leota Avenue	500 ft north of school property line	500 ft south of school property line	25
13	Vargas Elementary	Carson Drive	Leota Avenue	Mary Avenue	25
14	Cumberland Elementary	Cumberland Drive	Piper Avenue	Quetta Avenue	25
15	Cumberland Elementary	Quetta Avenue	500 ft north of school property line	500 ft south of school property line	25
16	Ellis Elementary	Central Avenue	500 ft north of school property line	Old San Francisco Road	25
17	Ellis Elementary	Olive Avenue	500 ft west of school property line	500 ft east of school property line	25
18	Brady Elementary	Gail Avenue	500 ft north of school property line	500 ft south of school property line	25
19	Ponderosa Elementary	Iris Avenue	500 ft west of school property line	500 ft east of school property line	25
20	Ponderosa Elementary	Ponderosa Avenue	Foxglove Drive	500 ft south of school property line	25
21	Cherry Chase Elementary	Grape Avenue	500 ft north of school property line	500 ft south of school property line	25
22	Cherry Chase Elementary	Heatherstone Way	500 ft west of school property line	500 ft east of school property line	25
23	Sunnyvale Middle School	Mango Avenue	500 ft north of school property line	Remington Drive	25
24	Peterson Middle School	Rosalia Avenue	Bryant Way	Poplar Avenue	25
25	Peterson Middle School	Poplar Avenue	500 ft north of school property line	Rosalia Avenue	25
26	Peterson Middle School	Bryant Avenue	Poplar Avenue	500 ft east of school property line	25
27	Stockmeir Elementary School	Dunholme Way	500 ft west of school property line	500 ft east of school property line	25
28	Stockmeir Elementary School	Condor Way	Dunholme Way	Harrow Way	25
29	Nimitz Elementary School	Cascade Drive	500 ft west of school property line	Yukon Drive	25
30	Nimitz Elementary School	Cheyenne Drive	500 ft west of school property line	Valcartier Drive	25
31	Cupertino Middle School	Bernardo Avenue	500 ft north of school property line	Homestead Road	30
32	Cupertino Middle School	Helena Drive	Bernardo Avenue	500 ft east of school property line	25
33	West Valley Elementary School	Belleville Way	500 ft north of school property line	500 ft south of school property line	25
34	West Valley Elementary School	Barton Drive	500 ft north of school property line	Bedford Avenue	25
35	West Valley Elementary School	Bedford Avenue	500 ft north of school property line	Barton Drive	25

**Council Meeting: February 28, 2012****SUBJECT: Resolution to Authorize and Establish New School Zone Speed Limits****REPORT IN BRIEF**

State law allows local jurisdictions by ordinance or resolution to establish 15 mile per hour zones during school arrival and departure times within 500 feet of school buildings or grounds on roadways with an established speed limit of 30 miles per hour or less. Sunnyvale's Municipal Code calls for speed limits to be established by resolution. At the suggestion of the Bicycle and Pedestrian Advisory Commission (BPAC), this report recommends that City Council adopt a resolution (Attachment A) to establish 15 mile per hour school zones at all public schools in Sunnyvale with the exception of Fremont High School.

EXISTING POLICY

Land Use and Transportation Element C3, Attain a transportation system that is effective, safe, pleasant and convenient.

DISCUSSION

State law was modified in 2008 to allow local jurisdictions the authority to establish 15 mile per hour speed zones near schools. Normally enforceable speed limits are set according to requirements of the California Vehicle Code (CVC) and procedures defined by the California Manual of Uniform Traffic Control Devices (CA-MUTCD). Establishment of speed limits involves measuring prevailing speeds, reviewing collision records, and identifying any roadway, traffic, or roadside conditions that are not readily apparent to the driver. The CA-MUTCD defines how these factors are considered to establish the posted speed. The CVC contains certain exceptions to the required method for establishing speed limits, including an exception to allow local authorities to adopt 15 mile per hour school area speed zones adjacent to schools in residential areas and on streets where the posted speed limit is 30 miles per hour or less. Reduced speeds can lower the severity of collisions. An ordinance or resolution must be approved in support of creation of the 15 mile per hour zones.

The BPAC considered this as a potential 2012 study issue (Attachment B). Council dropped the issue in accordance with staff's recommendation, which suggested the evaluation of appropriate speed limits was primarily an operational issue that would be accommodated within existing work programs. Thirty-seven locations in Sunnyvale meet the criteria defined by the CVC for 15

mile per hour school speed limit zones (See list of locations in Attachment A). Zones would extend 500 feet from the boundary of the subject school or to the nearest street terminus. Staff is not recommending creating zones near private schools because these facilities do not generate significant bicycle and pedestrian traffic, which is a focus of this initiative.

FISCAL IMPACT

Staff estimates the cost of installation at approximately \$30,000. Staff will present a funding recommendation as part of the FY 12/13 budget to utilize new funds added to vehicle registration fees. This new funding source becomes available to Sunnyvale starting July 1, 2012 and is available for roadway safety enhancements such as this.

PUBLIC CONTACT

Public contact was made by posting the Council agenda on the City's official-notice bulletin boards outside City Hall, in the Council Chambers lobby, the City Clerk's office, at the Library, Senior Center, Community Center, and Department of Public Safety; posting the agenda and report on the City's Web site; and making the report available at the Office of the City Clerk.

ALTERNATIVES

1. Approve the attached Resolution modifying speed limits to 15 miles per hour at 37 locations near schools in Sunnyvale.
2. Do not take action at this time.

RECOMMENDATION

Staff recommends Alternative No. 1: Approve the attached Resolution modifying speed limits to 15 miles per hour at 37 locations near schools in Sunnyvale. Reducing speed limits near schools will encourage cautious driving and potentially reduce the severity of collisions that occur near schools.

Reviewed by:

Kent Steffens, Director of Public Works

Prepared by: Jack Witthaus, Transportation and Traffic Manager

Approved by:

Gary M. Luebbers, City Manager

Attachments:

- A. Resolution
- B. DPW 12-02 Establishment of 15 Miles Per Hour School Zone and Flashing Warning Signs

RESOLUTION NO. _____

**A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF
SUNNYVALE AMENDING RESOLUTION NO. 203-95 TO
AUTHORIZE NEW SCHOOL ZONE SPEED LIMITS**

WHEREAS, Title 10 of the Sunnyvale Municipal Code provides that the designation of speed limits is to be accomplished by the City Council by resolution;

NOW, THEREFORE, BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF SUNNYVALE THAT it has been determined it is necessary to amend Sunnyvale Municipal Code Section 10.28.010 by adding the list of "School Zone Speed Limits" as set forth in Exhibit "A."

Adopted by the City Council at a regular meeting held on _____, 2012, by the following vote:

AYES:

NOES:

ABSTAIN:

ABSENT:

ATTEST:

APPROVED:

City Clerk
(SEAL)

Mayor

APPROVED AS TO FORM AND LEGALITY:

David E. Kahn, City Attorney

III. School Zone Speed Limits Pursuant to Municipal Code Section 10.28.010

Name of School	Name of Street Affected	From	To	Speed Limit in Miles per Hour
Fairwood	Sandia	500 ft. southeast of school property line	500 ft. west of school property line	15
	Fairwood	500 ft. north of school property line	500 ft. south of school property line	15
Lakewood	Meadow Lake	Lakebird Drive	500 ft. south of school property line	15
	Lakechime	Silverlake Drive	500 ft. west of school property line	15
Columbia Middle	Morse	Ahwanee Avenue	500 ft. south of school property line	15
	San Diego	Hemlock Avenue	500 ft. south of school property line	15
	Hemlock	San Diego Avenue	500 feet west of San Diego Avenue	15
San Miguel	San Miguel	Alvarado Drive	500 ft. south of school property line	15
	Alvarado	San Juan Drive	San Pablo Avenue	15
Bishop	Maude	500 ft. east of school property line	500 ft. west of school property line	15
	Sunnyvale	Maude Avenue	500 ft. south of school property line	15
	Hazelton	Sunnyvale Avenue	Bayview Avenue	15
	Bayview	Arbor Drive	500 ft. south of school property line	15
Vargas	Leota	500 ft. north of school property line	500 ft. south of school property line	15
	Carson	Mary Avenue	Leota Drive	15
Cumberland	Cumberland	Quetta Avenue	Piper Avenue	15
	Quetta	500 ft. north of school property line	500 ft. south of school property line	15
Ellis	Central	500 ft. north of school property line	Old San Francisco Road	15
	Olive	500 ft. east of school property line	500 ft. west of school property line	15
Braly	Gail	500 ft. north of school property line	500 ft. south of school property line	15
Ponderosa	Iris	500 ft. east of school property line	500 ft. west of school property line	15
	Ponderosa	Foxglove Drive	500 ft. south of school property line	15
Cherry Chase	Grape	500 ft. north of school property line	500 ft. south of school property line	15
	Heatherstone	500 ft. east of school property line	500 ft. west of school property line	15
Sunnyvale Middle	Mango	500 ft. north of school property line	Remington Drive	15
Peterson Middle	Rosalia	Poplar Avenue	Bryant Way	15
	Poplar	500 ft. north of school property line	500 ft. south of school property line	15
	Bryant	500 ft. east of school property line	500 ft. west of school property line	15
Stockmeir	Dunholme	500 ft. east of school property line	500 ft. west of school property line	15
	Condor	Harrow Way	Dunholme Drive	15
Nimitz	Cascade	Yukon Drive	500 ft. west of school	15

	Cheyenne	Valcartier Drive	property line 500 ft. west of school	15
Cupertino Middle	Bernardo	500 ft. north of school property line	property line Homestead Road	15
	Helena	500 ft. east of school property line	Bernardo Avenue	15
West Valley	Belleville	500 ft. north of school property line	500 ft. south of school property line	15
	Barton	500 ft. north of school property line	500 ft. south of school property line	15
	Ecola	Barton Drive	Bedford Drive	15

ATTACHMENT B

2012 Council Study Issue

DPW 12-02 Establishment of 15 Miles Per Hour School Zones and Flashing Warning Signs

Lead Department Public Works

History 1 year ago None 2 years ago None

1. What are the key elements of the issue? What precipitated it?

This issue would consider the establishment of 15 miles per hour zones near schools. The California Vehicle Code allows for the establishment of 15 MPH zones within 500 feet of schools in residential areas if the speed limit of the subject road is 30 MPH. Some jurisdictions have recently announced that they are pursuing uniform 15 MPH restrictions at schools, regardless of the underlying speed limit. Appropriate signs must be installed, and jurisdictions can enhance signs with flashing beacons at their discretion. The study would identify limitations and discuss efforts by other jurisdictions to broadly implement 15 MPH zones and how that would relate to the Vehicle Code and enforceability. It would also identify costs associated with erecting flashing beacon signs at school zones in Sunnyvale.

2. How does this relate to the General Plan or existing City Policy?

Law Enforcement Sub-Element A.5, Facilitate the safe movement of pedestrians, bicyclists, and vehicles.

3. Origin of issue

Board or Commission Bicycle and Pedestrian Advisory Commission

4. Staff effort required to conduct study Moderate**Briefly explain the level of staff effort required**

The study will require research into the issue of the City implementing enforceable speed limits beyond those allowed by the California Vehicle Code. It will require an inventory of associated signing needs to change speed zones and install flashing beacons, and associated cost estimating. An ordinance or resolution would be required to be prepared to implement new school zone speeds.

5. Multiple Year Project? No Planned Completion Year 2012**6. Expected participation involved in the study issue process?**

Does Council need to approve a work plan? No

Does this issue require review by a Board/Commission? Yes

If so, which? Bicycle and Pedestrian Advisory Commission

Is a Council Study Session anticipated? No

7. Briefly explain if a budget modification will be required to study this issue

Amount of budget modification required 0

ATTACHMENT B**Explanation**

8. Briefly explain potential costs of implementing study results, note estimated capital and operating costs, as well as estimated revenue/savings, include dollar amounts

Are there costs of implementation? Yes

Explanation

Should school zone speeds be approved for change and a flashing beacon project approved, costs would be associated with sign fabrication, beacon procurement, pavement legend striping and installation of signs and beacons. If staff determines that 15 mph speed zones are feasible and enforceable, a budget issue will be developed for consideration in FY2012/13.

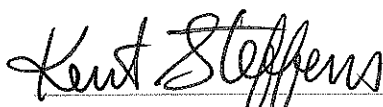
9. Staff Recommendation

Staff Recommendation Drop

If 'Support', 'Drop' or 'Defer', explain

Staff supports an effort to evaluate 15 MPH zones near schools. The evaluation of appropriate speed limits is primarily an operational issue that can be accommodated within existing work programs. Staff will begin this portion of the Study in 2011. However, staff feels the installation of flashing beacons would be cost prohibitive for the City and recommends this portion of the study be dropped.

Reviewed by

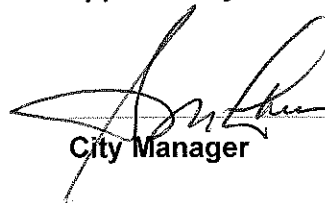


Department Director

10-25-11

Date

Approved by



City Manager

10-27-11

Date

**Council Meeting: December 4, 2012****SUBJECT: Discussion and Possible Action Regarding Comprehensive School Traffic Study - STUDY ISSUE****BACKGROUND**

The City Council approved a 2012 study issue to prepare a Comprehensive School Traffic Study (Attachment A – Study Issue DPW 09-01). This study per Council's revised direction as part of the 2012 budget issues process, focuses on evaluation of school traffic in Sunnyvale from an operational perspective and identifies potential traffic control improvements. The study identifies whether a set of actions exists beyond current traffic controls to improve school zone traffic flow and enhance pedestrian safety. The study maps school routes for Sunnyvale public elementary and middle schools per the California Manual of Uniform Traffic Control Devices (CA-MUTCD) methodology, and evaluates all school route intersections. Data inputs include existing intersection traffic control and approach signing and markings, traffic volume, collision information, speed limits, and roadway classification. Intersection improvement options are then developed using criteria based on guidance and requirements developed from a number of sources, including the (CA-MUTCD), the National Center for Safe Routes to Schools, and examples from other municipalities. The study presents recommendations for nine different types of traffic control modifications to improve pedestrian and bicycling conditions for school age travelers at all City public elementary and middle schools. Over 200 locations are recommended for further detailed evaluation (Attachment B).

This study is separate from a joint Council of Santa Clara Health Department/City of Sunnyvale project to evaluate travel behavior and interface with school administrations and parent groups to develop Transportation Demand Management (TDM) programs for City schools. This multi-year effort has a goal to establish school TDM programs in 80% of Sunnyvale schools.

EXISTING POLICY

Land Use and Transportation Chapter, LT-5.4g Conduct periodic analyses of roadway facilities and collision data in order to assure traffic safety.

Land Use and Transportation Chapter, LT-5.4b Install permanent and painted pavement markings.

Land Use and Transportation Chapter, LT-5.3d Make appropriate hardware and software improvements to traffic signals.

DISCUSSION

The comprehensive school traffic study issue considers new or enhanced traffic controls and pedestrian or bike features on school routes (Attachment C). The methodology is based on school route maps that illustrate existing controls and features and applying in a Geographic Information Systems-based query format a comprehensive set of data on traffic volume, safety, and controls on a school-specific basis. Criteria have been established to determine locations for subsequent detailed engineering evaluation for the installation of new or enhanced traffic devices (controls, warning signs, lighted crosswalks, paddle signs, enhanced striping, etc.). These maps will be used to guide future detailed study and implementation of controls, and also for scoping of possible future Safe Routes to School or other grant applications to implement traffic improvements.

The study considers nine different types of school area traffic control improvements. These are listed below and some examples are illustrated in Attachment D:

1. Rectangular rapid flashing beacons
2. In pavement lighted crosswalks
3. Raised crosswalks/traffic calming
4. Marked/improved crosswalks at uncontrolled intersections
5. High visibility crosswalks at Yield controlled intersections
6. Stop control and improved crosswalks at marked crosswalks
7. Marked crosswalks at Stop controlled intersections
8. Improved traffic signals
9. Fifteen mile per hour school zones

The criteria developed for determining traffic control improvement recommendations contains some flexibility in making recommendations for crosswalk and traffic signal improvements. Depending upon the conditions cited for making a recommendation, the study makes a range of conservative to liberal options for the recommendations on the marking of crosswalks and improvement of existing traffic signals. For example, when considering whether to mark crosswalks at stop controlled intersections along school routes, the study identifies five different criteria for installation. These range from proximity (mark crosswalks at all stop controlled intersections within $\frac{1}{2}$ mile of a school), to traffic volume (mark if volume is over 2,000 vehicles/day, VPD), to collision history and street classification, to marking of only three and four-way controlled intersections with a higher traffic volume (2,000 VPD). The result are options for marking high visibility crosswalks at a range of locations from two intersections to 257 intersections. Staff intends to apply the study results by conducting more detailed investigations beginning with the more conservative criteria results and moving to the more liberal. Location-specific investigations and judgments will need to be made to balance the in-the-field conditions with the resources available to install and maintain traffic controls.

The Comprehensive School Traffic Study provides a focused framework for moving forward on making school area traffic control improvements. It is anticipated that location specific investigations will advance over the next year utilizing funds approved by the City Council and contracting for professional services to conduct location-specific studies. Installation of improvements will primarily occur with grant funds that are pursued and secured based on completion of location-specific studies and development of competitive projects. Some items will be “just do it” items within the existing resources of the City and justified by the Comprehensive School Traffic Study, such as limited signing and striping improvements like crosswalks, yield bars, and warning signs. Existing operating budgets likely cannot cover the cost of widespread signing and striping improvements in the near term, however. Items such as higher cost lighted systems and traffic signal systems will require grant resources.

15 Mile Per Hour School Zones

At its February 28, 2012 meeting, the City Council considered the blanket establishment of 15 mile per hour school zones at qualifying locations per State law and acted not to enact the zones Citywide. This action still remains in the toolbox of school traffic controls, however. State law was modified in 2008 to allow local jurisdictions the authority to establish 15 mile per hour speed zones near schools. This is a certain exception in the California Vehicle Code (CVC) to the required method for establishing speed limits. Local authorities may adopt 15 mile per hour school area speed zones adjacent to schools in residential areas and on streets where the posted speed limit is 30 miles per hour or less. Reduced speeds can lower the rate and severity of collisions. An ordinance or resolution must be approved in support of creation of the 15 mile per hour zones

Staff has developed a criterion as part of the Comprehensive School Traffic Study to allow consideration of 15 mile per hour zones on those qualifying street segments that have documented higher traffic speeds. The criterion proposed is for those school area streets that have an 85 percentile speed greater than 25 miles per hour during school commute times, establishment of a 15 mile per hour zone would be recommended. Staff would utilize this criterion by conducting speed surveys to determine which school area streets are experiencing high traffic speeds, and may warrant corrective action. Any change in speed limits would still require Council action by resolution.

Bicycle and Pedestrian Advisory Commission Recommendation

The Sunnyvale Bicycle and Pedestrian Advisory Commission considered this item at its November 15, 2012 meeting and recommended that the City Council support the staff recommendation. During their discussion of the item, a BPAC member requested that the report be shared with school administrators. Staff will follow up to assure this happens.

FISCAL IMPACT

Additional detailed study of locations will be done utilizing funding approved for preparation and implementation of the Comprehensive School Traffic Study. Implementation of traffic control modifications will be done within the confines of the Public Works operating budget, augmented by grant funding as it is secured for future school traffic safety projects.

PUBLIC CONTACT

Public contact was made by posting the Council agenda on the City's official-notice bulletin board outside City Hall, at the Sunnyvale Senior Center, Community Center and Department of Public Safety; and by making the agenda and report available at the Sunnyvale Public Library, the Office of the City Clerk and on the City's Web site.

The Bicycle and Pedestrian Advisory Commission held a public hearing on a draft Report to Council at its November 15, 2012 meeting (Attachment E – Draft meeting minutes).

ALTERNATIVES

1. Accept the Comprehensive School Traffic Study and direct staff to implement the study results.
2. Do not accept the Comprehensive School Traffic Study and direct staff to conduct additional analysis.
3. Do not accept the Comprehensive School Traffic Study and take no further action.

RECOMMENDATION

Staff and the Bicycle and Pedestrian Advisory Commission recommend Alternative No. 1: Accept the Comprehensive School Traffic Study and direct staff to implement the study results.

The Comprehensive School Traffic Study provides a useful tool for planning and implementing school area traffic control improvements, and positioning the City for future grant funding opportunities for school traffic safety projects.

Reviewed by:

Kent Steffens, Director, Public Works

Prepared by: Jack Witthaus, Transportation and Traffic Manager

Approved by:

Gary M. Luebbers
City Manager

Attachments

- A. Study Issue DPW 09-01 Comprehensive School Traffic Study
- B. Summary of Study Recommendations
- C. Comprehensive School Traffic Study
- D. School Traffic Control Devices
- E. Draft Bicycle and Pedestrian Advisory Commission Meeting Minutes of November 15, 2012

2012 Council Study Issue

**DPW 09-01 Comprehensive School Traffic Study (Combined SI's School
TDM Opportunities & School Zone Traffic Controls and Enforcement)**

Lead Department Public Works

History 1 year ago Deferred 2 years ago Above the line

1. What are the key elements of the issue? What precipitated it?

This issue would comprehensively investigate and evaluate school traffic in Sunnyvale from both an operational and programmatic perspective. Three primary areas will be assessed: Transportation Demand Management (TDM), traffic controls, and traffic enforcement. Travel patterns and vehicle and pedestrian conditions at schools, including mode choice, alternative transportation resources, pedestrian patterns, location of pedestrian facilities (especially crosswalks), driving behaviors (especially speeding, right of way compliance and illegal turns), and speed controls will be assessed. For TDM, the study would look at appropriate levels of resources for the City to invest in encouraging effective TDM for schools within the City. The study would look at interfaces between school district and City operations, and opportunities for the City to invoke regulations or encourage TDM to school commuters. The outcome of the TDM evaluation would be recommendations for policy, actions, and resources for a transportation demand management program targeted at City schools. For traffic controls and enforcement, the study would identify whether a set of actions exists beyond current traffic controls and enforcement resources to improve school zone traffic flow and enhance pedestrian safety. This study would include a review of the applicability of CVC 22358.4 provisions regarding lowering of speed limits in school areas. The purpose of the study is to consider concerns that school area loading and unloading is chaotic in many areas and that a high proportion of parents drive their children to school. TDM, additional controls and/or enforcement may improve efficiency and safety.

As per Council action at the January 29, 2010 Study Issues Workshop, this study is the result of merging DPW 09-01, School Transportation Demand Management Opportunities, and DPW 10-08 School Zone Traffic Controls and Enforcement.

2. How does this relate to the General Plan or existing City Policy?

Land Use and Transportation Element Goal C3, Attain a transportation system that is effective, safe, pleasant and convenient.

3. Origin of Issue

Council Member(s) Hamilton, Howe
Board or Commission

4. Staff effort required to conduct study Major**Briefly explain the level of staff effort required**

This study would involve a citywide, school by school analysis of three significant topic areas - programmatic traffic demand actions, engineering/traffic control actions, and enforcement actions. Considerable field investigations, design efforts, and study of operating protocols would be involved. Significant coordination with school districts, individual schools, PTA's and other

stakeholders would be necessary. Such a comprehensive effort would require staffing augmentation by consultants and involvement of staff from several disciplines.

5. **Multiple Year Project?** Yes **Planned Completion Year** 2013

6. **Expected participation involved in the study issue process?**

Does Council need to approve a work plan? No

Does this issue require review by a Board/Commission? Yes

If so, which? Bicycle and Pedestrian Advisory
Commission

Is a Council Study Session anticipated? No

7. **Briefly explain if a budget modification will be required to study this issue**

Amount of budget modification required 500000

Explanation

A total of 28 schools would be targeted by the study. Staff estimates 200 consultant hours per school would be required for data collection, meetings with stakeholders, and development of school-specific action plans. A budget modification of approximately \$500,000 would be required. There would be staff time implications to the Department of Public Works and the Department of Public Safety.

8. **Briefly explain potential costs of implementing study results, note estimated capital and operating costs, as well as estimated revenue/savings, include dollar amounts**

Are there costs of implementation? Yes

Explanation

Should a TDM program be adopted, this could involve capital improvements to direct traffic or improve alternative transportation routes to schools. An ongoing program involving elements such as ridematching, walking school buses, or bike safety courses would require resources to manage the program, provide educational and promotional materials, etc. This study could also result in recommendations for new traffic controls at schools Citywide. This could represent a capital investment of considerable scope. The study could also result in recommendations for additional traffic enforcement or crossing guard resources, which can have a significant operating cost.

9. **Staff Recommendation**

Staff Recommendation Drop

If 'Support', 'Drop' or 'Defer', explain

Staff believes this issue is largely operational, and that a significant portion of the responsibility for school traffic should fall on school districts rather than the City. The City does, however, currently direct available resources to address school traffic issues as they arise. Also, the City, in partnership with the County Public Health Department, recently submitted a successful grant application for a comprehensive school traffic demand management program that will address many of the issues raised in the proposed study issue. This program will use a collaborative process to reach a minimum of 80% of Sunnyvale schools to design and implement transportation demand management programs and identify other measures that can be implemented within existing resource constraints. City staff from the Department of Public

Works and the Department of Public Safety are participating in the project, including site specific workshops with school staff and parents to design and implement transportation measures.

Reviewed by

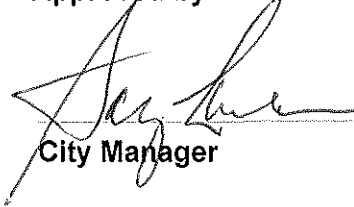


Department Director

10-3-11

Date

Approved by



City Manager

10-4-11

Date

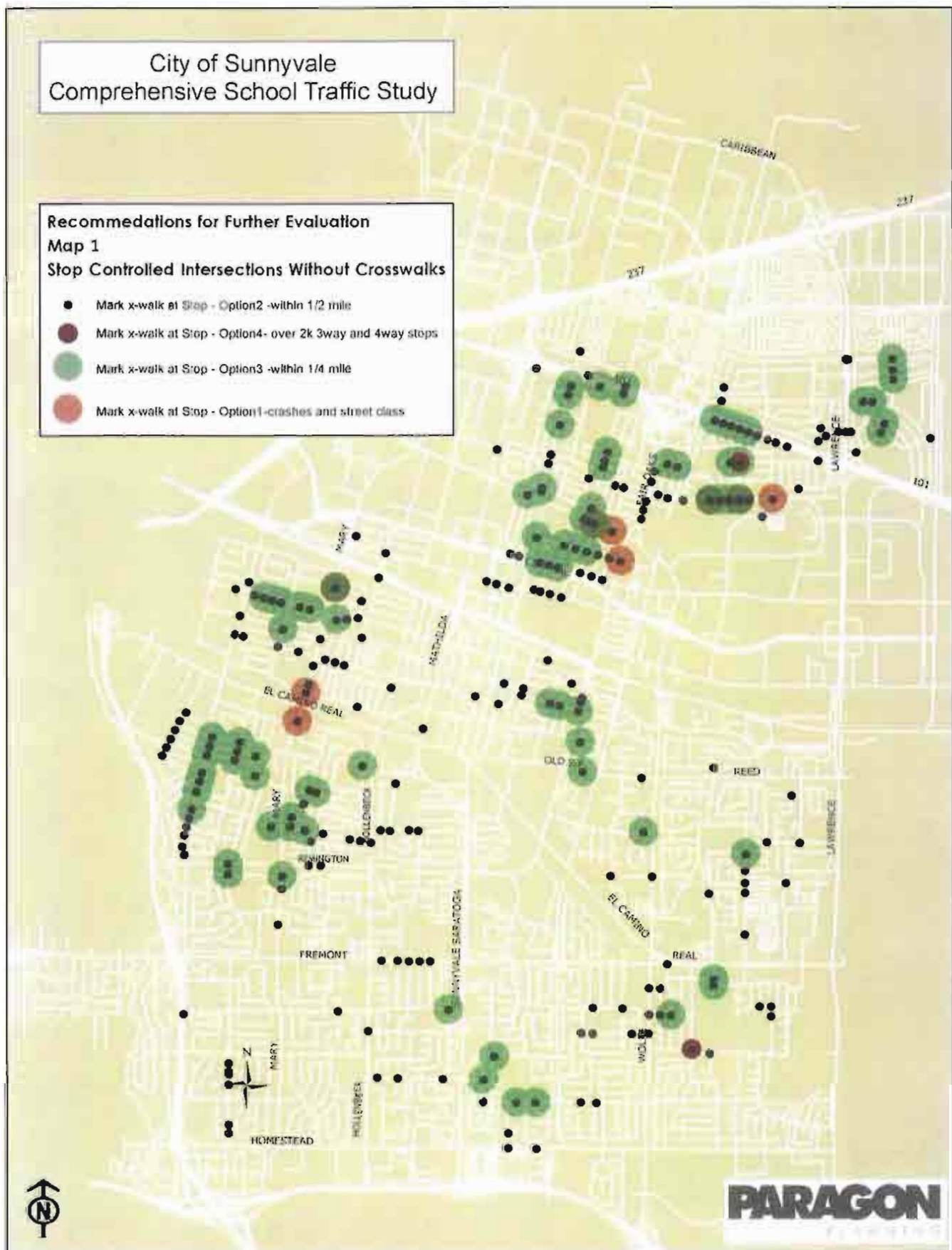
City of Sunnyvale Comprehensive School Traffic Study

Recommendations for Further Evaluation

Map 1

Stop Controlled Intersections Without Crosswalks

- Mark x-walk at Stop - Option2 - within 1/2 mile
- Mark x-walk at Stop - Option4 - over 2k 3way and 4way stops
- Mark x-walk at Stop - Option3 - within 1/4 mile
- Mark x-walk at Stop - Option1 - crashes and street class



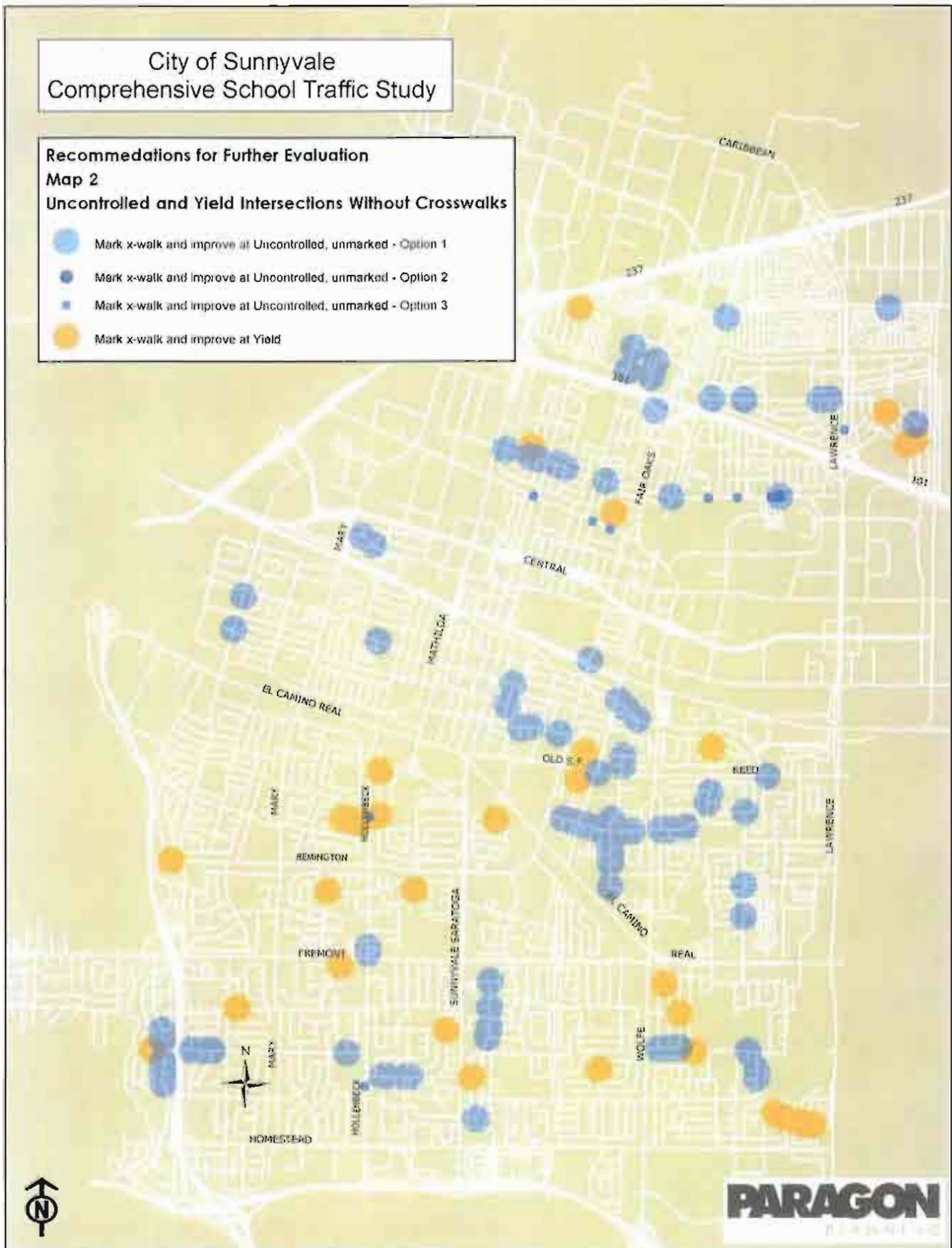
City of Sunnyvale Comprehensive School Traffic Study

Recommendations for Further Evaluation

Map 2

Uncontrolled and Yield Intersections Without Crosswalks

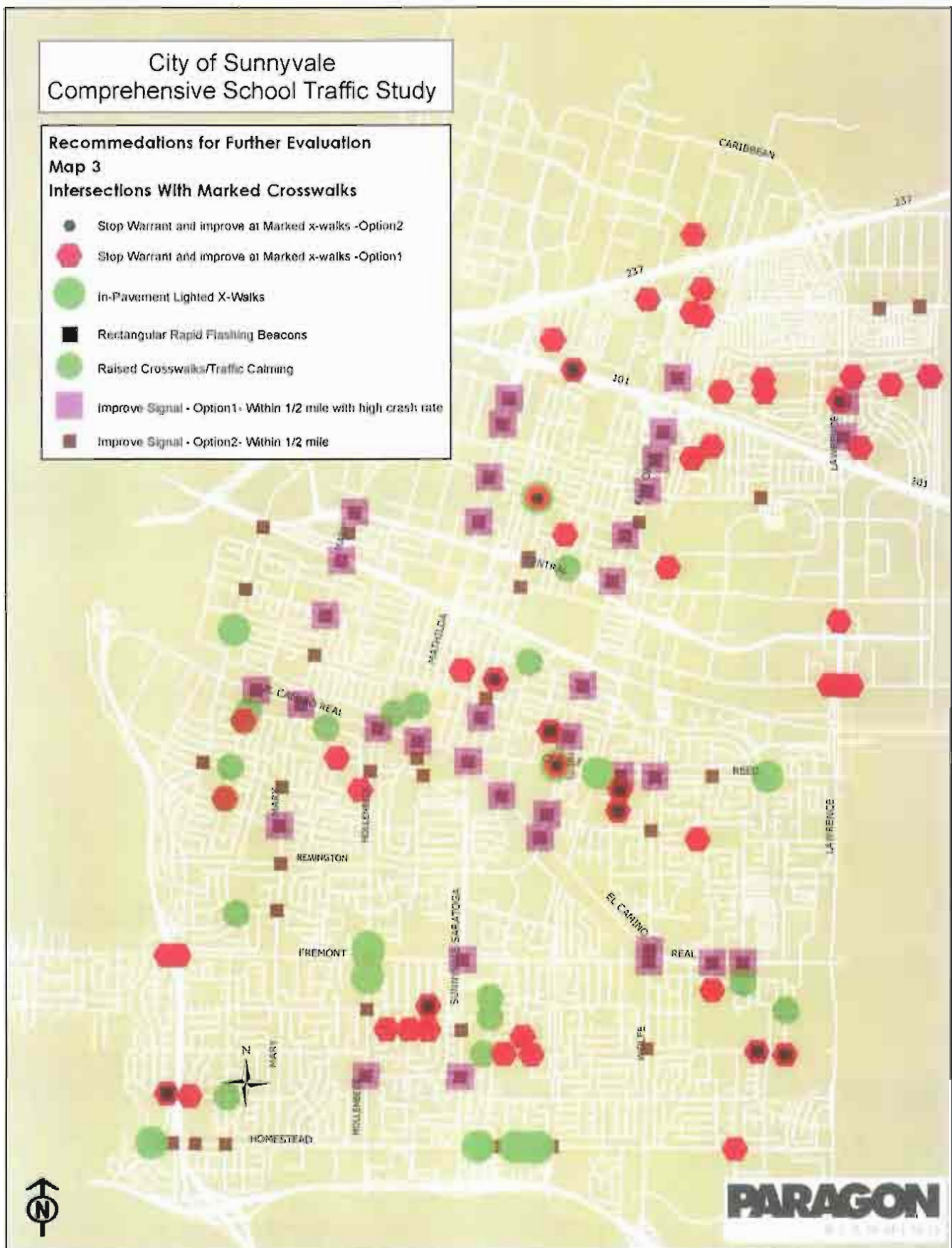
- Mark x-walk and improve at Uncontrolled, unmarked - Option 1
- Mark x-walk and improve at Uncontrolled, unmarked - Option 2
- Mark x-walk and improve at Uncontrolled, unmarked - Option 3
- Mark x-walk and improve at Yield



City of Sunnyvale Comprehensive School Traffic Study

Recommendations for Further Evaluation Map 3 Intersections With Marked Crosswalks

- Stop Warrant and improve at Marked x-walks -Option2
- Stop Warrant and improve at Marked x-walks -Option1
- In-Pavement Lighted X-Walks
- Rectangular Rapid Flashing Beacons
- Raised Crosswalks/Traffic Calming
- Improve Signal - Option1- Within 1/2 mile with high crash rate
- Improve Signal - Option2- Within 1/2 mile



City of Sunnyvale

Comprehensive School Traffic Study



November 2012

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Section 3 Intersection Improvement Identification	Page 18
Section 4 City-Wide Recommendation Tables and Maps	Page 31
Section 5 School Attendance Area Recommendation Maps	Page 36

SECTION 1

INTRODUCTION

The goal of the Sunnyvale Comprehensive School Traffic Study is to proactively and uniformly identify intersections for improvement and/or further study along routes used by children to walk and bike to school.

The study aggregated existing transportation data for all intersections along school routes within the City of Sunnyvale. The data included intersection traffic control, traffic volume, collision information, speed limit, roadway

classification, and existing signs and markings.

Intersection treatment options and criteria for implementation were then compiled to serve as a tool box for implementation of enhanced traffic control along routes used by children to walk to school. This was developed using the guidance and requirements from the Manual on Uniform Traffic Control Devices (MUTCD), current research from the National Center for Safe Routes to School, the 2007 Sunnyvale

Pedestrian Safety and Opportunities Study, examples from other municipalities, along with a number of other sources. This is detailed in Section 2 of the report.

The implementation criteria in conjunction with the transportation data were used to uniformly identify locations for further analysis. This is detailed in Section 3 of the report. The tables in Section 3 describe the existing traffic control and markings, the enhanced treatment options for consideration, and the criteria used to identify possible candidate intersections. In some cases multiple criteria were applied, varying from broad to restrictive. This is the case when “Options” are specified. The tables are also meant to accompany both the city wide and the school specific maps.

Future work will include a more detailed evaluation of each intersection and will identify locations where pedestrian counts, turning movement counts, speed surveys, and gap analysis should be collected.

SECTION 2

IMPROVEMENT OPTIONS AND CRITERIA

There are many different ways that an intersection can be modified to improve the safety, comfort, and convenience for children and families walking to school. This section describes a number of these intersection treatments that may be relevant for school routes in Sunnyvale. For each treatment there is a description, recommended implementation criteria for Sunnyvale, MUTCD Guidance, and a description of other precedence or details to consider.

Engineering criteria for devices on school routes allows for a lot of discretion. This toolbox of treatments and information associated with each treatment is aimed at standardizing Sunnyvale's application of improvements comprehensively, rather than on a reactionary basis. This section of the plan relates to the City of Sunnyvale General Plan Policy Policy LT -5.11 – The City should consider enhancing standards for pedestrian facilities.

Pavement Markings

Marked Crosswalks

Painted pedestrian crossings that specify proper locations for pedestrians to cross the street. Design may vary. Two parallel lines are standard. Ladder style is considered high-visibility.

Possible general criteria to consider in analysis:

- Speed limit under 40 mph
- Fewer than 4 lanes of traffic (unless there is a median island)
- Fewer than 12,000 ADT
- Over 20 student crossings in a peak hour.
- Consider crosswalks at intervals of 250 feet.

Recommended Implementation Criteria

Crosswalks should be marked at all intersections on established routes to a school where there is substantial conflict between motorists, bicyclists, and student movements; where students are encouraged to cross between intersections; where students would not otherwise recognize the proper place to cross; or where motorists or bicyclists might not expect students to cross.

Controlled Intersections (signal or stop): Use if a sidewalk exists on both sides of the street.

Uncontrolled Intersections: Must be convenient, accessible and in the direct pedestrian route to school.

Multi-lane or high volume marked crosswalks need substantial treatments so that crash risk does not increase.

Consider midblock crosswalks only if: there is adequate sight distance, protected intersection crossings are more than 200 feet away, the combination of traffic and pedestrian volume justify the installation, gap analysis shows that the frequency and adequacy of gaps in traffic is insufficient. Do not use in locations with speeds greater than 40 mph or Volumes greater than 20,000 vpd.

MUTCD Guidance

Section 3B -18 New marked crosswalks alone, without other measures designed to reduce traffic speeds, shorten crossing distances, enhance driver awareness of the crossing, and/or provide active warning of pedestrian presence, should not be installed across uncontrolled roadways where the speed limit exceeds 40 mph and either:

- A. The roadway has four or more lanes of travel without a raised median or pedestrian refuge island and an ADT of 12,000 vehicles per day or greater; or
- B. The roadway has four or more lanes of travel with a raised median or pedestrian refuge island and an ADT of 15,000 vehicles per day or greater.

Section 7A.03 Important to determine frequency and adequacy of gaps in the traffic stream. Use Traffic Control Devices Handbook Section 1A.11

Section 7C.02 Guidance: Crosswalks should be marked at all intersections on established routes to a school where there is substantial conflict between motorists, bicyclists, and student movements; where

Other Precedence/Details to Consider

Most installation guidance exempts school routes. ITE Recommended Practice on Design and Safety of Pedestrian Facilities Guidance for locations with

young pedestrians based on ped volume and ADT. Below are two examples from the table:

- Do not install with less than 15 peak hr ped crossings and under 7000 ADT
- 2 lane street, with 25 peak hour peds requires 6,000 ADT to meet requirements.

FHWA Study (and Ped SOS):

- Below 12,000ADT there is no significant difference in safety between marked and unmarked for 2 lane roads.
- For multi lane roads and speeds over 40mph, don't install a crosswalk.
- Roads with speeds less than 35mph, and under 12,000 ADT could be a candidate for a marked crosswalk.

San Luis Obispo Installation Guidelines:

- 40 or more peak hour peds, or 30 groupings of 2 or more during a 2 hr. period twice per day
- 85% speed below 40mph
- less than 3 travel lanes in each direction
- proper sight distance
- 2,700 ADT or more
- No controlled crosswalk within one block or 660ft

Sacramento Installation Guidelines:

- 20 peds per peak hour or 60 per 4 hours
- located near a school with the nearest marked crosswalk at least 300 feet away.
- 250 feet of visibility
- If it meets the criteria, a different level of crosswalk is recommended per level.

Transportation Association of Canada:

- counts each youth, or disabled as 2 adults, and each senior as 1.5 when considering ped volume they take crossing opportunity into account. ie. Analysis of vehicle gaps. And community size
- There is a warrant chart based on number of peds and crossing opportunity.

Brookline Guidelines:

- Speed limit 40mph or less

- 20 or more pedestrians during peak hour of vehicle traffic. Less can be considered for child population
- ADT exceeds 3000
- A sidewalk or adequate shoulder exists on both sides of the roadway
- no other crosswalks within 200ft
- adequate sight distance

AASHTO Green Book.

No marked crosswalks on ADT greater than 9000 with 3 or more lanes of traffic.

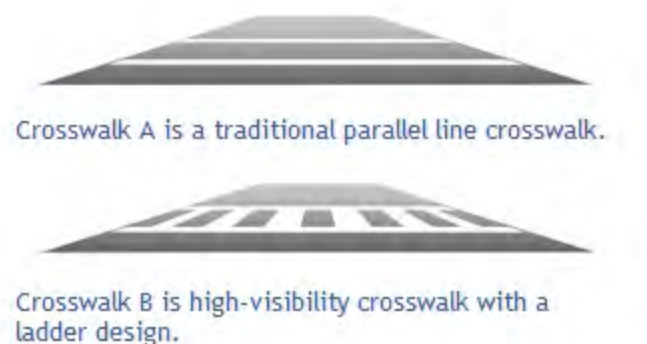


Image from National Center for Safe Routes to School. "Safe Routes to School Guide"

Advanced Stop/Yield Lines

Advance stop or yield lines are used to indicate the optimal stopping point for vehicles. They encourage drivers to stop/yield further back from the crosswalk.

Recommended Implementation Criteria

Use to promote better visibility between pedestrians and motorists, and help to prevent multiple-threat collisions particularly at mid-block or uncontrolled crossings. Consider advanced stop or yield signs at marked crosswalks with more than one lane of traffic in one direction. Install yield lines and signs at all marked crosswalks along a school route.

MUTCD Guidance

Yield here for pedestrians signs and markings may be used in advance of a marked crosswalk that crosses an uncontrolled multi lane approach. Should be placed 20 to 50 feet in advance of the nearest crosswalk line.

Section 3B.16 – If used, stop and yield lines should be placed a minimum of 4 feet in advance of the nearest crosswalk line at controlled intersections, and at midblock crosswalks.

Stop lines at midblock signalized locations should be placed at least 40 feet in advance of the nearest signal indication (see Section 4D.14).

If yield or stop lines are used at a crosswalk that crosses an uncontrolled multi-lane approach, the yield lines or stop lines should be placed 20 to 50 feet in advance of the nearest crosswalk line, and parking should be prohibited in the area between the yield or stop line and the crosswalk (see Figure 3B-17)

California MUTCD Section 7C.03 – The SCHOOL pavement marking may be used to guide, warn, or regulate traffic. CVC 21368. Should not be used at controlled intersections.

Other Precedence/ Details to Consider

Michael Cynecki Study:

–Typical stop lines are 4 feet in advance of the crosswalk, 20 feet for a mid-block location.

Angled or offset stop lines can be considered at signaled

intersections with a multi-lane approach to help improve sight distance in the right lane relative to pedestrians.

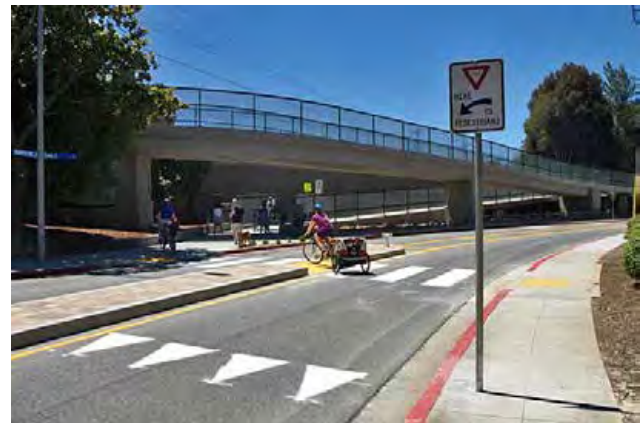
– Not used at most crosswalks

– Wider crosswalk or wider crosswalk lines can also be effective.

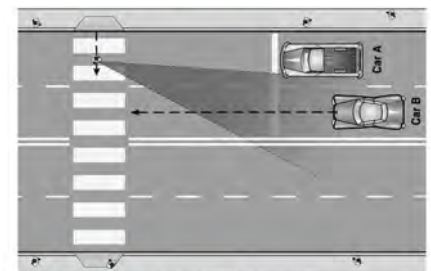
– They may occasionally be used at stop controlled intersections with unmarked crosswalks.

Brookline DPW Guidelines:

Install a stop line at all crosswalks at signalized intersections a min of 4 feet from the crosswalk line.



Heatherstone and Dale, at the SR 85 Ped Bridge



Solution: place advance stop/yield line so car 1 stops further back; car 1 no longer masks car 2, which can better see and be seen by the pedestrian.

Image from National Center for Safe Routes to School. "Safe Routes to School Guide"

Raised Crosswalk

A speed table the width of a typical crosswalk stretching across an entire intersection, slowing traffic and keeping the crossing at grade with the sidewalk.

Recommended Implementation Criteria

Behaves more like a traffic calming device. Use traffic calming protocol for implementation.

Other Precedence/ data to consider

National Center for Safe Routes to School, Safe Routes to School Guide:

- Speed tables may increase the rate that vehicles yield to pedestrians
- Decreases vehicle speed.



Image from National Center for Safe Routes to School. "Safe Routes to School Guide"

Signs, Lights, and Beacons

Rectangular Rapid Flashing Beacons

Rectangular rapid flashing beacons (RRFBs) are active warning devices used to alert motorists of crossing pedestrians at uncontrolled crossings. They remain dark until activated by pedestrians, at which point they emit a bright, rapidly flashing yellow light, which signals drivers to stop. Studies suggest that RRFBs can significantly increase yielding rates over standard pedestrian warning signs

- Consider for high volume and speed roadways
- Consider for ADT greater than 2000 ADT and 85% speed of 40mph or greater.

MUTCD Guidance

They are not currently included in the MUTCD, but jurisdictions can use them if they obtain approval from FHWA.

Recommended Implementation Criteria

- Consider RRFB for midblock crosswalks or uncontrolled marked crosswalks.
- Should be installed on both the right and left side of the crosswalk.
- Do not install within 300 feet of a controlled crossing

Other Precedence/Details to Consider

National Center for Safe Routes to School, Safe Routes to School Guidelines recommends their use at midblock or marked uncontrolled crosswalks.



Image from Pedestrian and Bicycle Information Center Image Library, Photographer Michael Frederick

Flashing Beacons and Overhead Signs

Overhead signs are easier for drivers to see in cases where on-street parking, street trees, or other visual obstructions. Flashing beacons at a marked crosswalk may draw additional attention to the crosswalk. In a busy urban environment, flashing beacons may not provide much benefit, while on a rural road, they may increase driver awareness of the crosswalk. Unlike the Rectangular Rapid Flashing Beacons, these are not pedestrian activated.

MUTCD Guidance

Section 4L.03 contains information regarding Warning Beacons to provide active warning of a pedestrian's presence.

Support: Typical applications of Warning Beacons include the following:

- A. At obstructions in or immediately adjacent to the roadway;
- B. As supplemental emphasis to warning signs;
- C. As emphasis for midblock

crosswalks;

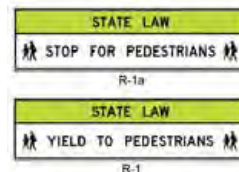
Recommended Implementation Criteria

- Consider placement at mid-block crossings but can be used at intersections with uncontrolled crossings.
- Do not install within 300 feet of a controlled crossing
- Consider overhead sign for all uncontrolled marked crosswalks along the school route with ADT over 6000.
- Add flashing beacon if there are 70-100 vehicles/peak school hour and 20 pedestrians per peak school hour.
- Consider beacons for ADT greater than 2000 ADT and 85% speed of 40mph or greater.

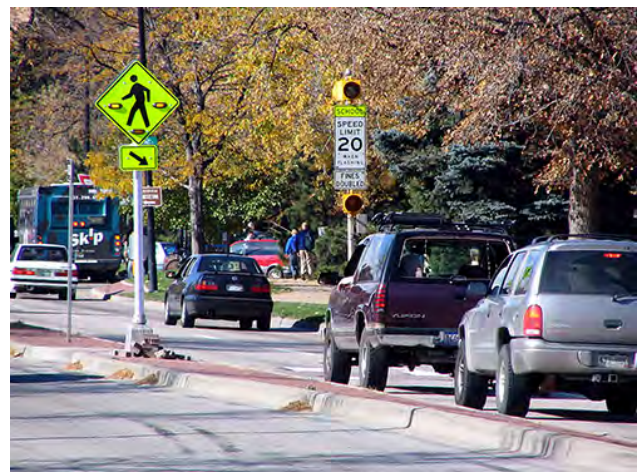
Other Precedence/Details to Consider

Los Angeles Guidelines:

- 300 feet of a controlled crossing
- Roadway to be crossed is 50 feet or more.
- Point system based on peds more than 136 per peak hour (youth ect count as 2), vehicle volume greater than 2001 ADT, speed 85% of 40mph or faster, more than 7 lanes of traffic, and collision info.



Images from Pedestrian and Bicycle Information Center Image Library, Photographer Dan Burden



In-Pavement Lights

Lights embedded in the crosswalk pavement that are activated when a pedestrian pushes a button or starts walking across the crosswalk.

provide additional warning to road users.

Other Precedence/Data to Consider

San Luis Obispo Guidelines:
100 or more peds per hour, or
100 groupings of 2 peds for a

2hours period twice per day.

Ped volume after dark is 75 or more for any one hour or 25 or more for a period of any four hours during the night time.

10,000 ADT or more

85% of 35mph or less

2 or more lanes in one direction but 4 lanes or less in both directions.

Uncontrolled crossing

National Center for Safe Routes to School, Safe Routes to School Guidelines recommends them for use at some uncontrolled marked crosswalks with high collision rate, high volumes and high speeds.

Recommended Implementation Criteria

- Consider at uncontrolled marked crosswalks.
- ADT greater than 10,000
- Ped volumes greater than 100 per hour.
- 85% speed less than 35 mph
- 2 or more lanes of traffic in one direction.

MUTCD Guidance

Section 4N.02 contains information regarding In-Road Warning lights at crosswalks. They must be located at a marked crosswalk at an uncontrolled intersection. In-roadway lights may be installed at certain marked crosswalks, based on an engineering study or engineering judgment, to

In-Street Signs

These signs are usually installed at un-signalized pedestrian crossings to make the crosswalk more visible and increase driver yielding.

Other Precedence/Data to Consider

San Luis Obispo Guidelines:
100 or more peds per hour, or
100 groupings of 2 peds for a
2hours period twice per day.

Ped volume after dark is 75 or more for any one hour or 25 or more for a period of any four hours during the night time.

10,000 ADT or more

85% of 35mph or less

2 or more lanes in one direction but 4 lanes or less in both directions.

Uncontrolled crossing

National Center for Safe Routes to School, Safe Routes to School Guidelines recommends them for use at some uncontrolled marked crosswalks with high collision rate, high volumes and high speeds.

Recommended Implementation Criteria

- Consider at uncontrolled marked crosswalks.
- ADT greater than 10,000
- Ped volumes greater than 100 per hour.
- 85% speed less than 35 mph
- 2 or more lanes of traffic in one direction.

MUTCD Guidance

Section 4N.02 contains information regarding In-Road Warning lights at crosswalks. They must be located at a marked crosswalk at an uncontrolled intersection. In-roadway lights may be installed at certain marked crosswalks, based on an engineering study or engineering judgment, to provide additional warning to road users.

Visibility and Crossing Distance

Curb Extensions

The extension of the curb out from the sidewalk and into the street, typically at an intersection. Curb extensions increase pedestrian visibility and decrease pedestrian exposure distance in the street, crossing time and vehicle turn speeds. Curb extensions can also provide additional space for curb ramps.

Recommended Implementation Criteria

Consider installation at intersections with: Wide streets, where visibility is limited, or on street parking is heavily utilized

Other Precedence/Data to Consider

This a traffic calming device. Use traffic calming criteria.

Refuge Islands

Raised medians in the middle of a street at an intersection, midpoint of the block, or continuously along street.

Recommended Implementation Criteria

Protect crossing pedestrians from oncoming traffic by serving as a barrier from motor vehicles, reduce crossing distance and allow pedestrians to focus on one direction of traffic at a time.

Best if used in streets with 4 lanes of traffic or more,

Waiting areas/Stand Back Line

Extra paving at busy crossings where large numbers of pedestrians can congregate before crossing the street without having to stand close to the busy street, or on landscaping, dirt or mud.

Recommended Implementation Criteria

Consider implementation if there are high volumes of pedestrians, waiting to cross streets with speeds of 35mph or greater.

Reduce Corner Radius

The reduction of a corner radius to produce a tighter turn results in decreases in turning speeds and improved motor vehicle and pedestrian sight distances, and a shortened pedestrian crossing distance.

Recommended Implementation Criteria

Similar to criteria for a bulb out, but can be considered on roadways without the presence of on street parking. Consider installation at intersections with: Wide streets, where visibility is limited.

Crossing Guards

Adult crossing guards assist elementary age children while crossing the street. They help provide a gap in traffic where engineering studies show that adequate gaps do not occur naturally.

Stop controlled Intersections:

Where the vehicular traffic volumes on undivided highways of four or more lanes exceeds 500 per hour during any period when the school pedestrians are going to or from school.

Signal Controlled Intersections:

Where the number of vehicular turning movements through the school crosswalk exceeds 300 per hour while school pedestrians are going to or from school; or
Where justified through analysis of the operations of the intersection.

Recommended Implementation Criteria

Consider an adult crossing guards if the following conditions exist:

Uncontrolled Intersections:

no alternate controlled crossing within 600 feet; and

In urban areas where the vehicular traffic volume exceeds 350 during each of any two hours (not necessarily consecutive) in which 40 or more school pedestrians cross daily while going to or from school;

or In rural areas where the vehicular traffic volume exceeds 300 during each of any two hours (not necessarily consecutive) in which 30 or more school pedestrians cross daily while going to or from school.

Whenever the critical (85th percentile) approach speed exceeds 40 mph, the guidelines for rural areas should be applied.

MUTCD Guidance

The State of California provides criteria for the placement of adult school crossing guards in the MUTCD 2012, California Supplement. Section 7D.02 Adult Crossing Guards

Intersection Control

Stop Sign and Signal Installation

The installation of a 2-way or 4-way stop sign at an intersection legally requires vehicles to stop before proceeding through an intersection. This provides an opportunity for pedestrians to cross. Traffic signals provide a protected phase where it is safe for pedestrians to cross.

Stop sign and signs installation must meet MUTCD/CA MUTCD warrants.

There are warrants for installing traffic control signals based on the volume of pedestrians. This is intended for situations where the vehicle volume is high creating excessive delay for pedestrians crossing.

Recommended Implementation Criteria

MUTCD

6000vpd/2500 vpd

3 collisions in 1 year

5 collisions in 2 years

must be classified residential collector

MUTCD Guidance

Other Precedence/Data to Consider

Brookline DPW Guidelines:

All signalized intersections shall have marked crosswalks on the roadway approaches that have sidewalks on both sides, or if adequate shoulder exists.

Crosswalk design should be two parallel lines 8-10 feet wide

Install a stop line all signalized intersections.

Pedestrian Actuated Signal / Pedestrian Hybrid Beacons

Traffic signals that are only activated when a pedestrian is present. They provide a controlled crossing for pedestrians without delaying motorists unnecessarily. They remain dark until activated by a pedestrian. Activation results in a sequence of amber and red beacon lights, which signal to drivers when to stop for crossing pedestrians and when to go again after pedestrians have cleared the crosswalk.

Recommended Implementation Criteria

hour.

Inadequate gaps in vehicle traffic to allow for crossing.

Vehicle speed too high

excessive pedestrian delay

MUTCD Guidance

Chapter 4F contains information on Pedestrian Hybrid Beacons.

Support: A pedestrian hybrid beacon is a special type of hybrid beacon used to warn and control traffic at an unsignalized location to assist pedestrians in crossing a street or highway at a marked crosswalk.

Recommended Implementation Criteria

No fewer than 20 pedestrian crossings per peak

Treatments for Signalized Intersections

The following should be considered at all signalized intersections along school routes: marked crosswalks on all legs, countdown pedestrian heads, ADA pedestrian push buttons, minimize pedestrian wait time, and increase pedestrian clearance intervals.

Recommended Implementation Criteria

Consider implementing these treatments at all signalized intersections along the school route.

MUTCD Guidance

4E.06 Pedestrian Intervals and Signal Phases requires this interval to be calculated based on a minimum walking speed of 3.5 feet per second. The additional time provided by an extended pushbutton press to satisfy pedestrian clearance time needs may be added to either the walk interval or the pedestrian change interval.

Guidance: Where pedestrians who walk slower than 3.5 feet per second, or pedestrians who use wheelchairs, routinely use the crosswalk, a walking

speed of less than 3.5 feet per second should be considered in determining the pedestrian clearance time.

Other Precedence/Data to Consider

National Center for Safe Routes to School, Safe Routes to School Guidelines:

Some pedestrians, especially large groups of children, may need additional time to cross. Consideration should be given to increasing the pedestrian clearance interval if a pedestrian signal must accommodate pedestrians that need more time to cross. However, these considerations should be balanced against the potential for increased wait times between 'Walk' signals. The longer people must wait to cross the street, the more likely they will decide to cross against the signal. Pedestrian wait time can be reduced by shortening the overall signal cycle length or by providing an actuated demand-responsive pedestrian signal.

Right-turn-on-red restrictions/Leading pedestrian interval

Pedestrian and motor vehicle conflicts are a common occurrence when motorists get a green light and pedestrians get a green light or a "Walk" signal at the same time. While motorists are required to stop for pedestrians, conflicts are likely to occur. One solution is to install a "leading pedestrian interval" (LPI) which illuminates the pedestrian 'Walk' signal, while the motor vehicle signal remains red. The LPI gives pedestrians an opportunity to start walking and establish a presence in the crosswalk before motorists can begin their turn. The leading pedestrian interval is usually about three seconds or more. Prohibiting right-turn-on-red is also an option to reduce pedestrian/vehicle conflict.

Recommended Implementation Criteria

LPI should be considered for all signalized intersections along the school route.

MUTCD Guidance

MUTCD 4E.06 Pedestrian Intervals and Signal Phases
Sections 4E.09 through 4E.13

References

National Center for Safe Routes to School, Safe Routes to School Guide
<http://guide.saferoutesinfo.org/engineering/index.cfm>

2009 MUTCD
http://mutcd.fhwa.dot.gov/kno_2009r1r2.htm

2012 California MUTCD Edition
http://www.dot.ca.gov/hq/traffops/signtech/mutcdsupp/ca_mutcd2012.htm

Crosswalks and Stop Lines
Michael J. Cynecki, PE

TCRP/NCHRP: Improving Pedestrian Safety at Unsignalized Crossings, Appendix F Pedestrian Crossing Installation Guidelines

Village of Brookline Department of Public Works, Crosswalk Policy and Design Guidelines

Sunnyvale Pedestrian Safety and Opportunities Study, 2007

Cover Image : Bcycle and Pedestrian Resource Center, Image Library, Photographer Dan Burden

SECTION 3

IMPROVEMENT IDENTIFICATION

There are sixteen schools with the City of Sunnyvale, most schools are within the Sunnyvale Unified School District, but some are within the Cupertino Unified School District or Santa Clara School District. Previous work by the Sunnyvale DPW, Division of Transportation and Traffic identified walking routes for all 16 schools.

For this study, all intersections on along these school routes were categorized by existing traffic control, signing and the


presence of marked crosswalks. A data base and geographic information systems (GIS) map was then created for these intersections with information about speed limit, roadway classification, collision data, and traffic volume for each intersection.

The implementation criteria identified in Section 2 was applied to the intersection data collected in the GIS data base to identify specific locations where improvements should be considered.

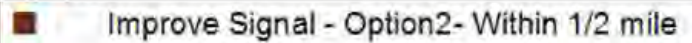
Traffic control devices typically have very specific rules standards for implementation. However, treatments for intersections along school routes allows for a significant amount of flexibility and use of engineering judgment. For this reason, many of the recommendations include a number of options using criteria that range from broad to conservative. For example, one query might identify all stop controlled intersections within a ½ mile of a school – this would be a broad option. A conservative option would include only stop controlled intersections within ½ mile of the school on collector streets, with more than 3 collisions in 5 years.

The information in this section details the type of intersection, the improvement to consider, the specific sql query that was used (so that it can be recreated in the future), the name of the GIS file, and the GIS map symbol (so that it can be identified on the accompanying maps)

Signalized Intersections


Improvements to Consider	Install High Visibility Crosswalks, Advanced Stop Bar, Leading Pedestrian Interval, Countdown Pedestrian signal, ADA Push Buttons, Minimize Ped Wait time, Increase Ped Clearance Intervals
Criteria	Option 1 Signalized intersections within ½ mile of a school, on a school route, with 3 or more crashes in 1 year, or 5 or more crashes in 2 years.
Query	"Control" = 'signalized' AND ("Crash1yr" = 3 OR "Crash2yr" = 5) AND "School" <> 'not school' AND "In_half_mi" = 'yes'
Result file name	signals_for_Improvement.shp 38 intersections
Map Label and Symbol	 Improve Signal - Option1- Within 1/2 mile with high crash rate

#	Signalized Intersections – Option 1	School
1	N MATHILDA AV & INDIO WY	Bishop
2	N MATHILDA AV & W MAUDE AV	Bishop
3	N MATHILDA AV & SAN ALESO AV	Bishop
4	ALMANOR AV & N MATHILDA AV & W AHWANEE AV	Bishop
5	OLD SAN FRANCISCO RD & GAIL AV	Braly
6	S MARY AV & W KNICKERBOCKER DR	Cumberland
7	HOLLENBECK AV & S PASTORIA AV & W EL CAMINO REAL	Cumberland
8	S MATHILDA AV & W EL CAMINO REAL	Ellis
9	S VALE SARATOGA RD & S VALE AV & E EL CAMINO REAL	Ellis
10	E OLIVE AV & S SUNNYVALE AV	Ellis
11	CEZANNE DR & E EL CAMINO REAL	Ellis
12	E REMINGTON DR & S FAIR OAKS AV & E EL CAMINO REAL	Ellis
13	S FAIR OAKS AV & IRIS AV	Ellis
14	OLD SAN FRANCISCO RD & S FAIR OAKS AV	Ellis
15	S FAIR OAKS AV & E OLIVE AV	Ellis
16	S FAIR OAKS AV & E EVELYN AV	Ellis
17	RAMP LAW SB N101 & LAWRENCE EX	Fairwood
18	SANDIA AV & LAKEHAVEN DR & LAWRENCE EX	Fairwood
19	RAMP N101 & N FAIR OAKS AV	Lakewood
20	REED AV & S WOLFE RD & OLD SAN FRANCISCO RD	Ponderosa
21	POPLAR AV & E EL CAMINO REAL	Ponderosa
22	HENDERSON AV & E EL CAMINO REAL	Ponderosa
23	E ARQUES AV & N FAIR OAKS AV	San Miguel
24	MAUDE AV & N FAIR OAKS AV	San Miguel
25	E DUANE AV & N FAIR OAKS AV	San Miguel
26	SAN CONRADO TE & N FAIR OAKS AV & CALIENTE DR	San Miguel
27	E AHWANEE AV & N FAIR OAKS AV	San Miguel
28	ALBERTA AV & HARWICK WY & SUNNYVALE SARATOGA RD	Stocklmeir
29	W FREMONT AV & SUNNYVALE SARATOGA RD	Stocklmeir
30	E FREMONT AV & S WOLFE RD	Stocklmeir
31	S WOLFE RDE EL CAMINO REAL	Stocklmeir
32	W EL CAMINO REAL & GRAPE AV	Vargas
33	W EL CAMINO REAL & S MARY AV	Vargas
34	S MARY AV & W WASHINGTON AV	Vargas
35	S MARY AV & W EVELYN AV	Vargas
36	N MARY AV & CX	Vargas
37	ALBERTA AV & HOLLENBECK AV	West Valley
38	W FREMONT AV & HOLLENBECK AV	West Valley


Improvements to Consider	Install High Visibility Crosswalks, Advanced Stop Bar, Leading Pedestrian Interval, Countdown Pedestrian signal, ADA Push Buttons, Minimize Ped Wait time, Increase Ped Clearance Intervals
Criteria	Option 2
Query	Signalized intersections within ½ mile of a school, on a school route.
Result file name	“Control” = ‘signalized’ AND “School” ⇔ ‘not school’ AND “In_half_mi” = ‘yes’ signals_for_Improvement_option2.shp 78 intersections
Map Label and Symbol	


Signalized Intersections – Option 2	School	#	Signalized Intersections – Option 2	School
1 N MATHILDA AV & INDIO WY	Bishop	39	REED AV & S WOLFE RD & OLD SAN FRANCISCO RD	Ponderosa
2 N MATHILDA AV & W MAUDE AV	Bishop	40	POPLAR AV & E EL CAMINO REAL	Ponderosa
3 N MATHILDA AV & SAN ALESO AV	Bishop	41	SEQUOIA DR & REED AV	Ponderosa
4 ALMANOR AV & N MATHILDA AV & W AHWANEE AV	Bishop	42	HENDERSON AV & E EL CAMINO REAL	Ponderosa
5 E CALIFORNIA AV & N SUNNYVALE AV & N SUNNYVALE AV*	Bishop	43	E EVELYN AV & REED AV	Ponderosa
6 CX & N SUNNYVALE AV	Bishop	44	E ARQUES AV & N FAIR OAKS AV	San Miguel
7 WCX & N SUNNYVALE AV	Bishop	45	MAUDE AV & N FAIR OAKS AV	San Miguel
8 E ARQUES AV & N SUNNYVALE AV	Bishop	46	N WOLFE RD & N FAIR OAKS AV	San Miguel
9 N SUNNYVALE AV & E MAUDE AV	Bishop	47	E DUANE AV & N FAIR OAKS AV	San Miguel
10 OLD SAN FRANCISCO RD & GAIL AV	Braly	48	SAN CONRADO TE & N FAIR OAKS AV & CALIENTE DR	San Miguel
11 S BERNARDO AV & HEATHERSTONE WY	Cherry Chase	49	E AHWANEE AV & N FAIR OAKS AV	San Miguel
12 S MARY AV & TICONDEROGA DR	Cherry Chase	50	E DUANE AV & DE GUIGNE DR	San Miguel
13 S MARY AV & W KNICKERBOCKER DR	Cumberland	51	ALBERTA AV & HARWICK WY & SUNNYVALE SARATOGA RD	StockImeir
14 S MARY AV & W REMINGTON DR	Cumberland	52	CHEYENNE DR & CONNEMARA WY & SUNNYVALE SARATOGA RD	StockImeir
15 HEATHERSTONE AV & S MARY AV	Cumberland	53	W FREMONT AV & SUNNYVALE SARATOGA RD	StockImeir
16 DANFORTH DR & HOLLENBECK AV	Cumberland	54	E HOMESTEAD RD & BLUE JAY DR	StockImeir
17 HOLLENBECK AV & S PASTORIA AV & W EL CAMINO REAL	Cumberland	55	BLUEJAY DR & HOMESTEAD RD	StockImeir
18 S MATHILDA AV & SENECA TE	Ellis	56	E HOMESTEAD RD & N BLANEY AV	StockImeir
19 S MATHILDA AV & W EL CAMINO REAL	Ellis	57	N BLANEY AV & HOMESTEAD RD	StockImeir
20 TENNIS CENTER WY & S MATHILDA AV	Ellis	58	MARION WY & S WOLFE RD	StockImeir
21 SVALE SARATOGA RD & SVALE AV & E EL CAMINO REAL	Ellis	59	E FREMONT AV & S WOLFE RD	StockImeir
22 E OLIVE AV & S SUNNYVALE AV	Ellis	60	S WOLFE RDE EL CAMINO REAL	StockImeir
23 S SUNNYVALE AV & E IOWA AV	Ellis	61	S BERNARDO AV & W WASHINGTON AV	Vargas
24 E MC KINLEY AV & S SUNNYVALE AV	Ellis	62	W EL CAMINO REAL & GRAPE AV	Vargas
25 CEZANNE DR & E EL CAMINO REAL	Ellis	63	S BERNARDO AV & W EVELYN AV & E EVELYN AVE	Vargas
26 E REMINGTON DR & S FAIR OAKS AV & E EL CAMINO REAL	Ellis	64	W EL CAMINO REAL & S MARY AV	Vargas
27 S FAIR OAKS AV & IRIS AV	Ellis	65	S MARY AV & W IOWA AV	Vargas
28 OLD SAN FRANCISCO RD & S FAIR OAKS AV	Ellis	66	S MARY AV & W WASHINGTON AV	Vargas
29 S FAIR OAKS AV & E OLIVE AV	Ellis	67	S MARY AV & W EVELYN AV	Vargas
30 S FAIR OAKS AV & E EVELYN AV	Ellis	68	W CALIFORNIA AV & N MARY AV & BUENA VISTA AV	Vargas
31 RAMP LAW SB N101 & LAWRENCE EX	Fairwood	69	N MARY AV & CX	Vargas
32 SANDIA AV & LAKEHAVEN DR & LAWRENCE EX	Fairwood	70	BARRANCA DR & HOMESTEAD RD	West Valley
33 TASMAN DR & BIRCHWOOD DR	Fairwood	71	BELLEVILLE WY & W HOMESTEAD RD	West Valley
34 BIRCHWOOD DR & LR	Fairwood	72	MAXINE AV & HOMESTEAD RD	West Valley
35 REAMWOOD AV & TASMAN DR	Fairwood	73	RAMP S85 HOMESTEAD & W HOMESTEAD RD	West Valley
36 REAMWOOD AV & LR	Fairwood	74	S BERNARDO AV & RAMP HOMESTEAD N85 & W HOMESTEAD *	West Valley
37 RAMP N101 & N FAIR OAKS AV	Lakewood	75	WRIGHT AV & W HOMESTEAD RD	West Valley
38 S WOLFE RD & IRIS AV	Ponderosa	76	ALBERTA AV & HOLLENBECK AV	West Valley
		77	HOLLENBECK AV & CASCADE DR	West Valley
		78	W FREMONT AV & HOLLENBECK AV	West Valley

Stop Controlled – Without Marked X-Walks

Improvements to Consider	Install High Visibility Crosswalks
Criteria	Option 1 Stop controlled intersections, along a school route, with no crosswalks, within ½ mile of a school, with 3 crashes in 1 year or 5 crashes in 2 years, on an arterial or collector
Query	"Control" = 'stop' AND "School" <> 'not school' AND "In_half_mi" = 'yes' AND "Crosswalk" = ' ' AND ("Crash1yr" = 3 OR "Crash2yr" = 5) AND ("Collector" = 'yes' OR "Arterial" = 'yes')
Result file name	Install_Crosswalks_stopcontrol_Option1.shp
Map Label and Symbol	9 Intersections  Mark x-walk at Stop - Option1-crashes and street class


#	Stop Controlled with no X-Walks Option 1	School
1	ROOSEVELT AV & E MAUDE AV	Bishop
2	WORLEY AV & E MAUDE AV	Bishop
3	S MARY AV & BLAIR AV	Cumberland
4	E TAYLOR AV & N FAIR OAKS AV	San Miguel
5	E DUANE AV & SAN LUISITO WY	San Miguel
6	SANTA PAULA AV & E DUANE AV	San Miguel
7	E DUANE AV & SAN RAFAEL ST	San Miguel
8	W OLIVE AVS MARY AV	Vargas
9	CARSON DR & CARSON DR & S MARY AV	Vargas

Improvements to Consider	Install High Visibility Crosswalks
Criteria	Option 2 Stop controlled intersections, along a school route, with no crosswalks, within ½ mile of a school
Query	"Control" = 'stop' AND "School" <> 'not school' AND "In_half_mi" = 'yes' AND "Crosswalk" = ' '
Result file name	stops_for_improvementbroad.shp
Map Label and Symbol	256 intersections  Mark x-walk at Stop - Option2 -within 1/2 mile

Improvements to Consider	Install High Visibility Crosswalks
Criteria	Option 3 Stop controlled intersections, along a school route, with no crosswalks, within ¼ mile of a school
Query	"Control" = 'stop' AND "School" <> 'not school' AND "quarter_mi" = 'Yes' AND "Crosswalk" = ' '
Result file name	Install_Crosswalks_stopcontrol_Option3.shp 88 intersections
Map Label and Symbol	 Mark x-walk at Stop - Option3 -within 1/4 mile


#	Stop Controlled with no X-Walks Option 3	School
1	STOWELL AV & W MAUDE AV	Bishop
2	E TAYLOR AV & N SUNNYVALE AV	Bishop
3	SCHROEDER ST & E ARQUES AV	Bishop
4	BORREGAS AV & E ARBOR AV	Bishop
5	W ARBOR AV & BORREGAS AV	Bishop
6	JACKSON ST & E ARQUES AV	Bishop
7	N BAYVIEW AV & E ARQUES AV	Bishop
8	BORREGAS AV & W FERNDAL AV	Bishop
9	N BAYVIEW AV & E TAYLOR AV	Bishop
10	BORREGAS AV & E HEMLOCK AV & W HEMLOCK AV	Bishop
11	BORREGAS AV & ALTURAS AV	Bishop
12	MORSE AV & E TAYLOR AV	Bishop
13	MORSE AV & E MAUDE AV	Bishop
14	ROOSEVELT AV & E TAYLOR AV	Bishop
15	MORSE AV & E MAUDE AV	Bishop
16	MORSE AV & E ARBOR AV	Bishop
17	ROOSEVELT AV & E MAUDE AV	Bishop
18	ALTURAS AV & E AHWANEE AV	Bishop
19	MORSE AV & WADDINGTON AV	Bishop
20	S FERNWOOD CL & MORSE AV	Bishop
21	N FERNWOOD CL & MORSE AV	Bishop
22	MORSE AV & E AHWANEE AV	Bishop
23	LUSTERLEAF DR & IRIS AV	Braly
24	S BERNARDO AV & LAFAYETTE & DR	Cherry Chase
25	S BERNARDO AV & JAMESTOWN DR	Cherry Chase
26	S BERNARDO AV & W CARDINAL DR	Cherry Chase
27	S BERNARDO AV & MORNINGSIDE DR	Cherry Chase
28	S BERNARDO AV & SUSAN WY & SUSAN WY	Cherry Chase
29	S BERNARDO AV & LYNN WY	Cherry Chase
30	S BERNARDO AV & PARKINGTON AV	Cherry Chase
31	ROCKEFELLER DR & LIME DR	Cherry Chase
32	W REMINGTON DR & LIME DR	Cherry Chase
33	SUSAN WY & GRAPE AV	Cherry Chase
34	LYNN WY & GRAPE AV	Cherry Chase
35	PARKINGTON AV & GRAPE AV	Cherry Chase
36	HEATHERSTONE AV & LOIS AV	Cherry Chase
37	LOIS AV & LYNN WY	Cherry Chase
38	MARANTA AV & W KNICKERBOCKER DR	Cherry Chase
39	MARY AV & ROCKEFELLER DR	Cherry Chase
40	W KNICKERBOCKER DR & ITHACA AV	Cumberland
41	NORFOLK PINE AV & ITHACA AV	Cumberland
42	PEPPER AV & W KNICKERBOCKER DR	Cumberland

43	PEACH AV & HANOVER AV & HEATHERSTONE AV	Cumberland
44	HANOVER AV & PIPPIN AV	Cumberland
45	CUMBERLAND DR & QUETTA AV	Cumberland
46	E MC KINLEY AV & CENTRAL AV	Ellis
47	E MC KINLEY AV & KENMORE AV	Ellis
48	E MC KINLEY AV & S FAIR OAKS AV	Ellis
49	FIRLOCH AV & E OLIVE AVE OLIVE AV	Ellis
50	OLD SAN FRANCISCO RD & GRAND FIR AV	Ellis
51	BURNTWOOD AV & SANDIA AV & BURNTWOOD CT	Fairwood
52	SANDIA AV & CANDLEWOOD CT & CANDLEWOOD AV	Fairwood
53	FAIRWOOD AV & TUCSON AV	Fairwood
54	FAIRWOOD AV & CANDLEWOOD AV & TORRANCE AV	Fairwood
55	PALAMOS AV & FAIRWOOD AV	Fairwood
56	PECOS WY & FAIRWOOD AV	Fairwood
57	FAIRWOOD AV & PRESCOTT AV	Fairwood
58	MORSE AV & E WEDDELL DR	Lakewood
59	S CASCADE TE & YUKON DR & CASCADE DR	Nimitz
60	ELIZABETH WY & RAMON DR	Peterson
61	BRYANT WY & POPLAR AV	Peterson
62	BRYANT WY & POPLAR AV	Peterson
63	HENDERSON AV & LUPINE DR	Ponderosa
64	BERNAL AV & CALIENTE DR	San Miguel
65	CALIENTE & JOHANNA AV & DR	San Miguel
66	E DUANE AV & SAN LUISITO WY	San Miguel
67	SAN JUNIPERO DR & E AHWANEE AV	San Miguel
68	SAN MIGUEL AV & E DUANE AV	San Miguel
69	SAN MATEO CT & E AHWANEE AV	San Miguel
70	E DUANE AV & SAN PATRICIO AV	San Miguel
71	SAN PABLO AV & AMADOR AV	San Miguel
72	SAN PABLO AV & E AHWANEE AV	San Miguel
73	SANTA PAULA AV & E DUANE AV	San Miguel
74	SANTA PAULA AV & AMADOR AV	San Miguel
75	SANTA PAULA AV & E AHWANEE AV	San Miguel
76	SAN RAMON AV & E AHWANEE AV	San Miguel
77	BITTERN DR & HARWICK WY	Stockmeir
78	DUNHOLME WY & BLACKHAWK DR & BLACKHAWK CT	Stockmeir
79	INVERNESS WY & CROW CT	Stockmeir
80	MARIANI DR & INVERNESS WY	Stockmeir
81	CORRAL AV & W WASHINGTON AV	Vargas
82	W WASHINGTON AV & GABILAN AV	Vargas
83	W WASHINGTON AV & LANITOS AV	Vargas
84	W MC KINLEY AV & LEOTA AV	Vargas
85	W WASHINGTON AV & LIEBRE CT	Vargas
86	LOMETA AV & W WASHINGTON AV	Vargas
87	CARSON DR & CARSON DR & S MARY AV	Vargas
88	W WASHINGTON AV & MATADERO DR	Vargas

Improvements to Consider	Install High Visibility Crosswalks
Criteria	Option 4 Stop controlled intersections, along a school route, with no crosswalks, within 1/2 mile of the a school, with traffic volume greater than 2,000 AWT or on a collector street, 3-way or 4-way stop only
Query	"Control" = 'stop' AND "School" <> 'not school' AND "Crosswalk" = '' AND "In_half_mi" = 'yes' AND("ADT_EW" > 2000 OR "ADT_NS" > 2000 OR "Collector" = 'yes') AND("Stop_Type" = '3-way' OR "Stop_Type" = '4-way')
Result file name	Install_Crosswalks_stopcontrol_Option4.shp 2 intersections
Map Label and Symbol	 Mark x-walk at Stop - Option4- over 2k 3way and 4way stops


#	Stop Controlled with no X-Walks Option 4	School
1	MARION WY & DUNFORD AV & NORMAN DR	Peterson
2	SANTA PAULA AV & AMADOR AV	San Miguel

Yield Controlled – Without Marked X-Walks

Improvements to Consider	Install High Visibility Crosswalks, Advanced Yield Lines, Consider in-street signs
Criteria	Yield controlled intersections, along a school route, with no crosswalks
Query	"Control" = 'yield' AND "Crosswalk" = '' AND "School" <> 'not school'
Result file name	yield_for_improvement.shp 32 intersections
Map Label and Symbol	 High Visibility Crosswalks at Yield intersections

#	Yield Controlled with no X-Walks	School
1	MADRONE AV & W EAGLEWOOD AV	Bishop
2	E ARBOR AV & WORLEY AV	Bishop
3	W REMINGTON DR & ROBIN WY	Cherry Chase
4	POME AV & SHERATON DR	Cherry Chase
5	HOLBROOK PL & HAVERHILL DR	Cumberland
6	PYRUS WY & HAVERHILL DR	Cumberland
7	HAVERHILL DR & QUETTA CT & QUETTA AV	Cumberland
8	RUBIS DR & HARVARD AV	Cumberland
9	RESEDA DR & DANFORTH DR	Cumberland
10	SPINOSA DR & TEMPLETON CT & TEMPLETON DR	Cumberland
11	CEZANNE DR & BRAHMS WY	Ellis
12	BELLFLOWER AV & GRAND FIR AV	Ellis
13	GRAND FIR AV & BEGONIA WY	Ellis
14	ASTER CT & SEQUOIA DR & ASTER AV	Ellis
15	SOCORRO AV & FAIRWOOD AV	Fairwood
16	HAVENWOOD AV & TORRANCE AV & TUCSON AV	Fairwood
17	SOCORRO AV & HAVENWOOD AV	Fairwood
18	PLAZA DR & FULTON AV	Lakewood
19	YUKON DR & CHEYENNE DR	Nimitz
20	ELEANOR WY & RAMON DR	Peterson
21	ELIZABETH WY & NAVARRO DR	Peterson
22	ORIOLE AV & DUNFORD AV	Peterson
23	LOCHINVAR AV & SWIFT CT	Peterson
24	BENTON ST & LOCHINVAR AV & BENTON CT	Peterson
25	VIREO AV & LOCHINVAR AV	Peterson
26	WARBLER AV & LOCHINVAR AV & WARBLER WY	Peterson
27	LOCHINVAR AV & WAXWING AV	Peterson
28	HARWICK WY & ALBATROSS DR	Stockmeir
29	HERON AV & FIFE WY	Stockmeir
30	BEDFORD AV & THE DALLES	West Valley
31	FRANCHERE PL & CASCADE DR	West Valley
32	ALLISON WY & NELSON WY	West Valley


Uncontrolled – With Marked X-Walks

Improvements to Consider	Conduct Stop Warrant Studies Install advanced Yield Lines and Yield Signs, and In-Street Signs
Criteria	Option 1 Uncontrolled marked crosswalks, on a school route
Query	"Control" = ' ' AND ("Crosswalk" = ' Yes' OR "Crosswalk" = 'yes') AND "School" <> 'not school'
Result file name	Stop_WarrantOption1.shp 51 intersections
Map Label and Symbol	 Stop Warrant and Improve at Marked x-walks -Option1


#	Uncontrolled with X-Walks Option 1	School
1	BORREGAS AV & W MAUDE AV	Bishop
2	N BAYVIEW AV & HAZELTON AV	Bishop
3	GAIL AV & GLADIOLA DR	Braly
4	GAIL AV & CALLA DR	Braly
5	JAMESTOWN DR & GRAPE AV	Cherry Chase
6	ANDOVER DR & GRAPE AV	Cherry Chase
7	CUMBERLAND DR & PIPER AV	Cumberland
8	ELMIRA DR & QUETTA AV	Cumberland
9	W MC KINLEY AV & S TAAFFE ST	Ellis
10	E MC KINLEY AV & SATURN TE	Ellis
11	E OLIVE AV & KENMORE AV	Ellis
12	OLD SAN FRANCISCO RD & BLACKWOOD TE	Ellis
13	KIFER RD & SAN ZENO WY	Ellis
14	KIFER RD & LAWRENCE STATION RD	Ellis
15	KIFER RD & GORDON AV	Ellis
16	BLAZINGWOOD DR & PECOS WY	Fairwood
17	BLAZINGWOOD DR & FAIRWOOD AV	Fairwood
18	FAIRWOOD AV & HH	Fairwood
19	HH & MANZANO WY	Fairwood
20	PED XING OVER CALABAZAS & MANZANO WY	Fairwood
21	DUNCAN AV & GARNER DR	Lakewood
22	BORREGAS AV & W WEDDELL DR	Lakewood
23	MORSE AV & PLENTY TE	Lakewood
24	MONTEGO TE & LE MANS TE	Lakewood
25	E JAVA DR & CROSSMAN AV	Lakewood
26	FAIR OAKS WY & N FAIR OAKS AV	Lakewood
27	ELDORADO MHP	Lakewood
28	HIDDENLAKE DR & HH	Lakewood
29	MEADOWLAKE DR & LAKEFAIR DR	Lakewood
30	HH & MEADOWLAKE DR	Lakewood
31	LAKEHAVEN DR & VELVET LAKE DR	Lakewood
32	OWEN SOUND DR & CHEYENNE DR	Nimitz
33	CHEYENNE DR & REVELSTOKE WY	Nimitz
34	CHEYENNE DR & SASKATCHEWAN DR	Nimitz
35	CASCADE DR & SELO DR	Nimitz
36	POPLAR AV & ROSALIA AV	Peterson
37	N TANTAU AV & HOMESTEAD RD	Peterson
38	DUNFORD WY & LOCHINVAR AV	Peterson
39	DUNFORD WY & THUNDERBIRD AV	Peterson
40	PONDEROSA AV & LANTANA DR	Ponderosa
41	N WOLFE RD & STEWART DR	San Miguel
42	SAN JUAN DR & BLYTHE AV	San Miguel
43	SAN JUNIPERO DR & ALVARADO AV	San Miguel
44	MIRALOMA WY & LAWRENCE EX	San Miguel
45	CHICKADEE CT & DUNHOLME WY	Stocklmeir
46	DUBLIN WY & FLOYD AV	Stocklmeir
47	CONDOR WY & DUNHOLME WY	Stocklmeir
48	RAMP FREMONT S85 & W FREMONT AV	West Valley
49	BELLEVILLE WY	West Valley
50	RAMP N85 & FREMONT AV	West Valley
51	CORONACH AV & HELENA DR	West Valley

Criteria	Option 2 Uncontrolled marked crosswalks, within ½ mile of the school, classified as a collector street, or with over 2000 AWDT volumes
Query	"Control" = ' ' AND "Crosswalk" = 'yes' AND "In_half_mi" = 'yes' AND ("ADT_EW" > 2000 OR "ADT_NS" > 2000 OR "Collector" = 'yes' OR "Arterial" = 'yes')
Result file name	Stop_WarrantOption2.shp 12 intersections
Map Label and Symbol	 Stop Warrant and improve at Marked x-walks -Option2


#	Uncontrolled with X-Walks Option 2	School
1	BORREGAS AV & W MAUDE AV	Bishop
2	GAIL AV & GLADIOLA DR	Braly
3	GAIL AV & CALLA DR	Braly
4	E MC KINLEY AV & SATURN TE	Ellis
5	E OLIVE AV & KENMORE AV	Ellis
6	OLD SAN FRANCISCO RD & BLACKWOOD TE	Ellis
7	BORREGAS AV & W WEDDELL DR	Lakewood
8	LAKEHAVEN DR & VELVET LAKE DR	Lakewood
9	CASCADE DR & SELO DR	Nimitz
10	DUNFORD WY & LOCHINVAR AV	Peterson
11	DUNFORD WY & THUNDERBIRD AV	Peterson
12	BELLEVILLE WY	West Valley

Improvements to Consider	Raised Crosswalk or other Traffic Calming
Criteria	All marked crosswalks, within ½ mile of a school, on a residential street, with volume higher than 1000 AWDT, without a traffic signal.
Query	"Crosswalk" = 'yes' AND "School" <> 'not school' AND "In_half_mi" = 'yes' AND "Collector" = ' ' AND "Arterial" = ' ' AND "Control" <> 'signalized' AND "Expressway" = ' ' AND ("ADT_EW" > 1000 OR "ADT_NS" > 1000)
Result file name	raised_crosswalks_potential.shp 16 intersections
Map Label and Symbol	 Raised Crosswalks/Traffic Calming

#	Uncontrolled with X-Walks – Raised X-walk/Traffic Calming	School
1	MORSE AV & E ARQUES AV	Bishop
2	JAMESTOWN DR & GRAPE AV	Cherry Chase
3	HEATHERSTONE AV & HEATHERSTONE WY & GRAPE AV	Cherry Chase
4	TICONDEROGA DR & LIME DR	Cherry Chase
5	ANDOVER DR & GRAPE AV	Cherry Chase
6	BLAIR AV & GRAPE AV	Cherry Chase
7	PEACH AV & BLAIR AV	Cumberland
8	ALL AMERICA WY & W OLIVE AV	Cumberland
9	CHARLES ST & W OLIVE AV	Cumberland
10	E WASHINGTON AV & S BAYVIEW AV	Ellis
11	HENDERSON AV & BRYANT WY	Peterson
12	THUNDERBIRD AV & LILICK DR	Peterson
13	BITTERN DR & DUNHOLME WY	Stocklmeir
14	BITTERN DR & CONNEMARA WY	Stocklmeir
15	BITTERN DR & CARLISLE WY	Stocklmeir
16	WRIGHT AV & HELENA DR	West Valley


Improvements to Consider	Rectangular Rapid Flashing Beacons
Criteria	All uncontrolled intersections, within ½ of a school, with marked crosswalks, on a school route, on a collector street, [criteria says 85 th % speed over 35mph and over 2000 ADT and 300 ft. distance from controlled crossing but data no or available in GIS. Distance from a controlled intersection was manually removed]
Query	"Control" = ' ' AND "In_half_mi" = 'yes' AND "School" <> 'not school' AND "Collector" = 'yes' AND "Crosswalk" = 'yes'
Result file name	RRFB.shp
Map Label and Symbol	8 intersections  Rectangular Rapid Flashing Beacons

#	Uncontrolled with X-Walks – RRFB	School
	1 GAIL AV & GLADIOLA DR	Braly
	2 GAIL AV & CALLA DR	Braly
	3 E OLIVE AV & KENMORE AV	Ellis
	4 BORREGAS AV & W WEDDELL DR	Lakewood
	5 CASCADE DR & SELO DR	Nimitz
	6 DUNFORD WY & LOCHINVAR AV	Peterson
	7 DUNFORD WY & THUNDERBIRD AV	Peterson
	8 BELLEVILLE WY	West Valley


Improvements to Consider	In-Pavement Lighted Crosswalks
Criteria	All uncontrolled intersections, with speed limit under 40mph, AWDT is over 10,000, on a school route, with in ½ mile of the a school, not on an arterial street.
Query	"Control" = ' ' AND "In_half_mi" = 'yes' AND "School" <> 'not school' AND "SPEED" <40 AND("ADT_EW" > 10000 OR "ADT_NS" > 10000) AND "Arterial" <> 'yes'
Result file name	Inpavement_Lighted_Crosswalks_potential.shp
Map Label and Symbol	14 Intersections  In-Pavement Lighted X-Walks

#	Uncontrolled with X-Walks – In-Pavement Lighted Xwalks	School
	1 BORREGAS AV & W MAUDE AV	Bishop
	2 WINDSOR TE & HOLLENBECK AV	Cumberland
	3 HOLLENBECK AV & YELLOWSTONE TE	Cumberland
	4 OLD SAN FRANCISCO RD & BLACKWOOD TE	Ellis
	5 OLD SAN FRANCISCO RD & IRONWOOD TE	Ellis
	6 REED AV & BOUGAINVILLEA TE	Ponderosa
	7 E HOMESTEAD RD & CUPERTINO SV BORDER	Stocklmeir
	8 NORTHSKY SQ & HOMESTEAD RD	Stocklmeir
	9 NORTHPOINT WY & HOMESTEAD RD	Stocklmeir
	10 NORTHWIND SQ & HOMESTEAD RD	Stocklmeir
	11 S BERNARDO AV & EATON TE	Vargas
	12 W HOMESTEAD RD & STEVENS CREEK	West Valley
	13 BEND DR & HOLLENBECK AV	West Valley


Uncontrolled – Without Marked X-Walks

Improvements to Consider	Install High Visibility Crosswalks, Advanced Yield Lines and In Street Signs, Conduct Stop Warrant Analysis
Criteria	Option 1 All uncontrolled intersections, with no crosswalks, with speed limit under 40mph, AWDT is over 2,000, under 12,000, on a school route, with in ½ mile of a school.
Query	"Control" = ' ' AND "Crosswalk" = ' ' AND "SPEED" < 40 AND "School" <> 'not school' AND "In_half_mi" = 'yes' AND (("ADT_EW" > 2000 AND "ADT_EW" < 12000) OR ("ADT_NS" > 2000 AND "ADT_NS" < 12000))
Result file name	Uncontrolled_Unmarked_crosswalks.shp
Map Label and Symbol	85 intersections  Mark x-walk and improve at Uncontrolled, unmarked - Option 1

#	Uncontrolled no X-Walks Option 1	School		
1	PINE AV & W DUANE AV	Bishop	42 LAKEHAVEN TE & LAKEHAVEN DR	Lakewood
2	MADRONE AV & W DUANE AV	Bishop	43 TASMAN DR & TASMAN CT	Lakewood
3	MANZANITA AV & W DUANE AV	Bishop	44 LAKEHAVEN DR & JADELAKE CT	Lakewood
4	WAITE & E DUANE AV & AV	Bishop	45 LAKEMUIR DR & LAKEHAVEN DR	Lakewood
5	KIRK AV & E DUANE AV	Bishop	46 LAKEHAVEN DR & TWINLAKE DR	Lakewood
6	E DUANE AV & MAPLE AV	Bishop	47 ALBERTA AV & OAK POINT TE	Nimitz
7	FAIR OAKS MOBILE LODGE & E AHWANEE AV	Bishop	48 PINE PASS TE & ALBERTA AV	Nimitz
8	PIERINO AV & IRIS AV	Braly	49 ALBERTA AV & RESTON TE	Nimitz
9	IRIS AV & HENRIETTA AV	Braly	50 ALBERTA AV & RIORDEN TE	Nimitz
10	GAIL AV & LINDEN AV	Braly	51 MARION WY & HAMPTON DR	Peterson
11	ANSHEN CT & GAIL AV	Braly	52 RAMON DR & MARION WY	Peterson
12	GAVELLO AV & GAIL AV	Braly	53 NAVARRO DR & MARION WY	Peterson
13	GAIL AV & DUFF CT	Braly	54 DUNFORD WY & SANDPIPER CT	Peterson
14	GAIL AV & GOLDENROD CT	Braly	55 LOCHINVAR AV & DURHAM CT	Peterson
15	KATON CT & IRIS AV	Braly	56 FIRTH CT & LOCHINVAR AV	Peterson
16	GAIL AV & CALICO CT	Braly	57 MAHOGANY LN & IRIS AV	Ponderosa
17	BLUE SAGE DR & GAIL AV	Braly	58 MESQUITE & PL & IRIS AV	Ponderosa
18	KOA CT & IRIS AV	Braly	59 IRIS AV & LADIS CT	Ponderosa
19	WINDSOR TE & HOLLENBECK AV	Cumberland	60 SHASTA FIR WY & SEQUOIA DR	Ponderosa
20	HOLLENBECK AV & YELLOWSTONE TE	Cumberland	61 ERICA DR & SEQUOIA DR	Ponderosa
21	PESCADERO TE & W MC KINLEY AV	Cumberland	62 HENDERSON AV & THUNDERBIRD MHP	Ponderosa
22	E IOWA AV & FLORA VISTA AV	Ellis	63 HENDERSON AV	Ponderosa
23	MC KINLEY AV & FLORA VISTA AV	Ellis	64 HENDERSON AV & GARDENIA WY	Ponderosa
24	MAXINE AV & E OLIVE AV & MAXINE AVE OLIVE AV	Ellis	65 REED AV & BOUGAINVILLEA TE	Ponderosa
25	E OLIVE AV & CARLYN CT	Ellis	66 E DUANE AV & N BRITTON AV	San Miguel
26	E OLIVE AV & WILSON AV	Ellis	67 SANTA ROSALIA TE & E DUANE AV	San Miguel
27	FOXTAIL DR & IRIS AV	Ellis	68 KIRBYHILL WY & BITTERN DR	StockImeir
28	GOLDEN OAK DR & IRIS AV	Ellis	69 DUBLIN WY & BITTERN DR	StockImeir
29	JACKPINE CT & IRIS AV	Ellis	70 CROMART CT & BITTERN DR	StockImeir
30	E HENDY AV & N FAIR OAKS AV	Ellis	71 BITTERN DR & CARLOW CT	StockImeir
31	OLD SAN FRANCISCO RD & IRONWOOD TE	Ellis	72 BITTERN DR & BERWICK WY	StockImeir
32	EVELYN AV & HOLLY TE	Ellis	73 S BERNARDO AV & EATON TE	Vargas
33	EVELYN AV & HOLLY TE	Ellis	74 S BERNARDO AV & DURAN TE	Vargas
34	EVELYN AV & BRISTOL COMMONS APTS	Ellis	75 W CALIFORNIA AV & LA MESA TE	Vargas
35	ADOBE WELLS MHP & TASMAN DR	Fairwood	76 BELMONT TE & LA MESA TE & W CALIFORNIA AV	Vargas
36	OAK CREEK WY & SANDIA AV	Fairwood	77 BELLEVILLE WY & BELLINGHAM WY	West Valley
37	HH & MORSE AV	Lakewood	78 BELLEVILLE WY & BELLEVILLE CT	West Valley
38	TIMOR TE & MORSE AV	Lakewood	79 BELLEVILLE WY & BELLEVILLE PL	West Valley
39	E WEDDELL DR	Lakewood	80 BELLEVILLE LN & BELLEVILLE WY	West Valley
40	JENA TE & E WEDDELL DR	Lakewood	81 BELLEVILLE WY	West Valley
41	JENA TE & E WEDDELL DR	Lakewood	82 ENDERBY WY & THE DALLS	West Valley
			83 CORONACH AV & THE DALLS	West Valley
			84 DOMINION AV & THE DALLS	West Valley
			85 THE DALLS & NEWFOUNDLAND DR	West Valley

Improvements to Consider	Install High Visibility Crosswalks, Advanced Yield Lines and In Street Signs, Conduct Stop Warrant Analysis
Criteria	Option 2 All uncontrolled intersections, with no crosswalks, with speed limit under 40mph, AWDT is over 2,000, under 12,000, on a school route, with in ½ mile of a school, with 3 collisions in 1 year or 5 collisions in 2 years.
Query	"Control" = ' ' AND "Crosswalk" = ' ' AND "SPEED" < 40 AND "School" <> 'not school' AND ("Crash1yr" = 3 OR "Crash2yr" = 5) AND "In_half_mi" = 'yes' AND (("ADT_EW" > 2000 AND "ADT_EW" < 12000) OR ("ADT_NS" > 2000 AND "ADT_NS" < 12000))
Result file name	Uncontrolled_Unmarked_crosswalks_Option2.shp
Map Label and Symbol	1 intersection  Mark x-walk and improve at Uncontrolled, unmarked - Option 2

#	Uncontrolled no X-Walks Option 2	School
1	SANTA ROSALIA TE & E DUANE AV	San Miguel

Improvements to Consider	Install High Visibility Crosswalks, Advanced Yield Lines and In Street Signs, Conduct Stop Warrant Analysis
Criteria	All 1-way or 2-way stops , with speed limit under 40mph, on a school route, not on an arterial, with in ½ mile of a school, with 3 collisions in 1 year or 5 collisions in 2 years.
Query	("Stop_Type" = '1-way' OR "Stop_Type" = '2-way') AND "SPEED" < 40 AND "School" <> 'not school' AND "In_half_mi" = 'yes' AND ("Crash1yr" = 3 OR "Crash2yr" = 5) AND "Arterial" <> 'yes'
Result file name	Uncontrolled_Unmarked_crosswalks_Option3.shp
Map Label and Symbol	9 intersections  Mark x-walk and improve at Uncontrolled, unmarked - Option 3

#	Uncontrolled no X-Walks Option 3	School
1	W MAUDE AV & N MURPHY AV	Bishop
2	ROOSEVELT AV & E MAUDE AV	Bishop
3	WORLEY AV & E MAUDE AV	Bishop
4	HARVARD AV & HOLLENBECK AV	Cumberland
5	LAWRENCE EX & LAKEWOOD DR & BRIDGEWOOD WY	Fairwood
6	E DUANE AV & SAN LUISITO WY	San Miguel
7	SANTA PAULA AV & E DUANE AV	San Miguel
8	E DUANE AV & SAN RAFAEL ST	San Miguel
9	GRAND COULEE AV & HOLLENBECK AV	West Valley

SECTION 4

Citywide Maps

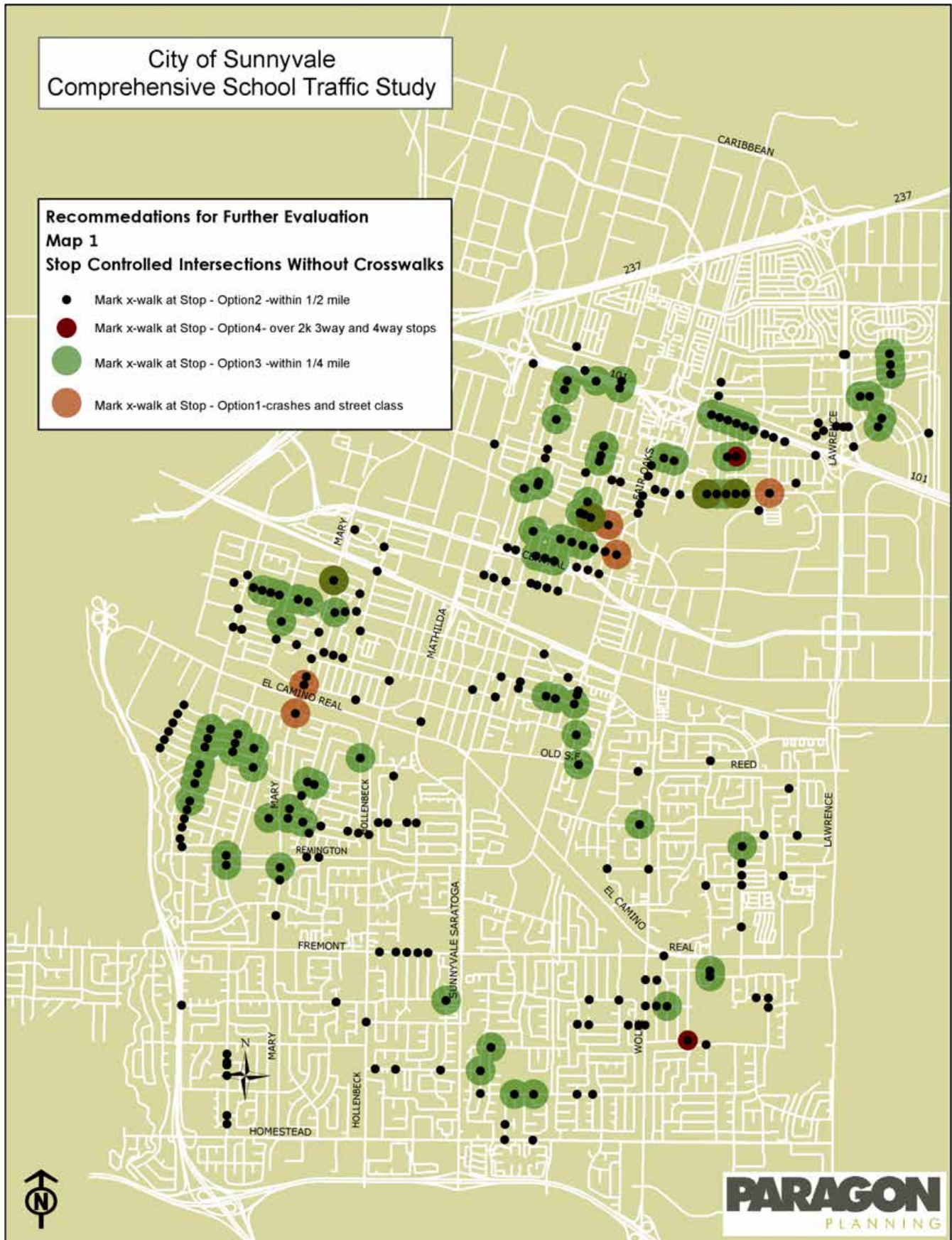
City of Sunnyvale Comprehensive School Traffic Study

Recommendations for Further Evaluation

Map 1

Stop Controlled Intersections Without Crosswalks

- Mark x-walk at Stop - Option2 -within 1/2 mile
- Mark x-walk at Stop - Option4- over 2k 3way and 4way stops
- Mark x-walk at Stop - Option3 -within 1/4 mile
- Mark x-walk at Stop - Option1-crashes and street class



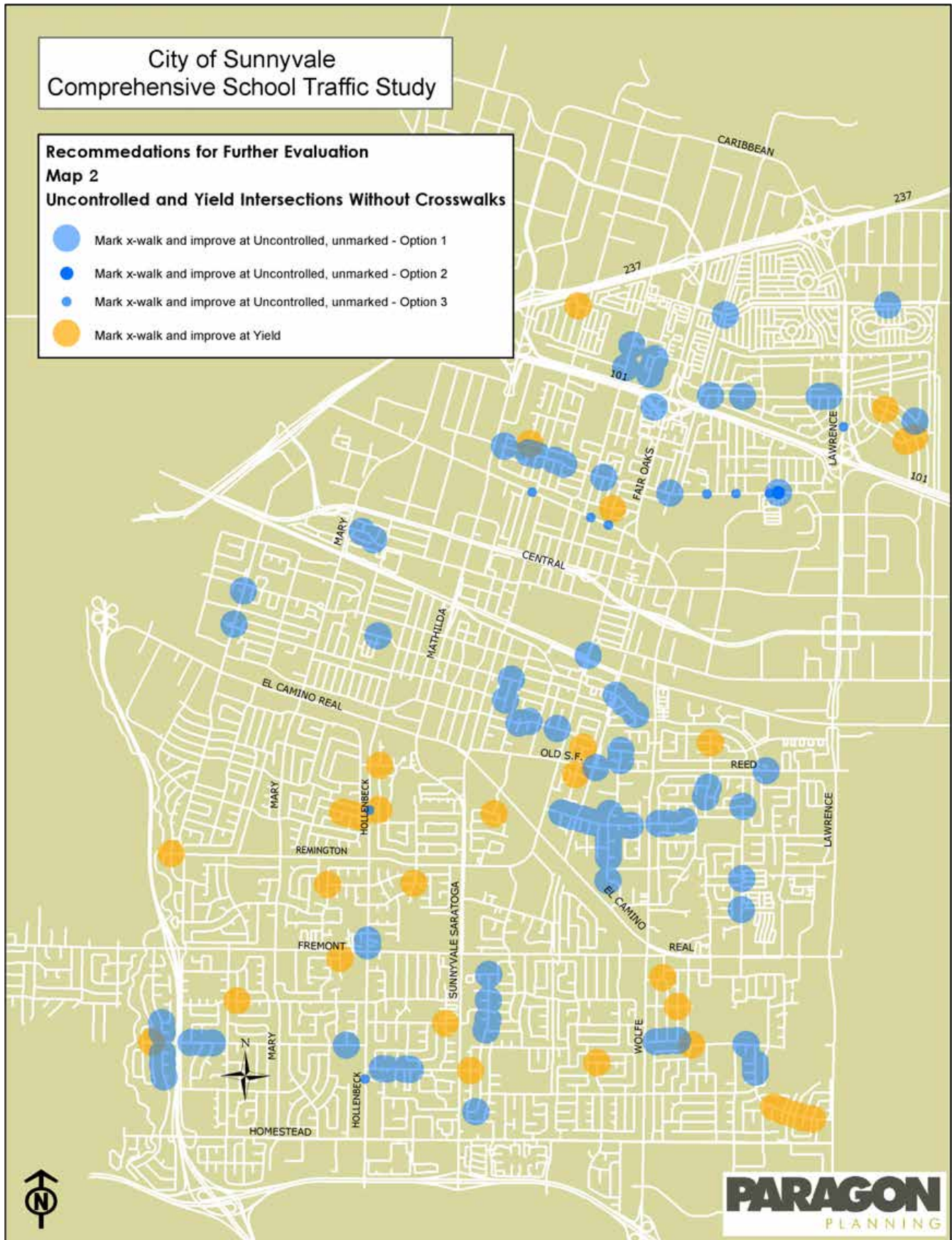
City of Sunnyvale Comprehensive School Traffic Study

Recommendations for Further Evaluation

Map 2

Uncontrolled and Yield Intersections Without Crosswalks

- Mark x-walk and improve at Uncontrolled, unmarked - Option 1
- Mark x-walk and improve at Uncontrolled, unmarked - Option 2
- Mark x-walk and improve at Uncontrolled, unmarked - Option 3
- Mark x-walk and improve at Yield



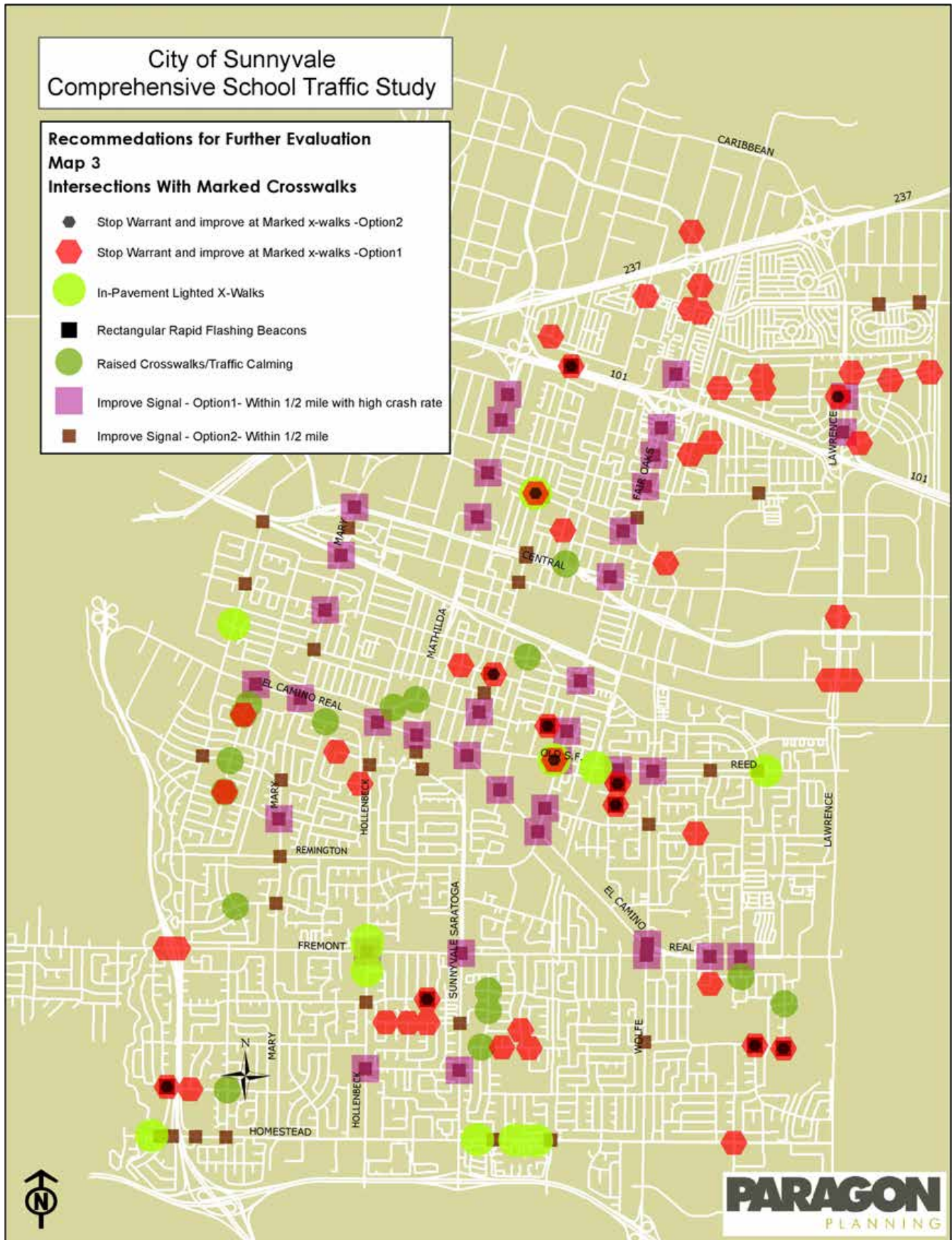
City of Sunnyvale Comprehensive School Traffic Study

Recommendations for Further Evaluation

Map 3

Intersections With Marked Crosswalks

- Stop Warrant and improve at Marked x-walks -Option2
- Stop Warrant and improve at Marked x-walks -Option1
- In-Pavement Lighted X-Walks
- Rectangular Rapid Flashing Beacons
- Raised Crosswalks/Traffic Calming
- Improve Signal - Option1- Within 1/2 mile with high crash rate
- Improve Signal - Option2- Within 1/2 mile



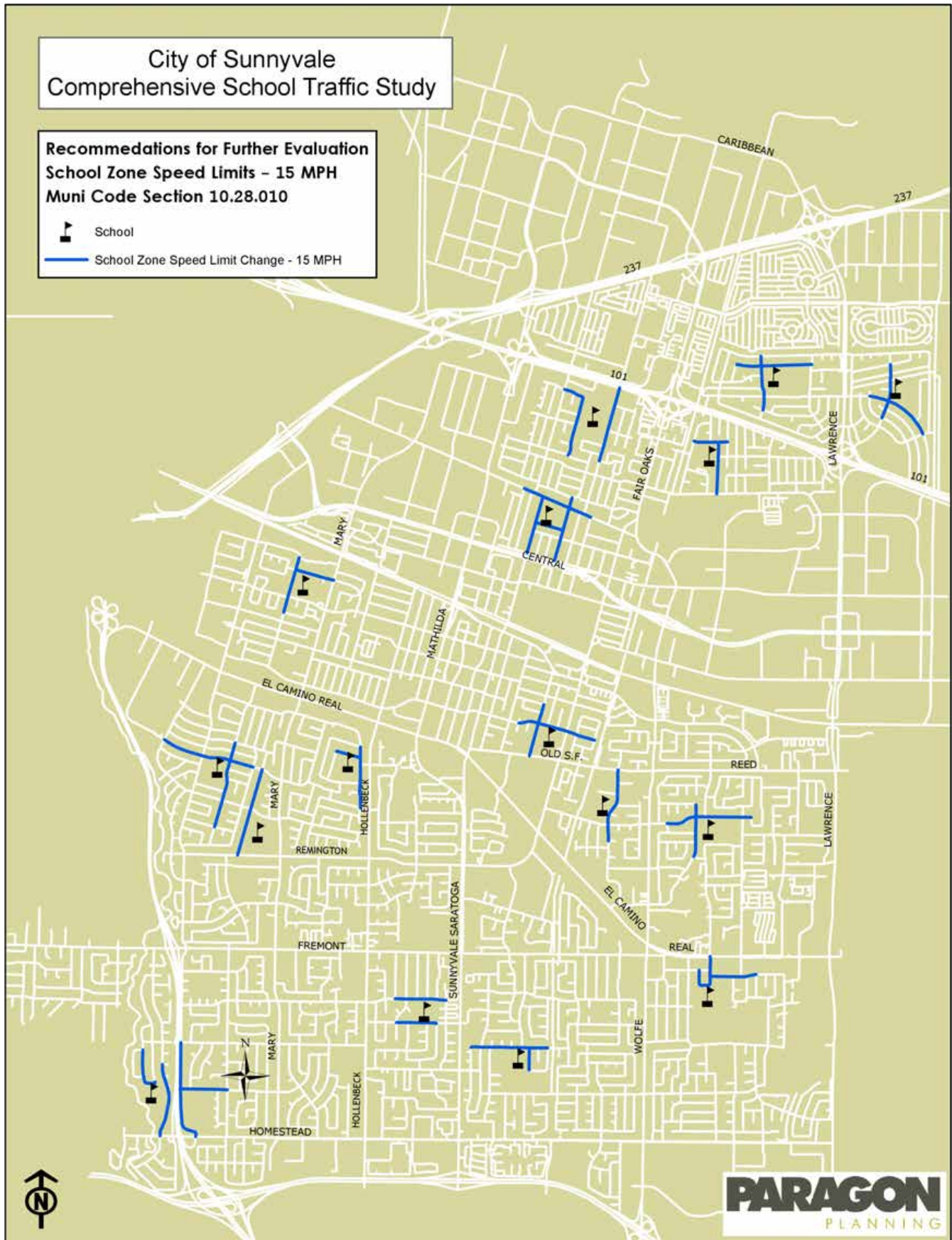
City of Sunnyvale Comprehensive School Traffic Study

Recommendations for Further Evaluation
School Zone Speed Limits - 15 MPH
Muni Code Section 10.28.010



School

 School Zone Speed Limit Change - 15 MPH



SECTION 5

School Attendance Area Maps

SUNNYVALE SCHOOL DISTRICT

Bishop Elementary and Columbia Middle	Page 38-39
Cherry Chase Elementary	Page 40-41
Cumberland Elementary	Page 42-43
Ellis Elementary	Page 44-45
Fairwood Elementary	Page 46-47
Lakewood Elementary	Page 48-49
San Miguel Elementary	Page 50-51
Vargas Elementary	Page 52-53

Basemap

- ### Recommendations for Intersections With Marked Crosswalks

- PARAGON**
-
- EQUINE INC.



Base Map



Safe Routes to School Cherry Chase Elementary Sunnyvale Middle

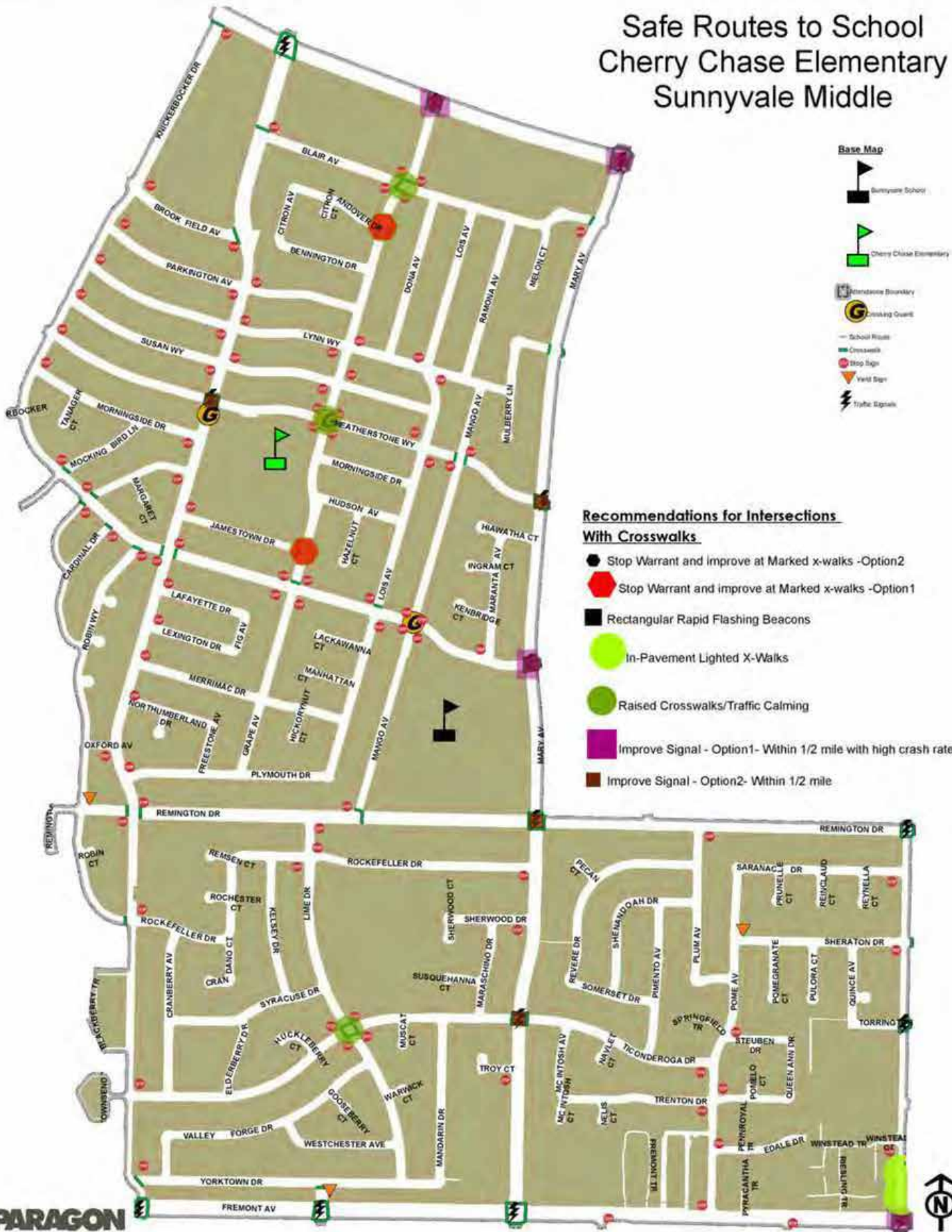
Recommendations for Intersections Without Crosswalks

- Mark x-walk and improve at Uncontrolled, unmarked - Option 1
- Mark x-walk and improve at Uncontrolled, unmarked - Option 2
- Mark x-walk and improve at Uncontrolled, unmarked - Option 3
- Mark x-walk and improve at Yield
- Mark x-walk at Stop - Option2 -within 1/2 mile
- Mark x-walk at Stop - Option4- over 2k 3way and 4way stops
- Mark x-walk at Stop - Option3 -within 1/4 mile
- Mark x-walk at Stop - Option1-crashes and street class



PARAGON
PLANNING

Safe Routes to School Cherry Chase Elementary Sunnyvale Middle











Safe Routes to School Cumberland Elementary

(Sunnyvale Middle School Feeder School)

Base Map

-  Cumberland Elementary
-  Attendance Boundary
-  School Route
-  Crossing Guard
-  Crosswalk
-  Traffic Signals
-  Stop Sign
-  Yield Sign

Recommendations for Intersections Without Crosswalks









-  Mark x-walk and improve at Uncontrolled, unmarked - Option 1
-  Mark x-walk and improve at Uncontrolled, unmarked - Option 2
-  Mark x-walk and improve at Uncontrolled, unmarked - Option 3
-  Mark x-walk and improve at Yield
-  Mark x-walk at Stop - Option2 -within 1/2 mile
-  Mark x-walk at Stop - Option4- over 2k 3way and 4way stops
-  Mark x-walk at Stop - Option3 -within 1/4 mile
-  Mark x-walk at Stop - Option1-crashes and street class










Safe Routes to School Cumberland Elementary

(Sunnyvale Middle School Feeder School)

Base Map

-  Cumberland Elementary
-  Attendance Boundary
-  School Route
-  Crossing Guard
-  Crosswalk
-  Traffic Signals
-  Stop Sign
-  Yield Sign

Recommendations for Intersections With Crosswalks

-  Stop Warrant and improve at Marked x-walks -Option2
-  Stop Warrant and improve at Marked x-walks -Option1
-  Rectangular Rapid Flashing Beacons
-  In-Pavement Lighted X-Walks
-  Raised Crosswalks/Traffic Calming
-  Improve Signal - Option1- Within 1/2 mile with high crash rate
-  Improve Signal - Option2- Within 1/2 mile



Safe Routes to School Ellis Elementary

(Sunnyvale Middle School Feeder School)

Pedestrian
Bridge
Access

Recommendations for Intersections Without Crosswalks

- Mark x-walk and improve at Uncontrolled, unmarked - Option 1
- Mark x-walk and improve at Uncontrolled, unmarked - Option 2
- Mark x-walk and improve at Uncontrolled, unmarked - Option 3
- Mark x-walk and improve at Yield
- Mark x-walk at Stop - Option2 -within 1/2 mile
- Mark x-walk at Stop - Option4- over 2k 3way and 4way stops
- Mark x-walk at Stop - Option3 -within 1/4 mile
- Mark x-walk at Stop - Option1-crashes and street class

Base Map

- Ellis Elementary
- Attendance Boundary
- School Route
- Crossing Guard
- Crosswalk
- Stop Sign
- Yield Sign
- Traffic Signals

Safe Routes to School Ellis Elementary

(Sunnyvale Middle School Feeder School)

Pedestrian
Bridge
Access

Recommendations for Intersections With Crosswalks

- Stop Warrant and improve at Marked x-walks -Option2
- Stop Warrant and improve at Marked x-walks -Option1
- Rectangular Rapid Flashing Beacons
- In-Pavement Lighted X-Walks
- Raised Crosswalks/Traffic Calming
- Improve Signal - Option1- Within 1/2 mile with high crash rate
- Improve Signal - Option2- Within 1/2 mile

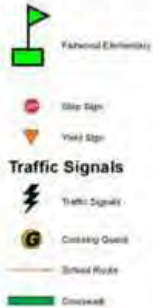
Base Map

- Ellis Elementary
- Attendance Boundary
- School Route
- Crossing Guard
- Crosswalk
- Stop Sign
- Yield Sign
- ⚡ Traffic Signals

Safe Routes to School Fairwood Elementary

(Columbia Middle School Feeder School)

Basemap



Recommendations for Intersections

Without Marked Crosswalks

- Mark x-walk and improve at Uncontrolled, unmarked - Option 1
- Mark x-walk and improve at Uncontrolled, unmarked - Option 2
- Mark x-walk and improve at Uncontrolled, unmarked - Option 3
- Mark x-walk and improve at Yield
- Mark x-walk at Stop - Option2 -within 1/2 mile
- Mark x-walk at Stop - Option4- over 2k 3way and 4way stops
- Mark x-walk at Stop - Option3 -within 1/4 mile
- Mark x-walk at Stop - Option1-crashes and street class

Pedestrian
Bridge Access

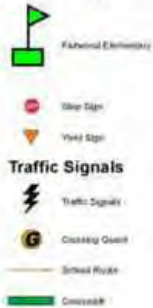
PARAGON
PLANNING



Safe Routes to School Fairwood Elementary

(Columbia Middle School Feeder School)

Basemap



Recommendations for Intersections With Marked Crosswalks

- Stop Warrant and improve at Marked x-walks -Option2
- Stop Warrant and improve at Marked x-walks -Option1
- Rectangular Rapid Flashing Beacons
- In-Pavement Lighted X-Walks
- Raised Crosswalks/Traffic Calming
- Improve Signal - Option1- Within 1/2 mile with high crash rate
- Improve Signal - Option2- Within 1/2 mile








Pedestrian
Bridge Access



Safe Routes to School Lakewood Elementary









(Columbia Middle School Feeder School)

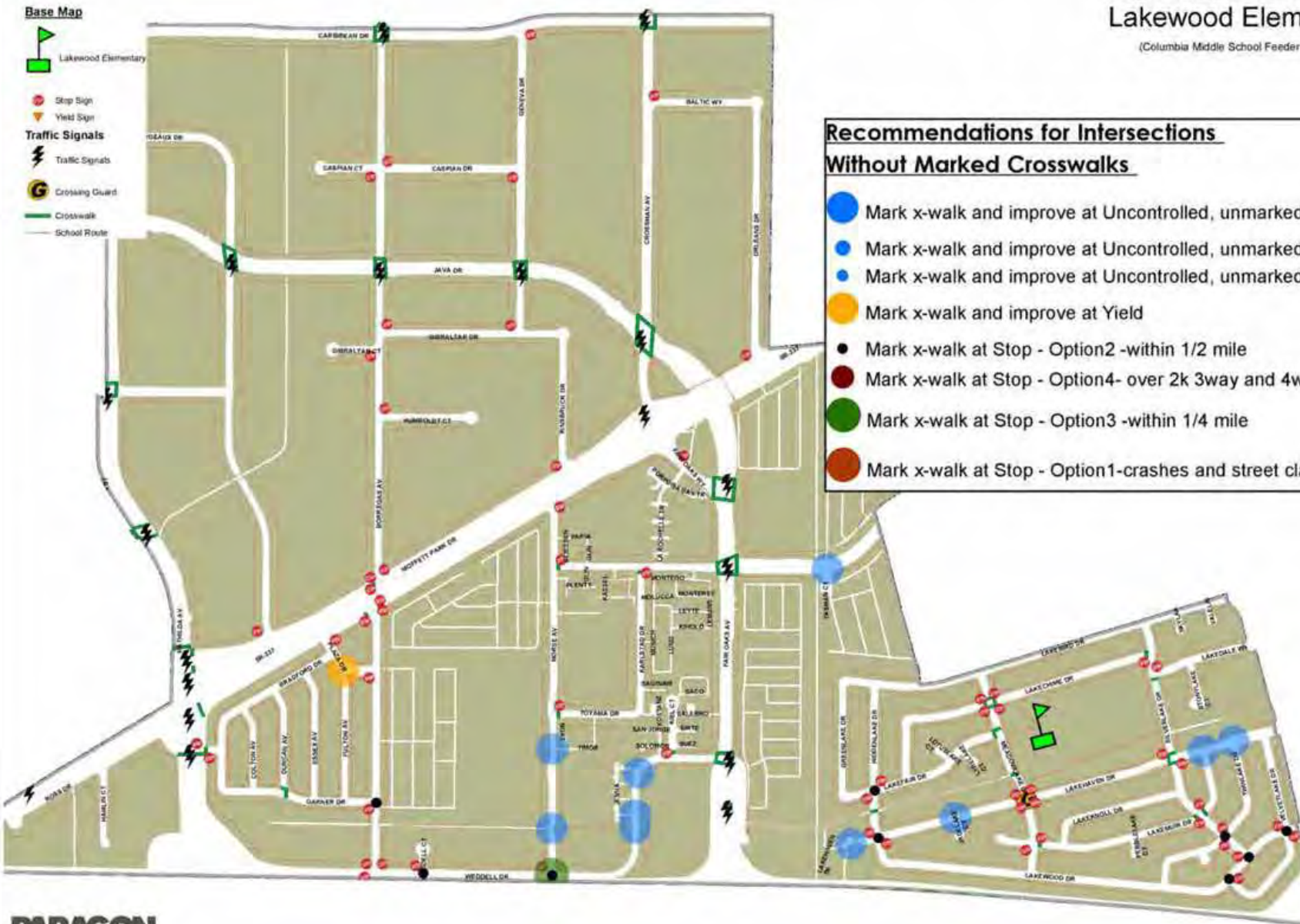
Base Map

-  Lakewood Elementary
-  Stop Sign
-  Yield Sign
- Traffic Signals**
-  Traffic Signals
-  Crossing Guard
-  Crosswalk
-  School Route

Recommendations for Intersections

Without Marked Crosswalks

-  Mark x-walk and improve at Uncontrolled, unmarked - Option 1
-  Mark x-walk and improve at Uncontrolled, unmarked - Option 2
-  Mark x-walk and improve at Uncontrolled, unmarked - Option 3
-  Mark x-walk and improve at Yield
-  Mark x-walk at Stop - Option2 -within 1/2 mile
-  Mark x-walk at Stop - Option4- over 2k 3way and 4way stops
-  Mark x-walk at Stop - Option3 -within 1/4 mile
-  Mark x-walk at Stop - Option1-crashes and street class











PARAGON
PLANNING



(Columbia Middle School Feeder School)



Base Map

-  San Miguel Elementary
-  Attendance Boundary
-  School Route
-  Crossing Guard
-  Crosswalk
-  Traffic Signals
-  Stop Sign
-  Yield Sign









Safe Routes to School San Miguel Elementary

(Columbia Middle School Feeder School)











Recommendations for Intersections

Without Marked Crosswalks

-  Mark x-walk and improve at Uncontrolled, unmarked - Option 1
-  Mark x-walk and improve at Uncontrolled, unmarked - Option 2
-  Mark x-walk and improve at Uncontrolled, unmarked - Option 3
-  Mark x-walk and improve at Yield
-  Mark x-walk at Stop - Option2 -within 1/2 mile
-  Mark x-walk at Stop - Option4- over 2k 3way and 4way stops
-  Mark x-walk at Stop - Option3 -within 1/4 mile
-  Mark x-walk at Stop - Option1-crashes and street class

Base Map

-  San Miguel Elementary
-  Attendance Boundary
-  School Route
-  Crossing Guard
-  Crosswalk
-  Traffic Signals
-  Stop Sign
-  Yield Sign

Safe Routes to School San Miguel Elementary

(Columbia Middle School Feeder School)



Safe Routes to School Vargas Elementary









(Sunnyvale Middle School Feeder School)

Base Map

-  Traffic Signals
-  Vargas Elementary
-  Stop Sign
-  Yield Sign
-  Crossing Guard
-  School Route
-  Attendance Boundary




Recommendations for Intersections Without Crosswalks

-  Mark x-walk and improve at Uncontrolled, unmarked - Option 1
-  Mark x-walk and improve at Uncontrolled, unmarked - Option 2
-  Mark x-walk and improve at Uncontrolled, unmarked - Option 3
-  Mark x-walk and improve at Yield
-  Mark x-walk at Stop - Option2 -within 1/2 mile
-  Mark x-walk at Stop - Option4- over 2k 3way and 4way stops
-  Mark x-walk at Stop - Option3 -within 1/4 mile
-  Mark x-walk at Stop - Option1-crashes and street class

Safe Routes to School Vargas Elementary

(Sunnyvale Middle School Feeder School)

Base Map


 Traffic Signals





Vargas Elementary

 Stop Sign

 Yield Sign








 Crossing Guard

 School Route

 Attendance Boundary



Recommendations for Intersections With Crosswalks

-  Stop Warrant and improve at Marked x-walks -Option2
-  Stop Warrant and improve at Marked x-walks -Option1
-  Rectangular Rapid Flashing Beacons
-  In-Pavement Lighted X-Walks
-  Raised Crosswalks/Traffic Calming
-  Improve Signal - Option1- Within 1/2 mile with high crash rate
-  Improve Signal - Option2- Within 1/2 mile



CUPERTINO UNION SCHOOL DISTRICT

Nimitz Elementary

Page 55-56

Stockleir Elementary

Page 56-57

West Valley Elementary









Page 58-59

Safe Routes to School Nimitz Elementary

(Cupertino Union School District)

Recommendations for Intersections

Without Crosswalks

-  Mark x-walk and improve at Uncontrolled, unmarked - Option 1
-  Mark x-walk and improve at Uncontrolled, unmarked - Option 2
-  Mark x-walk and improve at Uncontrolled, unmarked - Option 3
-  Mark x-walk and improve at Yield
-  Mark x-walk at Stop - Option2 -within 1/2 mile
-  Mark x-walk at Stop - Option4- over 2k 3way and 4way stops
-  Mark x-walk at Stop - Option3 -within 1/4 mile
-  Mark x-walk at Stop - Option1-crashes and street class

Base Map

-  Nimitz School
-  Traffic Signals
-  Stop Sign
-  Yield Sign
-  Crossing Guard
-  Crosswalk
-  School Route
-  Attendance Boundary










Safe Routes to School Nimitz Elementary

(Cupertino Union School District)

Base Map

-  Nimitz School
-  Traffic Signals
-  Stop Sign
-  Yield Sign
-  Crossing Guard
-  Crosswalk
-  School Route
-  Attendance Boundary

Recommendations for Intersections With Crosswalks

-  Stop Warrant and improve at Marked x-walks -Option2
-  Stop Warrant and improve at Marked x-walks -Option1
-  Rectangular Rapid Flashing Beacons
-  In-Pavement Lighted X-Walks
-  Raised Crosswalks/Traffic Calming
-  Improve Signal - Option1- Within 1/2 mile with high crash rate
-  Improve Signal - Option2- Within 1/2 mile

PARAGON
2015.09.17 - 11.02



Safe Routes to School Stockmeir Elementary

(Cupertino Union School District)

Recommendations for Intersections

Without Crosswalks

- Mark x-walk and improve at Uncontrolled, unmarked - Option 1
- Mark x-walk and improve at Uncontrolled, unmarked - Option 2
- Mark x-walk and improve at Uncontrolled, unmarked - Option 3
- Mark x-walk and improve at Yield
- Mark x-walk at Stop - Option2 -within 1/2 mile
- Mark x-walk at Stop - Option4- over 2k 3way and 4way stops
- Mark x-walk at Stop - Option3 -within 1/4 mile
- Mark x-walk at Stop - Option1-crashes and street class



Base Map

- Stockmeir School
- Attendance Boundary
- School Route
- G Crossing Guard
- Crosswalks
- Stop Sign
- ▲ Yield Sign
- ⚡ Traffic Signals

PARAGON
STATIONING

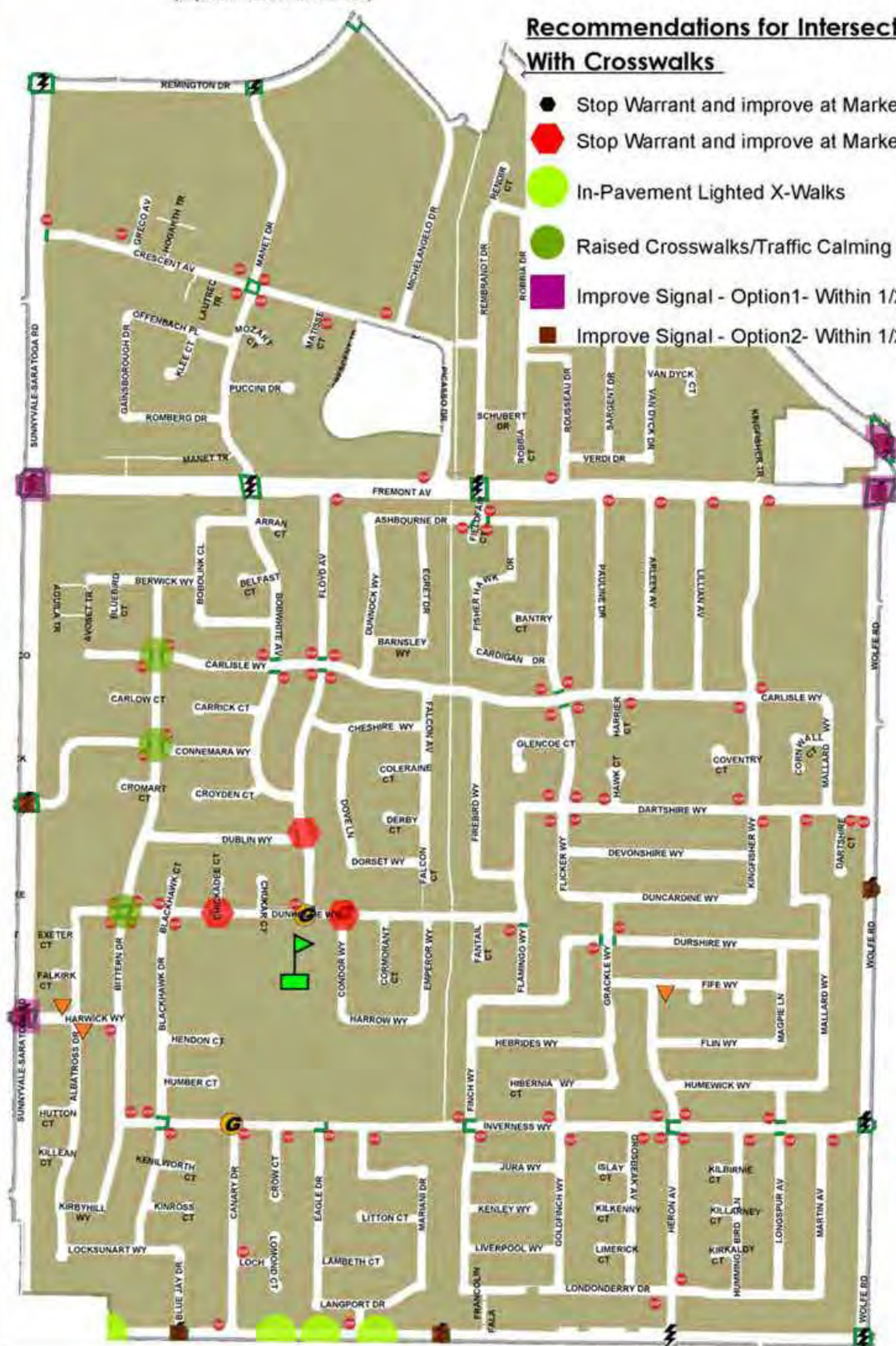


Safe Routes to School Stockmeir Elementary

(Cupertino Union School District)

Recommendations for Intersections With Crosswalks

- Stop Warrant and improve at Marked x-walks -Option2
- Stop Warrant and improve at Marked x-walks -Option1
- In-Pavement Lighted X-Walks
- Raised Crosswalks/Traffic Calming
- Improve Signal - Option1- Within 1/2 mile with high crash rate
- Improve Signal - Option2- Within 1/2 mile



Base Map

- Stockmeir School
- Attendance Boundary
- School Route
- Crossing Guard
- Crosswalks
- Stop Sign
- Yield Sign
- ⚡ Traffic Signals

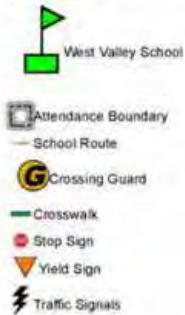
PARAGON
PLANNING



Safe Routes to School West Valley Elementary

(Cupertino Union School District)

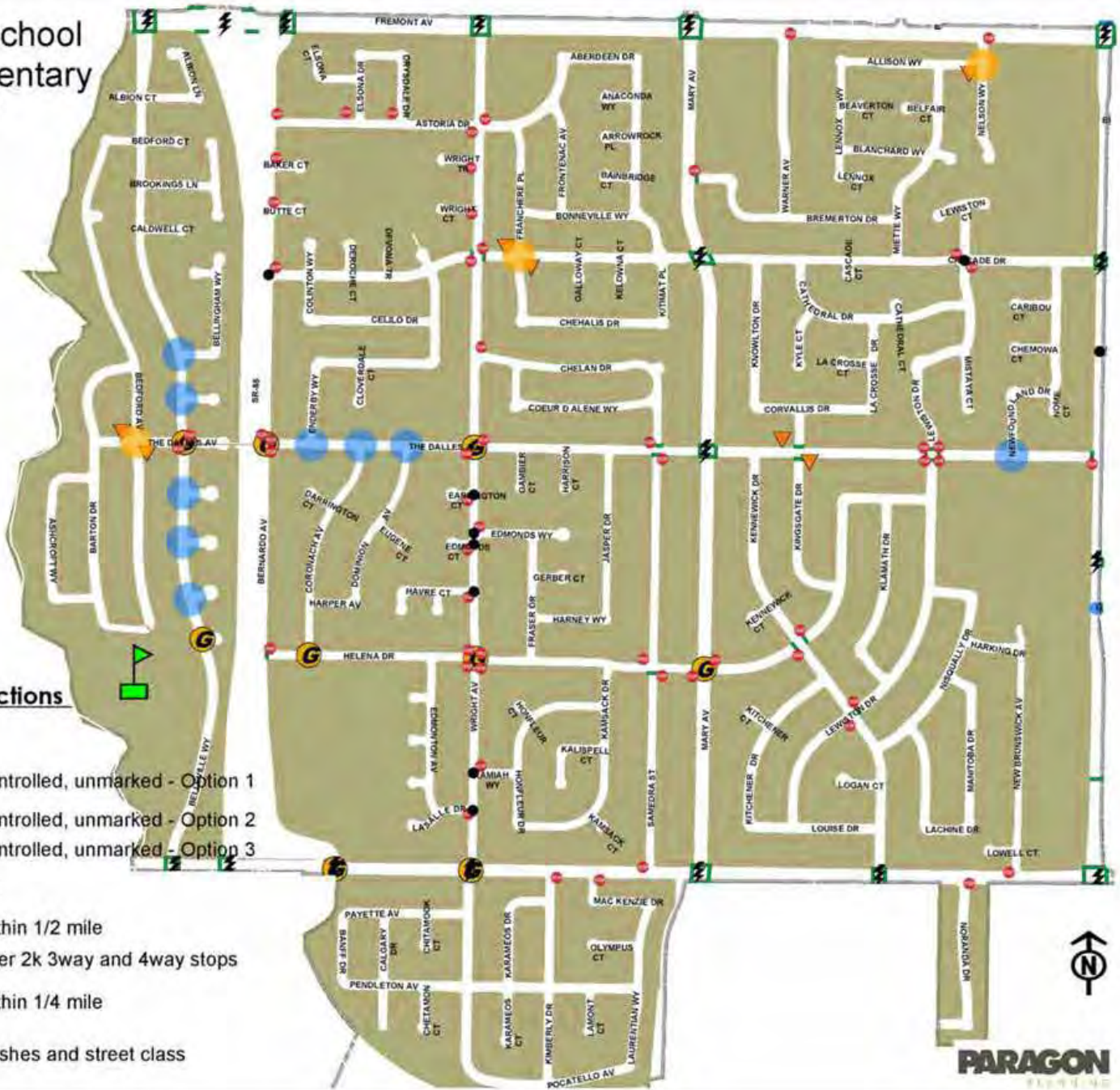
Base Map



Recommendations for Intersections

Without Crosswalks

- Mark x-walk and improve at Uncontrolled, unmarked - Option 1
- Mark x-walk and improve at Uncontrolled, unmarked - Option 2
- Mark x-walk and improve at Uncontrolled, unmarked - Option 3
- Mark x-walk and improve at Yield
- Mark x-walk at Stop - Option2 - within 1/2 mile
- Mark x-walk at Stop - Option4- over 2k 3way and 4way stops
- Mark x-walk at Stop - Option3 - within 1/4 mile
- Mark x-walk at Stop - Option1-crashes and street class



Safe Routes to School West Valley Elementary

(Cupertino Union School District)

Base Map



Recommendations for Intersections With Crosswalks

- Stop Warrant and improve at Marked x-walks - Option 2
- Stop Warrant and improve at Marked x-walks - Option 1
- Rectangular Rapid Flashing Beacons
- In-Pavement Lighted X-Walks
- Raised Crosswalks/Traffic Calming
- Improve Signal - Option 1- Within 1/2 mile with high crash rate
- Improve Signal - Option 2- Within 1/2 mile



SANTA CLARA UNIFIED SCHOOL DISTRICT

Braly Elementary

Page 62-63

Ponderosa Elementary

Page 64-65

Peterson Elementary

Page 66-67

Safe Routes to School Braly Elementary

(Santa Clara Unified School District)



**Recommendations for Intersections
Without Marked Crosswalks**

- Mark x-walk and improve at Uncontrolled, unmarked - Option 1
- Mark x-walk and improve at Uncontrolled, unmarked - Option 2
- Mark x-walk and improve at Uncontrolled, unmarked - Option 3
- Mark x-walk and improve at Yield
- Mark x-walk at Stop - Option2 -within 1/2 mile
- Mark x-walk at Stop - Option4- over 2k 3way and 4way stops
- Mark x-walk at Stop - Option3 -within 1/4 mile
- Mark x-walk at Stop - Option1-crashes and street class

Legend

- Stop
- Braly School
- School Route
- G Crossing Guard
- Crosswalk
- ▽ Yield Sign
- ⚡ Traffic Signals



Safe Routes to School Braly Elementary

(Santa Clara Unified School District)



Safe Routes to School Ponderosa Elementary

(Santa Clara Unified School District)

Base Map



Recommendations for Intersections

Without Marked Crosswalks

- Mark x-walk and improve at Uncontrolled, unmarked - Option 1
- Mark x-walk and improve at Uncontrolled, unmarked - Option 2
- Mark x-walk and improve at Uncontrolled, unmarked - Option 3
- Mark x-walk and improve at Yield
- Mark x-walk at Stop - Option2 -within 1/2 mile
- Mark x-walk at Stop - Option4- over 2k 3way and 4way stops
- Mark x-walk at Stop - Option3 -within 1/4 mile
- Mark x-walk at Stop - Option1-crashes and street class



PARAGON
LANDSCAPE

Safe Routes to School Ponderosa Elementary

(Santa Clara Unified School District)

Base Map



Recommendations for Intersections

With Marked Crosswalks

- Stop Warrant and improve at Marked x-walks -Option2
- Stop Warrant and improve at Marked x-walks -Option1
- Rectangular Rapid Flashing Beacons
- In-Pavement Lighted X-Walks
- Raised Crosswalks/Traffic Calming
- Improve Signal - Option1- Within 1/2 mile with high crash rate
- Improve Signal - Option2- Within 1/2 mile



PARAGON
LANDSCAPE

Safe Routes to School Peterson Middle School

(Santa Clara Unified School District)

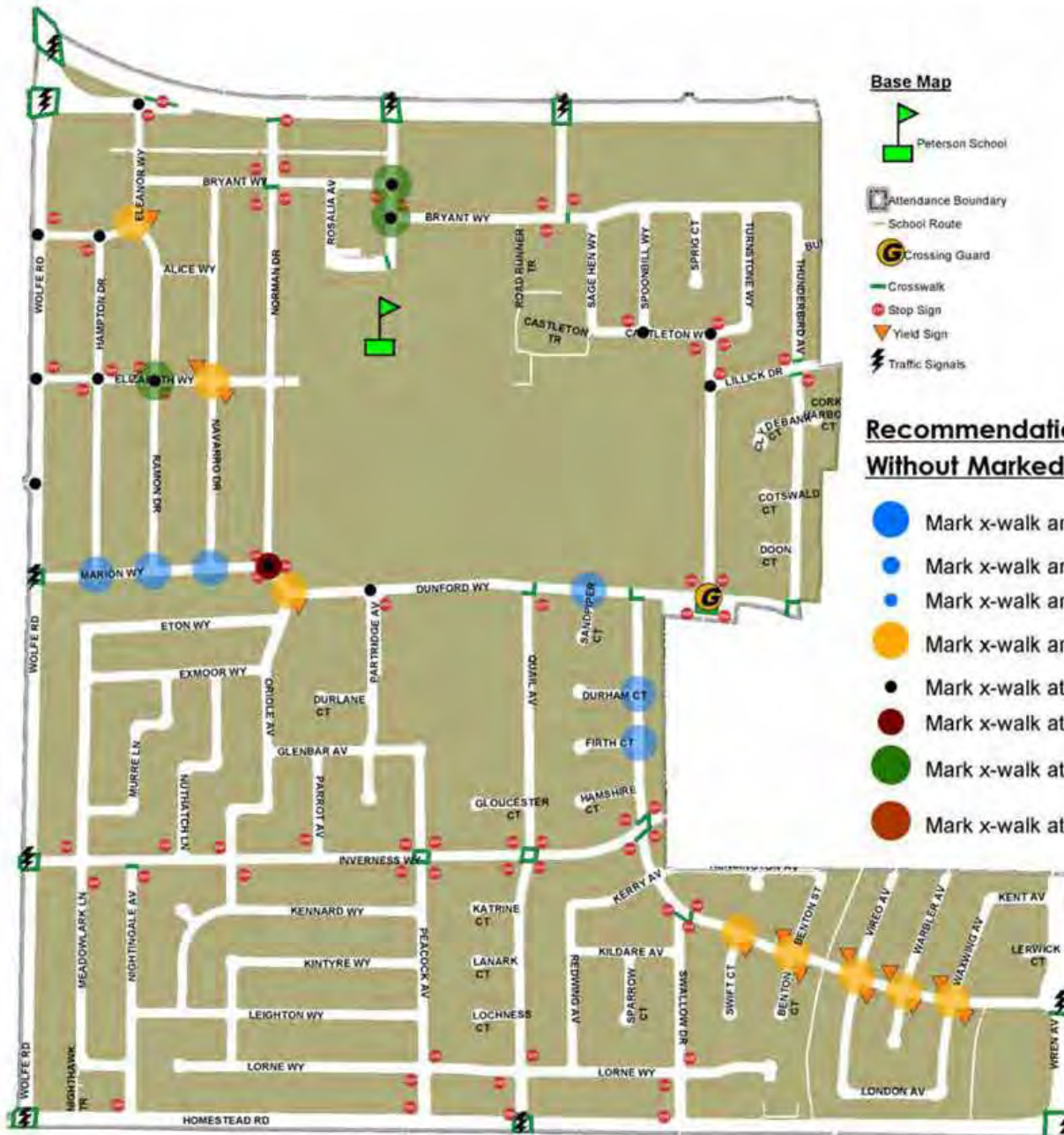
Base Map



Recommendations for Intersections Without Marked Crosswalks

Without Marked Crosswalks

- Mark x-walk and improve at Uncontrolled, unmarked - Option 1
- Mark x-walk and improve at Uncontrolled, unmarked - Option 2
- Mark x-walk and improve at Uncontrolled, unmarked - Option 3
- Mark x-walk and improve at Yield
- Mark x-walk at Stop - Option2 -within 1/2 mile
- Mark x-walk at Stop - Option4- over 2k 3way and 4way stops
- Mark x-walk at Stop - Option3 -within 1/4 mile
- Mark x-walk at Stop - Option1-crashes and street class



PARAGON
PLANNING

Safe Routes to School Peterson Middle School

(Santa Clara Unified School District)

Base Map



Recommendations for Intersections With Marked Crosswalks

- Stop Warrant and improve at Marked x-walks -Option2
- Stop Warrant and improve at Marked x-walks -Option1
- Rectangular Rapid Flashing Beacons
- In-Pavement Lighted X-Walks
- Raised Crosswalks/Traffic Calming
- Improve Signal - Option1- Within 1/2 mile with high crash rate
- Improve Signal - Option2- Within 1/2 mile



PARAGON
PLANNING

Attachment D
Examples of School Traffic Control Improvements



Pedestrian Flashing Sign



Crosswalk Refuge



In-Pavement Roadway Warning Lights



Marked School Crosswalk



Raised Crosswalk



High Visibility Ladder Crosswalk



DRAFT

SUNNYVALE BICYCLE AND PEDESTRIAN ADVISORY COMMISSION
Meeting Minutes – November 15, 2012

The Sunnyvale Bicycle and Pedestrian Advisory Commission met at 6:31 p.m. on November 15, 2012 with Commission Chair James Manidakos presiding. The meeting was held in the West Conference Room, City Hall, 456 West Olive Avenue, Sunnyvale.

ROLL CALL/CONSIDERATION OF ABSENCES

Members Present: James Manidakos
Angela Rausch
Kevin Jackson
Richard Kolber
David Jones
Cathy Switzer
Kyle Welch

Members Absent: None

Council Liaison
Present: Mayor Anthony Spitaleri

Staff Present: Jack Witthaus, Transportation and Traffic Division Manager, Department of Public Works

Visitors: David Simons, Valley Transportation Authority Bicycle and Pedestrian Advisory Committee representative

SPECIAL PRESENTATION

David Simons gave a summary of Valley Transportation Authority (VTA) BPAC events, including election of officers, pending review of One Bay Area Grant applications, and a pending workshop on comments on revisions to the VTA Bicycle Technical Guidelines. The Commission inquired about One Bay Area grant funding.

PUBLIC ANNOUNCEMENTS

(Speakers are limited to 3 minutes for announcements of related board/commission events, programs, resignations, recognitions, acknowledgments)

Commissioner Jackson announced the passing of Ellen Fletcher, a noted bicycle advocate. He gave updates on a Sunnyvale Cool Cities parking presentation, a Stevens Creek Trail public meeting, a Lawrence Station Area Plan citizens group meeting, and Safe Routes to School program activities. He commented on the importance of bicycles in the wake of the east coast Superstorm.

CONSENT CALENDAR

- 1.A) Approval of the Draft Minutes of October 18, 2012 Meeting
- 1.B) Updated 2012 BPAC Calendar

Chair Manidakos pulled item 1A, Approval of the Draft Minutes of October 18, 2012.

Item 1B was approved by consensus.

1.A) Approval of the Draft Minutes of October 18, 2012 Meeting

Chair Manidakos asked that the October minutes specifically reflect under item 1, Bicycle Parking for Non-Residential Uses, the BPAC's support for a ratio of 5% bicycle parking to vehicle parking. He also requested under Information Only items that the minutes reflect that the BPAC supports language in City driver training materials to state that bicycles and pedestrians are a "priority", not a "concern."

The minutes were approved as amended by consensus.

PUBLIC COMMENTS

This category is limited to 15 minutes, with a maximum of three minutes per speaker. If you wish to address the board or commission, please complete a speaker card and give it to the Recording Secretary or you may orally make a request to speak. If your subject is not on the agenda, you will be recognized at this time; but the Brown Act (Open Meeting Law) does not allow action by board or commission members. If you wish to speak to a subject listed on the agenda, you will be recognized at the time the item is being considered by the board or commission.

None.

PUBLIC HEARINGS/GENERAL BUSINESS

1. ACTION – Ranking of Study Issues

Chair Manidakos announced that the BPAC would follow the Study Issues ranking procedures provided by the Office of the City Manager.

Chair Manidakos inquired whether the current Stevens Creek Trail Joint Cities Feasibility Study would be considering property issues.

Motion by Jackson, second Manidakos, to drop issue CDD 13-01, Appropriate Locations for Bicycle Parking. Commissioner Jackson stated that he believed that the issue has the wrong emphasis, and that legitimate bicycle parking should be provided at locations where people are parking illegitimately, rather than regulating bike parking. Commissioner Jones noted that bicycle parking had been addressed well in the recent non-residential bicycle parking study. Motion approved, 7-0.

Motion by Jackson to drop issue ESD 13-04 regarding Transportation Demand Management (TDM) program enforcement. Commissioner Jackson stated that the level of effort was too great and was not justified based on his perceptions of compliance with

TDM requirements. Second by Manidakos. Friendly amendment by Kolber to defer the issue, not accepted by the maker. Commissioner Jones noted that the City Council had expressed concerns recently about questionable implementation and boilerplating of TDM programs. In response to an inquiry by Commissioner Kolber, staff stated that there are insufficient resources currently to conduct robust TDM program enforcement. Motion approved 6-1, Welch dissenting, stating he preferred deferral of the issue.

Motion by Manidakos to drop issue DPW 13-09 regarding resources for demarcating no parking zones at controlled intersections. He stated that Council has already acted to direct staff to conduct this work. Commissioner Jackson stated that he believes there are no resources to do this work. Staff clarified that there are resources, but it will take some time to accomplish the work. In response to an inquiry from Commissioner Jackson, staff stated that they are considering a budget issue to add resources to allow painting of red curbs as a means to implement the Council approved parking restrictions. Commissioner Switzer inquired if the parking restrictions would occur if neither a study issue or a budget issue were approved, and staff responded affirmatively. Commissioner Jackson asked if the BPAC could be given progress reports. Motion seconded by Jackson, motion approved 7-0.

Commissioner Kolber inquired about what a Cyclovia is.

Motion by Jones to drop issue DPW 13-07 regarding removal of bollards based on staff support for accomplishing this as an operational issue. Second Manidakos. Commissioner Jackson stated that he was not confident that progress would be made on this issue. In response to an inquiry from Chair Manidakos, staff indicated that work is proceeding on modification of two locations to remove bollards. Commissioner Jackson stated that the pedestrian overpass near Fair Oaks and Highway 101 should be a priority. Motion approved 5-2, Jackson and Kolber dissenting based on a desire for prioritization and scheduling of removal.

Motion by Manidakos to defer item DPW 13-12 regarding acquisition of property along Stevens Creek until the Stevens Creek Trail Joint Cities Feasibility Study is complete. Second Jackson. Motion approved, 7-0.

Motion by Jackson, second Manidakos, to defer the Cyclovia study issue due to a clear lack of resources to hold an event. Commissioner Switzer suggested contacting local cycling clubs to see if they would pursue an event. Motion approved, 7-0.

Motion by Jackson to defer the bicycle and pedestrian level of service study issue. Commissioner Jackson stated that it is not a practical role for a City to take the lead in adopting this type of measure. The Commission and staff discussed the status of various efforts to consider bicycle and pedestrian level of service, including efforts by the VTA. Second by Manidakos based on the VTA's ongoing consideration of bicycle and pedestrian level of service. Motion approved, 7-0.

Motion by Jackson to defer the bicycle boulevard pilot project study issue, based on his concerns that Sunnyvale would not implement an effective bicycle boulevard and that bicycle boulevards still require bicycles to ride side by side with motor vehicles without any

dedicated bike space. Commissioner Jones encouraged staff to refine the definition of bicycle boulevards. Motion failed, 2-5, Manidakos, Rausch, Switzer, Kolber, and Welch opposed.

Discussion of study issues for ranking occurred. Commissioner Jackson indicated support for issue DPW 13-06 regarding residential area parking measures to support bicycling. Commissioner Jones related that there was City Council support for DPW 13-01, the pedestrian anti-harassment ordinance.

The Commission ranked study issues as follows:

ISSUE	Switzer	Rausch	Manidakos	Jackson	Welch	Jones	Kolber	Total
DPW 13-04, vision triangle extension	1	3	3	3	5	3	4	22
DPW 13-06, residential parking measures	2	2	6	1	1	1	2	15
DPW 13-08, bikes use full lane signs	3	4	2	2	7	7	7	32
DPW 13-10, bike boulevard pilot	5	1	1	6	2	4	3	22
DPW 13-01, pedestrian anti-harassment	4	7	7	7	4	2	1	32
DPW 13-05 street amenities for disabled	6	6	5	5	2	3	6	33
DPW 13-02 broken yellow lines	7	5	4	4	6	6	5	37

Motion by Kolber, second Manidakos, to recommend the top five scoring issues to the City Council, and to defer the remaining two issues to the next year. Motion approved, 7-0.

2. ACTION - STUDY ISSUE – Consideration and Recommendation to Council on a Comprehensive School Traffic Study

Staff indicated that they could answer questions. In response to an inquiry from Commissioner Jackson, staff responded that the study had not been shared with school administrators. Commissioner Jackson encouraged staff to share the study with schools. Commissioner Jackson inquired about the Traffic Safe Communities Network effort. He requested editorial changes to the Report to Council to clarify that lower speeds can reduce the incidence of collisions as well, by increasing reaction times. He asked about the process for Council to approve speed limit changes. Commissioner Welch inquired about private schools. Commissioner Jones asked about the focus of the study on traffic controls and GIS formatting. Motion by Manidakos, second Kolber to recommend approval of the staff recommendation. Motion approved, 7-0.

NON-AGENDA ITEMS AND COMMENTS

- COMMISSIONERS ORAL COMMENTS

Chair Manidakos requested numbering of the meeting packet pages. He encouraged staff to prepare agendas as far ahead of time as possible.

The Commission discussed possible agenda items for the December 20 meeting.

- STAFF ORAL COMMENTS

None.

INFORMATION ONLY ITEMS

1. BPAC E-mail messages and/or letters since circulation of the agenda packet of the October 18, 2012 meeting.
2. BPAC Active Items List.

Commissioner Jackson inquired about the status of Bernardo Avenue Caltrain undercrossing planning.

ADJOURNMENT

Meeting adjourned at 8:45 p.m.

Respectfully submitted by:

Jack Witthaus
Transportation and Traffic Manager



City of Sunnyvale

Agenda Item

14-0092**Agenda Date: 4/8/2014**

REPORT TO COUNCIL

SUBJECT

Approve a Resolution Creating School Zone Speed Limits on Lakechime, Meadowlake, and Silverlake Drives near Lakewood Elementary School and Determine that the Action is Exempt from the California Environmental Quality Act

DISCUSSION

Staff received a citizen request to consider school zone speed limits on streets adjacent to Lakewood Elementary School. In response, staff has completed an engineering study of existing speeds on Lakechime Drive, Meadowlake Drive, and Silverlake Drive in the morning school hours in order to evaluate prevailing speeds against the Council-established threshold for consideration of 15 mile per hour school zones. At its December 4, 2012 meeting, the City Council adopted a criterion as part of a Comprehensive School Traffic Study to allow consideration of 15 mile per hour zones on those qualifying street segments that have documented higher traffic speeds (RTC 12-279). The criterion is for those school area streets that have an 85 percentile speed greater than 25 miles per hour during school commute times, establishment of a 15 mile per hour zone would be recommended.

Completed speed surveys for the 7:00 a.m. to 9:00 a.m. time period on Lakechime, Meadowlake and Silverlake Drives show that the 85 percentile speeds for the three streets are 29.9 miles per hour on Lakechime Drive, 25.2 miles per hour on Meadowlake and 34.1 miles per hour on Silverlake Drive. These prevailing speeds exceed the Council's established threshold for consideration of 15 mile per hour school zone establishment. Per the California Vehicle Code, modification of speed limits shall be done by ordinance or resolution of the governing body (Attachment 1). The proposed school zones are depicted in Attachment 2.

EXISTING POLICY

Safety and Noise Element SN-3.5, Facilitate the safe movement of pedestrians, bicycles, and vehicles.

ENVIRONMENTAL REVIEW

Approving the Resolution to establish prevailing speeds on certain streets in Sunnyvale is a project that is exempt from environmental review pursuant to CEQA Guideline 15301(c) because it concerns the operation of an existing street.

FISCAL IMPACT

Fabrication and installation of signs creating 15 mile per hour school zones is estimated at \$700. This cost would be absorbed within the operating budget for traffic signs and markings in the Pavement Maintenance Program.

PUBLIC CONTACT

Public contact was made by posting the Council agenda on the City's official-notice bulletin board outside City Hall, at the Sunnyvale Senior Center, Community Center and Department of Public Safety; and by making the agenda and report available at the Sunnyvale Public Library, the Office of the City Clerk and on the City's website.

RECOMMENDATION

Approve the attached resolution establishing 15 mile per hour school zones on Lakechime Drive, Meadowlake Drive, and Silverlake Drive adjacent to Lakewood Elementary School per the requirements of the California Vehicle Code.

Prepared by: Jack Witthaus, Transportation and Traffic Manager

Reviewed by: Kent Steffens, Director, Department of Public Works

Approved by: Robert A. Walker, Interim City Manager

ATTACHMENTS

1. Resolution
2. Proposed Reduced School Speed Limit Zone

DRAFT 3/12/14 *h3*

RESOLUTION NO. _____

**A RESOLUTION OF THE CITY COUNCIL OF THE CITY
OF SUNNYVALE AMENDING RESOLUTION NO. 203-95 TO
AUTHORIZE SCHOOL ZONE SPEED LIMITS ON
PORTIONS OF LAKECHIME DRIVE, MEADOWLAKE
DRIVE AND SILVERLAKE DRIVE**

WHEREAS, Title 10, Section 10.28.010 of the Sunnyvale Municipal Code provides that the designation of speed limits is to be accomplished by the City Council by resolution; and

WHEREAS, pursuant thereto, the City Council adopted Resolution No. 203-95 setting forth certain designated speed limits in the City of Sunnyvale, which Resolution has been amended from time to time by Resolution Nos. 159-97, 192-97, 114-98, 170-02, 159-03 and 632-14; and

WHEREAS, staff completed speed surveys around Lakewood Elementary School on Lakechime, Meadowlake and Silverlake Drives in the morning school hours in order to evaluate prevailing speeds against the City Council-established threshold for consideration of 15 mph school zones, and determined that the designated streets meet the criteria for a 15 mph school zone speed; and

WHEREAS, the City Council desires to adopt school zone speed limits to portions of Lakechime, Meadowlake and Silverlake Drives by decreasing the speed limit from 25 mph to 15 mph, when children are present, in accordance with California Vehicle Code 22358.4; and

WHEREAS, this project is exempt under the California Environmental Quality Act Section 15301(c), because it is a minor alteration to the operation of existing highways and streets.

NOW, THEREFORE, BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF SUNNYVALE THAT it has been determined it is necessary to amend the list of "Speed Limits" set forth in Resolution No. 203-95, as amended from time to time, by adding School Zone Speed Limits as set forth below:

III. School Zone Speed Limits Pursuant to Municipal Code Section 10.28.010

Name of Street or Portion Affected	From	To	Speed Limit (MPH)
Lakechime Drive	Silverlake Drive	500 feet west of Meadowlake Drive	15
Meadowlake Drive	Lakebird Drive	Lakehaven Drive	15
Silverlake Drive	Lakebird Drive	Lakehaven Drive	15

Adopted by the City Council at a regular meeting held on _____, 2014, by the following vote:

AYES:

NOES:

ABSTAIN:

ABSENT:

ATTEST:

APPROVED:

City Clerk
[SEAL]

Mayor

APPROVED AS TO FORM:

City Attorney

Attachment 2

Proposed Reduced School Speed Limit Zone



Not to Scale



Proposed 15MPH School Speed Limit Zone

RESOLUTION NO. _____

**A RESOLUTION OF THE CITY COUNCIL OF THE CITY
OF SUNNYVALE AMENDING RESOLUTION NO. 813-17
ESTABLISHING A 15 MPH SPEED LIMIT DURING
SCHOOL HOURS WHEN CHILDREN ARE PRESENT, AT
35 LOCATIONS ADJACENT TO 16 PUBLIC SCHOOLS**

WHEREAS, the City Council of the City of Sunnyvale ("City Council") sets forth all multi-way stop intersections and the speed limits on all streets in the City of Sunnyvale ("City") having a speed limit different than the maximum or minimum speed limits set forth in the California Vehicle Code ("CVC") in a master resolution pursuant to Chapter 10.28 of the Sunnyvale Municipal Code; and

WHEREAS, the City last adopted Resolution No. 813-17, a new master resolution setting forth the speed limits on roads within the City on April 11, 2017; and

WHEREAS, the City Council desires to adopt this new Resolution establishing a 15 mph speed limit during school hours, when children are present, at 35 locations adjacent to 16 public schools within the City, in accordance with CVC 22358.4; and

WHEREAS, the City has determined and declared on the basis of the CVC and/or taking into consideration engineering and traffic survey provisions that the speed limits set forth herein are the most reasonable, safe and appropriate for the orderly movement of traffic on the applicable portions of such streets; and

WHEREAS, this project is categorically exempt from environmental review pursuant to the California Environmental Quality Act Guidelines section 15301(c) because it involves the operation of an existing street involving negligible or no expansion of existing use.

NOW, THEREFORE, BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF SUNNYVALE THAT:

1. The speed limits set forth in Exhibit A of this Resolution, which is incorporated in its entirety, are determined to be reasonable, safe and appropriate for the orderly movement of traffic on City streets and shall be the prima facie speed limits on those streets and portions of streets set forth herein.
2. Exhibit A establishes a 15 mph speed limit during school hours, when children are present, at 35 locations adjacent to 16 public schools within the City, and shall remain at 25 mph and/ or 30 mph during all other times. This speed limit is determined based on the CVC and is declared by the City Council to be reasonable, safe and appropriate for the orderly movement of traffic.
3. All other provisions of Resolution No. 813-17 shall remain in effect.

Adopted by the City Council at a regular meeting held on _____, by
the following vote:

AYES:

NOES:

ABSTAIN:

ABSENT:

RECUSAL:

ATTEST:

APPROVED:

City Clerk
[SEAL]

Mayor

APPROVED AS TO FORM:

City Attorney

EXHIBIT A

CITY OF SUNNYVALE Candidate Locations for Establishing 15 MPH Speed Zones

No.	School Name	Street Name	Limit 1	Limit 2	Current Speed Limit
1	Fairwood Elementary	Sandia Avenue	500 ft west of school property line	500 ft southeast of school property line	25
2	Fairwood Elementary	Fairwood Avenue	500 ft north of school property line	500 ft south of school property line	25
3	Columbia Middle	Morse Avenue	Ahwanee Avenue	500 ft south of school property line	25
4	Columbia Middle	San Diego Avenue	Hemlock Avenue	500 ft south of school property line	25
5	Columbia Middle	Hemlock Avenue	500 ft west of San Diego Avenue	San Diego Avenue	25
6	San Miguel Elementary	San Miguel Avenue	Alvarado Drive	500 ft south of school property line	25
7	San Miguel Elementary	Alvarado Avenue	San Juan Drive	San Pablo Avenue	25
8	Bishop Elementary	Maude Avenue	500 ft west of school property line	500 ft east of school property line	30
9	Bishop Elementary	Sunnyvale Avenue	Maude Avenue	500 ft south of school property line	25
10	Bishop Elementary	Hazelton Avenue	Sunnyvale Avenue	Hazelton Avenue	25
11	Bishop Elementary	Bayview Avenue	Arbor Drive	500 ft south of school property line	25
12	Vargas Elementary	Leota Avenue	500 ft north of school property line	500 ft south of school property line	25
13	Vargas Elementary	Carson Drive	Leota Avenue	Mary Avenue	25
14	Cumberland Elementary	Cumberland Drive	Piper Avenue	Quetta Avenue	25
15	Cumberland Elementary	Quetta Avenue	500 ft north of school property line	500 ft south of school property line	25
16	Ellis Elementary	Central Avenue	500 ft north of school property line	Old San Francisco Road	25
17	Ellis Elementary	Olive Avenue	500 ft west of school property line	500 ft east of school property line	25
18	Brady Elementary	Gail Avenue	500 ft north of school property line	500 ft south of school property line	25
19	Ponderosa Elementary	Iris Avenue	500 ft west of school property line	500 ft east of school property line	25
20	Ponderosa Elementary	Ponderosa Avenue	Foxglove Drive	500 ft south of school property line	25
21	Cherry Chase Elementary	Grape Avenue	500 ft north of school property line	500 ft south of school property line	25
22	Cherry Chase Elementary	Heatherstone Way	500 ft west of school property line	500 ft east of school property line	25
23	Sunnyvale Middle School	Mango Avenue	500 ft north of school property line	Remington Drive	25
24	Peterson Middle School	Rosalia Avenue	Bryant Way	Poplar Avenue	25
25	Peterson Middle School	Poplar Avenue	500 ft north of school property line	Rosalia Avenue	25
26	Peterson Middle School	Bryant Avenue	Poplar Avenue	500 ft east of school property line	25
27	Stockmeir Elementary School	Dunholme Way	500 ft west of school property line	500 ft east of school property line	25
28	Stockmeir Elementary School	Condor Way	Dunholme Way	Harrow Way	25
29	Nimitz Elementary School	Cascade Drive	500 ft west of school property line	Yukon Drive	25
30	Nimitz Elementary School	Cheyenne Drive	500 ft west of school property line	Valcartier Drive	25
31	Cupertino Middle School	Bernardo Avenue	500 ft north of school property line	Homestead Road	30
32	Cupertino Middle School	Helena Drive	Bernardo Avenue	500 ft east of school property line	25
33	West Valley Elementary School	Belleville Way	500 ft north of school property line	500 ft south of school property line	25
34	West Valley Elementary School	Barton Drive	500 ft north of school property line	Bedford Avenue	25
35	West Valley Elementary School	Bedford Avenue	500 ft north of school property line	Barton Drive	25



City of Sunnyvale

Agenda Item

18-0882

Agenda Date: 10/30/2018

REPORT TO CITY COUNCIL

SUBJECT

File #: 2018-7048

Location: 669 & 673 Old San Francisco Rd. (APNs 209-17-050 & 209-17-051)

Proposed Project: Appeal by a neighbor of a decision by the Planning Commission to conditionally approve related applications on a .34-acre site:

SPECIAL DEVELOPMENT PERMIT and **VESTING TENTATIVE MAP** to allow a three-story six-unit townhouse development.

Zoning: R-3/PD

Applicant / Owner: Innovative Concepts / George Nejat

Environmental Review: Mitigated Negative Declaration

Project Planner: Ryan Kuchenig, (408) 730-7431, rkuchenig@sunnyvale.ca.gov

SUMMARY OF COMMISSION ACTION

This Special Development Permit and Vesting Tentative Map were first considered by the Planning Commission on July 9, 2018; however, a continuance was granted by request of the applicant to a later date to allow for revisions to the plans. A subsequent public hearing was held with the Planning Commission on August 13, 2018. The Report to the Planning Commission (PC) can be found in Attachment 11. Minutes of the August 13, 2018 Planning Commission public hearing can be found in Attachment 12. The project was approved by the Planning Commission with a vote of 7-0 with the following modification to the Conditions of Approval (COAs):

- Staff shall review the paint color of the garage doors to ensure it is not too light with respect to the rest of the building (Added to COA PS-1).

As stated in the Planning Commission report, a similar six-unit project was reviewed for the project site by the City Council in April of 2017. That project also included a rezone of the site from the R-0 to R-3/PD zoning district. The City Council approved the rezone request but denied the Special Development Permit and Tentative Map based on the scale and design. The City Council did not recommend a reduction in the unit count of the project, but noted that scale of the buildings appeared too large and should be reduced to decrease the massing.

APPEAL

On August 24, 2018, the project was appealed by a neighbor of the project site. The letter of appeal is included as Attachment 13. The appellant notes concern regarding the project size, architectural massing and scale, location of parking, and conformance to City guidelines as reasons for the appeal. The appellant also notes inconsistencies in the project plans.

The applicant has submitted a letter in response to the appeal, which is included in Attachment 14.

STAFF COMMENTS ON APPEAL

The appeal letter refers to the size of the previous project that was reviewed by Planning Commission and City Council in April, 2017, as 75% FAR, while the new project is listed as 81%. The previous project plans listed an incorrect tabulation under "Total" for floor area. When each of the units are added up together from that 2017 plan, the total floor area result is 13,615 square feet (90.9% FAR). The project plans for the current project have always included a correct tabulation for each of the units at approximately 12,190 square feet (81.4% FAR). It should be noted that the Zoning Code does not have FAR standards for developments in R-3 zoning. FAR is not a good measurement of massing due to factors such as the number of bedrooms in each unit, differing heights of floors, roof design, overhangs, etc.

Regarding parking, staff finds that the parking spaces are appropriately located on the site. A location in front of the building near the street would be less desirable for safety and circulation. The parking spaces would have to be accessed from a sloped driveway. The building as currently designed would need to be shifted and result in a setback deviation. Landscaping in the front of the buildings, as currently designed, provides for a better street presence for the project. Furthermore, a carport at the rear would not meet required setbacks.

As stated in the report to the Planning Commission, the project was modified to address the concerns of the Planning Commission and City Council with respect to mass and design. Further changes to the windows were made to address privacy concerns of the neighbors. A total of 1,425 square feet was removed compared to the previous project. The total floor area of the third story was reduced by 1,350 square feet. The second story was reduced by 307 square feet. A total of 232 square feet of the displaced area was reconfigured within the first floor/garage and entrance areas.

The four end units that were reduced from four to three bedrooms were reduced in size more than the two middle units that retained four bedrooms. Nonetheless, each of the units were reduced in size to varying degrees. It was estimated at the hearing that the reduction of floor area was approximately 8% from the first project; however, the floor area difference has been clarified and recalculated to an approximately 9.5% decrease from the previous project.

There are no deviations proposed as part of the project. The first project included requests for deviations from setbacks and separation between the buildings. Because of the reduction in floor area, the massing of the buildings has been improved through a more proportional and less top-heavy design. To address further design concerns, an additional horizontal siding material was introduced to the façade. The front entries of the units facing the street have been enhanced and added landscaping berms further softens the appearance to the public street. Several trees have been added to the rear to buffer the development from the existing neighborhood.

The project is situated in a neighborhood of mixed development with two-story condominiums to the west and north and one and two-story single family development to the east. Four-story apartments are situated directly across the street. The proposed three story buildings are designed slightly below grade to better blend in with the adjacent development.

Staff agrees with the appellant that the delay of the earlier scheduled Planning Commission hearing was mostly due to inconsistencies within the project plans that caused confusion. Further clarifications and corrections were needed to make sure that the civil, landscaping and architectural site plans were consistent and represent the intended proposal. In addition, the dates referenced within certain plan pages needed to be updated to reflect the latest proposal. The applicant also

requested time to respond to concerns from neighbors.

PUBLIC CONTACT

Public contact was made by posting the Council agenda on the City's official-notice bulletin board outside City Hall, at the Sunnyvale Senior Center, Community Center and Department of Public Safety; and by making the agenda and report available at the Sunnyvale Public Library, the Office of the City Clerk and on the City's website.

ALTERNATIVES

1. Deny the appeal and affirm the Planning Commission's determination to adopt the Mitigated Negative Declaration, and affirm the approval of the Special Development Permit and Vesting Tentative Map subject to the recommended conditions of approval in Attachment 4.
2. Deny the appeal and affirm the Planning Commission's determination to adopt the Mitigated Negative Declaration, and affirm the approval of the Special Development Permit and Vesting Tentative Map subject to modified conditions of approval in Attachment 4.
3. Grant the appeal and reverse the Planning Commission's determination to adopt the Mitigated Negative Declaration; and, deny the Special Development Permit and Vesting Tentative Map.

STAFF RECOMMENDATION

Alternative 1: Deny the appeal and affirm the Planning Commission's determination to adopt the Mitigated Negative Declaration, and affirm the approval of the Special Development Permit and Vesting Tentative Map subject to the recommended conditions of approval in Attachment 4 of the report.

Staff finds that project has been adequately redesigned to address the City Council's direction as part of the denial action for the previous Special Development Permit. Further improvements have also been incorporated to respond to neighbors and to better integrate the project with the surrounding development.

Prepared by: Ryan Kuchenig, Senior Planner

Reviewed by: Gerri Caruso, Principal Planner

Reviewed by: Andrew Miner, Assistant Director of Community Development

Reviewed by: Trudi Ryan, Director of Community Development

Reviewed by: Teri Silva, Assistant City Manager

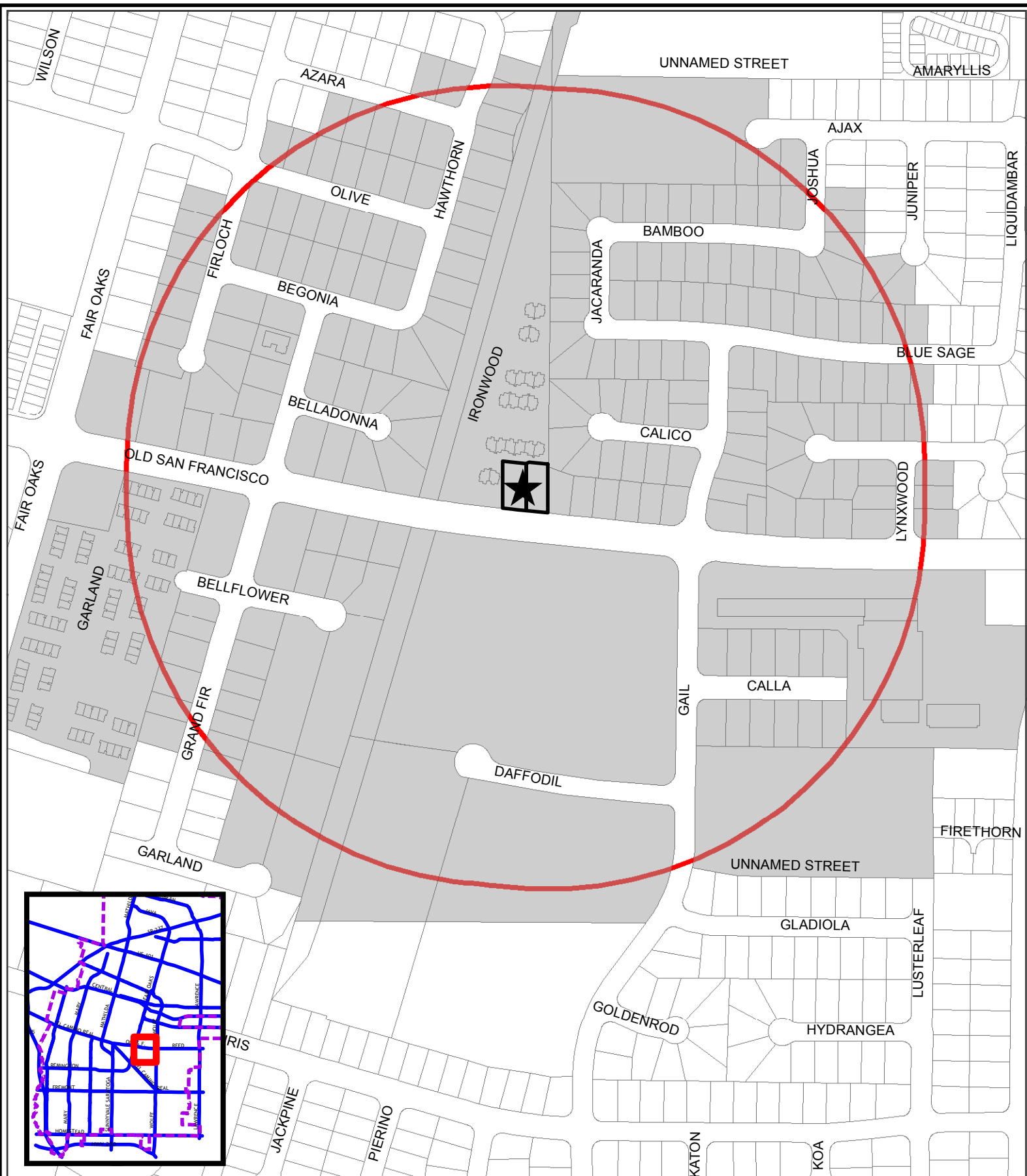
Approved by: Kent Steffens, City Manager

ATTACHMENTS

1. Vicinity and Noticing Radius Map
2. Project Data Table
3. Findings for Approval and General Plan Goals and Policies
4. Recommended Conditions of Approval
5. Project Plans and Tentative Map
6. Project Rendering
7. Mitigated Negative Declaration
8. Letters from Interested Parties
9. Excerpt of Minutes from the Planning Commission Meeting on July 9, 2018
10. Letter from the Applicant to the Planning Commission

Additional Attachments for Report to Council

11. Report to Planning Commission 18-0648, August 13, 2018 (without attachments)
12. Excerpt of Minutes of the Planning Commission Meeting of August 13, 2018
13. Appeal Letter
14. Applicant Response Letter to Appeal
15. Public Comment Letters Regarding the Appeal



File # 2018-7048
 669 Old San Francisco Rd.(APN:209-17-050 209-17-051)
 Special Development Permit | Tentative Map
 1000-ft Area Map

0 110 220 440 Feet



PROJECT DATA TABLE

	EXISTING	PROPOSED	REQUIRED/ PERMITTED
General Plan	Residential Medium Density	No Change	Residential Medium Density
Zoning District	R-3/PD	No Change	R-3/PD
Lot Size (s.f.)	14,977 (0.34 ac)	Same	8,000 min.
Gross Floor Area (s.f.)	4,102	12,190	No Max
Floor Area Ratio (FAR)	27%	81.3%	No Max
Lot Coverage	28%	35%	40% max.
No. of Units	N/A	6	8 max.
Density (units/acre)	N/A	18	24 max
Meets 75% min?	N/A	Yes	6 min.
Bedrooms/Unit	N/A	3 – Four Units 4 – Two Units	---
Unit Sizes (s.f.)	N/A	1,990 - 2,115 incl. garage	N/A
Lockable Storage/Unit	N/A	Within 2-car garage	300 cu. ft. min. storage or 2-car garage
No. of Buildings On-Site	3	2	N/A
Distance Between Buildings (ft.)	N/A	26'	26 min. for 3-story
Building Height (ft.)	Approx. 15'	29'	30 max.
No. of Stories	1	3	3 max.
Setbacks			
Front (ft.)	N/A	18' min. / 20' average	15' min./20' average
Side (ft.)	N/A	12' min.	12 min.
Rear (ft.)	N/A	23'1" – 28' 8"	20' min.
Landscaping			
Total Landscaping (sq. ft.)	N/A	3,330	2,550 min.
Landscaping (sq. ft. per unit)	N/A	555 per unit	425 min. per unit

	EXISTING	PROPOSED	REQUIRED/ PERMITTED
Usable Open Space (sq. ft. per unit)	N/A	500 per unit	400 min. per unit
Parking Lot Area Shading	0%	50%	50% min. in 15 years
Water-Efficient Landscaping Compliance	N/A	Water Budget (0% turf)	Water Budget Or 80% min. water-conserving plants and limit 25% turf
Parking			
Total Spaces	2 covered and 2 uncovered per unit) – 8 Total	2 Covered/ assigned per unit 4 unassigned - 16 Total	1 covered/ assigned per unit; 4 unassigned - 10 Total
Accessible Spaces	N/A	Per Building Code	Per Building Code
Covered Spaces	N/A	2 per Unit	1 per Unit
Aisle Width (ft.)	N/A	26	20-26 min.
Bicycle Parking	N/A	6 Secured in Garages	4 Secured
Impervious Surface Area (sq. ft.)	7,185	8,420	None per zoning code
Impervious Surface (%)	48%	56%	None per zoning code

RECOMMENDED FINDINGS
2018-7048
669-673 Old San Francisco Road

Adoption of Negative Declaration

In order to adopt the Negative Declaration, the City Council must make the following findings per CEQA Guidelines Section 15074:

1. The Negative Declaration was prepared and circulated for public review in accordance with the requirements of the California Environmental Quality Act.
2. The City Council has read and considered the Negative Declaration and finds on the basis of the whole record before it, including the Initial Study and any comments received, that there is no substantial evidence that the proposed Project will have a significant effect on the environment.
3. The Negative Declaration reflects the City Council independent judgment and analysis.
4. The mitigation measures listed in the Mitigated Negative Declaration have been incorporated as conditions of approval of the Project, including a program for reporting and monitoring the measures required to mitigate or avoid significant environmental effects.
5. The Department of Community Development, Planning Division, is the custodian of the records of the proceedings on which this decision is based. The records are located at Sunnyvale City Hall, 456 West Olive Ave., Sunnyvale, CA 94086.

Special Development Permit

Goals and Policies that relate to this project are:

- **Policy LT-2.1** Recognize that the City is composed of residential, industrial and commercial neighborhoods, each with its own individual character; and allow change consistent with reinforcing positive neighborhood values.
- **Policy LT-3.2** Encourage the development of ownership housing to maintain a majority of housing in the city for ownership choice.
- **Policy LT-4.1** Protect the integrity of the City's neighborhoods; whether residential, industrial or commercial.
- **Policy LT-4.2** Require new development to be compatible with the neighborhood, adjacent land uses, and the transportation system.

- **Policy LT-4.4** Preserve and enhance the high-quality character of residential neighborhoods.
- **Policy CC-1.3** Ensure that new development is compatible with the character of special districts and residential neighborhoods.
- **Policy HE-4.3** Require new development to build to at least 75 percent of the maximum zoning density, unless an exception is granted by the City Council.

In order to approve the Special Development Permit, the decision maker must be able to make at least one of the following findings:

1. The proposed use attains the objectives and purposes of the General Plan of the City of Sunnyvale. (*Finding met.*)

The proposed project meets the goals and policies of the General Plan as listed above by providing six ownership housing units in compliance with the planned residential density for the area. The project has been improved since the previously denied project through the use of an additional high quality material (horizontal siding) along with reduced massing of the upper stories which results in a greatly improved architectural design. The design of the proposed townhomes will enhance the neighborhood aesthetics and contribute positively to the streetscape and character.

2. The proposed use ensures that the general appearance of proposed structures, or the uses to be made of the property to which the application refers, will not impair either the orderly development of, or the existing uses being made of, adjacent properties. (*Finding met.*)

The project proposal consists of three-story townhomes which differs from two story development to the north and west and one-to-two story single family uses to the east; however, taller three-story development is located directly across the street. The grade of the site slopes down approximately two feet, which allows for a reduced height for three-story structures and does not exceed what the current zoning would already allow (30 feet). Directly across the street, the three-story Renaissance Apartment development (built in 1998) is approximately 43 feet tall. Similar to the surrounding development, the proposed setback is approximately 20 feet from the property line adjacent to Old San Francisco Road. Currently no trees are located within the front yards of the existing single family homes. The proposal includes six trees (including four street trees) and landscaping within the front setback, which will further soften the appearance of the new structures from the street. Approximately 23 trees are added overall to the site (plus four street trees) over current conditions. These trees are mostly located near the side and rear property lines of the development, which buffer the view and provide more privacy from neighboring development. The project slightly reduces the amount of curb cut needed off the public street using a singular driveway. The garages face the interior of the site, allowing the entrances of the end units to face the public street and better relate to the surrounding neighborhood. Improvements to the to the architecture include new materials and reduced upper-story massing to further contribute positively to the street presence.

Tentative Map

In order to approve the Tentative Map, the proposed subdivision must be consistent with the general plan. Staff finds that the Tentative Map is in conformance with the General Plan. However, if any of the following findings can be made, the Tentative Map shall be denied.

1. That the subdivision is not consistent with the General Plan.
2. That the design or improvement of the proposed subdivision is not consistent with the General Plan.
3. That the site is not physically suitable for the proposed type of development.
4. That the site is not physically suitable for the proposed density of development.
5. That the design of the subdivision or proposed improvements is likely to cause substantial environmental damage or substantially and avoidably injure fish or wildlife or their habitat.
6. That the design of the subdivision or type of improvements is likely to cause serious public health problems.
7. That the design of the subdivision or the type of improvements will conflict with easements, acquired by the public at large, for access through or use of property within the proposed subdivision.
8. That the map fails to meet or perform one or more requirements or conditions imposed by the "Subdivision Map Act" or by the Municipal Code.

Staff was not able to make any of the following findings and recommends approval of the Tentative Map.

**RECOMMENDED
CONDITIONS OF APPROVAL AND
STANDARD DEVELOPMENT REQUIREMENTS
OCTOBER 30, 2018**

Planning Application 2018-7048

669-673 Old San Francisco Road

Special Development Permit for the construction of six three-story
attached townhouse units, and

Vesting Tentative Map to subdivide two lots into six townhouse lots
and one common lot.

The following Conditions of Approval [COA] and Standard Development Requirements [SDR] apply to the project referenced above. The COAs are specific conditions applicable to the proposed project. The SDRs are items which are codified or adopted by resolution and have been included for ease of reference, they may not be appealed or changed. The COAs and SDRs are grouped under specific headings that relate to the timing of required compliance. Additional language within a condition may further define the timing of required compliance. Applicable mitigation measures are noted with "Mitigation Measure" and placed in the applicable phase of the project.

In addition to complying with all applicable City, County, State and Federal Statutes, Codes, Ordinances, Resolutions and Regulations, Permittee expressly accepts and agrees to comply with the following Conditions of Approval and Standard Development Requirements of this Permit:

GC: THE FOLLOWING GENERAL CONDITIONS AND STANDARD DEVELOPMENT REQUIREMENTS SHALL APPLY TO THE APPROVED PROJECT.

GC-1. CONFORMANCE WITH APPROVED PLANNING APPLICATION:

All building permit drawings and subsequent construction and operation shall substantially conform with the approved planning application, including: drawings/plans, materials samples, building colors, and other items submitted as part of the approved application. Any proposed amendments to the approved plans or Conditions of Approval are subject to review and approval by the City. The Director of Community Development shall determine whether revisions are considered major or minor. Minor changes are subject to review and approval by the Director of Community Development. Major changes are subject to review at a public hearing. [COA] [PLANNING]

GC-2. PERMIT EXPIRATION:

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- The permit shall be null and void two years from the date of approval by the final review authority at a public hearing if the approval is not exercised, unless a written request for an extension is received prior to expiration date and is approved by the Director of Community Development. [SDR] [PLANNING]
- GC-3. INDEMNITY:
The applicant/developer shall defend, indemnify, and hold harmless the City, or any of its boards, commissions, agents, officers, and employees (collectively, "City") from any claim, action, or proceeding against the City to attack, set aside, void, or annul, the approval of the project when such claim, action, or proceeding is brought within the time period provided for in applicable state and/or local statutes. The City shall promptly notify the developer of any such claim, action or proceeding. The City shall have the option of coordinating the defense. Nothing contained in this condition shall prohibit the City from participating in a defense of any claim, action, or proceeding if the City bears its own attorney's fees and costs, and the City defends the action in good faith. [COA] [OFFICE OF THE CITY ATTORNEY]
- GC-4. NOTICE OF FEES PROTEST:
As required by California Government Code Section 66020, the project applicant is hereby notified that the 90-day period has begun as of the date of the approval of this application, in which the applicant may protest any fees, dedications, reservations, or other exactions imposed by the city as part of the approval or as a condition of approval of this development. The fees, dedications, reservations, or other exactions are described in the approved plans, conditions of approval, and/or adopted city impact fee schedule. [SDR] [PLANNING / OCA]
- GC-5. ON-SITE AMENITIES:
Swimming pools, pool equipment structures, play equipment and other accessory utility buildings, except as otherwise subject to Planning Commission review, may be allowed by the Director of Community Development subject to approval of design, location and colors. [COA] [PLANNING]
- GC-6. STORMWATER MANAGEMENT PLAN:
Project is subject to Provision C3, of the Municipal Regional Stormwater Permit Order No. R2-2009-0074, as determined by a completed "Stormwater Management Plan Data Form", and therefore must submit a Stormwater Management Plan as per SMC 12.60.140 prior to issuance of the building permit. [SDR] [PLANNING]

GC-7. PUBLIC IMPROVEMENTS:

The developer is required to install, per Sunnyvale Municipal Code Sections 18.08, all public improvements, which may include but not be limited to, curb & gutter, sidewalks, driveway approaches, curb ramps, street pavements, utility extensions and connections, meters/vaults, trees and landscaping, signage, striping, street lights, etc.

All public improvements shall be designed and constructed in accordance with current City design standards, standard details and specifications, and Americans with Disabilities Act (ADA) requirements where applicable, unless otherwise approved by the Department of Public Works.

The developer is required to complete the installation of all public improvements and other improvements deemed necessary by the Public Works Department, prior to occupancy of the first building, or to the satisfaction of the Department of Public Works. [COA] [PUBLIC WORKS]

GC-8. OFF-SITE IMPROVEMENT PLANS:

Submit off-site improvement plans separate from the Building on-site improvement plans as the off-site improvement plans are approved through a Public Works Encroachment Permit process. Civil Sheets C-1 to C-3 dated 01/10/18 is subject to change during the plan check process. [SDR] [PUBLIC WORKS]

GC-9. ENCROACHMENT PERMIT:

Prior to any work in the public right-of-way, obtain an encroachment permit with insurance requirements for all public improvements including a traffic control plan per the latest California Manual on Uniform Traffic Control Devices (MUTCD) standards to be reviewed and approved by the Department of Public Works. [COA] [PUBLIC WORKS]

GC-10. FINAL MAP RECORDATION:

This project is subject to, and contingent upon the approval of a vesting tentative map and recordation of a Final Map. The submittal, approval and recordation of the Final Map shall be in accordance with the provisions of the California Subdivision Map Act and Sunnyvale Municipal Code Title 18 Subdivision requirements. All existing and proposed property lines, easements, dedications shown on the vesting tentative map are subject to City's technical review and approval during the Final Map process prior to any grading or building permit. [COA] [PUBLIC WORKS]

PS: THE FOLLOWING CONDITIONS SHALL BE MET PRIOR TO SUBMITTAL OF BUILDING PERMIT, AND/OR GRADING PERMIT.

PS-1. EXTERIOR MATERIALS REVIEW:

Final exterior building materials and color scheme are subject to review and approval by the Director of Community Development prior to submittal of a building permit. ***Staff shall review the paint color of the garage doors to ensure it is not too light with respect to the rest of the building (Per Planning Commission approval on August 13, 2018)*** [COA] [PLANNING]

PS-2. HYDRAULIC MODELING:

Prior to first off-site improvement plan check submittal, developer shall coordinate with the City for a Water System Hydraulic Modeling analysis to ensure that water main servicing the proposed project would meet various City design guidelines and other statutory requirements for fire, domestic and irrigation flows in terms of pipe size, demands, pressure and velocity.

Upgrade of existing water main by the developer may be required as determined by the City and shall be incorporated into first off-site improvement plan check submittal. Developer shall receive fair-share credit as determined by the City if upgrades of water main are required. Contact Environmental Services Department/Water Operations at 408-730-2744 for more information. [COA] [ENVIRONMENTAL SERVICES/PUBLIC WORKS]

PS-3. SANITARY SEWER ANALYSIS:

Prior to first off-site plan check submittal, submit a focused sanitary sewer analysis, to be reviewed and approved by the City, identifying the overall project impact to the City's existing sanitary sewer main(s). This includes, but is not limited to, the following:

- a) A detailed estimate of water consumption in gallons per day or estimate of sanitary sewer discharge in gallons per day; and
- b) Any incremental impact that will result from the new project in comparison to the existing sewer capacity of the immediate downstream mainline as needed, and allocation of wastewater discharge from the project site to each of the proposed laterals. Any deficiencies in the existing system in the immediate vicinity of the project will need to be addressed and resolved at the expense of the developer as part of the off-site improvement plans. Sewer flow monitoring data may be required as needed. Any mitigation

improvements needed shall be incorporated in the first plan check submittal. [COA] [PUBLIC WORKS]

BP: THE FOLLOWING CONDITIONS SHALL BE ADDRESSED ON THE CONSTRUCTION PLANS SUBMITTED FOR ANY DEMOLITION PERMIT, BUILDING PERMIT, GRADING PERMIT, AND/OR ENCROACHMENT PERMIT AND SHALL BE MET PRIOR TO THE ISSUANCE OF SAID PERMIT(S).

BP-1. CONDITIONS OF APPROVAL:

Final plans shall include all Conditions of Approval included as part of the approved application starting on sheet 2 of the plans. [COA] [PLANNING]

BP-2. RESPONSE TO CONDITIONS OF APPROVAL:

A written response indicating how each condition has or will be addressed shall accompany the building permit set of plans. [COA] [PLANNING]

BP-3. NOTICE OF CONDITIONS OF APPROVAL:

A Notice of Conditions of Approval shall be filed in the official records of the County of Santa Clara and provide proof of such recordation to the City prior to issuance of any City permit, allowed use of the property, or Final Map, as applicable. The Notice of Conditions of Approval shall be prepared by the Planning Division and shall include a description of the subject property, the Planning Application number, attached conditions of approval and any accompanying subdivision or parcel map, including book and page and recorded document number, if any, and be signed and notarized by each property owner of record.

For purposes of determining the record owner of the property, the applicant shall provide the City with evidence in the form of a report from a title insurance company indicating that the record owner(s) are the person(s) who have signed the Notice of Conditions of Approval. [COA] [PLANNING]

BP-4. BLUEPRINT FOR A CLEAN BAY:

The building permit plans shall include a "Blueprint for a Clean Bay" on one full sized sheet of the plans. [SDR] [PLANNING]

BP-5. SOLID WASTE DISPOSAL PLAN:

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- A detailed recycling and solid waste disposal plan shall be submitted for review and approval by the Director of Community Development prior to issuance of building permit. [COA] [PLANNING]
- BP-6. ROOF EQUIPMENT:
Roof vents, pipes and flues shall be combined and/or collected together on slopes of roof or behind parapets out of public view as per Title 19 of the Sunnyvale Municipal Code and shall be painted to match the roof. [COA] [PLANNING]
- BP-7. FEES AND BONDS:
The following fees and bonds shall be paid in full prior to issuance of building permit.
- a) TRANSPORTATION IMPACT FEE - Pay Traffic Impact fee for the net new trips resulting from the proposed project, estimated at **\$5,358.00**, prior to issuance of a Building Permit. (SMC 3.50). [SDR] [PLANNING]
 - b) PARK IN-LIEU - Pay Park In-lieu fees estimated at **\$197,588.16**, prior to approval of the Final Map or Parcel Map. (SMC 18.10). [SDR] [PLANNING]
- BP-8. MECHANICAL EQUIPMENT (EXTERIOR):
Detailed plans showing the locations of individual exterior mechanical equipment/air conditioning units shall be submitted and subject to review and approval by the Director of Community Development prior to issuance of building permits. Proposed locations shall have minimal visual and minimal noise impacts to neighbors and ensure adequate usable open space. Individual exterior mechanical equipment/air conditioning units shall be screened with architecture or landscaping features. [PLANNING] [COA]
- BP-9. LANDSCAPE PLAN:
Landscape and irrigation plans shall be prepared by a certified professional, and shall comply with Sunnyvale Municipal Code Chapter 19.37 requirements. Landscape and irrigation plans are subject to review and approval by the Director of Community Development through the submittal of a Miscellaneous Plan Permit (MPP). The landscape plan shall include the following elements:
- a) All areas not required for parking, driveways or structures shall be landscaped.
 - b) Ten percent (10%) shall be 24-inch box size or larger and no tree shall be less than 15-gallon size.

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- c) Any “protected trees”, (as defined in SMC 19.94) approved for removal, shall be replaced with a specimen tree of at least 36-inch box size.
 - d) Ground cover shall be planted so as to ensure full coverage eighteen months after installation.
 - e) Backflow device and other appurtenances are to include screening and covers as approved by the Director of Community Development. This includes all devices (irrigation, DCDA, etc.) located in the front yard landscape areas. Covers should be black, metal mesh with rounded top covers (e.g. “mailbox style”).
 - f) As shown on the approved plans, decorative pervious paving shall be used on the drive aisle and on the pedestrian path connecting to Old San Francisco Road.
 - g) Staff shall work with the developer to ensure appropriate placement and selection of native, large species trees [COA] [PLANNING]

BP-10. FENCE AGREEMENT:

A signed agreement with the neighboring Pebble Creek Condos homeowner’s association detailing the final design of the shared fence shall be provided. [SDR] [PLANNING]

BP-11. PRE-APPROVED WATER-EFFICIENT LANDSCAPE PLANS:

The developer must install all landscaping and irrigation within the common lots and the private usable open space areas per approved site water-efficient landscape plans, and must submit a Landscaping Certificate of Completion, Irrigation Audit Report and Irrigation Schedule, and Landscaping Maintenance Schedule. Any future changes to the approved landscaped areas or planting changes that may affect the approved landscaping water budget calculations are subject to approval by the Director of Community Development. [COA] [PLANNING]

BP-12. LANDSCAPE MAINTENANCE PLAN:

Prepare a landscape maintenance plan subject to review and approval by the Director of Community Development prior to issuance of building permit. [COA] [PLANNING]

BP-13. TREE PROTECTION PLAN:

Prior to issuance of a Demolition Permit, a Grading Permit or a Building Permit, whichever occurs first, obtain approval of a tree protection plan from the Director of Community Development. Two copies are required to be submitted for review. The tree protection plan shall include

measures noted in Title 19 of the Sunnyvale Municipal Code and at a minimum:

- a) An inventory shall be taken of all existing trees on the plan including the valuation of all 'protected trees' by a certified arborist, using the latest version of the "Guide for Plant Appraisal" published by the International Society of Arboriculture (ISA).
- b) All existing (non-orchard) trees on the plans, showing size and varieties, and clearly specify which are to be retained.
- c) Provide fencing around the drip line of the trees that are to be saved and ensure that no construction debris or equipment is stored within the fenced area during the course of demolition and construction.
- d) The tree protection plan shall be installed prior to issuance of any Building or Grading Permits, subject to the on-site inspection and approval by the City Arborist and shall be maintained in place during the duration of construction and shall be added to any subsequent building permit plans. [COA] [PLANNING/CITY ARBORIST]

BP-14. STORMWATER MANAGEMENT CALCULATIONS:

Submit two copies of the City of Sunnyvale Impervious Surface Calculation worksheet prior to issuance of a Building Permit. [COA] [PLANNING]

BP-15. STORMWATER MANAGEMENT PLAN:

Submit two copies of a Stormwater Management Plan subject to review and approval by Director of Community Development, pursuant to SMC 12.60, prior to issuance of building permit. The Stormwater Management Plan shall include as updated Stormwater Management Data Form. [COA] [PLANNING/ENVIRONMENTAL SERVICES]

BP-16. STORMWATER MANAGEMENT PLAN THIRD-PARTY CERTIFICATION:

Third-party certification of the Stormwater Management Plan is required per the following guidance: City of Sunnyvale – Stormwater Quality BMP Applicant Guidance Manual for New and Redevelopment Projects – Addendum: Section 3.1.2 Certification of Design Criteria Third-Party Certification of Stormwater Management Plan Requirements. The third-party certification shall be provided prior to building permit issuance. [SDR] [PLANNING/ENVIRONMENTAL SERVICES]

BP-17. BEST MANAGEMENT PRACTICES - STORMWATER:

The project shall comply with the following source control measures as outlined in the BMP Guidance Manual and SMC 12.60.220. Best

management practices shall be identified on the building permit set of plans and shall be subject to review and approval by the Director of Public Works:

- a) Storm drain stenciling. The stencil is available from the City's Environmental Division Public Outreach Program, which may be reached by calling (408) 730-7738.
- b) Landscaping that minimizes irrigation and runoff, promotes surface infiltration where possible, minimizes the use of pesticides and fertilizers, and incorporates appropriate sustainable landscaping practices and programs such as Bay-Friendly Landscaping.
- c) Appropriate covers, drains, and storage precautions for outdoor material storage areas, loading docks, repair/maintenance bays, and fueling areas.
- d) Covered trash, food waste, and compactor enclosures.
- e) Plumbing of the following discharges to the sanitary sewer, subject to the local sanitary sewer agency's authority and standards:
 - i) Discharges from indoor floor mat/equipment/hood filter wash racks or covered outdoor wash racks for restaurants.
 - ii) Dumpster drips from covered trash and food compactor enclosures.
 - iii) Discharges from outdoor covered wash areas for vehicles, equipment, and accessories.
 - iv) Swimming pool water, spa/hot tub, water feature and fountain discharges if discharge to onsite vegetated areas is not a feasible option.
 - v) Fire sprinkler test water, if discharge to onsite vegetated areas is not a feasible option. [SDR] [PLANNING]

BP-18. CITY STREET TREES (SUBDIVISION):

At the expense of the subdivider, City staff shall install required street trees of a species determined by the Public Works Department. Obtain approval of a detailed landscape and irrigation plan from the Director of Community Development (SMC 19.37) prior to issuance of a Building Permit. [SDR] [PLANNING/PUBLIC WORKS]

BP-19. EXTERIOR LIGHTING PLAN:

Prior to issuance of a Building Permit submit an exterior lighting plan, including fixture and pole designs, for review and approval by the Director of Community Development. Driveway and parking area lights shall include the following:

- a) Sodium vapor (or illumination with an equivalent energy savings).

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- b) Pole heights to be uniform and compatible with the areas, and shall not exceed 8 feet.
 - c) Provide photocells for on/off control of all security and area lights.
 - d) All exterior security lights shall be equipped with vandal resistant covers.
 - e) Wall packs shall not extend above the roof of the building.
 - f) Lights shall have shields to prevent glare onto adjacent residential properties. [COA] [PLANNING]

BP-20. PHOTOMETRIC PLAN:

Prior to issuance of a Building Permit submit a contour photometric plan for approval by the Director of Community Development. The plan shall meet the specifications noted in the Standard Development Requirements. [COA] [PLANNING]

BP-21. LIGHTING SPACING:

Installation of lights at a minimum of 50 feet intervals along all private streets. [COA] [PLANNING]

BP-22. PARKING MANAGEMENT PLAN (RESIDENTIAL MULTI-FAMILY):

A Parking Management Plan is subject to review and approval by the Director of Community Development prior to issuance of a building permit. The Parking Management Plan shall include the following:

- a) A clear definition of “guest” as proposed by the property manager/homeowner’s association and subject to review and approval by the Director of Community Development.
- b) The property manager/homeowner’s association may specify that 25% to 75% of unassigned spaces be reserved for guest use.
- c) Clearly indicate that the property manager/homeowner’s association shall not rent unassigned spaces, except that a nominal fee may be charged for parking management.
- d) Tenants shall use their assigned parking spaces prior to using unassigned parking spaces.
- e) Prohibit tenants from parking RV’s, trailers, or boats in assigned spaces.
- f) Notify potential residents that number of parking spaces provided for each unit on-site as per the approved plans. [PLANNING] [COA]

BP-23. GREEN BUILDING:

The plans submitted for building permits shall demonstrate the project achieves a minimum of 80 points on the Green Point Rated checklist, or the minimum points required effective at the time of building permit

submittal. The project plans shall be accompanied with a letter from the project's Green Point Rater/LEED AP verifying the project is designed to achieve the required points. [COA] [PLANNING] [BUILDING]

BP-24. CONSTRUCTION MANAGEMENT PLAN:

The project applicant shall implement a Construction Management Plan (CMP) to minimize impacts of construction on surrounding residential uses to the extent possible. The CMP shall be subject to review and approval by the Director of Community Development prior to issuance of a demolition permit, grading permit, or building permit. The CMP shall identify measures to minimize the impacts of construction including the following:

- a) Measures to control noise by limiting construction hours to those allowed by the SMC, avoiding sensitive early morning and evening hours, notifying residents prior to major construction activities, and appropriately scheduling use of noise-generating equipment.
- b) Use 'quiet' models of air compressors and other stationary noise sources where such technology exists.
- c) Equip all internal combustion engine-driven equipment with mufflers, which are in good condition and appropriate for the equipment.
- d) Locate all stationary noise-generating equipment, such as air compressors and portable power generators, as far away as possible from residences or other noise-sensitive land uses.
- e) All construction related materials, equipment, and construction workers parking need to be managed on-site and not located in the public right-of-ways or public easements.
- f) Locate staging areas and construction material areas as far away as possible from residences or noise-sensitive land uses.
- g) Route all construction traffic to and from the project site via designated truck routes where possible. Prohibit construction-related heavy truck traffic in residential areas where feasible. Obtain approval of proposed construction vehicle truck routes from the Department of Public Works.
- h) Manage construction parking so that neighbors are not impacted by construction vehicles. When the site permits, all construction parking shall be on-site and not on the public streets.
- i) Prohibit unnecessary idling of internal combustion engine-driven equipment and vehicles.
- j) Notify all adjacent business, residents, and noise-sensitive land uses of the construction schedule in writing. Notify nearby

residences of significant upcoming construction activities at appropriate stages in the project using mailing or door hangers.

- k) Designate a “disturbance coordinator” who would be responsible for responding to any local complaints about construction noise. The disturbance coordinator will determine the cause of the noise complaint and will require that reasonable measures warranted to correct the problem be implemented. Conspicuously post a telephone number for the disturbance coordinator at the construction site and include it in the notice sent to neighbors regarding the construction schedule. [COA] [PLANNING]

BP-25. DEMOLITION/CONSTRUCTION/RECYCLING WASTE REPORT FORM: To mitigate the impacts of large projects on local waste disposal and recycling levels, demolition waste weights/volumes, construction weights/volumes, and recycling weights/volumes are to be reported to the City, per City’s “waste & recycling reporting form” (electronic copy available) or a similar chart approved by the City. As part of the project’s construction specifications, the developer shall track the type, quantity, and disposition of materials generated, and forward a complete report to the Department of Environmental Services, Solid Waste Division both periodically and at project completion. [COA] [ENVIRONMENTAL SERVICES]

BP-26. SOLID WASTE DISPOSAL AND RECYCLING DESIGN PLAN:
A detailed solid waste disposal and recycling design plan shall be submitted for review and approval by the Director of Community Development prior to issuance of building permit. The solid waste disposal plan and building permit plans shall demonstrate compliance with current City requirements and guidelines for residential/multi-family projects. [COA] [PLANNING/ENVIRONMENTAL SERVICES]

BP-27. UNDERGROUND UTILITIES:
All utilities shall be undergrounded per Sunnyvale Municipal Code Chapter 19.38.095. [COA] [PLANNING/PUBLIC WORKS]

BP-28. HISTORIC AND CULTURAL RESOURCES:
Final construction drawings shall incorporate all mitigation measures related to historic and cultural resources as set forth under “Mitigation Measures” in the approved environmental document and as noted below. [COA] [PLANNING]

MITIGATION MEASURE - HISTORIC AND CULTURAL RESOURCES
WHAT:

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1. A qualified archaeologist shall conduct further archival and field study to identify cultural resources. Field study may include, but is not limited to, pedestrian survey, hand auger sampling, shovel test units, or geoarchaeological analyses as well as other common methods used to identify the presence of archaeological resources. Please refer to the list of consultants who meet the Secretary of Interior's Standards at <http://www.chrisinfo.org>.
 2. If archaeological resources are encountered during construction, work shall be temporarily halted in the vicinity of the discovered materials and workers shall not alter the materials and their context until a qualified professional archaeologist has evaluated the situation and provided appropriate recommendations. Project personnel shall not collect cultural resources. Native American resources include chert or obsidian flakes, projectile points, mortars, and pestles; and dark friable soil containing shell and bone dietary debris, heat-affected rock, or human burials. Historic-period resources include stone or adobe foundations or walls; structures and remains with square nails; and refuse deposits or bottle dumps, often located in old wells or privies.
 3. Any identified cultural resources shall be recorded on DPR 523 historic resource recordation forms.

WHEN: These mitigations shall be incorporated into conditions of approval for the Special Development Permit (SDP) prior to its final approval by the City Council. The conditions will become valid when the SDP is approved and prior to building permit issuance.

WHO: The project applicant or property owner shall be solely responsible for implementation and maintenance of these mitigation measures.

HOW: The conditions of approval will require these mitigation measures to be incorporated into the construction plans.

BP 29. HAZARDS AND HAZARDOUS MATERIALS.

Final construction drawings shall incorporate all mitigation measures related to hazards and hazardous materials as set forth under "Mitigation Measures" in the approved environmental document and as noted below. [COA] [PLANNING]

MITIGATION MEASURE - HAZARDS AND HAZARDOUS MATERIALS

WHAT:

1. Hazardous building materials surveys shall be conducted by a qualified and licensed professional for all structures. All loose

and peeling lead-based paint and asbestos-containing material shall be abated by certified contractor(s) in accordance with local, state, and federal requirements. All other hazardous materials shall be removed from buildings prior to demolition in accordance with California Department of Industrial Relations, Division of Occupational Safety and Health regulations. The completion of the abatement activities shall be documented by a qualified environmental professional(s) and submitted to the City for review with applications for issuance of construction and demolition permits.

WHEN: These mitigations shall be incorporated into conditions of approval for the Special Development Permit (SDP) prior to its final approval by the City Council. The conditions will become valid when the SDP is approved and prior to building permit issuance.

WHO: The project applicant or property owner shall be solely responsible for implementation and maintenance of these mitigation measures.

HOW: The conditions of approval will require these mitigation measures to be incorporated into the construction plans.

EP: THE FOLLOWING CONDITIONS SHALL BE ADDRESSED AS PART OF AN ENCROACHMENT PERMIT APPLICATION.

EP-1 COMPLETE OFF-SITE IMPROVEMENT PLAN SET:

A complete plan check set applicable to the project, which may include street improvement plans, streetscape plans, streetlight plans, photometric analysis, signing/stripping plans, erosion control plans, traffic signal plans and traffic control plans shall be submitted as part of the first off-site improvement plans, including on-site and off-site engineering cost estimate and the initial Engineering and Inspection plan review fee. Joint trench plans may be submitted at a later date. No partial sets are allowed unless otherwise approved by the Department of Public Works. Civil Sheets C-1 to C-3 dated 1/10/18 is subject to change during plan check process. See Improvement Plan Checklist and Improvement Plan Submittal Checklist at the following 2 links:

<https://sunnyvale.ca.gov/civicax/filebank/blobdload.aspx?BlobID=24002>

<https://sunnyvale.ca.gov/civicax/filebank/blobdload.aspx?BlobID=23625> [COA] [PUBLIC WORKS]

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- EP-2. **UPGRADE OF EXISTING PUBLIC IMPROVEMENTS:**
As part of the off-site improvement plan review and approval, any existing public improvements to be re-used by the project, which are not in accordance with current City standards and are not specifically identified in the herein project conditions (such as backflow preventers, sign posts, etc.), shall be upgraded to current City standards and as required by the Department of Public Works. [COA] [PUBLIC WORKS]
- EP-3. **BENCHMARKS:**
The improvement plans shall be prepared by using City's latest benchmarks (NAVD88) available on City's website <https://sunnyvale.ca.gov/civicax/filebank/blobdload.aspx?BlobID=23803> Plans based on NAVD29 will not be accepted. [COA] [PUBLIC WORKS]
- EP-4. **UTILITY CONNECTION:**
This project requires connection to all City utilities or private utilities operating under a City or State franchise which provide adequate levels of service. [COA] [PUBLIC WORKS]
- EP-5 **UTILITY CONNECTION TO THE MAIN:**
All sanitary sewer laterals connecting to the existing main line shall be at a new sanitary sewer manhole. All storm drain laterals connecting to the main shall be at a new storm drain manhole, except where a pipe to pipe connection is permitted if the mainline is 36" or larger, or a junction structure is permitted where the point of connection is within close vicinity of an existing down-stream manhole. Pursuant to City design standards, any new and retrofitted manholes require Sewpercoat, Mainstay or Sancon calcium aluminate cementitious mortar coating of the interior. [SDR] [PUBLIC WORKS]
- EP-6. **POTHOLING OF EXISTING DRY UTILITIES:**
Concurrent with the initial submittal of off-site improvement plans, obtain an encroachment permit for potholing purposes to locate existing dry utilities. Use pothole information to identify possible conflict between the proposed location of City trees and existing utilities, proposed joint trench, and proposed connection of gravity utilities. Potholing is to take place in a timely manner so that this does not hold up the review of the improvement plans. [COA] [PUBLIC WORKS]
- EP-7. **EXISTING UTILITY ABANDONMENT/RELOCATION:**
Developer is responsible for research on all existing utility lines to ensure that there are no conflicts with the project. All existing utility lines (public or private) and/or their appurtenances not serving the project and/or have conflicts with the project, shall be capped,

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- abandoned, removed, relocated and/or disposed of to the satisfaction of the City. Existing public facilities within the street right-of-way shall be abandoned per City's Abandonment Notes and procedures, including abandonment by other utility owners. [COA] [PUBLIC WORKS]
- EP-8. **MODIFICATIONS TO EXISTING PUBLIC UTILITIES:**
Developer is required to pay for all changes or modifications to existing City utilities, streets and other public utilities within or adjacent to the project site, including but not limited to utility facilities/conduits/vaults relocation due to grade change in the sidewalk area, caused by the development. [COA] [PUBLIC WORKS]
- EP-9. **DRY UTILITIES:**
Submit dry utility plans and/or joint trench plans (PG&E, telephone, cable TV, fiber optic, etc.) to the Public Works Department for review and approval prior to the issuance of any permits for utility work within any public right-of-way or public utility easements. Separate encroachment permits shall be required for various dry utility construction. [SDR] [PUBLIC WORKS]
- EP-10. **WET UTILITIES:**
All wet utilities (water, sanitary sewer, storm drain) on private property shall be privately owned and maintained. The fire and domestic water systems shall be privately owned and maintained beyond the meter. [COA] [PUBLIC WORKS]
- EP-11. **RE-USE OF EXISTING CITY UTILITY SERVICE LINES:**
The re-use of existing City water service lines is not allowed. Re-use of existing City sanitary sewer and storm drain service lines and appurtenances is subject to City's review and approval. Developer's contractor shall expose the existing facilities during construction for City's evaluation or provide video footage of the existing pipe condition. Developer's contractor shall replace any deficient facilities as deemed necessary by the Department of Public Works. Civil Sheets C-1 to C-3 dated 1/10/18 is subject to change during plan check process. [COA] [PUBLIC WORKS]
- EP-12. **PUBLIC FIRE HYDRANTS:**
New fire hydrant barrel with current City standard Clow-Rich 75 per current City standard. New fire hydrant locations shall be per current City standard detail 2B and 2B-2. Public fire hydrant shall be maintained free and clear of all trees, vines, shrubs, bushes, ivy, etc. for a minimum of three feet. [COA] [PUBLIC WORKS/PUBLIC SAFETY-FIRE PROTECTION]

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- EP-13. **WATER METER:**
Install new radio-read domestic water meter(s) and RPBFP(s) at each point of connection to the water main. For water meter size two (2) inch or larger, provide meter sizing calculations to the Department of Public Works for approve of meter size. Civil Sheets C-1 to C-3 dated 1/10/18 is subject to change during plan check process. [COA] [PUBLIC WORKS]
- EP-14. **IRRIGATION SERVICE LINE AND BACKFLOW PREVENTORS:**
Install a separate irrigation water service line (separate from the domestic/fire water service line) with a water meter and backflow prevention device.
- All landscape and irrigation systems, located in the public park strip areas shall be connected to the water system metered to the property owner. Install new reduced pressure backflow prevention devices on the discharge side of irrigation line on private property. Install backflow preventer enclosure where applicable. Backflows must adhere to City's Cross-Connection Program. Backflow inspection permit and tags are required for all backflow devices. [COA] [PUBLIC WORKS]
- EP-15. **SEWER MANHOLE:**
Install new sewer manhole at the street right-of-way lines for all existing and proposed sanitary sewer laterals to be used for the project. Sheet C-3 Utility Plan dated 1/10/2018 [SDR] [PUBLIC WORKS]
- EP-16. **SANITARY SEWER VIDEO:**
The contractor shall make a video copy of the interior of the new sanitary sewer lateral installed prior to it is put into service. [COA] [PUBLIC WORKS]
- EP-17. **SANITARY SEWER AND STORM DRAIN TRIBUTARY PATTERN:**
This project is required to follow the existing sanitary sewer and storm drain tributary pattern. Any deviations would require additional analysis and subject to approval by the Public Works Department as part of the off-site improvement plan review process. This project shall not cause any negative impact on the drainage pattern for adjacent properties. [COA] [PUBLIC WORKS]
- EP-18. **CATCH BASIN TRASH CAPTURE DEVICES:**
Pursuant to SMC 12.60.130, install full trash capture devices on the project site, prior to connecting to the City's storm drain collection system, the developer shall be responsible for perpetual maintenance of those trash capture devices. All storm drain inlet facilities located in the public right-of-way shall be stenciled and/or have a badge that read "NO DUMPING" as supplied by the Environmental Services

Department.[COA] [PLANNING/ENVIRONMENTAL SERVICES/PUBLIC WORKS]

EP-19. UTILITY METER/VAULT:

No existing or new utility meters or vaults shall be located within the new driveway approach. All existing or new utility vaults serving the project site shall be located on-site and not within the public utility easement, if any. [COA] [PUBLIC WORKS]

EP-20. DRIVEWAY APPROACHES:

Remove existing driveway approaches and install new driveway approaches along the project frontage to comply with Americans with Disabilities Act (ADA) requirements and City standard details and specifications. All unused driveway approaches shall be replaced with new curbs, gutters, and sidewalks per current City standards. [SDR] [PUBLIC WORKS]

EP-21. STREETScape IMPROVEMENTS:

Along project frontage on Old San Francisco Road, remove existing concrete curb, gutter and 5' sidewalk and install new concrete curb and 2' gutter and 6' sidewalk per current City standards. [SDR] [PUBLIC WORKS]

EP-22. ROOT BARRIER:

Install a continuous root barrier along new sidewalk adjacent to City trees per City standard details and specifications. [SDR] [PUBLIC WORKS]

EP-23. DECORATIVE PAVEMENT:

Any and all proposed decorative pavement and vertical curb pertaining to on-site development shall not be located within the City right-of-way. [COA] [PUBLIC WORKS]

EP-24. SIGNING AND STRIPING PLANS:

Submit a signing and striping plan in accordance with the latest edition of the CA MUTCD to City for review and approval by the Public Works Department. [COA] [PUBLIC WORKS]

EP-25. TRAFFIC CONTROL PLAN:

Submit a traffic control plan with the off-site improvement plans for review and approval. Per the City's Temporary Traffic Control Checklist, the traffic control plan shall include a summary of the traffic control types, dates, times and blocks affected. All construction related materials, equipment, and construction workers parking need to be stored on-site and the public streets need to be kept free and clear of construction debris. [COA] [PUBLIC WORKS]

EP-26. SLURRY SEAL:

Developer shall be responsible to install Type II slurry seal on Old San Francisco from lip of gutter to lip of gutter along project frontage to the median turn lane or as determined by the Department of Public Works. [COA] [PUBLIC WORKS]

EP-27. CITY STREET TREES:

The developer shall install required street trees in proposed within the public right-of-way along the project frontage as follows: Quercus Agrifolia (California Coast Live Oak. Street trees and frontage landscaping shall be included in the detailed landscape and irrigation plan subject to review and approval by the Department of Public Works prior to issuance of encroachment permit. New street trees shall be 24-inch box size or 15 gallon size spaced approximately 35 feet apart. No street trees are to be planted within 10' of a sanitary sewer lateral. Sheets C1-1 to C-3 dated 1/10/18 is subject to change during plan check process. [SDR] [PUBLIC WORKS]

EP-28. PROTECTION OF EXISTING TREES:

No utility trench shall be allowed within 15' radius of an existing mature tree. Boring, air spade or other excavation method as approved by the City Arborist shall be considered to protect existing mature tree. Consult with the City Arborist prior to adjusting locations of utility lines. [SDR] [PUBLIC WORKS]

EP-29. DAMAGE TO EXISTING PUBLIC IMPROVEMENTS:

Developer shall be responsible to rectify any damage to the existing public improvements fronting and adjacent to the project site as a result of project construction, to City's satisfaction by the Public Works Department. All existing traffic detector loops and conduits shall be protected in place during construction. Any damaged detector loops shall be replaced within 7 days at the expense of the developer. [COA] [PUBLIC WORKS]

EP-30. RECORD DRAWINGS:

Stamped and signed hard copy record drawings of the off-site improvements (including off-site street, sewer, water, storm drain and landscaping plans) shall be submitted to the City prior to encroachment permit sign-off. In addition, streetlight record drawings shall be in AutoCAD format. Developer shall pay the record drawing fee. [COA] [PUBLIC WORKS] (SMC 13.08.160(a))

TM: THE FOLLOWING CONDITIONS SHALL BE MET PRIOR TO THE APPROVAL OF THE FINAL MAP OR PARCEL MAP.
--

TM-1. CONDITIONS, COVENANTS AND RESTRICTIONS (CC&RS) (DRAFT REVIEW):

Any proposed deeds, covenants, restrictions and by-laws relating to the subdivision are subject to review and approval by the Director of Community Development and the City Attorney. Four (4) sets of the CC&Rs including all information required below shall be submitted to the Engineering Division of the Public Works Department for routing. In addition to requirements as may be specified elsewhere, the CC&R's shall include the following provisions:

- a) Membership in and support of an association controlling and maintaining all common facilities shall be mandatory for all property owners within the development.
- b) The owners association shall obtain approval from the Director of Community Development prior to any modification of the CC&R's pertaining to or specifying the City.
- c) The developer shall maintain all private utilities and landscaping for a period of three (3) years following installation of such improvements or until the improvements are transferred to a owners association, following sale of at least 75% of the units, whichever comes first.
- d) The Standard Development Requirements and Conditions of Approval included as part of the approved Planning Application, Permit #**2018-7048**, and associated map shall be incorporated into the CC&Rs as an exhibit or attachment. The included map shall clearly indicate all public/private easements as disclosure for property owners. The CC&Rs shall include a list of all attachments and/or exhibits.
- e) The CC&Rs shall contain language for Best Management Practices "Agreement to Maintain" pursuant to Sunnyvale Municipal Code 12.60.200.
- f) The CC&Rs shall contain the following provisions:
 - i) The owners association shall maintain parkstrip landscaping in perpetuity along the public street fronting the project site.
 - ii) Property owners are prohibited from modifying drainage facilities and/or flow patterns unless reviewed and approval granted from the Public Works Department.
- g) The CC&Rs shall contain the following language:
 - i) "Right to Remedy Failure to Maintain Common Area. In the event that there is a failure to maintain the Common Area so that owners, lessees, and their guests suffer, or will suffer, substantial diminution in the enjoyment, use, or property

value of their Project, thereby impairing the health, safety and welfare of the residents in the Project, the City, by and through its duly authorized officers and employees, will have the right to enter upon the subject Property, and to commence and complete such work as is necessary to maintain said Common Area. The City will enter and repair only if, after giving the Association and Owners written notice of the failure to maintain the Common Area, they do not commence correction of such conditions in no more than thirty (30) days from the giving of the notice and proceed diligently to completion. All expenses incurred by the City shall be paid within thirty (30) days of written demand. Upon a failure to pay within said thirty (30) days, the City will have the right to impose a lien for the proportionate share of such costs against each lot in the Project.

- iii) It is understood that by the provisions hereof, the City is not required to take any affirmative action, and any action undertaken by the City will be that which, in its sole discretion, it deems reasonable to protect the public health, safety and general welfare, and to enforce it and the regulations and ordinances and other laws.
- iv) It is understood that action or inaction by the City, under the provisions hereof, will not constitute a waiver or relinquishment of any of its rights to seek redress for the violation of any of the provisions of these restrictions or any of the rules, regulations and ordinances of the City, or of other laws by way of a suit in law or equity in a court of competent jurisdiction or by other action.
- v) It is further understood that the remedies available to the City by the provision of this section or by reason of any other provisions of law will be cumulative and not exclusive of the maintenance of any other remedy. In this connection, it is understood and agreed that the failure to maintain the Common Area will be deemed to be a public nuisance and the City will have the right to abate said condition, assess the costs thereof, and cause the collection of said assessments to be made on the tax roll in the manner provided by appropriate provisions of the Sunnyvale Municipal Code or any other applicable law.
- vi) No Waiver. No failure of the City of Sunnyvale to enforce any of the covenants or restrictions contained herein will in any event render them ineffective.
- vii) Hold Harmless. Declarant, Owners, and each successor in interest of Declarant and said Owners, hereby agree to save,

defend and hold the City of Sunnyvale harmless from any and all liability for inverse condemnation which may result from, or be based upon, City's approval of the Development of the subject Property." [COA] [PUBLIC WORKS/PLANNING/CITY ATTORNEY]

TM-2: HOA CREATION:

The developer/Owner shall create a Homeowner's Association that comports with the state law requirements for Common Interest Developments. Covenants, conditions and restrictions (CC&Rs) relating to the development are subject to review for consistency with the Conditions of Approval by the City Attorney and Director of Community Development prior to approval of the Final Map. The Conditions of Approval shall be attached as an exhibit to the CC&Rs created for this subdivision. [COA] [PLANNING]

TM-3. HOA TRANSFER:

At the time the homeowners association is transferred from the developer to the individual property owners (typically at election of board members or officers), the developer shall schedule a meeting between the board members or officers, the City of Sunnyvale and the developer to review the Conditions of Approval of the development and other applicable City requirements. [COA] [PLANNING]

TM-4: PUBLIC/PRIVATE STREETS:

All streets, both public and private, shall be shown on the Final Map. The common lot shall be assigned a private street name in accordance with the official Street Name System, as selected by the Community Development Department. Private streets shall be designated as "Terrace". [COA] [PLANNING]

TM-5. FINAL MAP COMPLIANCE WITH VESTING TENTATIVE MAP: The Final Map shall be substantially the same as the vesting tentative map. Any alteration of the vesting tentative map after the vesting tentative map is approved is subject to additional approval by the City and may require a public hearing. [COA] [PLANNING/PUBLIC WORKS]

TM-6. TITLE 18 AND SUBDIVISION MAP ACT:

The submittal, approval and recordation of the Final Map shall be in accordance with the provision of the California Subdivision Map Act and Sunnyvale Municipal Code Title 18 Subdivision requirements. [COA] [PUBLIC WORKS]

TM-7. RESERVATION/ABANDONMENT OF EASEMENTS:

-
- Reservation of new and/or abandonment of existing public/private utility easement(s), ingress/egress easement(s) necessary for the project shall be delineated on the map or recorded concurrently with the map with a separate instrument. (SMC 18.16.010 (c), Subdivision Map Act §66475 and §66499.20-1/2 for public easements) Quitclaim deed is required for abandonment of private easements prior to map recordation. All easements shall be kept open and free from buildings and structures of any kind except those appurtenances associated with the defined easements. [COA] [PUBLIC WORKS]
- TM-8. UTILITY COMPANY APPROVAL:
Obtain map approval letters from the utility companies in regards to any existing or new easements associated with their facilities. [COA] [PUBLIC WORKS]
- TM-9. PUBLIC WORKS DEVELOPMENT FEES:
The developer shall pay all applicable Public Works development fees associated with the project, including but not limited to, utility frontage and/or connection fees and off-site improvement plan check and inspection fees, prior to map recordation or any permit issuance, whichever occurs first. The exact fee amount shall be determined based upon the fee rate at the time of fee payment. [COA] [PUBLIC WORKS]
- TM-10. SUBDIVISION AGREEMENT AND IMPROVEMENT SECURITIES:
The developer shall execute a subdivision agreement and provide improvement securities and/or cash deposit(s) for all proposed public improvements prior to map recordation or any permit issuance, whichever occurs first. Provide an itemized engineer's estimate for all off-site public improvements for the entire project for determination of security amount. [COA] [PUBLIC WORKS]

<p>PF: THE FOLLOWING CONDITIONS SHALL BE ADDRESSED ON THE CONSTRUCTION PLANS AND/OR SHALL BE MET PRIOR TO RELEASE OF UTILITIES OR ISSUANCE OF A CERTIFICATE OF OCCUPANCY.</p>
--

- PF-1. LANDSCAPING AND IRRIGATION:
All landscaping and irrigation as contained in the approved building permit plan shall be installed prior to occupancy. [COA] [PLANNING]
- PF-2. PARKING LOT STRIPING:
All parking lot striping, carpool and compact spaces shall be striped as per the approved plans and Public Works standards. [COA] (PLANNING/ENGINEERING)

PF-3. CONDITIONS, COVENANTS AND RESTRICTIONS (CC&RS) (RECORDATION):

The Developer/Owner shall submit a copy of the recorded CC&Rs and a letter from the Developer/Owner either indicating that the recorded CC&Rs are in conformance with the approved draft CC&Rs or summary of changes shall be provided to the Director of Community Development prior to release if utilities or certificate of occupancy. [COA] [PUBLIC WORKS/PLANNING/CITY ATTORNEY]

PF-4. HOA ESTABLISHMENT:

The developer shall submit to the Planning Division the names, addresses and telephone numbers of the officers of the homeowners association, architectural review committee or similar committee, at the time the organization is granted autonomy. Until such information is supplied, the developer shall remain a Responsible Person for purposes of maintaining all common property. The chairperson, secretary or principal officer of any committee or association shall notify the City of any change in officers and provide the names, addresses and telephone numbers of the new officers within thirty (30) days after the change becomes effective. [COA] [PLANNING]

PF-5. COMPLETION OF PUBLIC IMPROVEMENTS:

Developer shall complete all required public improvements as required and in accordance with City approved plans, prior to any building occupancy. [COA] [PUBLIC WORKS]

DC: THE FOLLOWING CONDITIONS SHALL BE COMPLIED WITH AT ALL TIMES DURING THE CONSTRUCTION PHASE OF THE PROJECT.

DC-1. BLUEPRINT FOR A CLEAN BAY:

The project shall be in compliance with stormwater best management practices for general construction activity until the project is completed and either final occupancy has been granted. [SDR] [PLANNING]

DC-2. TREE PROTECTION:

All tree protection shall be maintained, as indicated in the tree protection plan, until construction has been completed and the installation of landscaping has begun. [COA] [PLANNING]

DC-3. CLIMATE ACTION PLAN – OFF ROAD EQUIPMENT REQUIREMENT:

OR 2.1: Idling times will be minimized either by shutting equipment off when not in use or reducing the maximum idling time to 5 minutes (as

required by the California airborne toxics control measure Title 13, Section 2485 of California Code of Regulations [CCR]), or less. Clear signage will be provided at all access points to remind construction workers of idling restrictions.

OR 2.2: Construction equipment must be maintained per manufacturer's specifications.

OR 2.3: Planning and Building staff will work with project applicants to limit GHG emissions from construction equipment by selecting one of the following measures, at a minimum, as appropriate to the construction project:

- a) Substitute electrified or hybrid equipment for diesel- and gasoline-powered equipment where practical.
- b) Use alternatively fueled construction equipment on-site, where feasible, such as compressed natural gas (CNG), liquefied natural gas (LNG), propane, or biodiesel.
- c) Avoid the use of on-site generators by connecting to grid electricity or utilizing solar-powered equipment.
- d) Limit heavy-duty equipment idling time to a period of 3 minutes or less, exceeding CARB regulation minimum requirements of 5 minutes. [COA] [PLANNING]

DC-4. DUST CONTROL:

At all times, the Bay Area Air Quality Management District's CEQA Guidelines and "Basic Construction Mitigation Measures Recommended for All Proposed Projects", shall be implemented. [COA] [PLANNING]

AT: THE FOLLOWING CONDITIONS SHALL BE COMPLIED WITH AT ALL TIMES THAT THE USE PERMITTED BY THIS PLANNING APPLICATION OCCUPIES THE PREMISES.

AT-1. LANDSCAPE MAINTENANCE:

All landscaping shall be installed in accordance with the approved landscape plan and shall thereafter be maintained in a neat, clean, and healthful condition. Trees shall be allowed to grow to the full genetic height and habit (trees shall not be topped). Trees shall be maintained using standard arboriculture practices. [COA] [PLANNING]

AT-2. PARKING MANAGEMENT:

On-Site parking management shall conform with the approved parking management plan. [COA] [PLANNING]

AT-3. PARKING LOT MAINTENANCE:

The parking lot shall be maintained in accordance with the approved plans and as follows:

- a) Clearly mark all employee, customer, and compact spaces. This shall be specified on the Building Permit plans and completed prior to occupancy.
- b) Maintain all parking lot striping and marking.
- c) Assure that adequate lighting is available in parking lots to keep them safe and desirable for the use.
- d) Require signs to direct vehicles to additional parking spaces on-site, as needed.
- e) Clearly mark all compact spaces as per approved plans. [COA] [PLANNING]

AT-4. OFF-STREET PARKING:

Off-street parking for both residents and guests shall be maintained at all times in accordance with approved plans. [COA] [PLANNING]

AT-5. PARKING LOT MAINTENANCE:

The parking lot shall be maintained as follows:

- a) Garage and carport spaces shall be maintained at all times so as to allow for parking of vehicles.
- b) Clearly mark all assigned, guest, and compact spaces. This shall be specified on the Building Permit plans and completed prior to occupancy.
- c) Maintain all parking lot striping and marking.
- d) Maintain parking lot lighting and exterior lighting to ensure that the parking lot is maintained in a safe and desirable manner for residents and/or patrons. [COA] [PLANNING]

AT-6. RECREATIONAL VEHICLE STORAGE PROHIBITED:

Unenclosed storage of any vehicle intended for recreation purposes, including land conveyances, vessels and aircraft, but excluding attached camper bodies and motor homes not exceeding 18 feet in length, shall be prohibited on the premises. [COA] [PLANNING]

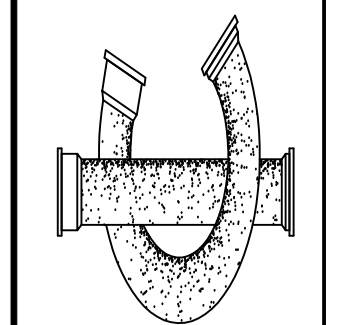
AT-7. HOA REVIEW AND APPROVAL:

In common interest developments, any future applications to the City for physical modifications on commonly owned property shall require

-
- consent of the board of directors of the homeowners association, architectural review committee or similar committee; applications for physical modifications on privately owned property shall require the individual property owner's signature. Individual property owners submitting an application for physical modifications on private property shall comply with any approval processes outlined as such in the conditions, covenants & restrictions (CC&Rs) of their respective development. [COA] [PLANNING]
- AT-8. HOA RESPONSIBILITIES:
The chairperson, secretary or principal officer of any committee or association shall notify the Planning Division and the Neighborhood and Community Resources Division of any change in officers and provide the names, addresses and telephone numbers of the new officers within thirty (30) days after the change becomes effective. [COA] [PLANNING DIVISION/NEIGHBORHOOD AND COMMUNITY RESOURCES DIVISION]
- AT-9. BMP MAINTENANCE:
The project applicant, owner, landlord, or HOA, must properly maintain any structural or treatment control best management practices to be implemented in the project, as described in the approved Stormwater Management Plan and indicated on the approved building permit plans. [SDR] [PLANNING]
- AT-10. BMP RIGHT OF ENTRY:
The project applicant, owner, landlord, or HOA, shall provide access to the extent allowable by law for representatives of city, the local vector control district, and the Regional Water Quality Control Board, strictly for the purposes of verification of proper operation and maintenance for the storm water treatment best management practices contained in the approved Storm Water Management Plan. [SDR] [PLANNING]
- AT-11. SOLID WASTE RECYCLING MANAGEMENT:
Waste and recycling services for residential uses shall be maintained under a master account held by the applicant, owner or landlord. The account holder will be responsible for ensuring adequate services and that all locations, private sidewalks and streets are kept free of litter and stains. Requirements shall be specified in the approved documents and be submitted for approval by the City. [COA] [ENVIRONMENTAL SERVICES]

Revisions	By

INNOVATIVE CONCEPTS
PROFESSIONAL BUILDING DESIGN AND PLANNING
3550 Stevens Creek Blvd, Suite 225
San Jose, CA 95117
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E-Mail: inncp@abcglobal.net



A Major Use Permit/ Special Development Permit for:
George Nejat
669 & 673 Old San Francisco Road
Sunnyvale, CA. 94086

Date	05/08/2018
Scale	
Drawn	
Job	
Sheet	0
Of	Sheets


A SIX UNIT DEVELOPMENT FOR: GEORGE NEJAT



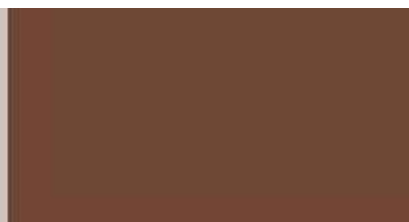
669-673 OLD SAN FRANCISCO ROAD
SUNNYVALE, CA 94086

COLOR PALETTE


- 1




STANDARD WEIGHT
CONCRETE TILE ROOF
BY BORAL ROOFING
STYLE: SAXONY 900 SLATE
COLOR: CHARCOAL BROWN BLEND
- 2




WOOD CAP RAILING
COLOR: SANTANA SOUL
BY KELLY MOORE
- 3




VINYL WINDOWS
BY MILGARD
STYLE MONTECITO SERIES
COLOR: CLASSIC BROWN
- 4




JELD-WEN ENTRY DOOR
STYLE: CRAFTSMAN 3 - LITE PRE-HUNG
COLOR: MESA RED GLOSS FINISH
- 5



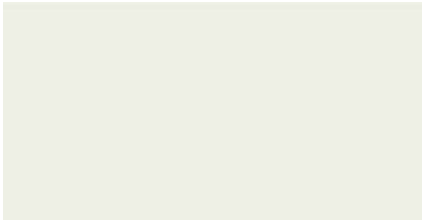
GARAGE DOORS SECTION STEEL
BY CLOPAY
STYLE: NON INSULATED ALMOND GARAGE
DOOR WITH PLAIN WINDOW
COLOR: ALMOND
- 6




GLASS PANEL RAILING BY CRYSTALITE
ALUMINUM WITH BRONZE FINISH
- 7




GALVANIZED STEEL K-STYLE GUTTER
BROWN (AMERIMAX HOME PRODUCT)
BY KELLY MOORE
COLOR: SANTANA SOUL
- 8




STUCCO EXTERIOR
SAND FINISH
COLOR: SWISS COFFEE
BY KELLY MOORE
- 9



SIDING EXTERIOR
SMOOTH FINISH
COLOR: SPANISH SAND
BY KELLY MOORE
- 10



WOOD TRIM EXTERIOR
SMOOTH FINISH
COLOR: SANTANA SOUL
BY KELLY MOORE
- 11



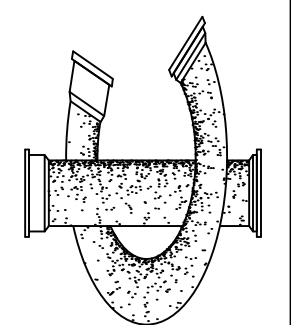
STACKED STONE
BY ELDORADO STONE
STYLE: LIMESTONE
COLOR: SAN MARTIN

A SIX UNIT DEVELOPMENT FOR:
GEORGE NEJAT
669-673 OLD SAN FRANCISCO ROAD
SUNNYVALE, CA 94086



Revisions	By

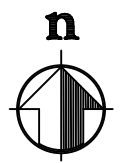
INNOVATIVE CONCEPTS
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A Major Use Permit/ Special Development Permit for:
George Nejat
669 & 673 Old San Francisco Road
Sunnyvale, CA. 94086

Date	05/08/2018
Scale	
Drawn	
Job	
Sheet	

A0
Of Sheets



VICINITY MAP

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- MATERIAL BOARD
- TOPOGRAPHIC MAP
- SITE PLAN
- COVER SHEET / NOTES / DETAILS
- GRADING & DRAINAGE PLAN
- UTILITY PLAN
- STORM WATER & MANAGEMENT PLAN / NOTES
- STORM WATER & MANAGEMENT PLAN / DETAILS
- STORM WATER & MANAGEMENT PLAN / IMPERVIOUS SURFACE
- VESTING TENTATIVE MAP
- FIRE ACCESS PLAN
- PROPOSED FLOOR PLANS
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- ELEVATIONS
- ELEVATIONS
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- SOLAR STUDY
- SOLAR STUDY
- LANDSCAPE PLANS

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- SHEET T-1
- SHEET A0
- SHEET C-1
- SHEET C-2
- SHEET C-3
- SHEET STRM-1
- SHEET STRM-2

PROJECT DESCRIPTION:

BUILDING OCCUPANCY	: R-3/ U
ZONING	: RO
TYPE OF CONSTRUCTION	: V-B
STORIES	: 3
LOT SIZE	: 14,977 SF
SITE COVERAGE	: 5,254 SF = 35%
FLOOR AREA	: 12,190 SF = 81.3%
PRIVATE OPEN SPACE	: 1,736 SF
COMMON OPEN SPACE	: 1,267 SF
TOTAL	: 3,003 SF
LANDSCAPED AREA	: 3,330 SF

- SHEET STRM-3
- SHEET TM-1
- SHEET A1
- SHEET A2
- SHEET A3
- SHEET A4
- SHEET A5
- SHEET A6
- SHEET A7
- SHEET A8
- SHEET LI

BUILDING DATA:

FIRST FLOOR	SECOND FLOOR	GARAGE	GARAGE ENTRANCE	FLOOR AREA (TOTAL)
UNIT 1 811 SF	585 SF	541 SF	53 SF	1,990 SF
UNIT 2 791 SF	704 SF	545 SF	75 SF	2,115 SF
UNIT 3 811 SF	585 SF	541 SF	53 SF	1,990 SF
UNIT 4 811 SF	585 SF	541 SF	53 SF	1,990 SF
UNIT 5 791 SF	704 SF	545 SF	75 SF	2,115 SF
UNIT 6 811 SF	585 SF	541 SF	53 SF	1,990 SF
TOTAL: 4,826 SF	3,748 SF	3,254 SF	362 SF	12,190 SF

AVERAGE FRONT SETBACKS:

BUILDING 1

1ST FLOOR:
21.0 ft
18.0 ft
19.25 ft
24.41ft

TOTAL: 82.66 ft / 4

20.7 ft Average Front 1st Floor Setback

2ND FLOOR:

23.25 ft
25.66 ft
30.25 ft
35.66 ft

TOTAL: 114.8 ft / 4

28.7 ft Average Front 2nd Floor Setback

BUILDING 2

1ST FLOOR:
23.75 ft
20.91 ft
22.25 ft
27.16 ft

TOTAL: 94.07 ft / 4

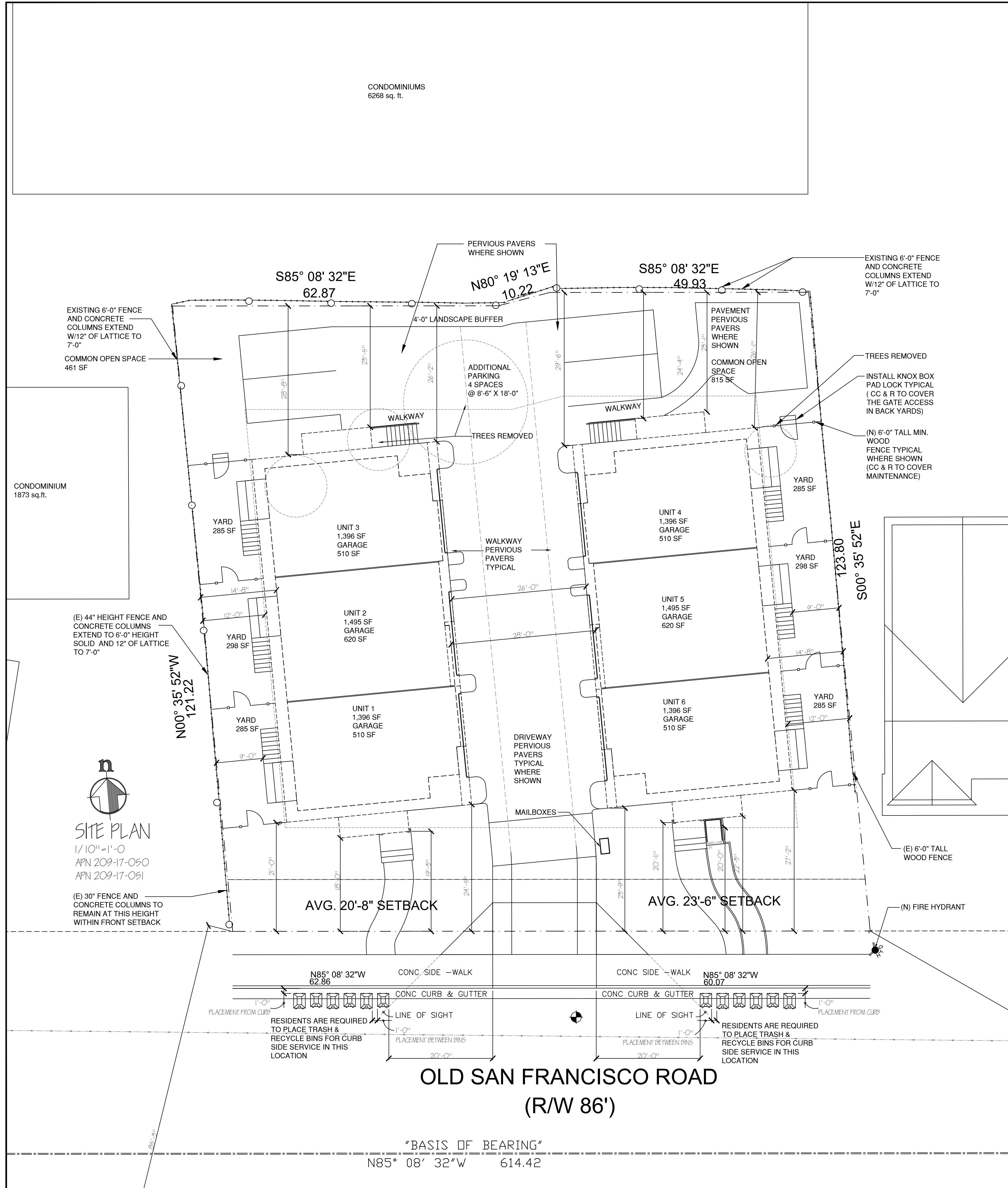
23.5 ft Average Front 1st Floor Setback

2ND FLOOR:

35.08 ft
29.91 ft
26.5 ft
26.91 ft

TOTAL: 118.4 ft / 4

29.6 ft Average Front 2nd Floor Setback





ENGINEERS
CIVIL ENGINEERS

1534 CAROB LANE
LOS ALTOS, CA 94024
TEL: (650) 941-8055
FAX: (650) 941-8755

OWNER/APPLICANT:

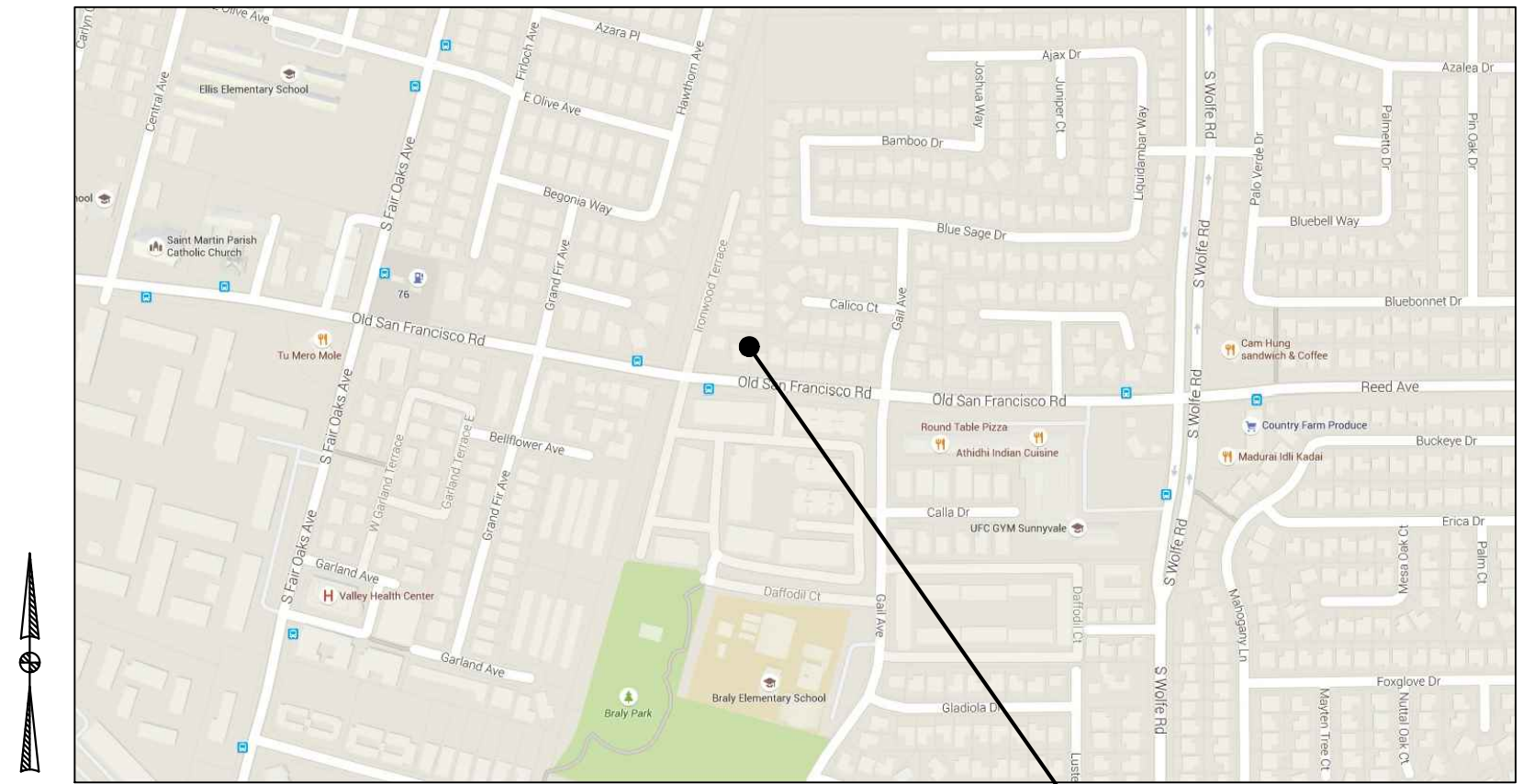
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CIVIL ENGINEERS

GRADING AND DRAINAGE PLANS
SIX (6) NEW TOWN HOUSES SUBDIVISION
APN: 209-17-051 & 050
669 AND 673 OLD SAN FRANCISCO ROAD, SUNNYVALE, CA
COVER SHEET

Revisions:



Date: 7/25/2018
Scale: 1"=10'
Prepared by: S.P.
Checked by: S.R.
Job #: 215001



LOCATION MAP
N.T.S.

SHEET INDEX:

- C-1 COVER SHEET/ NOTES/ DETAILS
- C-2 GRADING AND DRAINAGE PLAN
- C-3 UTILITY PLAN

REFERENCED BENCHMARK:

REFERENCED CITY OF SUNNYVALE B.M# 9, EL: 92.481'.
BRASS DISC IN TOP OF CURB NEXT TO CATCH BASIN AT THE SOUTHWEST CURB RETURN,
INTERSECTION OF WOLFE ROAD AND OLD SAN FRANCISCO ROAD. NAVD88

BASIS OF BEARINGS:

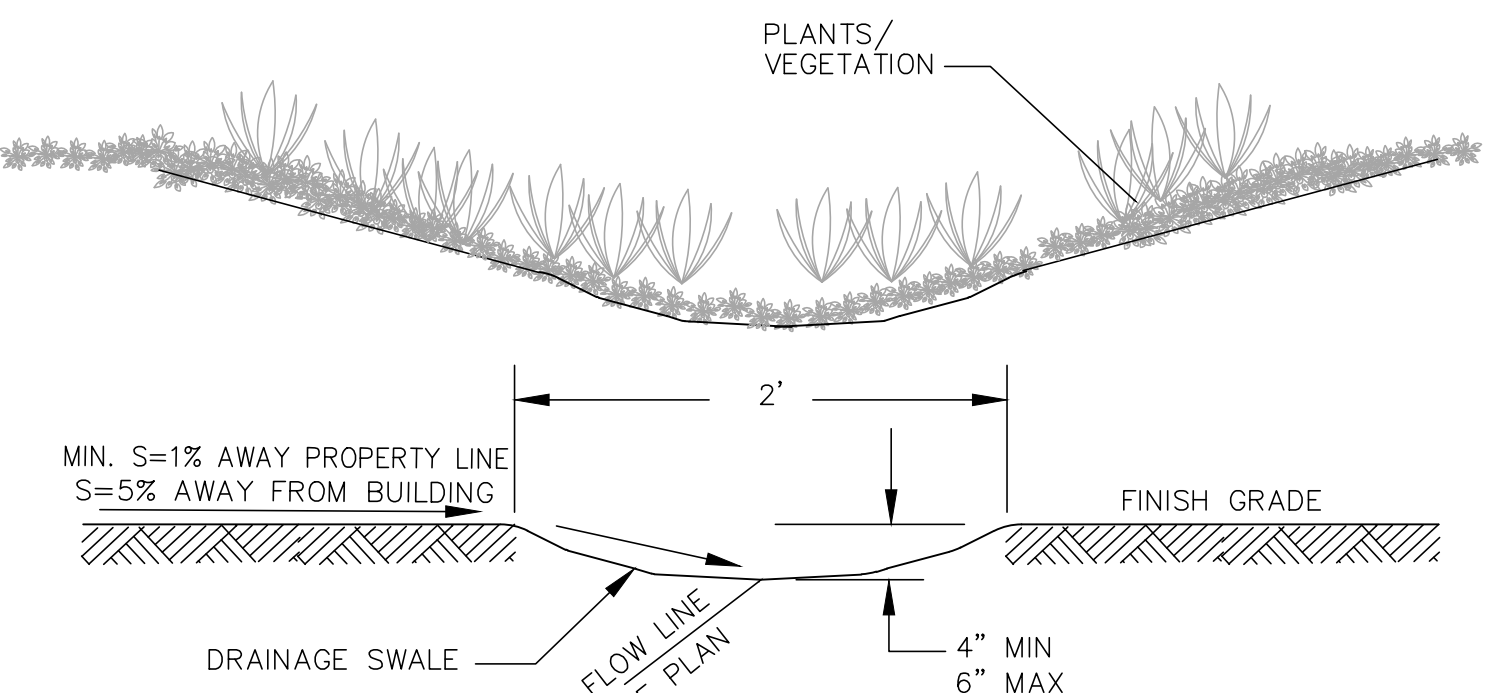
THE BEARING N 74°13'40" W BETWEEN FOUND MONUMENTS ON CALIFORNIA AVE.,
AS SHOWN ON THAT CERTAIN PARCEL MAP, RECORDED IN BOOK 350 OF MAPS
AT PAGE 48, SANTA CLARA COUNTY RECORDS, WAS USED AS THE BASIS
OF BEARINGS SHOWN ON THIS MAP

DRAINAGE NOTES

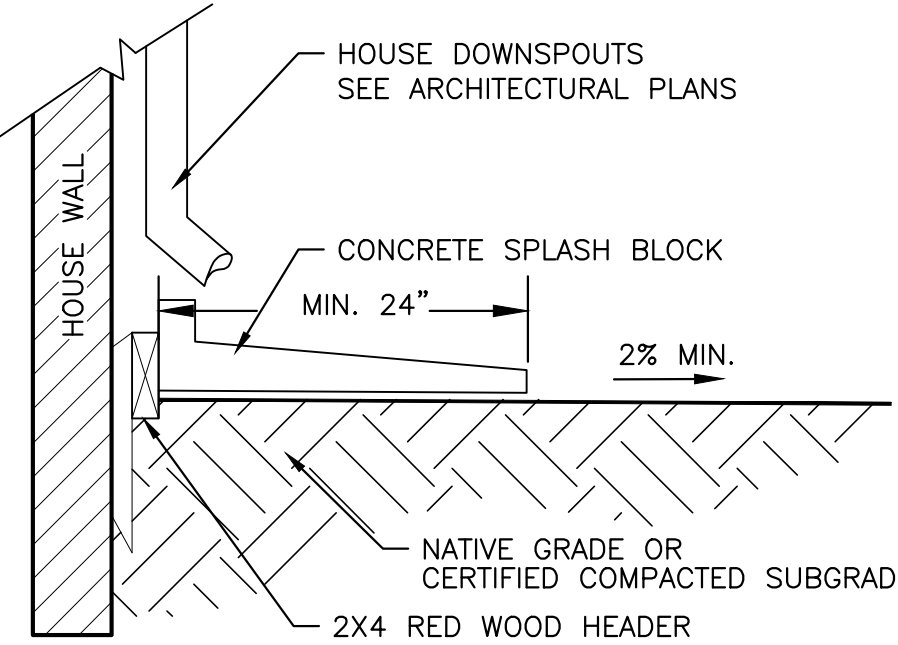
- Surface water shall be directed away from all buildings into drainage swales, gutters, storm drain inlets and drainage systems.
- All roof downspouts shall discharge to concrete splash pads draining away from the foundation. See architectural plans for roof downspout locations.
- On site storm drain lines shall consist of solid PVC-SCH 40 minimum or better. Use PVC SCH80 for pipes running under driveway.
- Storm drain inlets shall be precast concrete, Christy U23 type or equivalent.

GRADING AND DRAINAGE PLANS

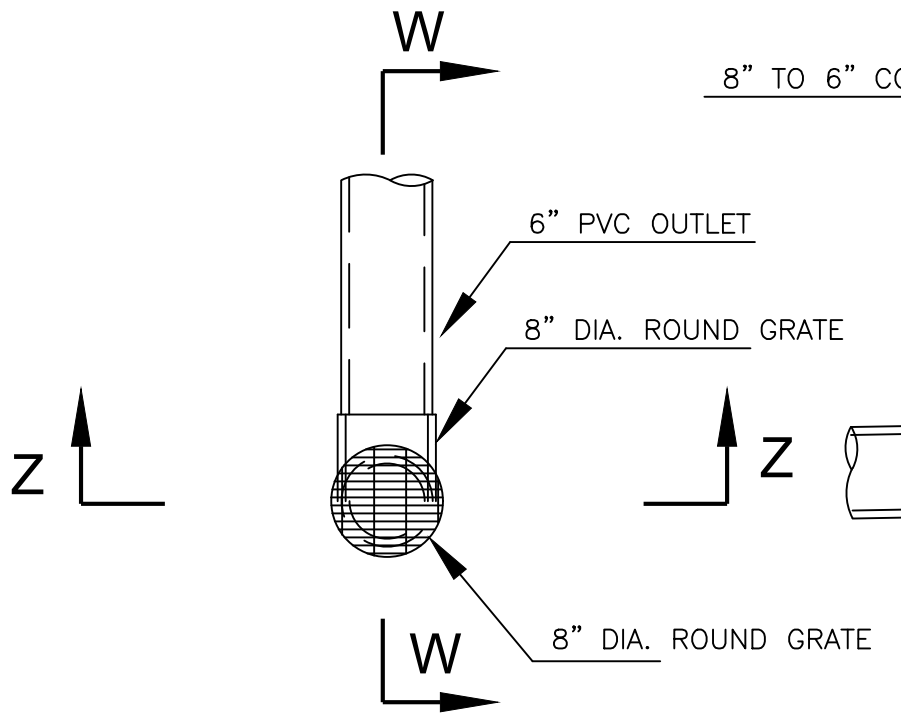
SIX (6) NEW TOWN HOUSES SUBDIVISION
669 AND 673 OLD SAN FRANCISCO ROAD, SUNNYVALE, CA



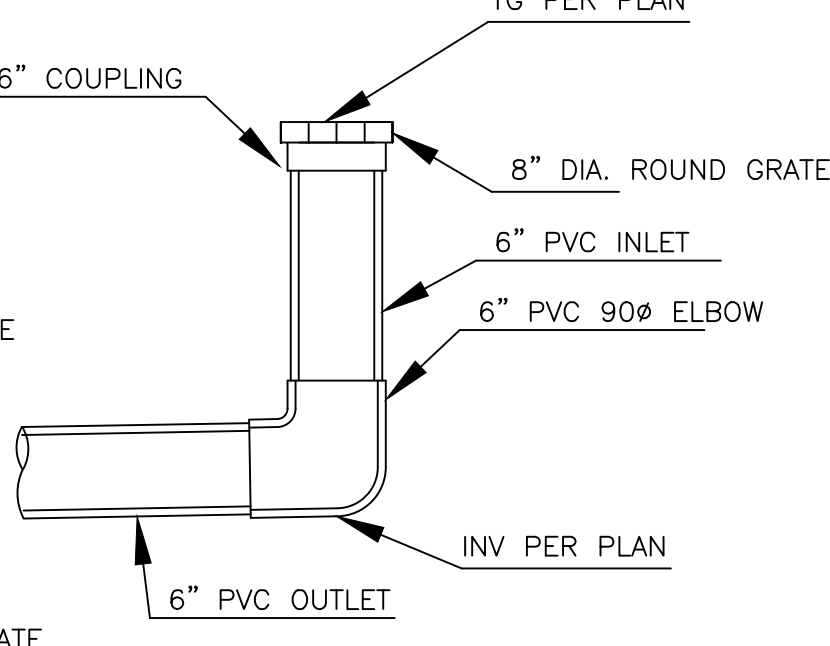
GRASSY SWALE DETAIL
NTS



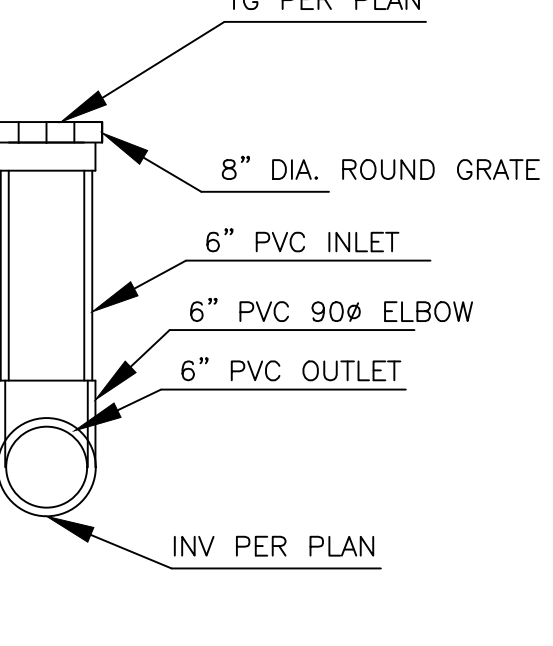
SPLASH BLOCK
NTS



PLAN

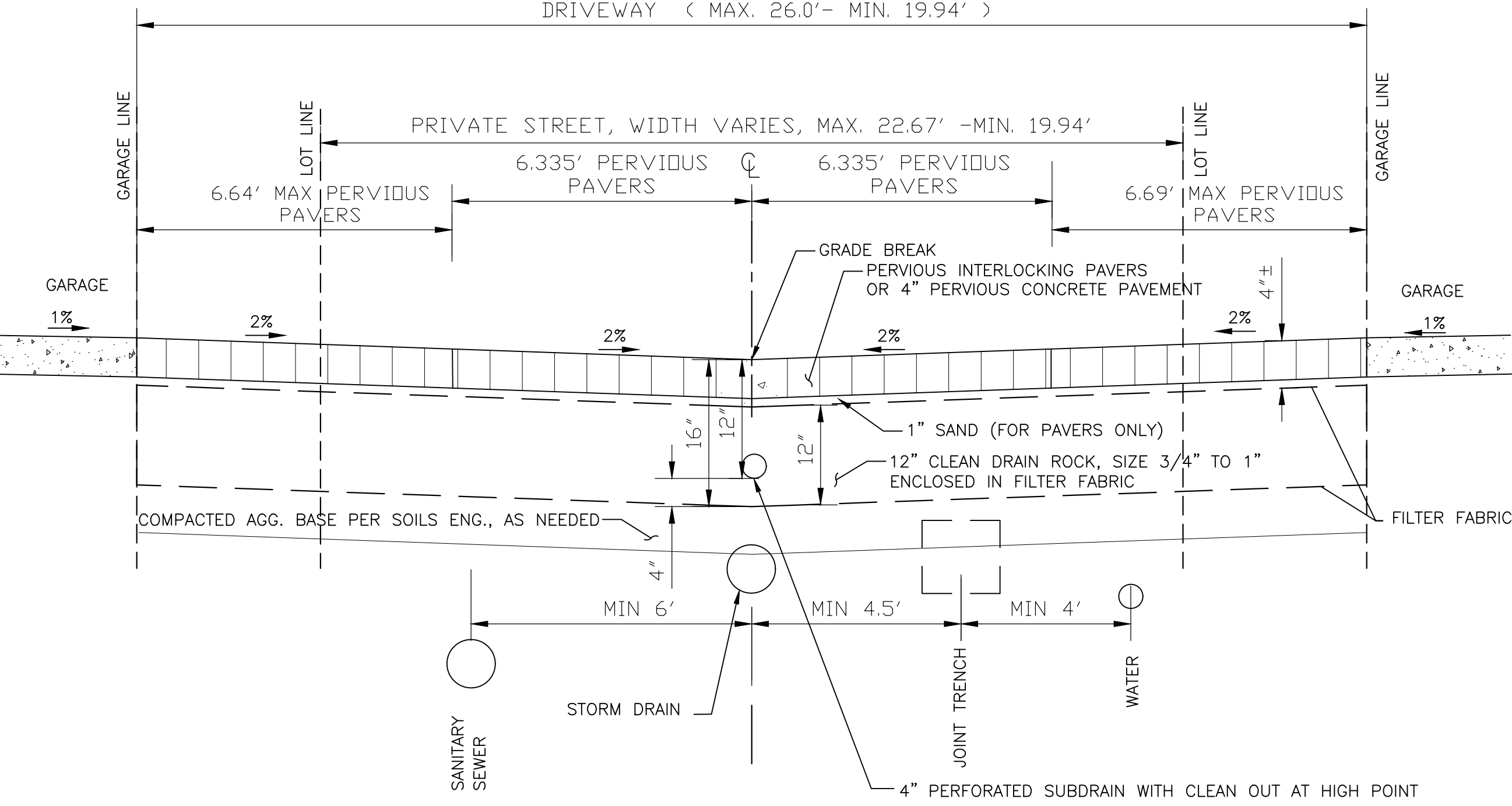


SECTION W-W



SECTION Z-Z

STORM DRAIN AREA DRAIN
NTS



TYPICAL PRIVATE STREET SECTION
NTS

ABBREVIATIONS		
	DESCRIPTION	DESCRIPTION
AB	AGGREGATE BASE	LIP LIP OF GUTTER
AC	ASPHALT CONCRETE	LP LOW POINT
AD	AREA DRAIN	MON MONUMENT
BC	BACK OF CURB	(N) NEW
BFL	BACKFLOW PREVENTOR	OG ORIGINAL GROUND
BW	BOTTOM OF WALL	PB PULL BOX
C&G	CURB AND GUTTER	PG&E PG&E VAULT
E.C/L	CENTERLINE	P/L PROPERTY LINE
CLSW	CENTERLINE SWALE	PP POWER POLE
CO	CLEANOUT	PPP PLASTIC PERFORATED PIPE
CP	CONTROL POINT	PSE PUBLIC SERVICE EASEMENT
DWY	DRIVEWAY	PVC POLYVINYL CHLORIDE
DI	DROP INLET	R/W RIGHT OF WAY
DTL	DETAIL	RCP REINFORCED CONCRETE PIPE
ELCT	ELECTRIC	SD STORM DRAIN
EP	EDGE OF PAVEMENT ELEVATION	SDMH STORM DRAIN MANHOLE
EUC	EUCALYPTUS TREE	STD STANDARD
(E),EX	EXISTING	SS SANITARY SEWER
FF	FINISH FLOOR	SSMH SANITARY SEWER MANHOLE
FG	FINISH GRADE	SW SIDEWALK
FH	FIRE HYDRANT	TC TOP OF CURB
FL	FLOWLINE	TF TOP OF FOUNDATION
FNC	FENCE	TG TOP OF GRATE
FOC	FACE OF CURB	TOS TOP OF SLAB
GB	GRADE BREAK	TP TOP OF PAVEMENT
GUY	GUY WIRE	TW TOP OF WALL
HP	HIGH POINT	(TYP) TYPICAL
DIP	DUCTILE IRON PIPE	VCP VITRIFIED CLAY PIPE
INV	INVERT	WL WHITE LINE STRIPE
JP	JOINT POLE	WLK WALKWAY
JB	JUNCTION BOX (UTILITY)	WM WATER METER
		WV WATER VALVE

LEGEND

EXISTING	PROPOSED	DESCRIPTION
---	---	PROPERTY LINE
---	---	FILL AREA LIMIT
---	---	CUT AREA LIMIT
102	102	CONTOUR
---	---	WATER LINE
SD	SD	STORM DRAIN PIPE (SOLID)
SS	SS	SANITARY SEWER PIPE
SD	SD	SUBDRAIN PIPE (PERFORATED)
OH	OH	OVERHEAD UTILITIES WITH POLE
G	G	GAS LINE
E	E	ELECTRIC LINE (UNDERGROUND)
JT	JT	JOINT TRENCH (UNDERGROUND)
SLV	SLV	STREET LIGHT VAULT
SSCO	SSCO	SANITARY SEWER CLEANOUT
SSCO	SSCO	SANITARY SEWER MANHOLE
SSCO	SSCO	STORM DRAIN MANHOLE
SSCO	SSCO	SURVEY CITY MONUMENT
SSCO	SSCO	ELECTROLIER
SSCO	SSCO	WATER METER
SSCO	SSCO	TREE WITH TRUNK
SSCO	SSCO	6' WOODEN FENCE
SSCO	SSCO	SPOT ELEVATION
SSCO	SSCO	TREE PROTECTION FENCE
SSCO	SSCO	5' TALL CHAIN LINK
SSCO	SSCO	EARTH SWALE
SSCO	SSCO	CONCRETE SWALE
SSCO	SSCO	AREA DRAIN/ INLET
SSCO	SSCO	OVERLAND RELEASE PATH
SSCO	SSCO	GRADE TO DRAIN, 2% MIN. AWAY FROM HOUSE
SSCO	SSCO	1% MIN. FROM PROPERTY LINE TO SWALE
SSCO	SSCO	(E) TREE TO BE REMOVE
SSCO	SSCO	DOWN-SPOUT
SSCO	SSCO	POP-UP EMITTER
SSCO	SSCO	ROOF DOWN-SPOUT, CONNECTED TO STORM DRAIN SYSTEM



ENGINEERS
CIVIL ENGINEERS

1534 CAROB LANE
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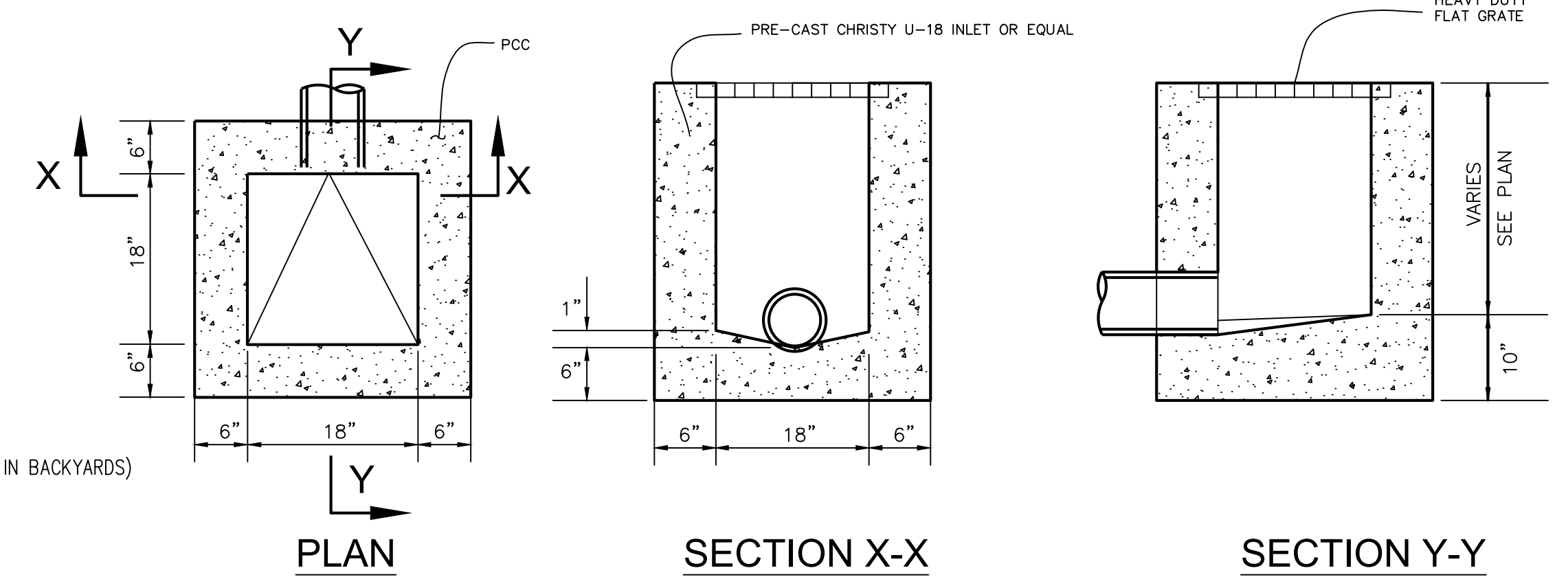
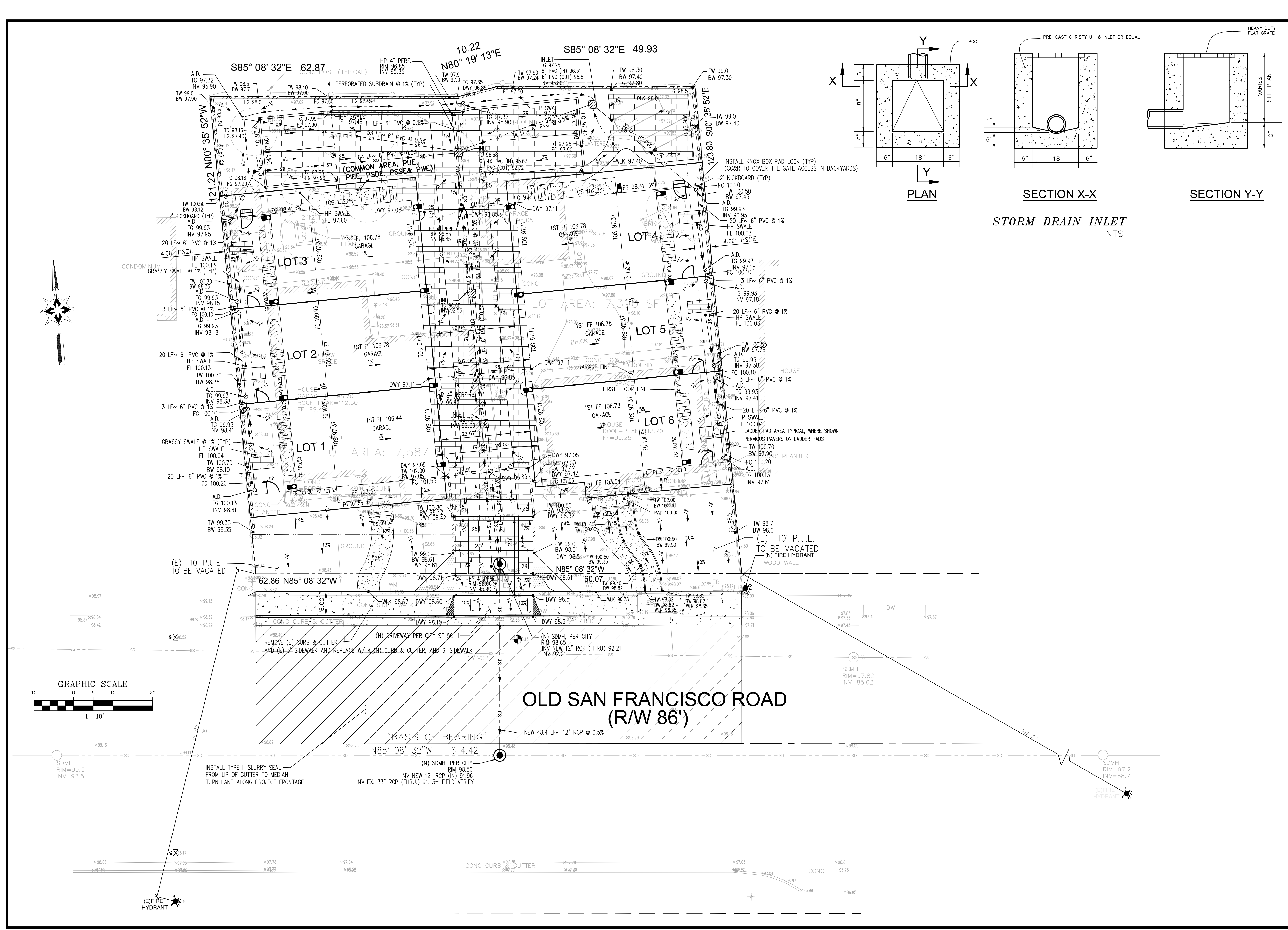
GRADING AND DRAINAGE PLANS
SIX (6) NEW TOWN HOUSES SUBDIVISION
APN: 209-17-051 & 050
669 AND 673 OLD SAN FRANCISCO ROAD, SUNNYVALE, CA
GRADING AND DRAINAGE PLAN

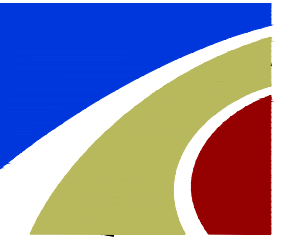
Revisions:



Date: 7/25/2018
Scale: 1"=10'
Prepared by: S.P.
Checked by: S.R.
Job #: 215001

Sheet: 2 OF 3
C-2





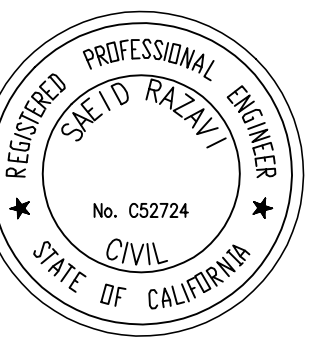
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GRADING AND DRAINAGE PLANS
SIX (6) NEW TOWN HOUSES SUBDIVISION
APN: 209-17-051 & 050
669 AND 673 OLD SAN FRANCISCO ROAD, SUNNYVALE, CA

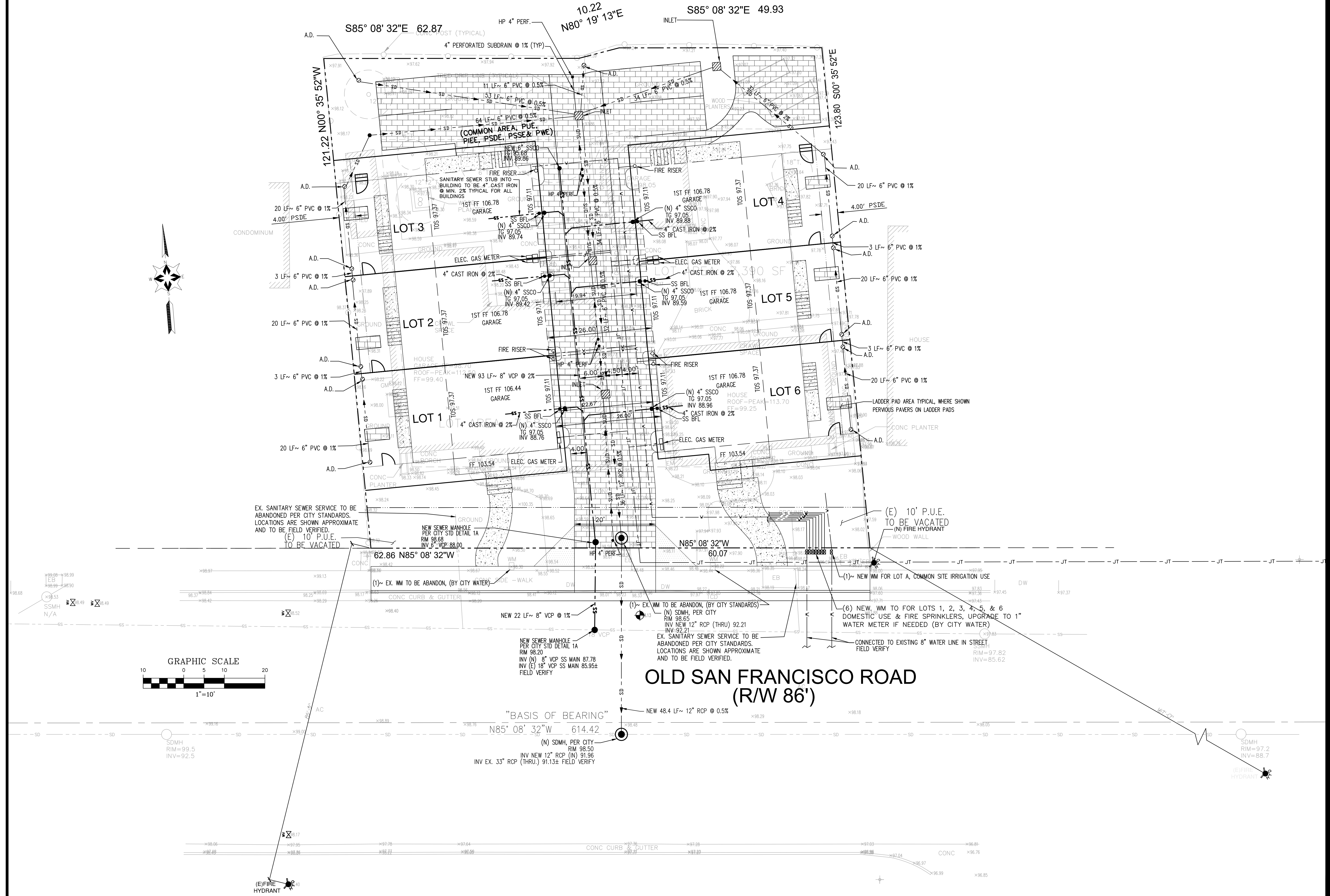
UTILITY PLAN

Revisions:



Date: 7/25/2018
Scale: 1"=10'
Prepared by: S.P.
Checked by: S.R.
Job #: 215001

Sheet: 3 OF 3
C-3





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STORMWATER MANAGEMENT PLANS
SIX (6) NEW TOWN HOUSES SUBDIVISION
APN: 209-17-051 & 050
669 AND 673 OLD SAN FRANCISCO ROAD, SUNNYVALE, CA
STORMWATER MANAGEMENT NOTES MEASURES

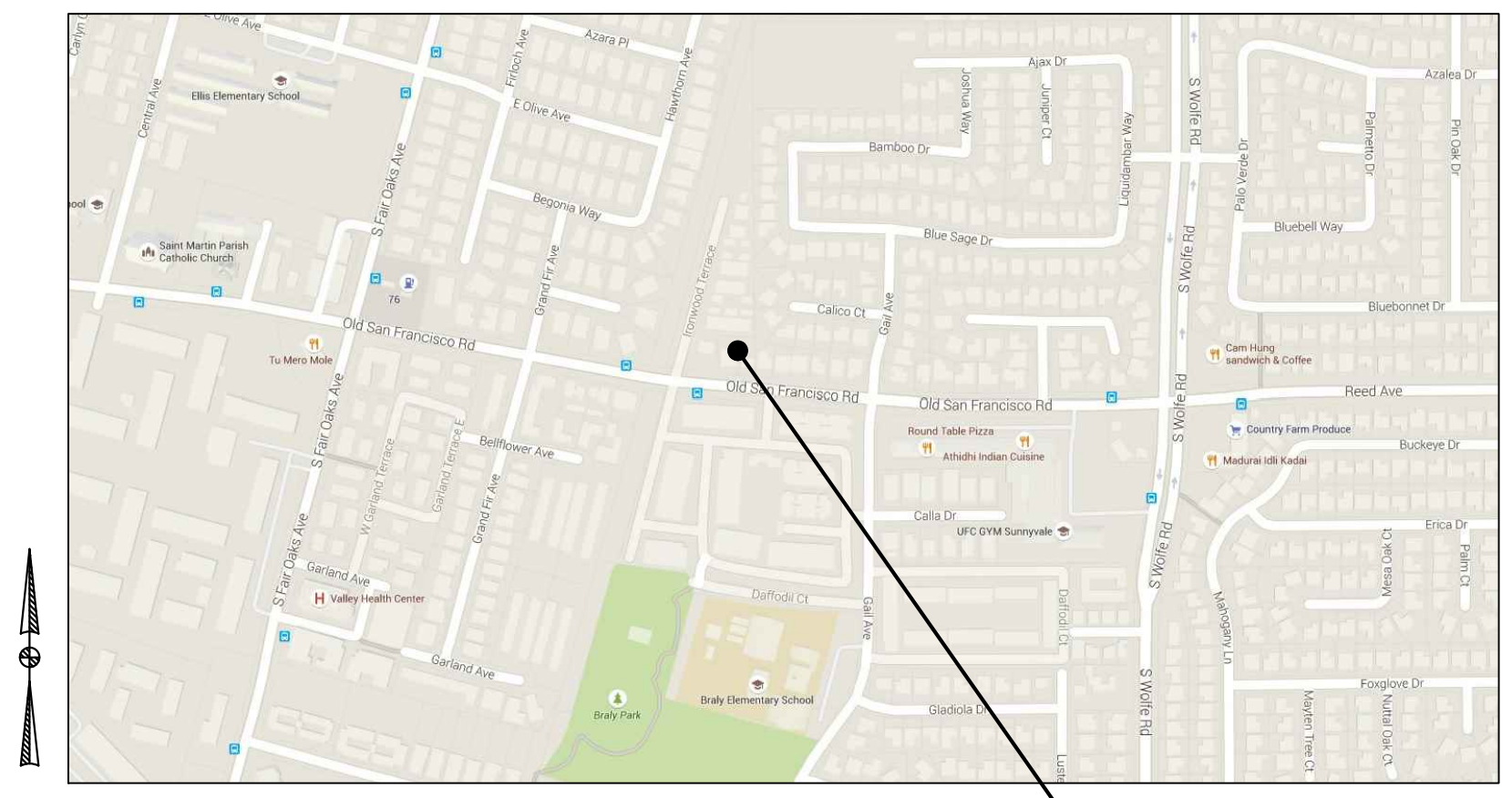
Revisions:



Said Raza

Date: 7/25/2018
Scale: NTS
Prepared by: S.P.
Checked by: S.R.
Job #: 215001

Sheet: 1 OF 3
STRM-1



LOCATION MAP
N.T.S.

PROJECT SITE

SHEET INDEX:

STRM-1	COVER SHEET/ NOTES/ MEASURES
STRM-2	GRADING AND DRAINAGE PLAN, CALCULATIONS, DETAILS
STRM-3	IMPERVIOUS SURFACE

GRADING AND DRAINAGE NOTES:

1. Surface water shall be directed away from all buildings into drainage swales, gutters, storm drain inlets and drainage systems.
2. All roof downspouts shall discharge to concrete splash pads draining away from the foundation. See architectural plans for roof downspout locations.
3. On site storm drain lines shall consist of solid PVC-SCH 40 minimum or better. Use PVC SCH80 for pipes running under driveway.
4. Storm drain inlets shall be precast concrete, Christy U23 type or equivalent.

STORMWATER MANAGEMENT PLANS
SIX (6) NEW TOWN HOUSES SUBDIVISION
669 AND 673 OLD SAN FRANCISCO ROAD, SUNNYVALE, CA

Compliance with NPDES Permit Provision C.3:

The San Francisco Bay Regional Water Quality Control Board (SFRWQCB) incorporated updated requirements into Santa Clara County's National Pollution Discharge Elimination System (NPDES) Permit in August 06. These updated stormwater quality control requirements are predominantly in the category of new development discharge controls. The Permit requires that permanent, post-construction stormwater quality control measures be implemented as part of development projects.

Updated stormwater quality control measures include:
- Source Control Measures
- Site Design Measures
- Treatment Control Measures

Beginning August 15, 2006, all projects creating or replacing 10,000 sq. ft. or more of impervious surface area must design and install a permanent post-construction stormwater treatment facility on the site. The system must be design and installed according to numeric sizing criteria.

All projects, regardless of size that create or replace impervious surface may be required to install stormwater quality controls to the maximum extent practicable.

Source Control Measures

STREET MAINTENANCE, BENEFICIAL LANDSCAPING.

Site Design Measures

DISCONNECTED DOWNSPOUTS, PERVIOUS DRIVEWAY DESIGN, MINIMIZE IMPERVIOUS SURFACE.

Storm water Treatment Measures:

PERVIOUS PAVEMENT DRIVEWAY WITH UNDERGROUND PERVIOUS DRAIN ROCK, TO TREAT RUN-OFF PRIOR TO ENTERING TO CITY STORM DRAINAGE SYSTEM, REDUCED RUN-OFF AND PEAK FLOW COMPARED TO PRE-CONSTRUCTION CONDITION.

Vegetated Swale Maintenance

(per California BMP Handbook-New and Redevelopment):

Maintenance activities should include periodic mowing (with grass never cut shorter that the design flow depth), weed control, watering during drought conditions, reseeding of bare areas, and clearing of debris and blockages. Cutting should be removed from the channel and disposed in a local composting facility. Accumulated sediment should also be removed manually to avoid concentrated flows in the swale. The application of fertilizers and pesticides should be minimal.

If channels develop ruts or holes, they should be repaired utilizing a suitable soil that is properly tamped and seeded. The grass cover should be thick; if it is not, reseed as necessary. Any standing water removed during the maintenance operation must be disposed to a sanitary sewer at an approved discharge location. Residuals (e.g. silt, grass cuttings) must be disposed in accordance with local or State requirements. Maintenance of grassed swales mostly involves maintenance of the grass or wetland plant cover. Typical maintenance activities are summarized below:

- a) Inspect swales at least twice annually for erosion, damage to vegetation, and sediment and debris accumulation preferably at the end of the wet season to schedule summer maintenance and before major fall runoff to be sure the swale is ready for winter. However, additional inspection after periods of heavy runoff is desirable. The swale should be checked for debris and litter, and areas of sediment accumulation.
- b) Grass height and mowing frequency may not have a large impact on pollutant removal. Consequently, mowing may only be necessary once or twice per year for safety or aesthetics or to suppress weeds and woody vegetation.
- c) Trash tends to accumulate in swale areas. The need for litter removal is determined through periodic inspection, but litter should be removed prior to mowing.
- d) Sediment accumulating near Inlets and Area Drains should be removed when it builds up to 75 mm (3 in.) at any spot, or covers vegetation.
- e) Regularly inspect swales for pools of standing water. Swales can become nuisance due to mosquito breeding in standing water if obstructions develop (e.g. debris accumulation, invasion vegetation) and/or if proper drainage slopes are not implemented and maintained.

Pervious Walkways and Driveway Maintenance:

The maintenance activity schedule presented below is based on recommendations provided in the California Stormwater BMP Handbook-New and Redevelopment, and the Interlocking Concrete Pavement Institute Manual (Second Edition).

The following maintenance activities should be performed on an ongoing basis:

- a) Keep landscaped areas well maintained;
- b) Prevent soil being washed onto pavement;

The following maintenance activities should be performed 2-3 times per year:

- a) Vacuum clean surface using commercially available sweeping machines at the following times:

- End of Winter (April)
- Mid-Summer (July/August)
- After Autumn-leaf fall(November)

The following maintenance activities should be performed on as-needed (infrequent) basis, maximum 15-20 years:

- a) If routine cleaning does not restore infiltration rates, then reconstruction of part of the whole of a previous surface may be required;
- b) The surface area affected by any observed hydraulic failure should be lifted for inspection of the internal materials to identify the location and the extent of the blockage;
- c) Surface materials should be lifted and replaced after brush cleaning. Geotextiles may need complete replacement;
- d) Subsurface layers may need cleaning and replacing;
- e) Removed silts may need to be disposed of as controlled weeds;
- f) Repair ruts or deformations in pavement exceeding ½-inch or 13 mm;
- g) Replace broken paver units that impair the structural integrity of the surface;
- h) Replenish aggregate surface joint materials.

ABBREVIATIONS		
	DESCRIPTION	DESCRIPTION
AB	AGGREGATE BASE	LIP LIP OF GUTTER
AC	ASPHALT CONCRETE	LP LOW POINT
AD	AREA DRAIN	MON MONUMENT
BC	BACK OF CURB	(N) NEW
BFL	BACKFLOW PREVENTOR	OG ORIGINAL GROUND
BW	BOTTOM OF WALL	PB PULL BOX
C&G	CURB AND GUTTER	PG&E PG&E VAULT
E.C./L	CENTERLINE	P/L PROPERTY LINE
CLSW	CENTERLINE SWALE	PP POWER POLE
CO	CLEANOUT	PPP PLASTIC PERFORATED PIPE
CP	CONTROL POINT	PSE PUBLIC SERVICE EASEMENT
DWY	DRIVEWAY	PVC POLYVINYL CHLORIDE
DI	DROP INLET	R/W RIGHT OF WAY
DTL	DETAIL	RCP REINFORCED CONCRETE PIPE
ELCT	ELECTRIC	SD STORM DRAIN
EP	EDGE OF PAVEMENT ELEVATION	SDMH STORM DRAIN MANHOLE
EUC	EUCALYPTUS TREE	STD STANDARD
(E),EX	EXISTING	SS SANITARY SEWER
FF	FINISH FLOOR	SSMH SANITARY SEWER MANHOLE
FG	FINISH GRADE	SW SIDEWALK
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FL	FLOWLINE	TF TOP OF FOUNDATION
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FOC	FACE OF CURB	TOS TOP OF SLAB
GB	GRADE BREAK	TP TOP OF PAVEMENT
GUY	GUY WIRE	TW TOP OF WALL
HP	HIGH POINT	(TYP) TYPICAL
DIP	DUCTILE IRON PIPE	VCP VITRIFIED CLAY PIPE
INV	INVERT	WL WHITE LINE STRIPE
JP	JOINT POLE	WLK WALKWAY
JB	JUNCTION BOX (UTILITY)	WM WATER METER
		WV WATER VALVE

LEGEND

EXISTING	PROPOSED	DESCRIPTION
---	---	PROPERTY LINE
-F-	-F-	FILL AREA LIMIT
-C-	-C-	CUT AREA LIMIT
102	102	CONTOUR
-W-	-W-	WATER LINE
-SD->	-SD->	STORM DRAIN PIPE (SOLID)
-SS-	-SS-	SANITARY SEWER PIPE
-OH- e,T,TV	-OH- e,T,TV	SUBDRAIN PIPE (PERFORATED)
-G-	-G-	OVERHEAD UTILITIES WITH POLE
-E-	-E-	GAS LINE
-JT-	-JT-	ELECTRIC LINE (UNDERGROUND)
SLV	SLV	JOINT TRENCH (UNDERGROUND)
SSCO	SSCO	STREET LIGHT VAULT
SSCO	SSCO	SANITARY SEWER CLEANOUT
SSCO	SSCO	SANITARY SEWER MANHOLE
SSCO	SSCO	STORM DRAIN MANHOLE
SSCO	SSCO	SURVEY CITY MONUMENT
SSCO	SSCO	ELECTROLIER
SSCO	SSCO	WATER METER
SSCO	SSCO	TREE WITH TRUNK
SSCO	SSCO	6' WOODEN FENCE
SSCO	SSCO	SPOT ELEVATION
SSCO	SSCO	TREE PROTECTION FENCE
SSCO	SSCO	5' TALL CHAIN LINK
SSCO	SSCO	EARTH SWALE
SSCO	SSCO	CONCRETE SWALE
SSCO	SSCO	AREA DRAIN/ INLET
SSCO	SSCO	OVERLAND RELEASE PATH
SSCO	SSCO	GRADE TO DRAIN, 2% MIN. AWAY FROM HOUSE
SSCO	SSCO	1% MIN. FROM PROPERTY LINE TO SWALE
SSCO	SSCO	(E) TREE TO BE REMOVE
SSCO	SSCO	DOWN-SPOUT
SSCO	SSCO	POP-UP EMITTER
SSCO	SSCO	ROOF DOWN-SPOUT, CONNECTED TO STORM DRAIN SYSTEM



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STORMWATER MANAGEMENT PLANS
SIX (6) NEW TOWN HOUSES SUBDIVISION
APN: 209-17-051 & 050
669 AND 673 OLD SAN FRANCISCO ROAD, SUNNYVALE, CA
IMPERVIOUS SURFACE

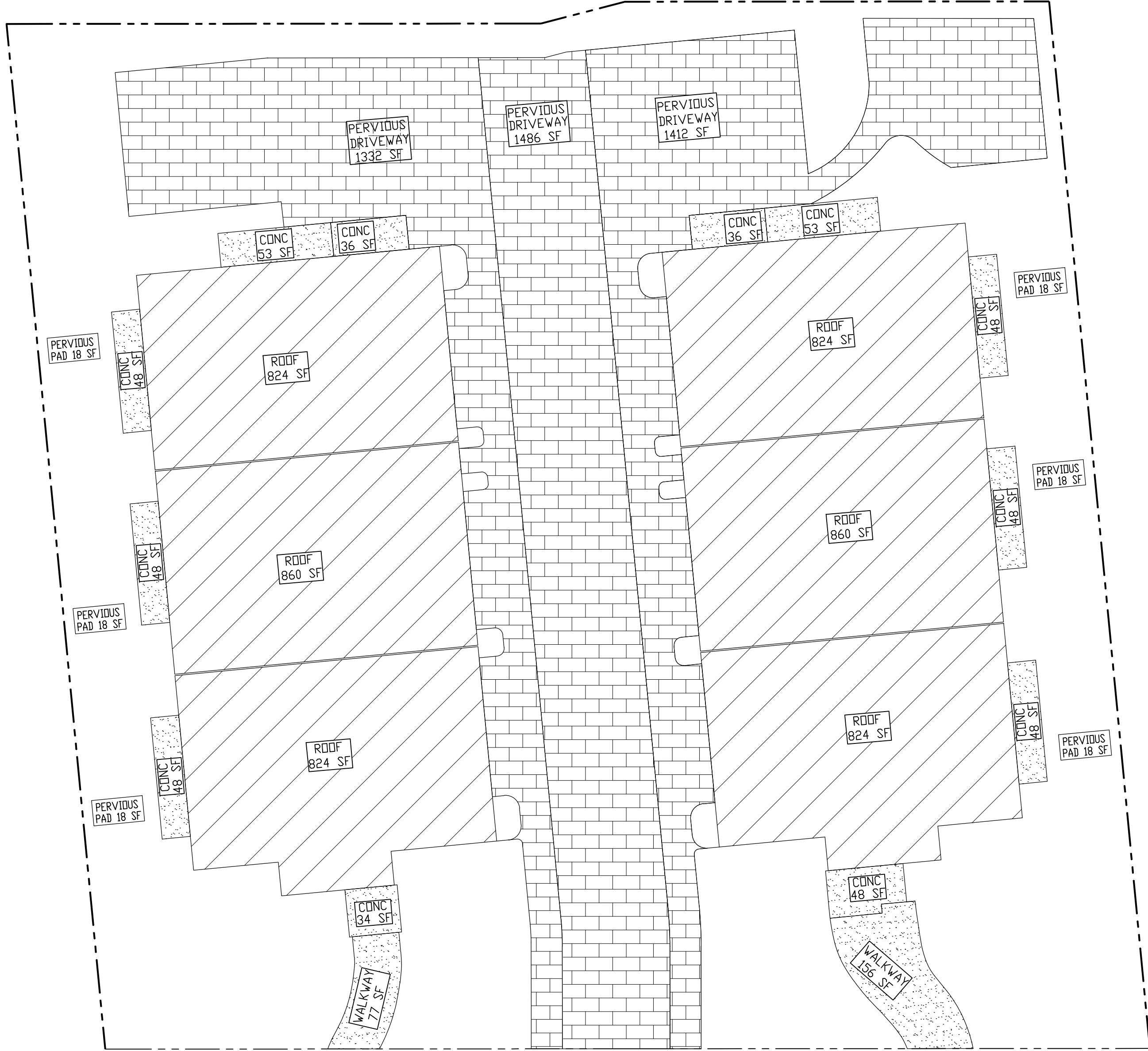
Revisions:



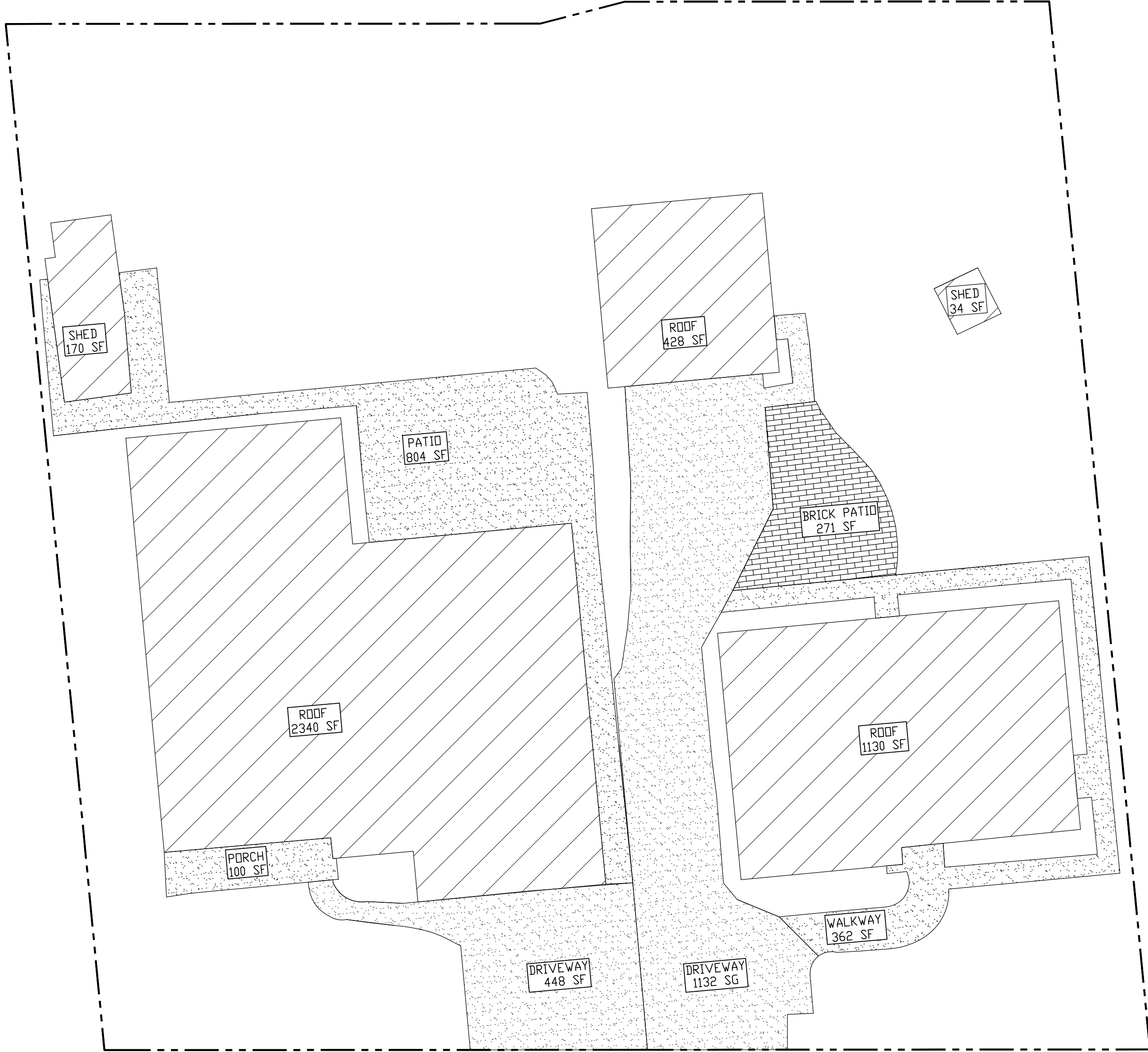
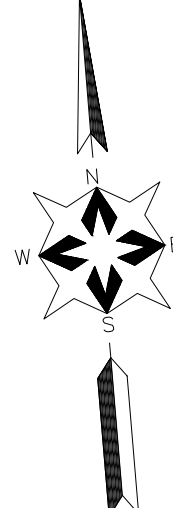
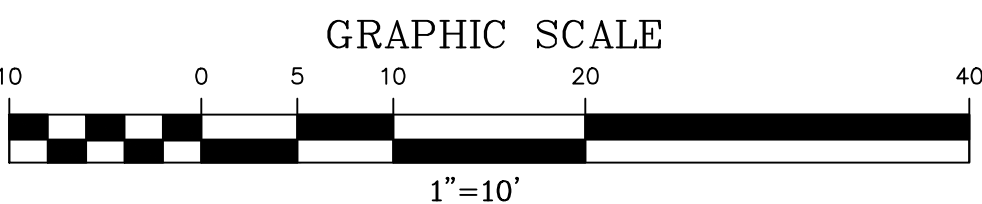
Sakto Rajani

Date: 7/25/2018
Scale: 1"=10'
Prepared by: S.P.
Checked by: S.R.
Job #: 215001

Sheet: 3 OF 3
STRM-3



NEW PERVIOUS AND IMPERVIOUS AREA



EXISTING PERVIOUS AND IMPERVIOUS AREA



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SIX (6) NEW TOWN HOUSES SUBDIVISION
APN: 209-17-051 & 050
669 AND 673 OLD SAN FRANCISCO ROAD, SUNNYVALE, CA
VESTING TENTATIVE MAP

Revisions:



Date: 7/25/2018
Scale: 1"=10'
Prepared by: S.P.
Checked by: S.R.
Job #: 215001

Sheet: 1 OF 1
TM-1

VESTING TENTATIVE MAP

SIX (6) NEW TOWN HOUSE DEVELOPMENT
CONSISTING OF ONE (1) SHEET
669 & 673 OLD SAN FRANCISCO RD.
SUNNYVALE, CALIFORNIA 94087
APN: 209-17-050 & 209-17-051

LOT 1 & 2, AS SHOWN IN THAT CERTAIN TRACT ENTITLED, ORCHARD GREEN TRACT 6830
FILED FOR RECORD ON OCTOBER 15, 1980, IN BOOK 473 OF MAPS, PAGES 16,17,18,19
AND 20, SANTA CLARA COUNTY RECORDS.

LYING ENTIRELY WITHIN THE
CITY OF SUNNYVALE, COUNTY OF SANTA CLARA, CALIFORNIA

JULY 2018
SMP ENGINEERS
1534 CAROB LANE
LOS ALTOS, CA 94024

LEGEND AND ABBREVIATIONS

- STREET CENTER LINE
- DISTINCTIVE BORDER LINE
- PROPOSED LOT LINE
- EASEMENT LINE
- CONCEPTUAL PROPOSED BUILDING FOOTPRINT
- OTHER PROPERTY LINES
- PIEE PRIVATE INGRESS EGRESS EASEMENT
- PSDE PRIVATE STORM DRAINAGE EASEMENT
- PSSE PRIVATE SANITARY SEWER EASEMENT
- PWE PRIVATE WATER EASEMENT
- PUE PUBLIC UTILITY EASEMENT
- PFSE PRIVATE FIRE SERVICE EASEMENT

GENERAL NOTES & STATEMENTS

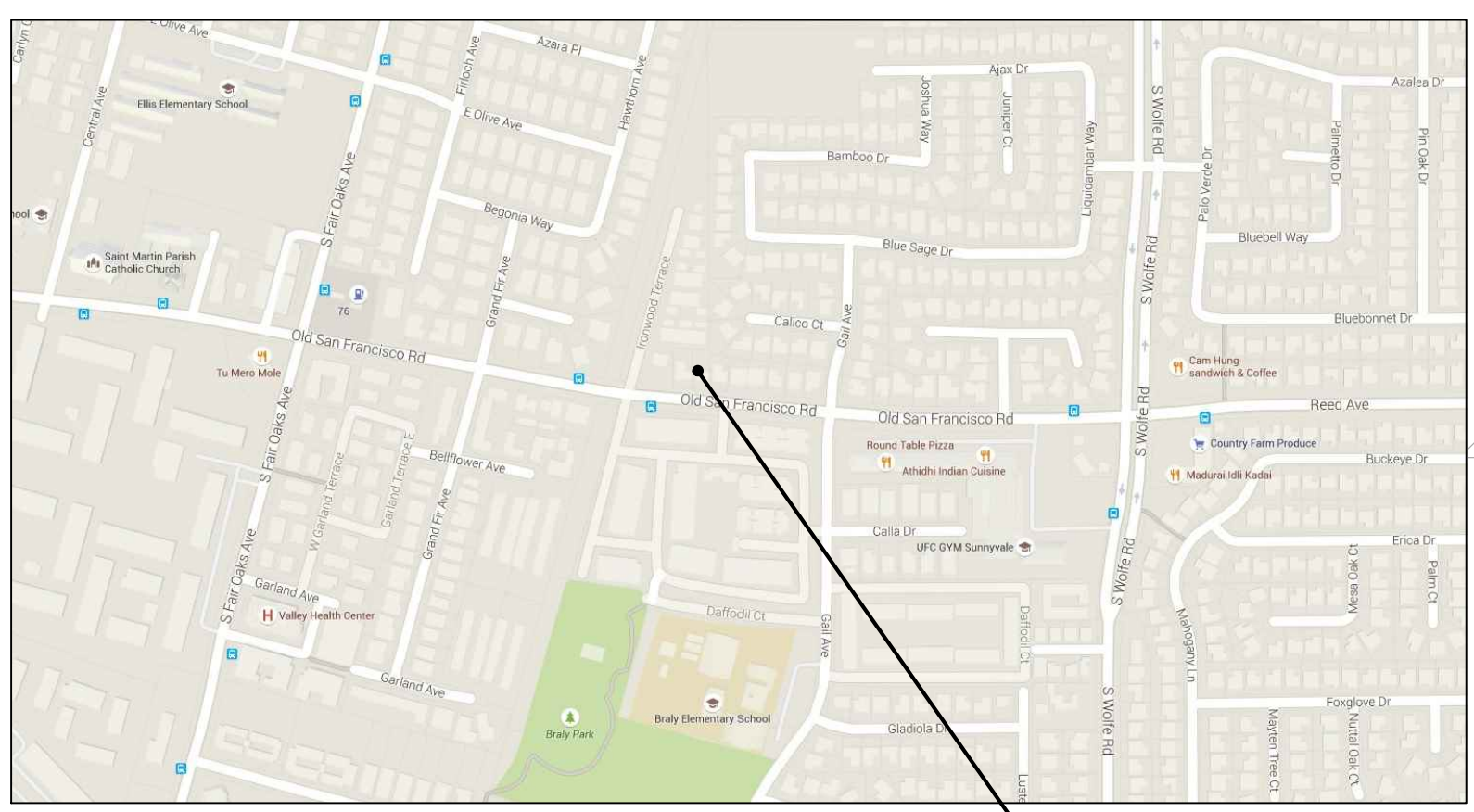
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- APPLICANT: SAME AS ABOVE.
- EXISTING ZONING: R3PD
- EXISTING APN: 209-17-050 & 209-17-051
- EXISTING USE: SINGLE FAMILY RESIDENTIAL
- PROPOSED USE: SINGLE FAMILY RESIDENTIAL
- EXISTING WELLS: UNKNOWN
- FLOOD ZONE: AO
- STREETS: ALL PROPOSED STREET MODIFICATIONS WILL BE DONE TO THE SATISFACTION OF THE DIRECTOR OF PUBLIC WORKS
- EXISTING USE OF ADJACENT PROPERTIES: RESIDENTIAL
- WATER: CITY OF SUNNYVALE
- FIRE PROTECTION: CITY OF SUNNYVALE
- STORM/SANITARY SEWER: CITY OF SUNNYVALE
- POWER AND GAS: PACIFIC GAS AND ELECTRIC
- TELEPHONE/CABLE: AT&T/COMCAST
- STREET TREES: STREET TREE(S) TO BE PLANTED PER CITY OF SUNNYVALE DIRECTION.
- BUILDING FOOTPRINTS SHOWN ARE CONCEPTUAL AND ARE SUBJECT TO CHANGE IN FINAL CONSTRUCTION PLANS.

REFERENCED BENCHMARK:

REFERENCED CITY OF SUNNYVALE B.M.# 9, EL: 92.481'.
BRASS DISC IN TOP OF CURB NEXT TO CATCH BASIN AT THE SOUTHWEST CURB RETURN,
INTERSECTION OF WOLFE ROAD AND OLD SAN FRANCISCO ROAD. NAVD88

BASIS OF BEARINGS:

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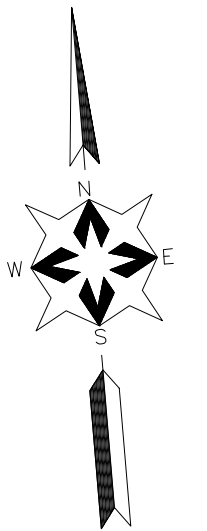
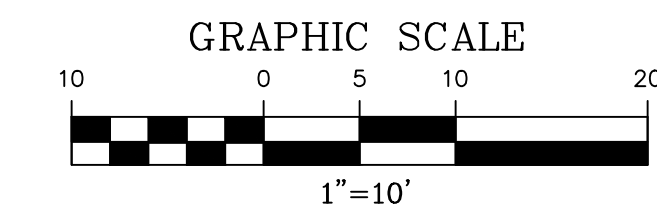


LOCATION MAP
N.T.S.

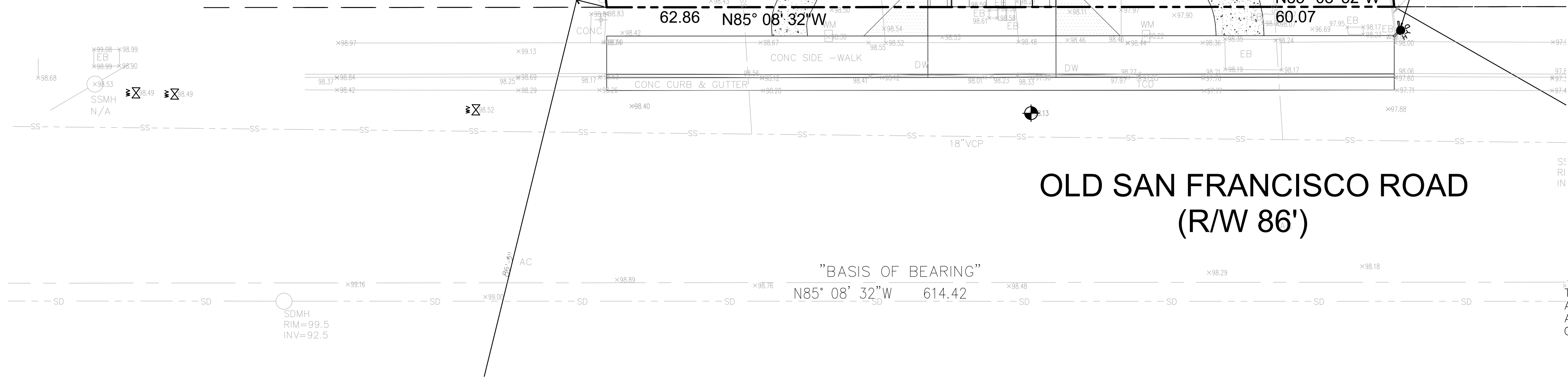
PROJECT SITE

TRACT #7462
PEBBLE CREEK CONDOMINIUMS

APARTMENTS



LOT AREA TABLE	GROSS AREA	
DESCRIPTION	AREA (SQFT.)	AREA (ACRES)
LOT 1	1,434	0.03
LOT 2	1,205	0.03
LOT 3	1,434	0.03
LOT 4	1,433	0.03
LOT 5	1,204	0.03
LOT 6	1,383	0.03
LOT A	6,883	0.16
TOTAL (BOUNDARY)	14,976	0.34



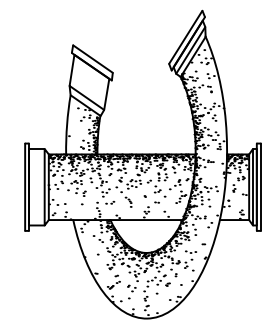
OLD SAN FRANCISCO ROAD
(R/W 86')

"BASIS OF BEARING"
N85° 08' 32"W 614.42

[illegible]

INNOVATIVE CONCEPTS
PROFESSIONAL BUILDING DESIGN AND PLANNING

3550 Stevens Creek Blvd. Suite 225
San Jose, CA 95117
Phone: (408) 985-1078 Fax: (408) 985-1343
E-Mail: imncpt@sbcglobal.net



A Major Use Permit/ Special Development Permit for:

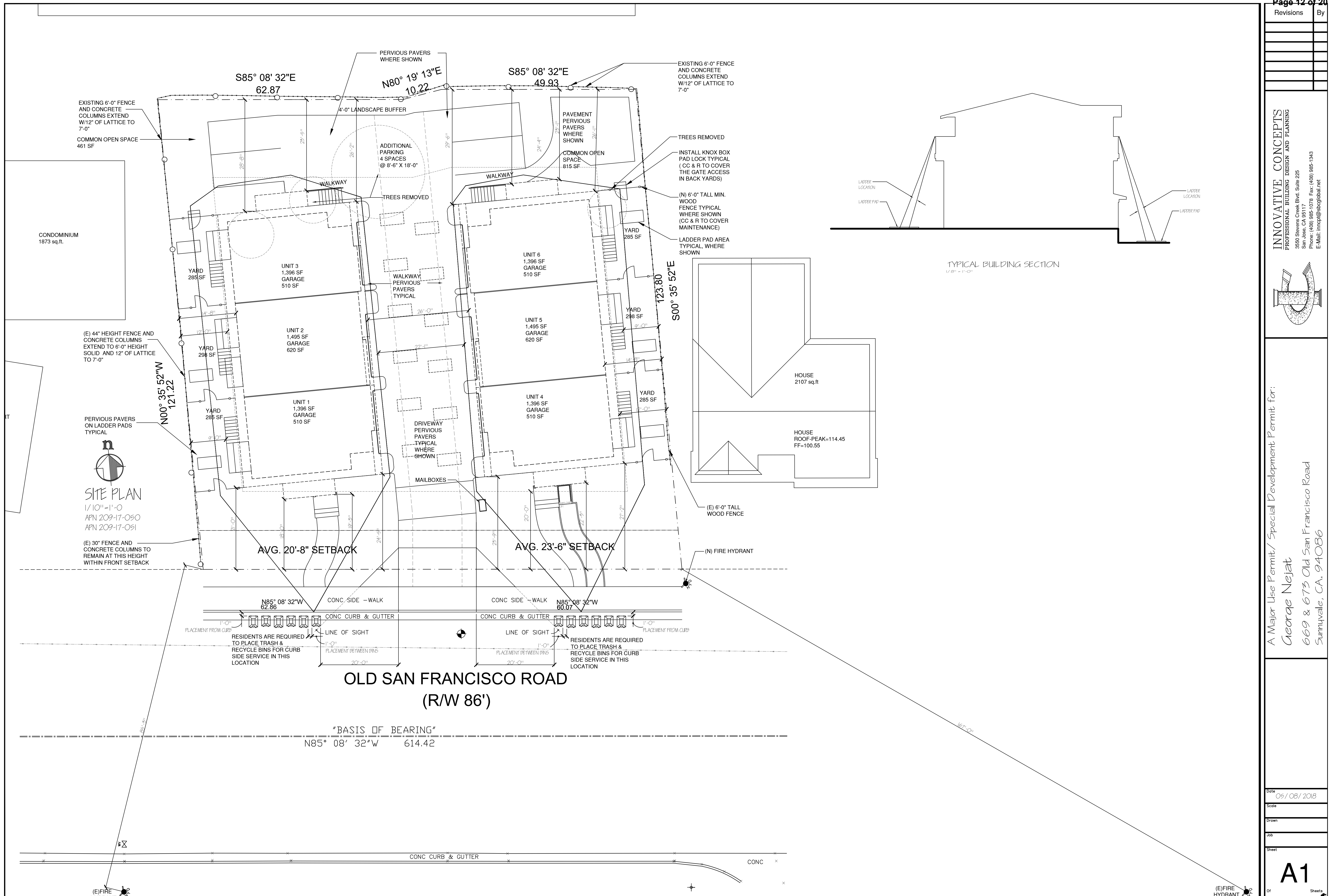
George Nejat

669 & 673 Old San Francisco Road
Sunnyvale, CA. 94086

Date	05 / 08 / 2018
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Drawn	
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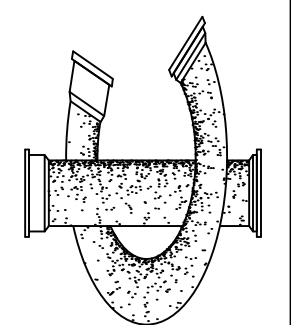
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Of Sheets



Revisions	By

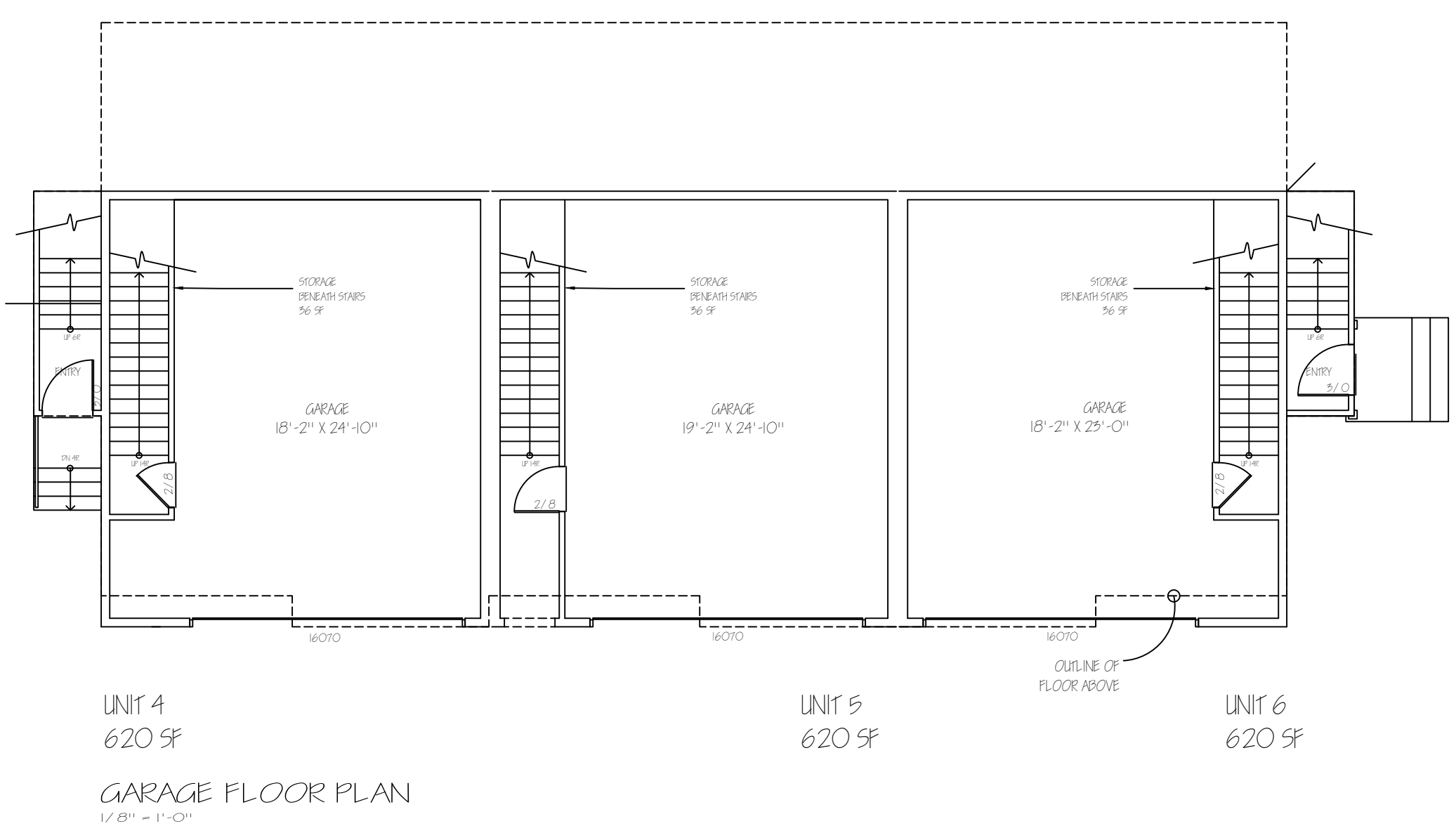
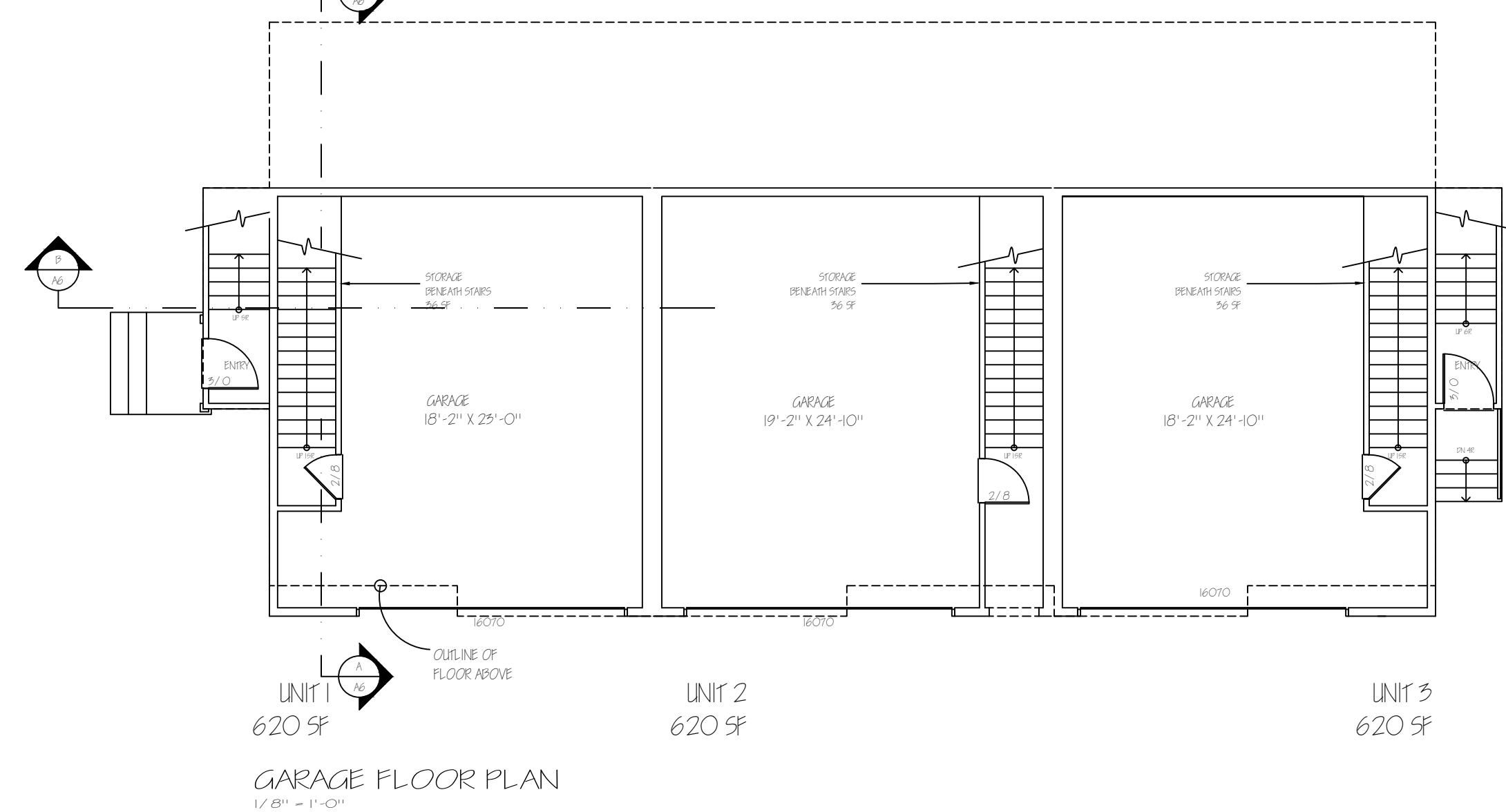
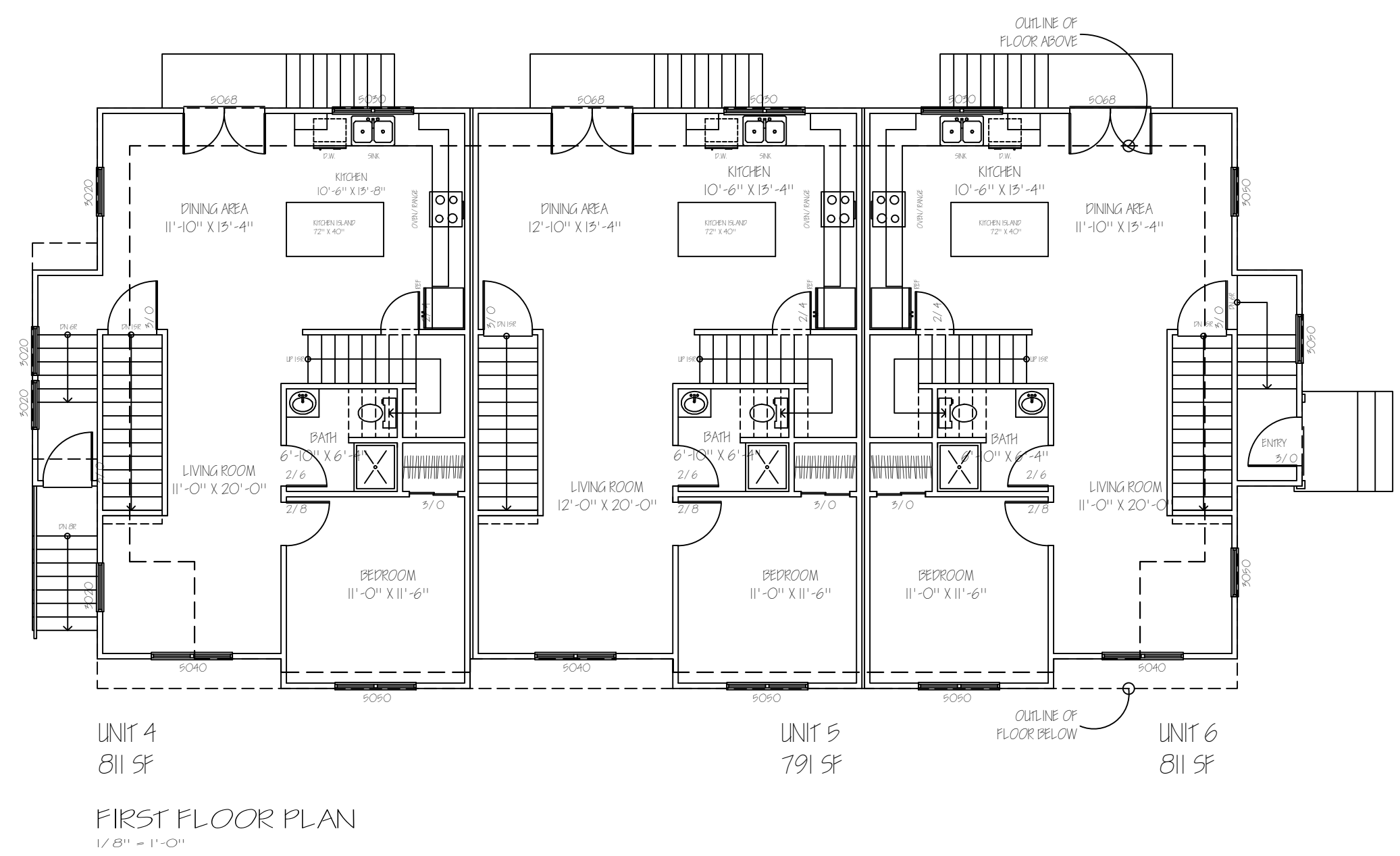
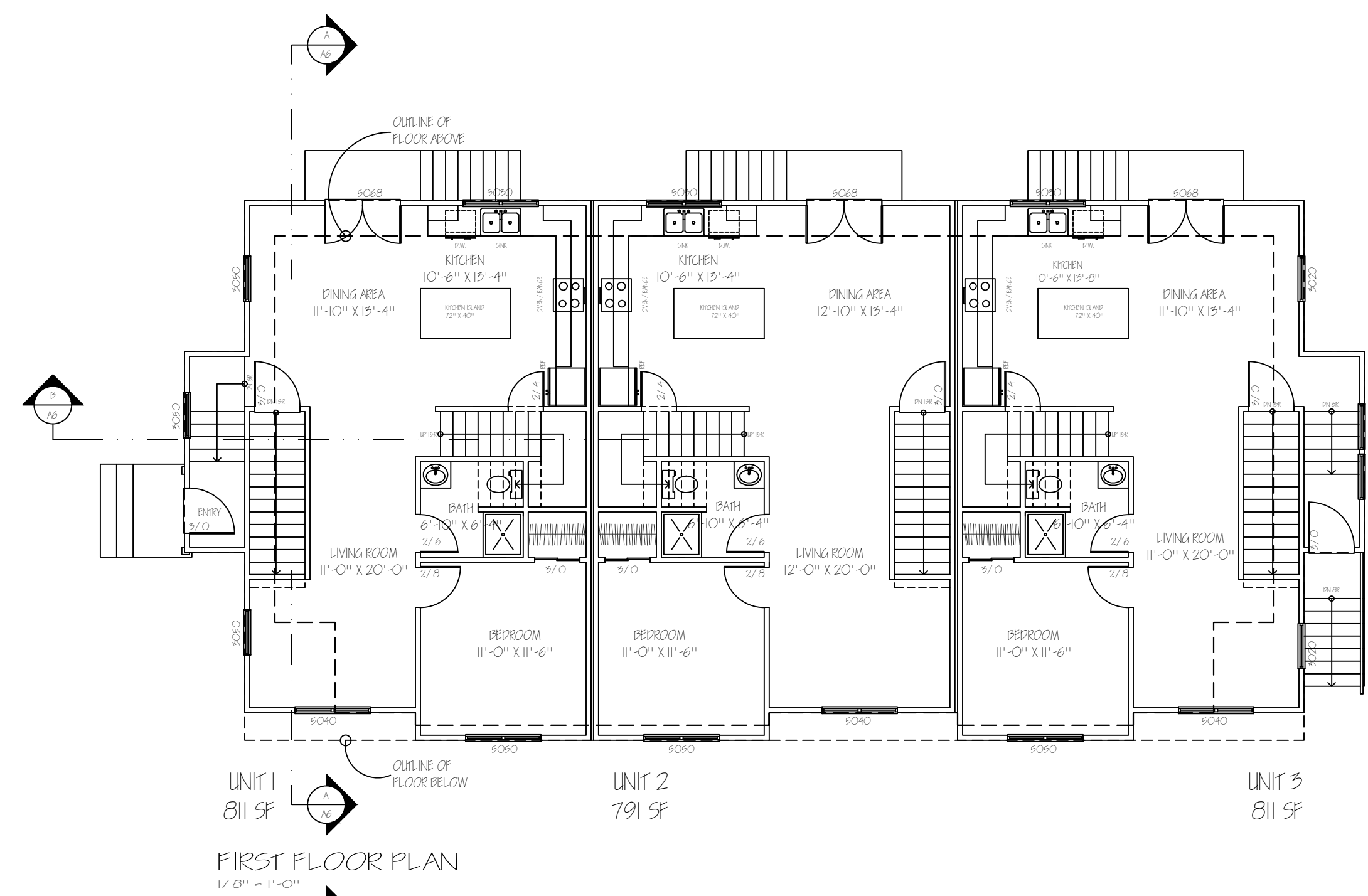
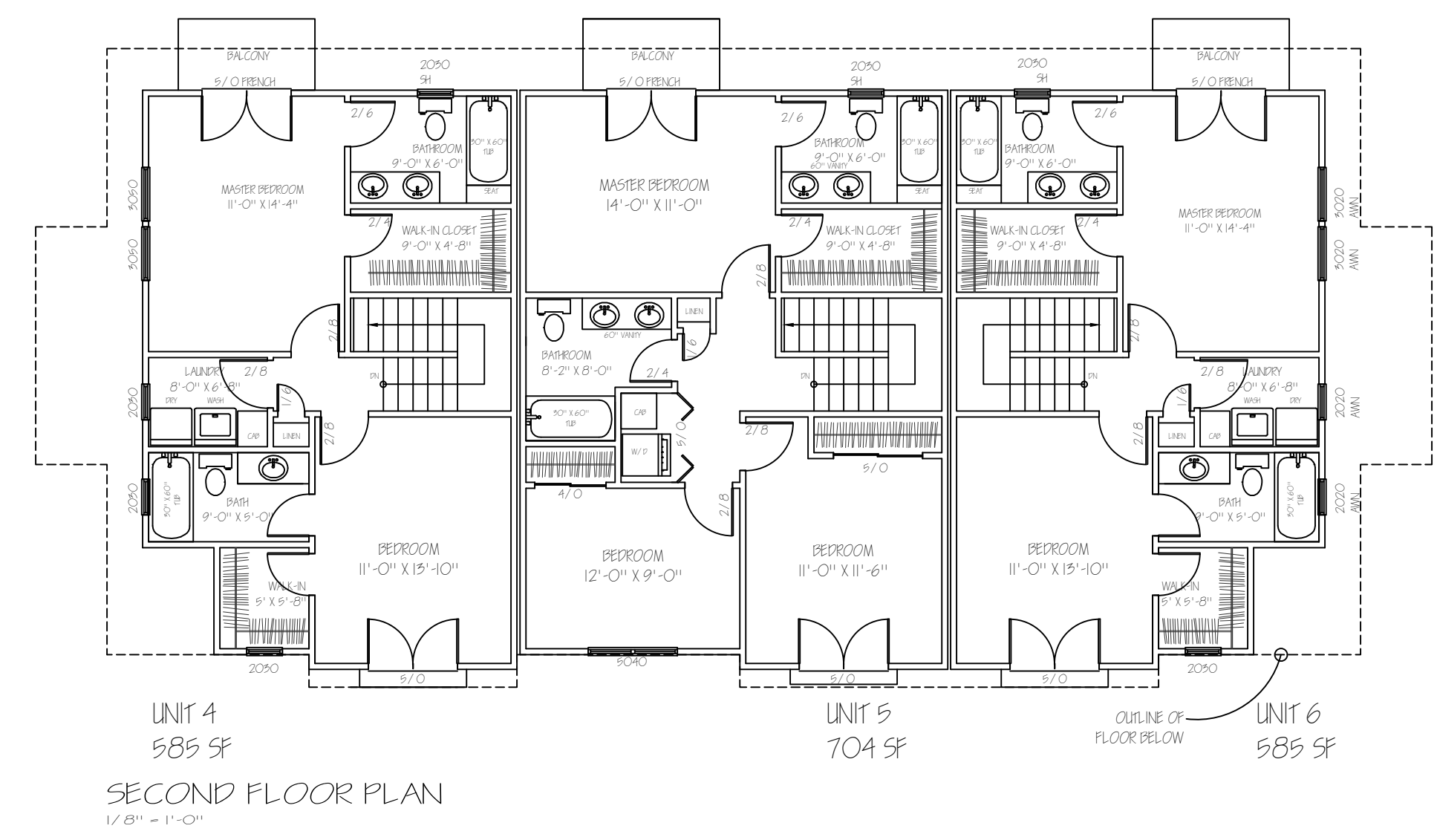
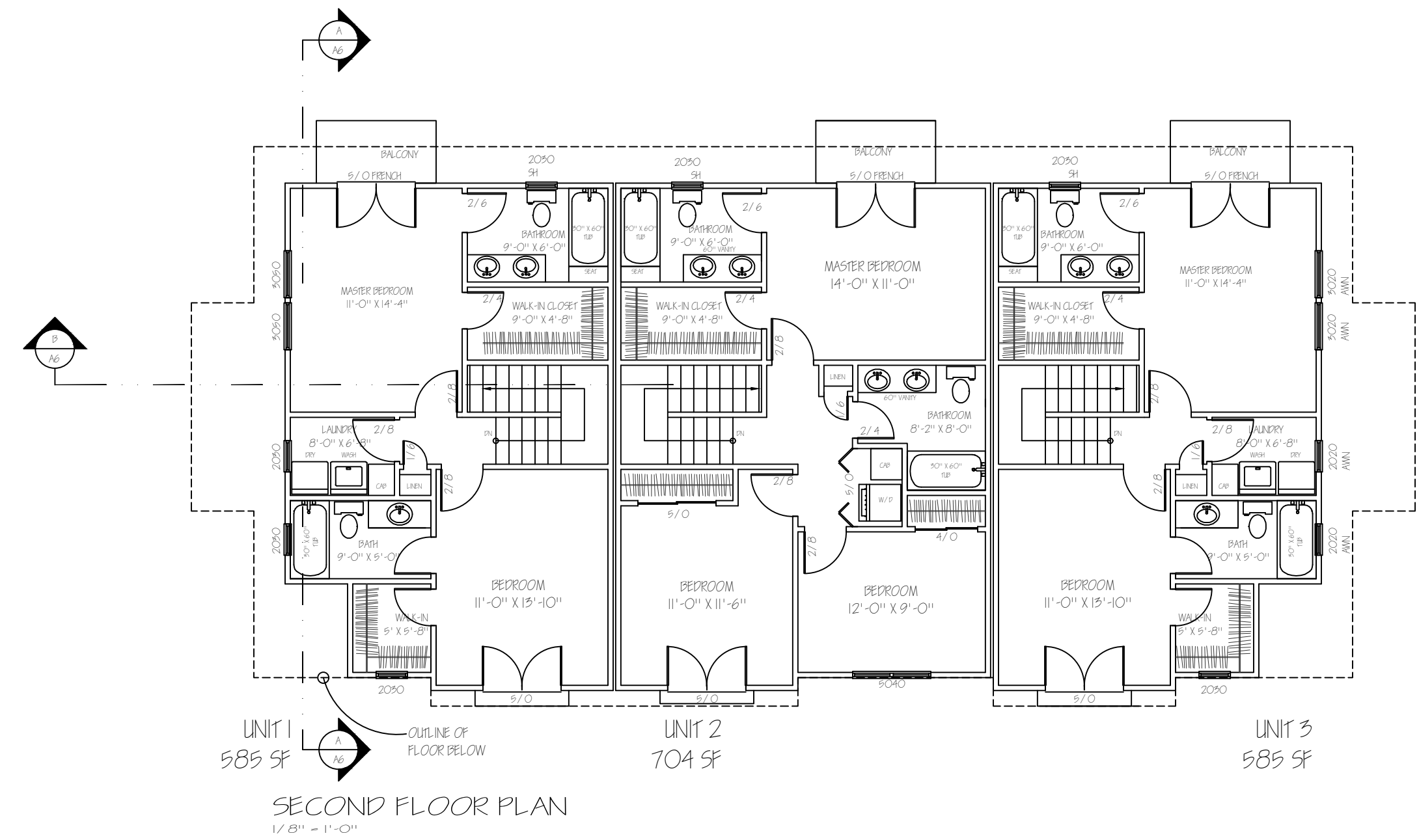
INNOVATIVE CONCEPTS
PROFESSIONAL BUILDING DESIGN AND PLANNING
3550 Stevens Creek Blvd, Suite 225
San Jose, CA 95117
Phone: (408) 985-1078 Fax: (408) 985-1943
E-Mail: inncp@abglobal.net



A Major Use Permit/ Special Development Permit for:
George Nejat
669 & 673 Old San Francisco Road
Sunnyvale, CA. 94086

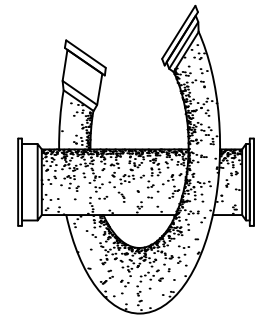
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of Sheets



Revisions	By

INNOVATIVE CONCEPTS
PROFESSIONAL BUILDING DESIGN AND PLANNING
3550 Stevens Creek Blvd, Suite 225
San Jose, CA 95117
Phone: (408) 985-1078 Fax: (408) 985-1343
E-Mail: inncep@aboglobal.net

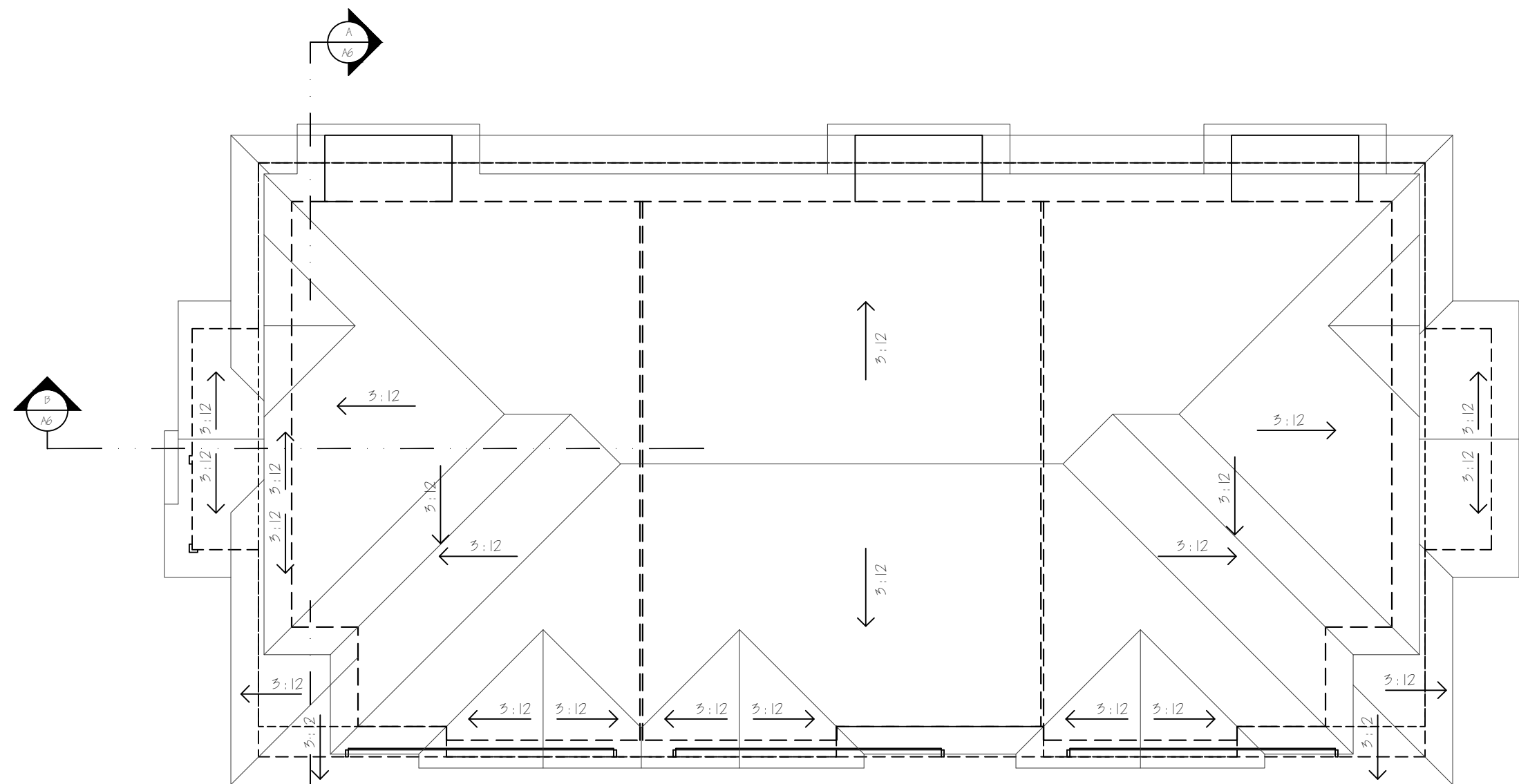


A Major Use Permit/ Special Development Permit for:
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Sunnyvale, CA. 94086

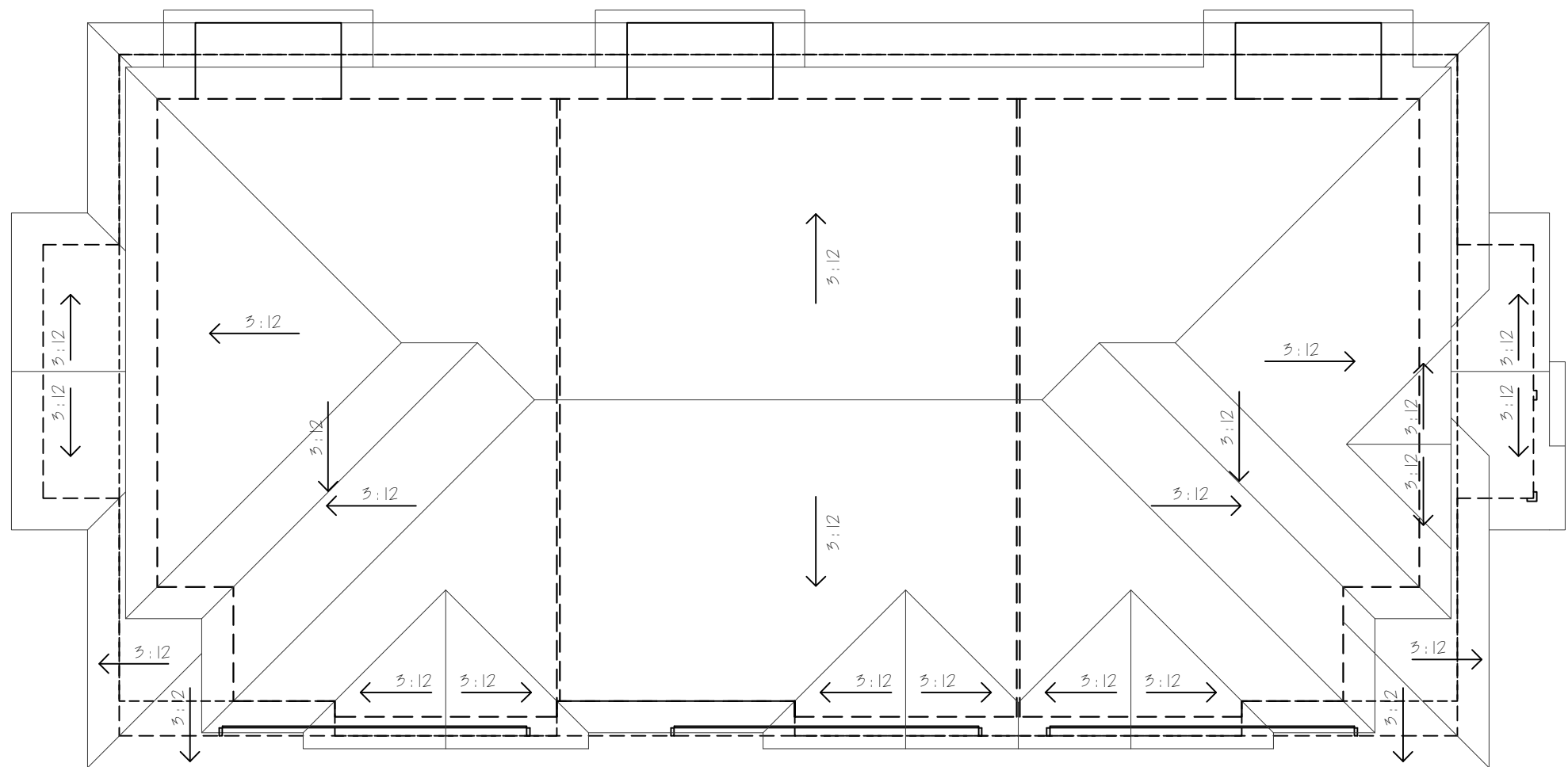
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ROOF PLAN
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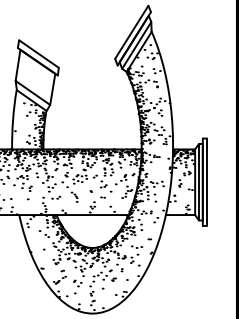


ROOF PLAN
1/8" = 1'-0"

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INNOVATIVE CONCEPTS
PROFESSIONAL BUILDING DESIGN AND PLANNING

3550 Stevens Creek Blvd. Suite 225
San Jose, CA 95117
Phone: (408) 985-1078 Fax: (408) 985-1343
E-Mail: imncpt@sbcglobal.net




A Major Use Permit/ Special Development Permit for:

George Nejat

669 & 673 Old San Francisco Road
Sunnyvale, CA. 94086

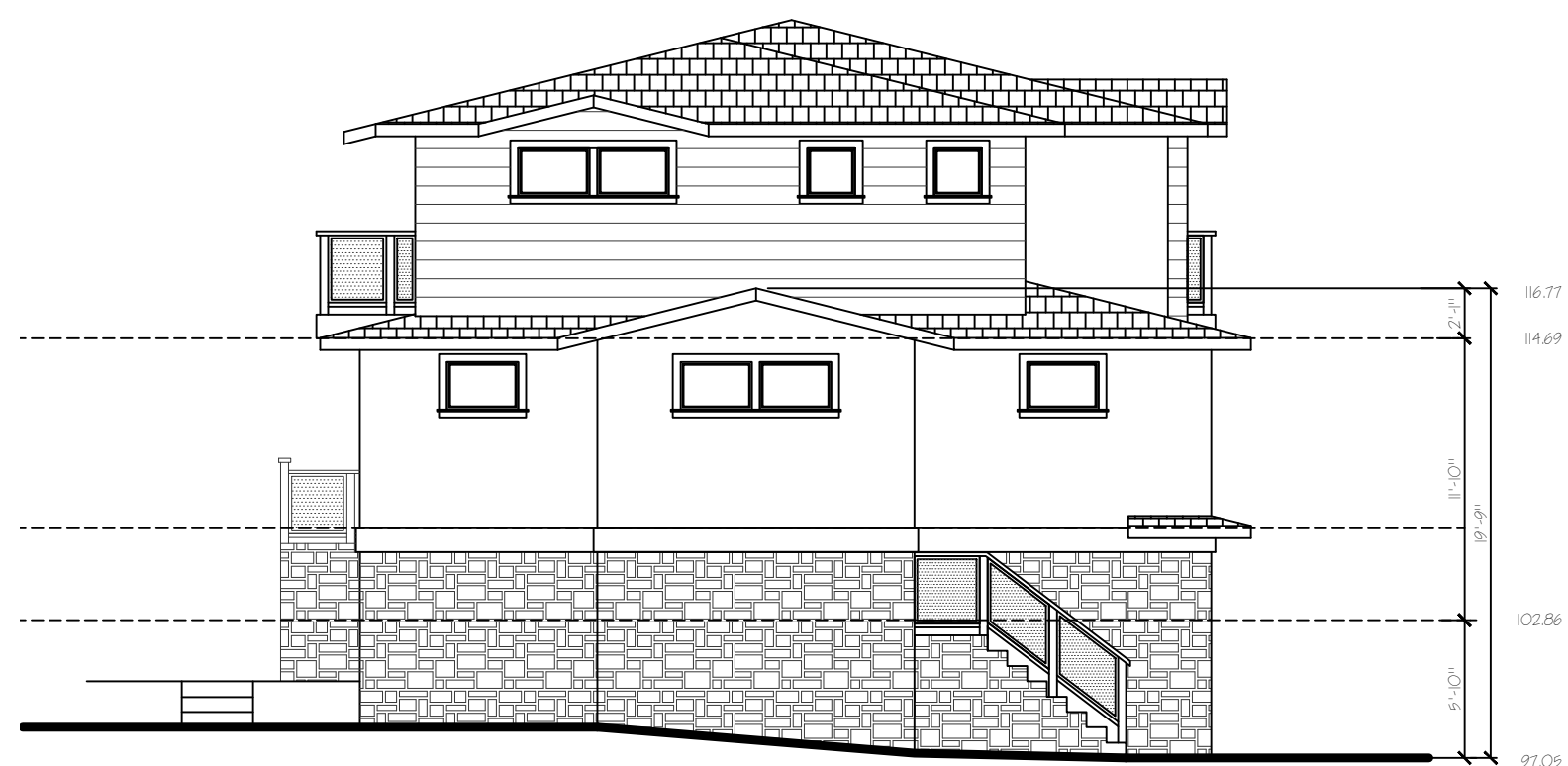
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BUILDING 2
SOUTH ELEVATION
1/8" = 1'-0"



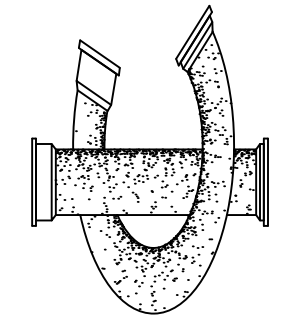
BUILDING 2
NORTH ELEVATION
1/8" = 1'-0"



BUILDING 2
WEST ELEVATION
1/8" = 1'-0"



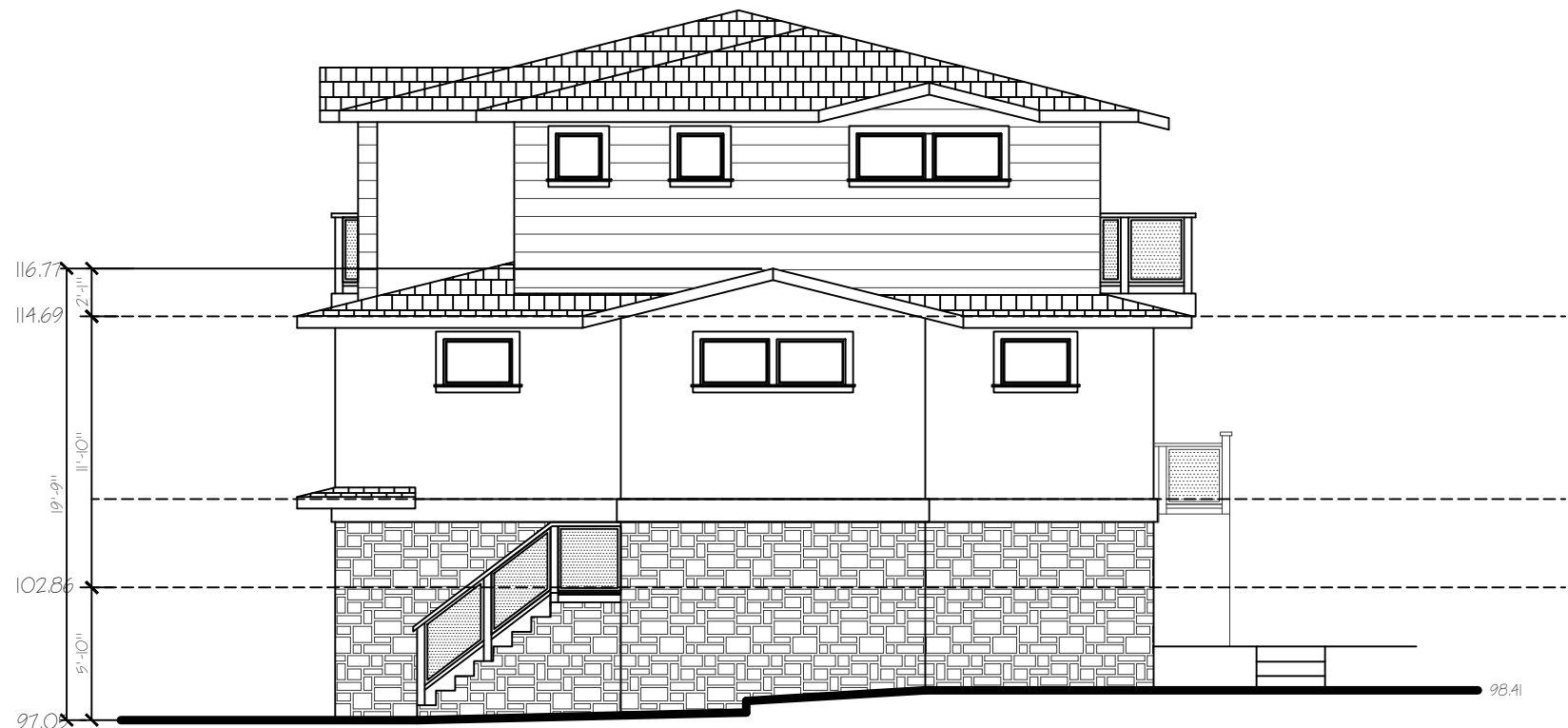
BUILDING 2
WEST ELEVATION
1/8" = 1'-0"



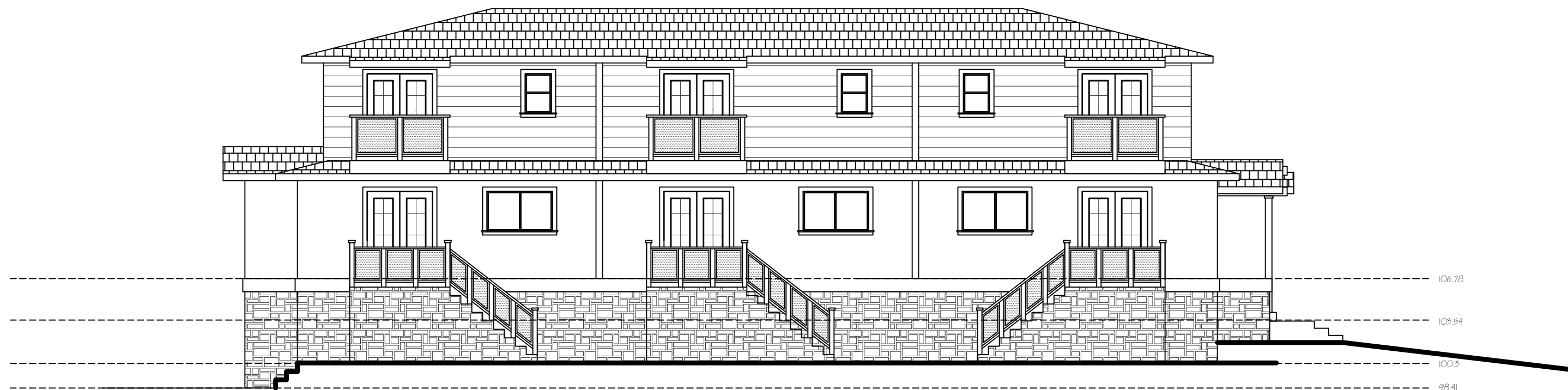
BUILDING 1
SOUTH ELEVATION
1/8" = 1'-0"



BUILDING 1
EAST ELEVATION
1/8" = 1'-0"



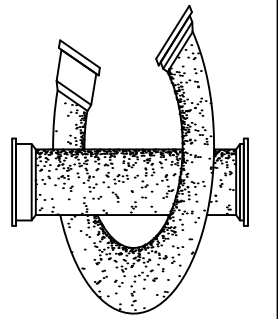
BUILDING 1
NORTH ELEVATION
1/8" = 1'-0"



BUILDING 1
WEST ELEVATION
1/8" = 1'-0"

Revisions	By

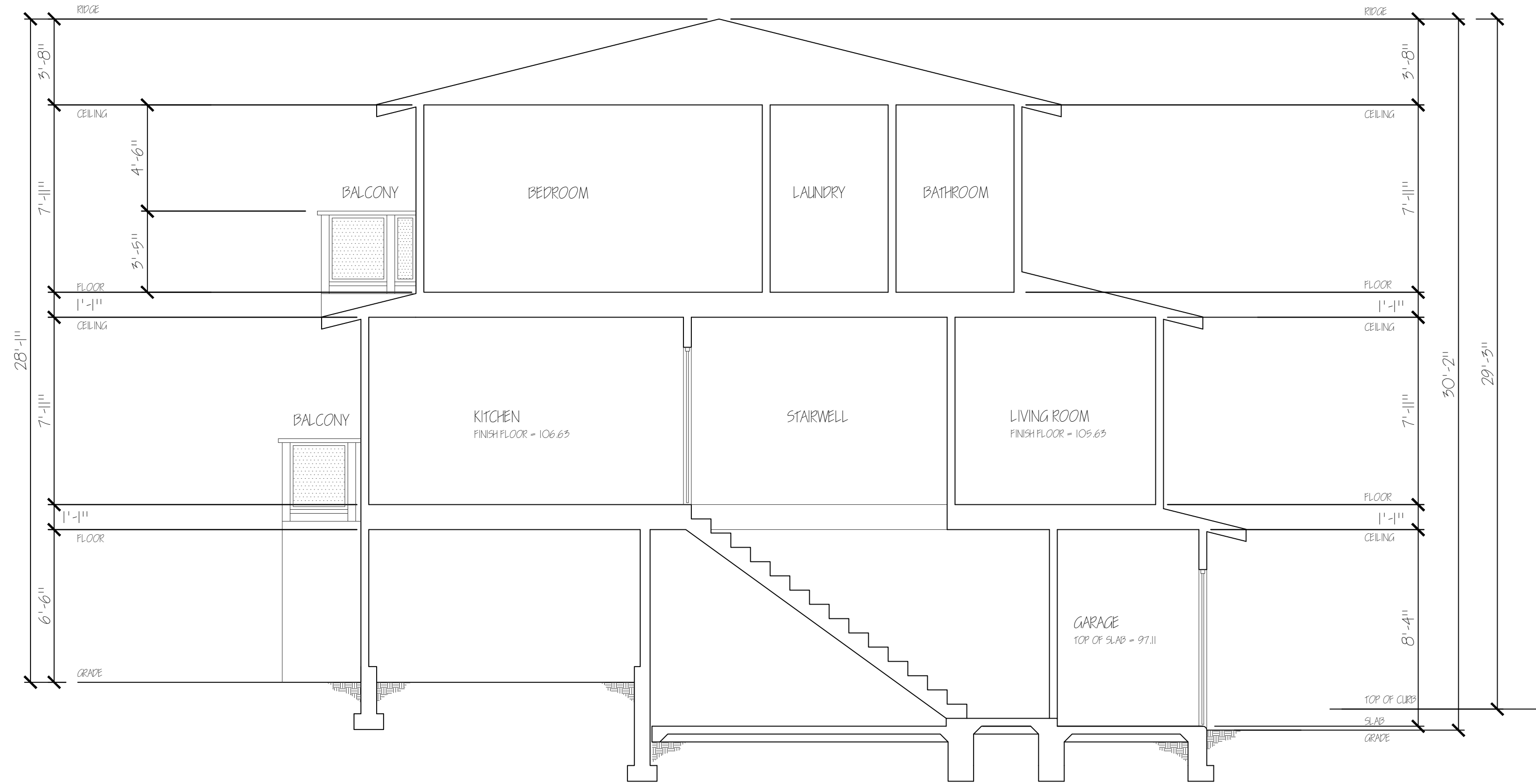
INNOVATIVE CONCEPTS
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3550 Stevens Creek Blvd, Suite 225
San Jose, CA 95117
Phone: (408) 985-1078 Fax: (408) 985-1343
E-Mail: inncpj@aboglobal.net



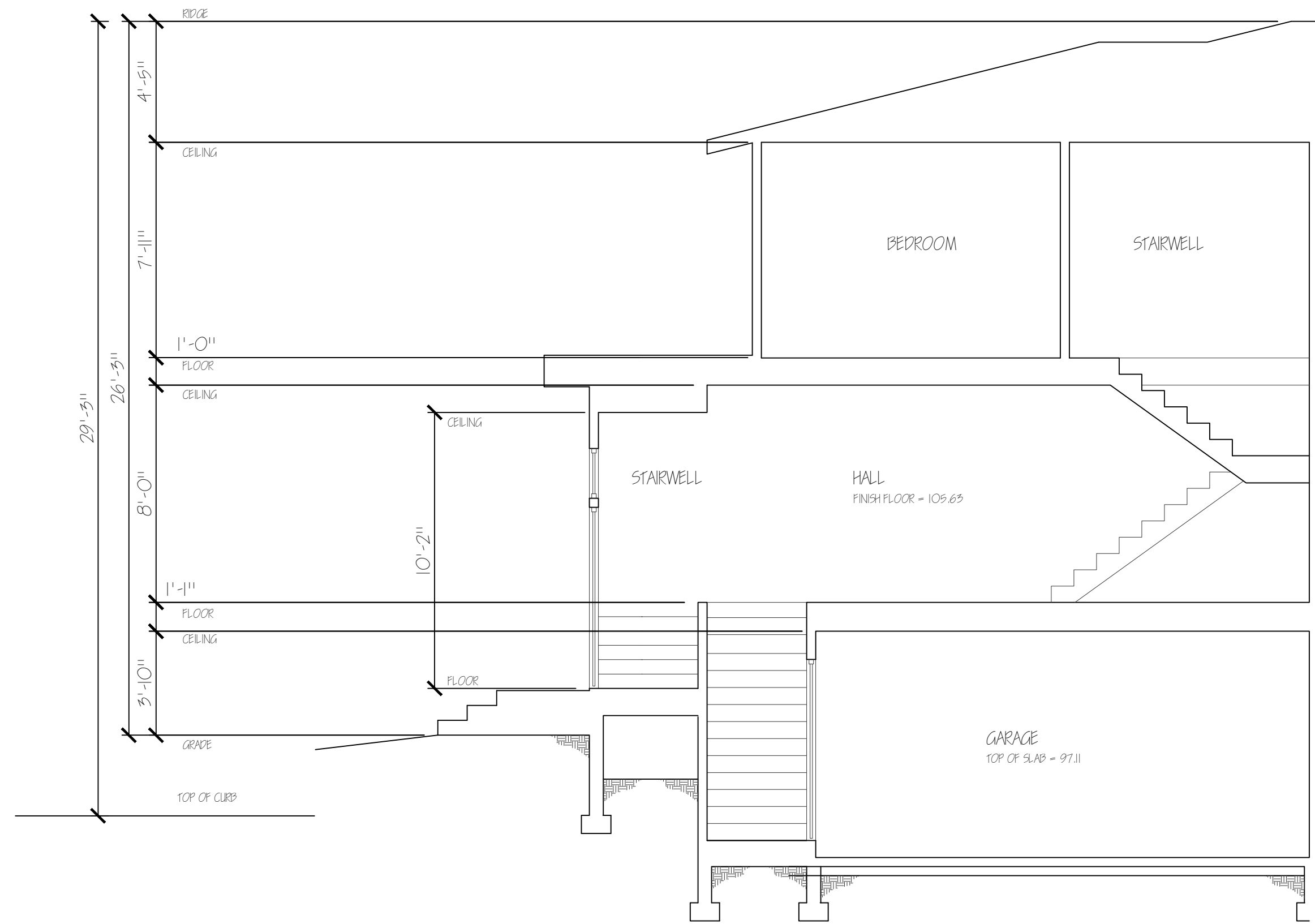
A Major Use Permit/ Special Development Permit for:
George Nejat
669 & 673 Old San Francisco Road
Sunnyvale, CA. 94086

Date	05/08/2018
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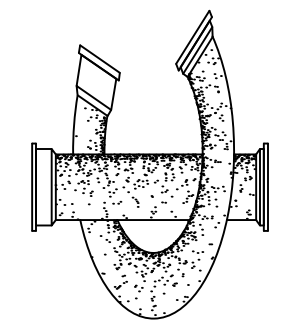
SECTION 'A'-'A'
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SECTION 'B'-'B'
1/4" = 1'-0"

Revisions	By

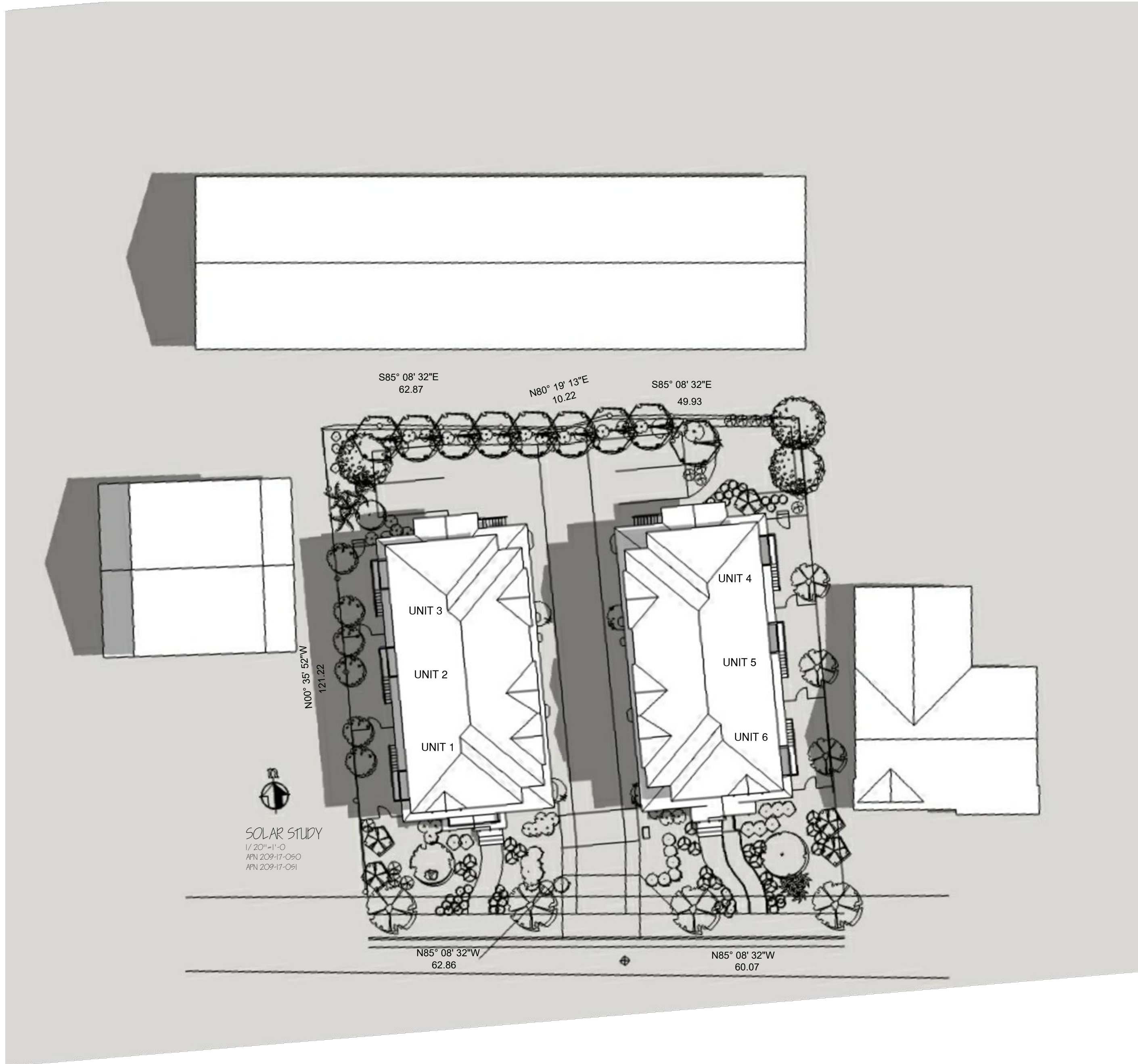
INNOVATIVE CONCEPTS
PROFESSIONAL BUILDING DESIGN AND PLANNING
2801 Moorpark Ave., Suite 4
San Jose, CA 95128
Phone: (408) 985-1078 Fax: (408) 985-1943
E-Mail: inncepi@aboglobal.net



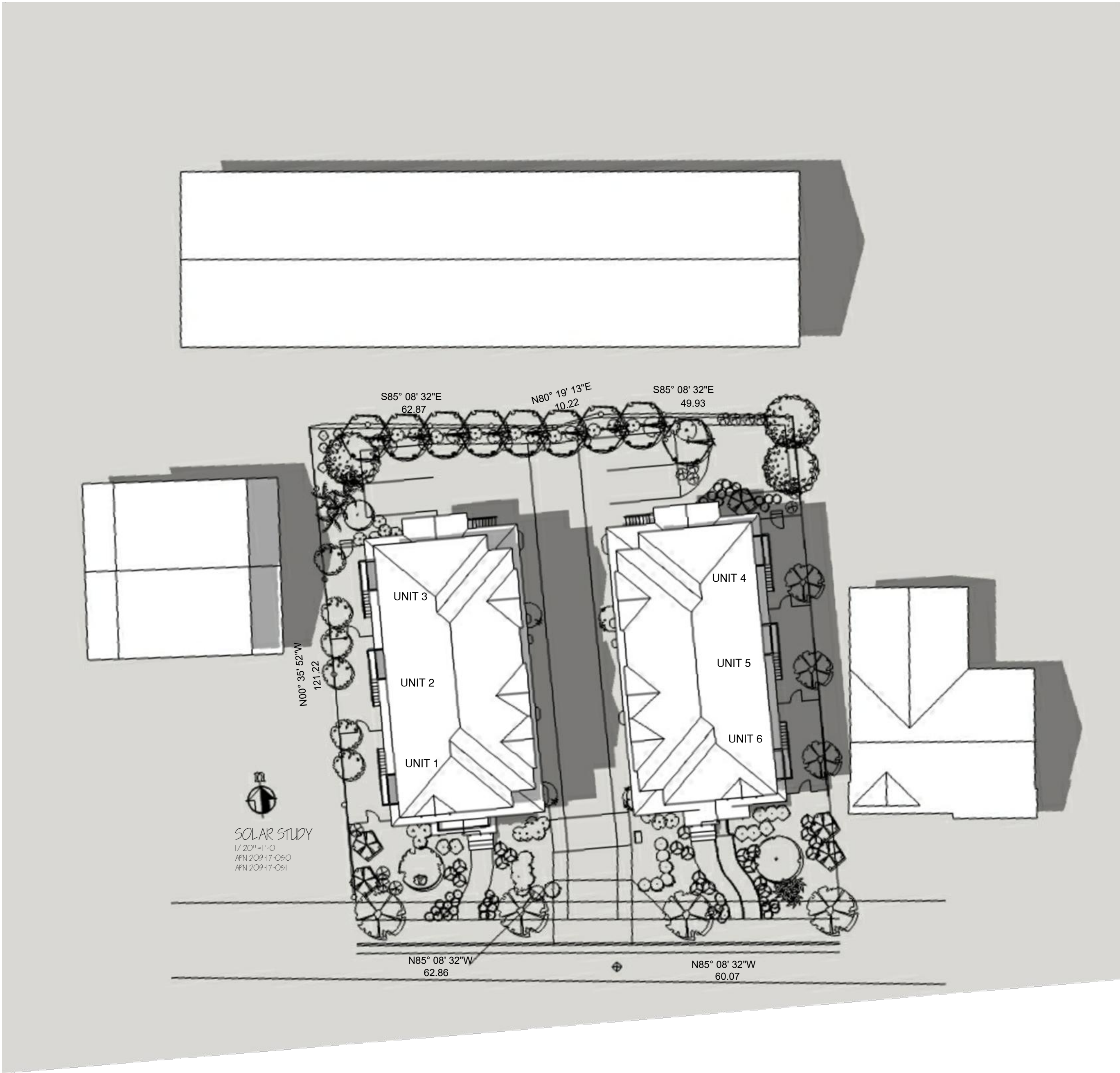
A Major Use Permit/ Special Development Permit for:
George Nejat
669 & 673 Old San Francisco Road
Sunnyvale, CA. 94086

Date	05/08/2018
Scale	
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Job	
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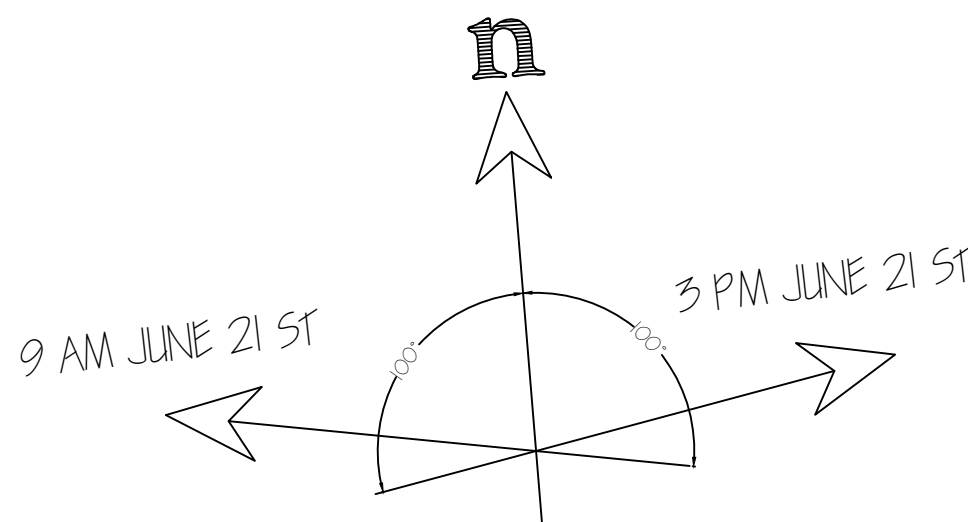
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OLD SAN FRANCISCO ROAD
(R/W 86')



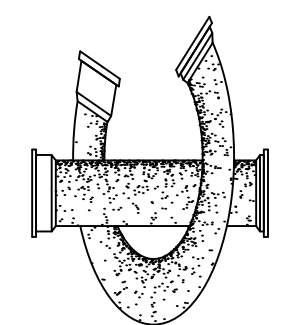
OLD SAN FRANCISCO ROAD
(R/W 86')



SHADOW ANALYSIS
JUNE 21 AT 9 AM AND 3 PM
100° AZIMUTH AND 47° ALTITUDE

Revisions	By

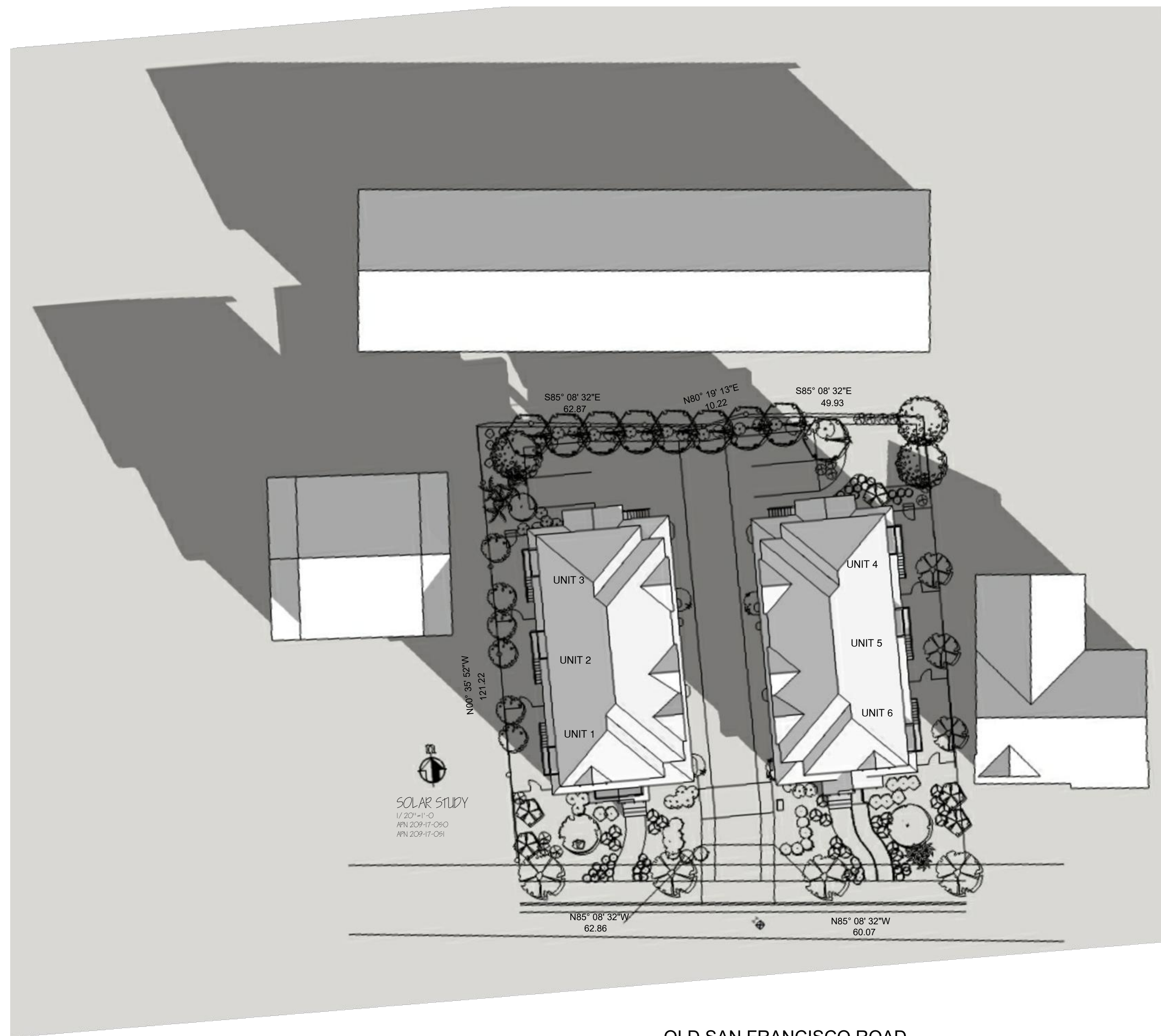
INNOVATIVE CONCEPTS
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San Jose, CA 95128
Phone: (408) 985-1078 Fax: (408) 985-1343
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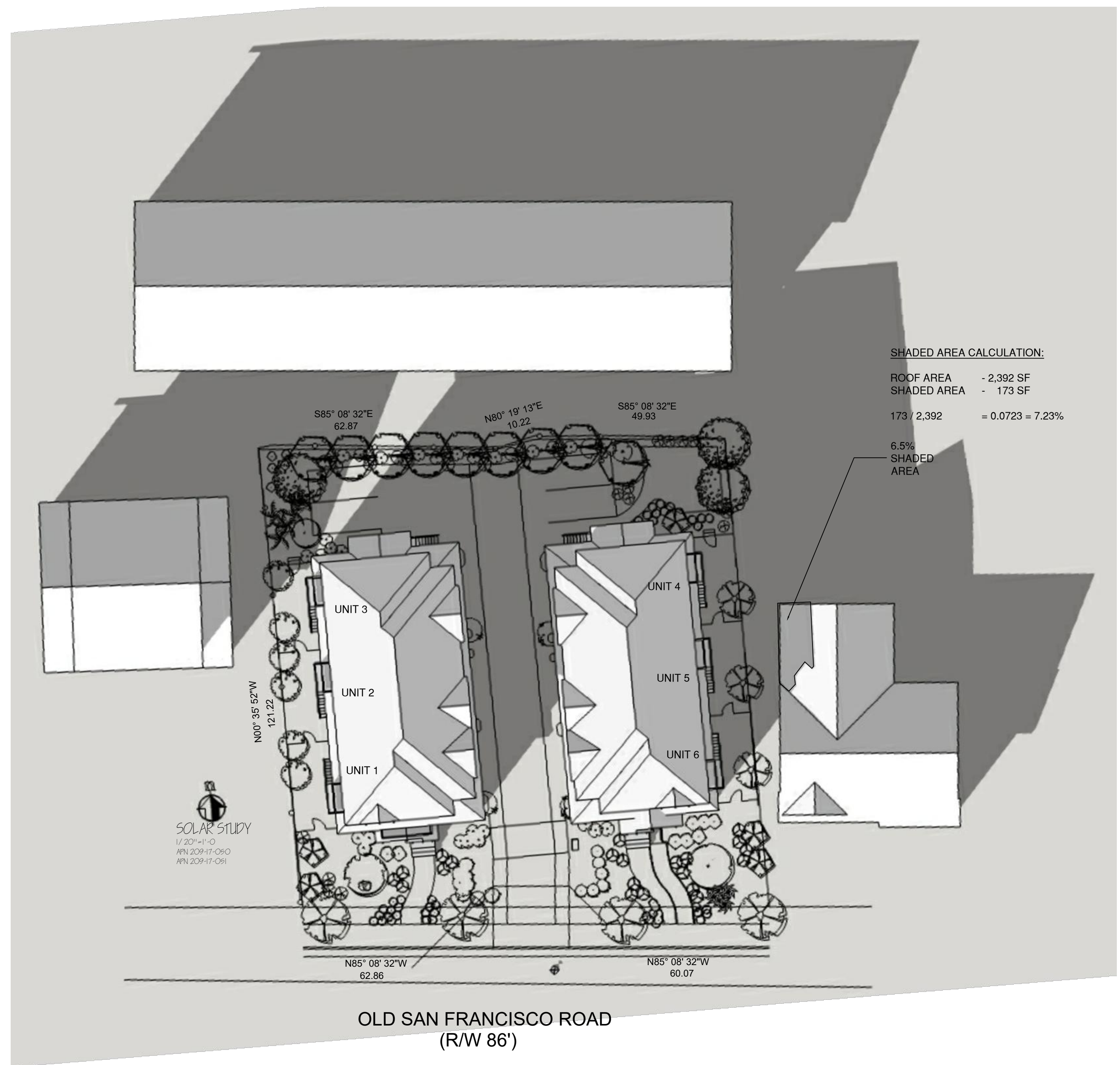
A Major Use Permit/ Special Development Permit for:
George Nejat
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Sunnyvale, CA. 94086

Date	05/08/2018
Scale	
Drawn	
Job	
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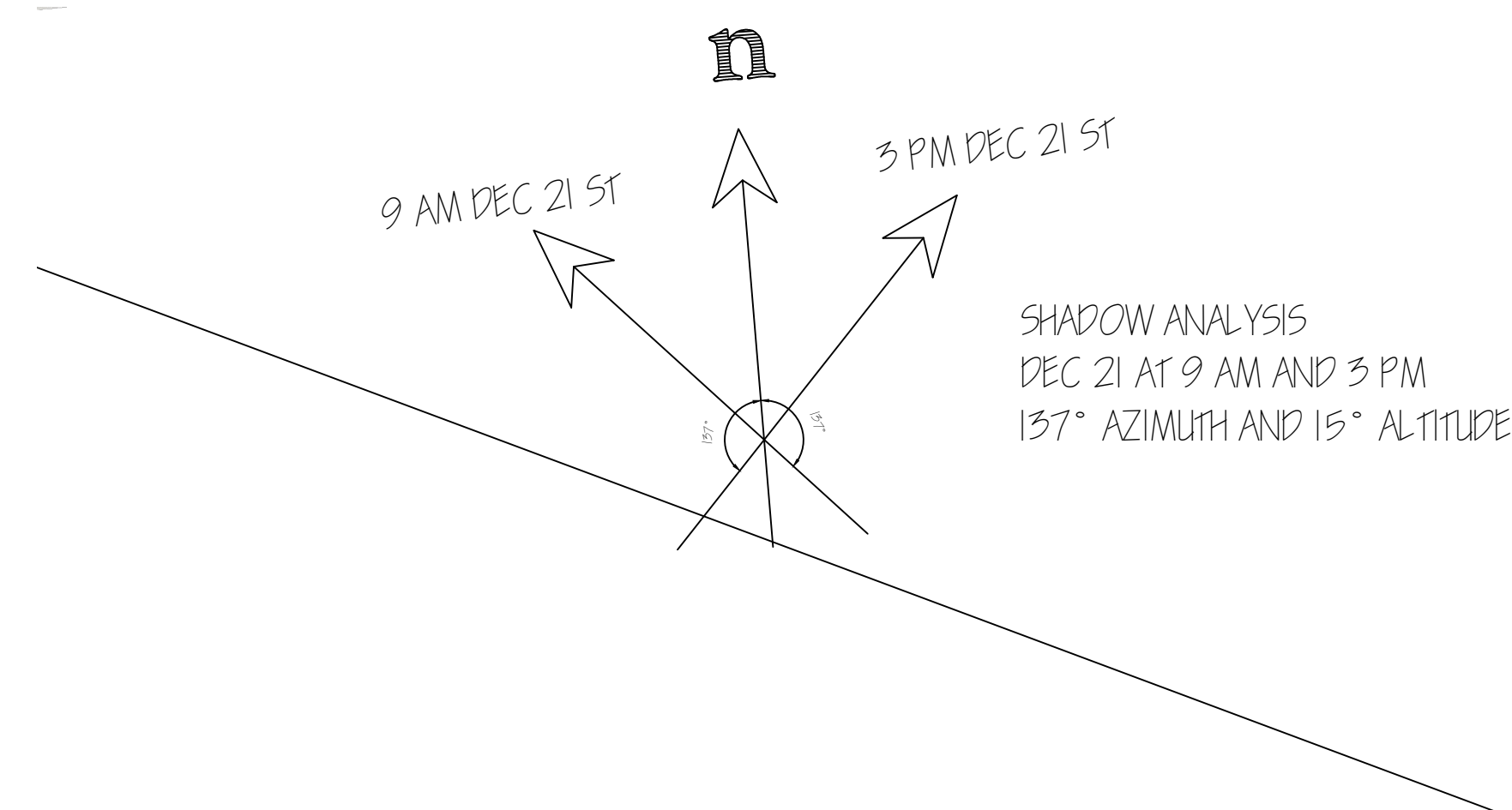
A8
of Sheets



OLD SAN FRANCISCO ROAD
(R/W 86')



OLD SAN FRANCISCO ROAD
(R/W 86')





County of Santa Clara
Office of the County Clerk-Recorder
Business Division

County Government Center
70 West Hedding Street, E. Wing, 1st Floor
San Jose, California 95110 (408) 299-5688



Santa Clara County - Clerk-Recorder Office
State of California

File Number: ENV21611

ENVIRONMENTAL FILING

No. of Pages: 35

Total Fees: \$0.00

File Date: 06/18/2018

Expires: 07/07/2018

CEQA DOCUMENT DECLARATION

ENVIRONMENTAL FILING FEE RECEIPT

PLEASE COMPLETE THE FOLLOWING:

1. LEAD AGENCY: City of Sunnyvale
2. PROJECT TITLE: 669 & 673 Old San Francisco Road - Six Three-Story Townhouses
3. APPLICANT NAME: Innovative Concepts PHONE: 408-985-1078
4. APPLICANT ADDRESS: 3550 Stevens Creek Blvd., Ste. 225, San Jose, CA 95117
5. PROJECT APPLICANT IS A: ☐ Local Public Agency ☐ School District ☐ Other Special District ☐ State Agency ☒ Private Entity
6. NOTICE TO BE POSTED FOR 20 DAYS.
7. **CLASSIFICATION OF ENVIRONMENTAL DOCUMENT**

a. PROJECTS THAT ARE SUBJECT TO DFG FEES

- | | | |
|--|-------------|----------------|
| <input type="checkbox"/> 1. <u>ENVIRONMENTAL IMPACT REPORT</u> (PUBLIC RESOURCES CODE §21152) | \$ 3,168.00 | \$ <u>0.00</u> |
| <input type="checkbox"/> 2. <u>NEGATIVE DECLARATION</u> (PUBLIC RESOURCES CODE §21080(C)) | \$ 2,280.75 | \$ <u>0.00</u> |
| <input type="checkbox"/> 3. <u>APPLICATION FEE WATER DIVERSION</u> (STATE WATER RESOURCES CONTROL BOARD ONLY) | \$ 850.00 | \$ <u>0.00</u> |
| <input type="checkbox"/> 4. <u>PROJECTS SUBJECT TO CERTIFIED REGULATORY PROGRAMS</u> | \$ 1,077.00 | \$ <u>0.00</u> |
| <input type="checkbox"/> 5. <u>COUNTY ADMINISTRATIVE FEE</u> (REQUIRED FOR a-1 THROUGH a-4 ABOVE)
Fish & Game Code §711.4(e) | \$ 50.00 | \$ <u>0.00</u> |

b. PROJECTS THAT ARE EXEMPT FROM DFG FEES

- | | | |
|--|----------|----------------|
| <input type="checkbox"/> 1. NOTICE OF EXEMPTION (\$50.00 COUNTY ADMINISTRATIVE FEE REQUIRED) | \$ 50.00 | \$ <u>0.00</u> |
| <input type="checkbox"/> 2. A COMPLETED "CEQA FILING FEE NO EFFECT DETERMINATION FORM" FROM THE DEPARTMENT OF FISH & GAME, DOCUMENTING THE DFG'S DETERMINATION THAT THE PROJECT WILL HAVE NO EFFECT ON FISH, WILDLIFE AND HABITAT, OR AN OFFICIAL, DATED RECEIPT / PROOF OF PAYMENT SHOWING PREVIOUS PAYMENT OF THE DFG FILING FEE FOR THE "SAME" PROJECT IS ATTACHED (\$50.00 COUNTY ADMINISTRATIVE FEE REQUIRED) | | |
| DOCUMENT TYPE: <input type="checkbox"/> ENVIRONMENTAL IMPACT REPORT <input type="checkbox"/> NEGATIVE DECLARATION | \$ 50.00 | \$ <u>0.00</u> |

c. NOTICES THAT ARE NOT SUBJECT TO DFG FEES OR COUNTY ADMINISTRATIVE FEES

- | | | | |
|--|--|--------|------------------|
| <input type="checkbox"/> NOTICE OF PREPARATION | <input checked="" type="checkbox"/> NOTICE OF INTENT | NO FEE | \$ <u>NO FEE</u> |
|--|--|--------|------------------|

8. OTHER: _____ FEE (IF APPLICABLE): \$ _____

9. TOTAL RECEIVED..... \$ 0.00

*NOTE: "SAME PROJECT" MEANS NO CHANGES. IF THE DOCUMENT SUBMITTED IS NOT THE SAME (OTHER THAN DATES), A "NO EFFECT DETERMINATION" LETTER FROM THE DEPARTMENT OF FISH AND GAME FOR THE SUBSEQUENT FILING OR THE APPROPRIATE FEES ARE REQUIRED.

THIS FORM MUST BE COMPLETED AND ATTACHED TO THE FRONT OF ALL CEQA DOCUMENTS LISTED ABOVE (INCLUDING COPIES) SUBMITTED FOR FILING. WE WILL NEED AN ORIGINAL (WET SIGNATURE) AND TWO (2) COPIES. IF THERE ARE ATTACHMENTS, PLEASE PROVIDE THREE (3) SETS OF ATTACHMENTS FOR SUBMISSION. (YOUR ORIGINAL WILL BE RETURNED TO YOU AT THE TIME OF FILING.)

CHECKS FOR ALL FEES SHOULD BE MADE PAYABLE TO: SANTA CLARA COUNTY CLERK-RECORDER

PLEASE NOTE: FEES ARE ANNUALLY ADJUSTED (Fish & Game Code §711.4(b); PLEASE CHECK WITH THIS OFFICE AND THE DEPARTMENT OF FISH AND GAME FOR THE LATEST FEE INFORMATION.

"... NO PROJECT SHALL BE OPERATIVE, VESTED, OR FINAL, NOR SHALL LOCAL GOVERNMENT PERMITS FOR THE PROJECT BE VALID, UNTIL THE FILING FEES REQUIRED PURSUANT TO THIS SECTION ARE PAID." Fish & Game Code §711.4(c)(3)

(Fees Effective 01-01-2018)



PLANNING DIVISION
CITY OF SUNNYVALE
P.O. BOX 3707
SUNNYVALE, CALIFORNIA 94088-3707

**NOTICE OF INTENT TO ADOPT A
MITIGATED NEGATIVE DECLARATION**

This form is provided as a notification of an intent to adopt a Mitigated Negative Declaration which has been prepared in compliance with the provisions of the California Environmental Quality Act of 1970, as amended, and Resolution #118-04.

PROJECT TITLE:

669 & 673 Old San Francisco Road - Six Three-Story Townhouses

PROJECT DESCRIPTION AND LOCATION (APN):

FILE #: 2018-7048
Location: 669 & 673 Old San Francisco Rd. (APNs 209-17-050 & 209-17-051)
Proposed Project: SPECIAL DEVELOPMENT PERMIT and VESTING TENTATIVE MAP to allow a three-story six-unit townhouse development.
Applicant / Owner: Innovative Concepts / George Nejat
Environmental Review: Mitigated Negative Declaration
Staff Contact: Ryan Kuchenig, (408) 730-7431 rkuchenig@sunnyvale.ca.gov

WHERE TO VIEW THIS DOCUMENT:

The **Mitigated Negative Declaration**, its supporting documentation and details relating to the project are on file and available for review and comment in the Office of the Secretary of the Planning Commission, City Hall, 456 West Olive Avenue, Sunnyvale.

This **Mitigated Negative Declaration** may be protested in writing by any person prior to 5:00 p.m. on July 9, 2018. Protest shall be filed in the Department of Community Development, 456 W. Olive Avenue, Sunnyvale and shall include a written statement specifying anticipated environmental effects which may be significant. A protest of a **Mitigated Negative Declaration** will be considered by the adopting authority, whose action on the protest may be appealed.

HEARING INFORMATION:

A Planning Commission public hearing on the project is scheduled for: Monday, July 9, 2017 at 7:00 p.m. in the Council Chambers, City Hall, 456 West Olive Avenue, Sunnyvale.

TOXIC SITE INFORMATION:

(No) listed toxic sites are present at the project location.

Circulated On June 18, 2018

Signed:

A handwritten signature in cursive script, appearing to read "Gerri Caruso".
Gerri Caruso, Principal Planner

Project Title	Six Unit Development at 669 & 673 Old San Francisco Road
Lead Agency Name and Address	City of Sunnyvale P.O. Box 3707, Sunnyvale, CA 94088-3707
Contact Person	Ryan Kuchenig, Senior Planner
Phone Number	(408) 730-7431
Project Location	669-673 Old San Francisco Road Sunnyvale, CA 94086 (APNs: 209-17-050 and 209-17-051)
Applicant's Name	George Nejat (applicant/owner)
Zoning	R-3/PD (Medium Density Residential / Planned Development)
General Plan	Residential Medium Density
Other Public Agencies whose approval is required	None

BRIEF PROJECT DESCRIPTION

Related applications on a 0.34-acre site:

Special Development Permit for six residential townhome units; and
Vesting Tentative Map to subdivide two lots into six lots plus a common lot.

DETAILED PROJECT DESCRIPTION

Surrounding Uses and Setting: The 0.34-acre project site is located at 669 & 673 Old San Francisco Road. The site is bordered along the north and west by multi-family residential development (condominiums), multi-family residential (apartments) to the south across Old San Francisco Road, and single-family residential to the east. Nearby and further east and west of the site, along Old San Francisco Road, are neighborhood commercial uses.

On-site Development: The project includes full demolition of the existing two single family homes and related accessory structures (built in the 1950s), and construction of six townhomes with site improvements. The new units will be grouped into two three-story buildings; each building comprised of three townhome units. The two buildings will be arranged around a central driveway off Old San Francisco Road that will serve as the only vehicular access to the development. Each unit will have two-car garages. Four guest spaces are proposed at the rear of the site. The common lot will include landscaped areas, uncovered parking and the driveway aisle. Five trees

on-site will be removed while 23 new trees will be added to the site. Three of the trees removed are defined as protected, per Sunnyvale Municipal Code.

A Special Development Permit (SDP) is required for site and architectural review on project sites with the PD designation. The Tentative Map is required prior to a Final Map for the creation of six individual ownership lots and one common lot.

Construction Activities and Schedule: Construction activities include full demolition of all existing buildings and paving on the project site and construction of six townhomes with associated on-site and off-site improvements. The project will be subject to the Sunnyvale Municipal Code requirements for construction noise and hours of construction contained in Chapter 16.08.030.

Construction of the project is estimated to span 18 months. Demolition is anticipated to commence in early 2019. Demolition and site work will span approximately four months. The remaining time will include construction of buildings, on-site improvements and off-site improvements. Construction will not include deep pile foundations or pile driving, jackhammers or other extremely high noise-generating activities or significant vibration.

Off-site Improvements: Existing curb cuts and driveways off Old San Francisco Road will be removed and one new curb cut and driveway will be installed to serve the new homes. New sidewalks and street trees will be installed in the public right-of-way, per City standards. Standard water, sewer, right-of-way and utility upgrades will be provided as required by the Municipal Code.

EVALUATION OF ENVIRONMENTAL IMPACTS:

1. A brief explanation is required for all answers except "No Impact" answers that are adequately supported by the information sources a lead agency cites in the parentheses following each question. A "No Impact" answer is adequately supported if the referenced information sources show that the impact simply does not apply to projects like the one involved (e.g. the project falls outside a fault rupture zone). A "No Impact" answer should be explained where it is based on project-specific factors as well as general standards (e.g. the project will not expose sensitive receptors to pollutants, based on a project-specific screening analysis).
2. All answers must take account of the whole action involved, including off-site as well as on-site, cumulative as well as project-level, indirect as well as direct, and construction as well as operational impacts.
3. Once the lead agency has determined that a particular physical impact may occur, then the checklist answers must indicate whether the impact is potentially significant, less than significant with mitigation, or less than significant. "Potentially Significant Impact" is appropriate if there is substantial evidence that an effect may be significant. If there are one or more "Potentially Significant Impact" entries when the determination is made, an EIR is required.
4. "Negative Declaration: Potentially Significant Unless Mitigation Incorporated" applies where the incorporation of mitigation measures has reduced an effect from "Potentially Significant Impact" to a "Less Significant Impact." The lead agency must describe the mitigation measures, and briefly explain how they reduce the effect to a less than significant level (mitigation measures from Section 17, "Earlier Analysis," may be cross-referenced).
5. Earlier analysis may be used where, pursuant to the tiering, program EIR, or other CEQA process, an effect has been adequately analyzed in an earlier EIR or negative declaration. Section 15063 (c) (3) (d). In this case, a brief discussion should identify the following:
6. Earlier Analysis Used. Identify and state where they are available for review.
7. Impacts Adequately Addressed. Identify which effects from the above checklist were within the scope of and adequately analyzed in an earlier document pursuant to applicable legal standards, and state whether such effects were addressed by mitigation measures based on the earlier analysis.
8. Mitigation Measures. For effects that are "Less than Significant with Mitigation Measures Incorporated," describe the mitigation measures which were incorporated or refined from the earlier document and the extent to which they address site-specific conditions for the project
9. Lead agencies are encouraged to incorporate into the checklist references to information sources for potential impacts (e.g. general plans, zoning ordinances). Reference to a previously prepared or outside document should, where appropriate, include a reference to the page or pages where the statement is substantiated.

ENVIRONMENTAL FACTORS POTENTIALLY AFFECTED:

The environmental factors checked below would be potentially affected by this project, involving at least one impact that is a "Potentially Significant Impact" as indicated by the checklist on the following pages.

- | | | |
|---|--|---|
| <input type="checkbox"/> Aesthetics | <input type="checkbox"/> Hazards & Hazardous Materials | <input type="checkbox"/> Public Services |
| <input type="checkbox"/> Agricultural Resources | <input type="checkbox"/> Hydrology/Water Quality | <input type="checkbox"/> Recreation |
| <input type="checkbox"/> Air Quality | <input type="checkbox"/> Land Use/Planning | <input type="checkbox"/> Transportation/Traffic |
| <input type="checkbox"/> Biological Resources | <input type="checkbox"/> Mineral Resources | <input type="checkbox"/> Utilities/Service Systems |
| <input type="checkbox"/> Cultural Resources | <input type="checkbox"/> Noise | <input type="checkbox"/> Mandatory Findings of Significance |
| <input type="checkbox"/> Geology/Soils | <input type="checkbox"/> Population/Housing | |

MANDATORY FINDINGS OF SIGNIFICANCE (see checklist for further information):

Does the project have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal, or eliminate important examples of the major periods of California history or prehistory?

☐ Yes
☒ No

Mandatory Findings of Significance? Does the project have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of the past projects, the effects of other current projects, and the effects of probable future projects)?

☐ Yes
☒ No

Mandatory Findings of Significance? Does the project have environmental effects which will cause substantial adverse effects on human beings, either directly or indirectly?

☐ Yes
☒ No

DETERMINATION:

On the basis of this initial evaluation:

I find that the proposed project COULD NOT have a significant effect on the environment, and a NEGATIVE DECLARATION will be prepared. ☐

I find that although the proposed project could have a significant effect on the environment, there will not be a significant effect in this case because revisions in the project have been made by or agreed to by the project proponent. A MITIGATED NEGATIVE DECLARATION will be prepared. ☒

I find that the proposed project MAY have a significant effect on the environment, and an ENVIRONMENTAL IMPACT REPORT is required. ☐

I find that the proposed project MAY have a "potential significant impact" or "potentially significant unless mitigated" impact on the environment, but at least one effect (1) has been adequately analyzed in an earlier document pursuant to applicable legal standards, and (2) has been addressed by mitigation measures based on the earlier analysis as described on attached sheets. An ENVIRONMENTAL IMPACT REPORT is required, but it must analyze only the effects that remain to be addressed. ☐

I find that although the proposed project could have a significant effect on the environment, because all potentially significant effects (a) have been analyzed in an earlier EIR or NEGATIVE DECLARATION pursuant to applicable standards and (b) have been avoided or mitigated pursuant to that earlier EIR or NEGATIVE DECLARATION, including revisions or mitigation measures that are imposed upon the proposed project, nothing further is required. ☐

Checklist Prepared By: Ryan Kuchenig

Date: 6/14/2018

Title: Senior Planner

City of Sunnyvale

Signature:

Planning	Potentially Significant Impact	Less than Sig. With Mitigation	Less Than Significant	No Impact	Source Other Than Project Description and Plans
1. Aesthetics -Substantially damage scenic resources, including, but not limited to trees, historic buildings?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Sunnyvale General Plan Map, Land Use and Transportation Chapter 3, and Community Character Chapter 4 of the Sunnyvale General Plan www.sunnyvaleplanning.com
2. Aesthetics -Substantially degrade the existing visual character or quality of the site and its surroundings including significant adverse visual changes to neighborhood character	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Sunnyvale General Plan Map, Land Use and Transportation Chapter 3, and Community Character Chapter 4 of the Sunnyvale General Plan www.sunnyvaleplanning.com
3. Aesthetics -Create a new source of substantial light or glare which would adversely affect day or nighttime views in the area?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Sunnyvale General Plan Map, Land Use and Transportation Chapter 3, and Community Character Chapter 4 of the Sunnyvale General Plan www.sunnyvaleplanning.com
4. Population and Housing - Induce substantial population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of roads or other infrastructure) in a way that is inconsistent with the Sunnyvale General Plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Sunnyvale General Plan Map, and Land Use and Transportation Chapter 3 of the Sunnyvale General Plan www.sunnyvaleplanning.com
5. Population and Housing -Displace substantial numbers of existing housing, necessitating the construction of replacement housing elsewhere?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Land Use and Transportation Chapter 3, and Housing Chapter 5 of the Sunnyvale General Plan and General Plan Map www.sunnyvaleplanning.com
6. Population and Housing -Displace substantial numbers of people, necessitating the construction of replacement housing elsewhere?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Housing Chapter 5 of the Sunnyvale General Plan and General Plan Map www.sunnyvaleplanning.com
7. Land Use Planning - Physically divide an established community?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Sunnyvale General Plan Map www.sunnyvaleplanning.com
8. Land Use Planning conflict - With the Sunnyvale General Plan, Zoning Ordinance, San Francisco Bay Conservation and Development Commission (BCDC) area or related specific plan adopted for the purpose of avoiding or mitigating an environmental effect?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Land Use and Transportation Chapter 3 of the Sunnyvale General Plan, Title 19 (Zoning) of the Sunnyvale Municipal Code http://qcode.us/codes/sunnyvale/view.php?topic=19&frames=off
9. Transportation and Traffic - Result in inadequate parking capacity?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Parking Requirements (Section 19.46) in the Sunnyvale Municipal Code http://qcode.us/codes/sunnyvale/view.php?topic=19-4-19_46&frames=off

Planning	Potentially Significant Impact	Less than Sig. With Mitigation	Less Than Significant	No Impact	Source Other Than Project Description and Plans
10. For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Moffett Field AICUZ , Sunnyvale Zoning Map, Sunnyvale General Plan Map www.sunnyvaleplanning.com
11. For a project within the vicinity of a private airstrip, would the project result in a safety hazard for people residing or working in the project area?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	There are no private airstrips in or in the vicinity of Sunnyvale
12. For a project within the vicinity of Moffett Federal Airfield, would the project result in a safety hazard for people residing or working in the project area?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Moffett Field AICUZ
13. Agricultural Resources - Conflict with existing zoning for agricultural use, or a Williamson Act contract?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Sunnyvale Zoning Map www.sunnyvaleplanning.com
14. Noise - Exposure of persons to or generation of noise levels in excess of standards established in the Noise Sub-Element, Noise limits in the Sunnyvale Municipal Code, or applicable standards of the California Building Code?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Safety and Noise Chapter of the Sunnyvale General Plan, SMC www.sunnyvaleplanning.com 19.42 Noise Ordinance http://qcode.us/codes/sunnyvale/view.php?topic=19&frames=off
15. Noise - Exposure of persons to or generation of excessive ground borne vibration?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Safety and Noise Chapter of the Sunnyvale General Plan www.sunnyvaleplanning.com Project Description Project Construction Schedule
16. Noise - A substantial permanent or periodic increase in ambient noise levels in the project vicinity above levels existing without the project?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Safety and Noise Chapter of the Sunnyvale General Plan www.sunnyvaleplanning.com Project Description
17. Biological Resources - Have a substantially adverse impact on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations, or by the California Department of Fish and Game or U.S Wildlife Service?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Environmental Management Chapter 7 of the Sunnyvale General Plan www.sunnyvaleplanning.com

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18. Biological Resources -Have a substantial adverse effect on federally protected wetlands as defined by Section 404 of the Clean Water Act (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Environmental Management Chapter 7 of the Sunnyvale General Plan www.sunnyvaleplanning.com Project Description
19. Biological Resources -Interfere substantially with the movement of any resident or migratory fish or wildlife species or with established native resident migratory wildlife corridors, or impede the use of native wildlife nursery sites?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Environmental Management Chapter 7 of the Sunnyvale General Plan www.sunnyvaleplanning.com Project Description
20. Biological Resources -Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	SMC 19.90 Tree Preservation Ordinance Sunnyvale Inventory of Heritage Trees Tree Inventory Report by Anderson's Tree Care Specialists, Inc., dated 3/10/15
21. Biological Resources -Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Conservation Community Plan, other approved local, regional, or state habitat conservation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Land Use and Transportation Chapter 3 of the Sunnyvale General Plan and General Plan Map www.sunnyvaleplanning.com
22. Historic and Cultural Resources - Cause a substantial adverse change in the significance of a historical resource or a substantial adverse change in an archeological resource?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Community Character Chapter 4 of the Sunnyvale General Plan, Sunnyvale Inventory of Heritage Resources The United States Secretary of the Interior's "Guidelines for Rehabilitation" Criteria of the National Register of Historic Places
23. Historic and Cultural Resources - Disturb any human remains, including those interred outside of formal cemeteries?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	California Health and Safety Code Section 7050.5(b), CEQA Guidelines Section 15064.5(e) Project description CHRIS/NWIC Letter dated 1/21/15
24. Public Services - Would the project result in substantial adverse physical impacts associated with the provision of new or expanded public schools, the construction of which could cause significant environmental impacts, in	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	The following public school districts are located in the City of Sunnyvale: Fremont Union High School District, Sunnyvale Elementary School District, Cupertino Union School

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order to maintain acceptable performance objectives?					District and Santa Clara Unified School District.
25. Air Quality - Conflict with or obstruct implementation of the BAAQMD air quality plan? How close is the use to a major road, hwy. or freeway?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	BAAQMD CEQA Guidelines 1999 Thresholds Sunnyvale General Plan Map Sunnyvale Air Quality Sub-Element www.sunnyvaleplanning.com
26. Air Quality - Would the project generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	BAAQMD CEQA Guidelines 1999 Thresholds AB 32
27. Air Quality -Would the project conflict with any applicable plan, policy or regulation of any agency adopted for the purpose of reducing the emissions of greenhouse gases?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	BAAQMD CEQA Guidelines 1999 Thresholds Sunnyvale Climate Action Plan 2014 AB 32
28. Air Quality -Violate any air quality standard or contribute substantially to an existing or projected air quality violation.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	BAAQMD CEQA Guidelines 1999 Thresholds Sunnyvale General Plan Map Environmental Management Chapter 7 of the Sunnyvale General Plan www.sunnyvaleplanning.com
29. Air Quality -Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard (including releasing emissions which exceed quantitative thresholds for ozone precursors)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	BAAQMD CEQA Guidelines 1999 Thresholds Sunnyvale General Plan Map Environmental Management Chapter 7 of the Sunnyvale General Plan www.sunnyvaleplanning.com
30. Air Quality -Expose sensitive receptors to substantial pollutant concentrations?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	BAAQMD CEQA Guidelines 1999 Thresholds Sunnyvale General Plan Map Environmental Management Chapter 7 of the Sunnyvale General Plan www.sunnyvaleplanning.com
31. Seismic Safety -Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Safety and Noise Chapter 6 of the Sunnyvale General Plan www.sunnyvaleplanning.com
32. Seismic Safety - Inundation by seiche, tsunami, or mudflow?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Safety and Noise Chapter 6 of the Sunnyvale General Plan www.sunnyvaleplanning.com

Planning	Potentially Significant Impact	Less than Sig. With Mitigation	Less Than Significant	No Impact	Source Other Than Project Description and Plans
33. Seismic Safety-Strong seismic ground shaking?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Safety and Noise Chapter 6 of the Sunnyvale General Plan www.sunnyvaleplanning.com
34. Seismic Safety-Seismic-related ground failure, including liquefaction?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Safety and Noise Chapter 6 of the Sunnyvale General Plan www.sunnyvaleplanning.com

Further Discussion if “Less Than Significant” with or without mitigation:

2. Aesthetics – Visual Character (Less than Significant) – Construction of the project will alter the visual character of the site. The current buildings are one-story single family residential buildings, which will be demolished to construct two three-story buildings with a maximum height of 30 ft. The proposed buildings are consistent in height to nearby development. The design and site layout are in general conformance with the Citywide Design Guidelines. The project architectural style is contemporary. The design of the project is consistent with the contemporary and traditional architectural styles found in the area. The City’s implementation of the City-Wide Design Guidelines and staff’s review of final development plans, which will be submitted for final Building Permit review, will ensure that the final design of the project is consistent with the plans reviewed by the Planning Commission. The project will not substantially degrade the existing visual character or quality of the site and its surroundings. Therefore, impacts would be less than significant.

4. Population and Housing (Less than Significant) – The General Plan designation for this site is Residential Medium Density and the zoning is R03/PD (Medium Density Residential / Planned Development). The six-unit project results in a project density of 18 units per acre, which is less than the maximum 24 unit per acre for R-3 Zoning District. This density would meet the Council policy of at least 75% of the maximum density allowed on a site. The project density is consistent with the densities of the residential developments in the vicinity. Therefore, the impact is less than significant.

9. Transportation and Traffic – Parking (No Impact) – Based on the two covered spaces (within individual garages) and three bedrooms for four units and four bedrooms for two units, a minimum of four guest parking spaces are required to be provided, per Sunnyvale Municipal Code (SMC) Section 19.46. The project complies with the parking requirement by providing 12 covered spaces and four guest parking spaces. All new residential construction will be required to pre-wire 12.5% of the open parking spaces and all the garage spaces are required to allow for a future Level 2 electric car charger. Therefore, no impact is expected.

20. Biological Resources (Less than Significant) – A Tree Inventory Report was prepared by Anderson’s Tree Care Specialists, Inc., dated December 1, 2017. A total of 20 trees were included in the report: however, 15 of these trees are located on neighboring properties (near the subject site). The five trees located on the site are in “poor” to “very poor” condition. Each of these trees are located towards the rear of the site. No City street trees currently exist

Of the five trees, located on the subject site, three are considered “protected” under Sunnyvale Municipal Code Chapter 19.94. A protected tree is defined as having a trunk size of at least 38 inches in circumference, as measured 4.5 feet from the ground. Each of the five trees will be removed due to their poor condition. Most of the trees are also located within the proposed building footprints or circulation areas. The project will add approximately 23 trees to the site, which will mitigate the loss of the existing trees in accordance with Sunnyvale Municipal Code Section 19.94, to a less than significant level.

23. Historic and Cultural Resources (Less than Significant with Mitigation) – The project construction will include grading and land disturbance. A records search by the California Historical Resources Information System/Northwest Information Center of Sonoma State University (CHRIS/NWIC) was conducted for the project area in January 21, 2015. Review of the obtained information indicates that there have been no cultural resources studies conducted within the project area. The project area contains no recorded archaeological resources. The State Office of Historic Preservation Historic Property Directory (OHP HPD) (which includes listings of the California Register of Historical Resources, California State Historical Landmarks, California State Points of Historical Interest, and the National Register of Historic Places) lists no recorded buildings or structures within the proposed project area. In addition to these inventories, the NWIC base maps show no recorded buildings or structures within the proposed project area. In addition to these inventories, the NWIC base maps show no recorded buildings or structures within the proposed project area.

Based on an evaluation of the environmental setting and features associated with known sites, there is a moderate to high potential of unrecorded Native American resources in the proposed project area. Review of historical literature and maps gave no indication of the possibility of historic-period archaeological resources within the proposed project area, but there is a low potential of unrecorded historic-period archaeological resources.

The following mitigations are recommended to reduce the potential impact to less than significant level:

MITIGATION

WHAT:

- (1) A qualified archaeologist shall conduct further archival and field study to identify cultural resources. Field study may include, but is not limited to, pedestrian survey, hand auger sampling, shovel test units, or geoarchaeological analyses as well as other common methods used to identify the presence of archaeological resources. Please refer to the list of consultants who meet the Secretary of Interior's Standards at <http://www.chrisinfo.org>.
- (2) If archaeological resources are encountered during construction, work shall be temporarily halted in the vicinity of the discovered materials and workers shall not alter the materials and their context until a qualified professional archaeologist has evaluated the situation and provided appropriate recommendations. Project personnel shall not collect cultural resources. Native American resources include chert or obsidian flakes, projectile points, mortars, and pestles; and dark friable soil containing shell and bone dietary debris, heat-affected rock, or human burials. Historic-period resources include stone or adobe foundations or walls; structures and remains with square nails; and refuse deposits or bottle dumps, often located in old wells or privies.
- (3) Any identified cultural resources shall be recorded on DPR 523 historic resource recordation forms.

WHEN: These mitigations shall be incorporated into conditions of approval for the Special Development Permit (SDP) prior to its final approval by the City Council. The conditions will become valid when the SDP is approved and prior to building permit issuance.

WHO: The project applicant or property owner shall be solely responsible for implementation and maintenance of these mitigation measures.

HOW: The conditions of approval will require these mitigation measures to be incorporated into the construction plans.

24. Public Services – Schools (Less than Significant) – The project site is located within the Santa Clara School District and Santa Clara Unified High School District. In both districts, all new residential developments

are required to fully offset their anticipated impact on demand for schools by paying a school impact fee as set by the Districts. The City requires evidence of school impact fee payment prior to issuance of building permits. Therefore, impacts on public schools will be less than significant.

27. Air Quality – Greenhouse Gases (No Impact) – A Sunnyvale “Climate Action Plan CEQA Checklist” was completed for the project, which provides further analysis related to project greenhouse gas emissions. The Checklist demonstrates conformance with the City's adopted Climate Action Plan; therefore, the project is not expected to have an impact on greenhouse gas emissions.

Responsible Division: Planning Division

Completed by: Ryan Kuchenig Date: 6/14/2018

Transportation	Potentially Significant Impact	Less than Sig. With Mitigation	Less Than Significant	No Impact	Source Other Than Project Description and Plans
35. Exceeds the capacity of the existing circulation system, based on an applicable measure of effectiveness (as designated in a general plan policy, ordinance, etc.), taking into account all modes of transportation including nonmotorized travel and all relevant components of the circulation system, including but not limited to intersections, streets, highways and freeways, pedestrian walkways, bicycle paths, and mass transit?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Land Use and Transportation Chapter 3 of the Sunnyvale General Plan www.sunnyvaleplanning.com Santa Clara Valley Transportation Authority Congestion Management Program http://www.vta.org/cmp/
36. Conflict with an applicable congestion management program, including, but not limited to level of service standards and travel demand measurements, or other standards established by the county congestion management agency for designated roads or highways?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Santa Clara Valley Transportation Authority Congestion Management Program http://www.vta.org/cmp/
37. Results in a change in air traffic patterns, including either an increase in air traffic levels or a change in flight patterns or location that results in substantial safety risks to vehicles, bicycles, or pedestrians?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Land Use and Transportation Chapter 3 of the Sunnyvale General Plan www.sunnyvaleplanning.com Project Description
38. Substantially increase hazards to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g. farm equipment)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Land Use and Transportation Chapter 3 of the Sunnyvale General Plan www.sunnyvaleplanning.com Project Description
39. Conflict with adopted policies, plans, or programs regarding public transit or nonmotorized transportation?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Land Use and Transportation Chapter 3 of the Sunnyvale General Plan www.sunnyvaleplanning.com Santa Clara Valley Transportation Authority Congestion Management Program http://www.vta.org/cmp/
40. Affect the multi-modal performance of the highway and/or street and/or rail and/or off road nonmotorized trail transportation facilities, in terms of structural, operational, or perception-based measures of effectiveness (e.g. quality of service for nonmotorized and transit modes)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Land Use and Transportation Chapter 3 of the Sunnyvale General Plan www.sunnyvaleplanning.com Santa Clara Valley Transportation Authority Congestion Management Program http://www.vta.org/cmp/

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Transportation	Potentially Significant Impact	Less than Sig. With Mitigation	Less Than Significant	No Impact	Source Other Than Project Description and Plans
41. Reduce, sever, or eliminate pedestrian or bicycle circulation or access, or preclude future planned and approved bicycle or pedestrian circulation?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Land Use and Transportation Chapter 3 of the Sunnyvale General Plan www.sunnyvaleplanning.com Santa Clara Valley Transportation Authority Congestion Management Program http://www.vta.org/cmp/
42. Cause a degradation of the performance or availability of all transit including buses, light or heavy rail for people or goods movement?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Land Use and Transportation Chapter 3 of the Sunnyvale General Plan www.sunnyvaleplanning.com Santa Clara Valley Transportation Authority Congestion Management Program http://www.vta.org/cmp/

Further Discussion if “Less Than Significant” with or without mitigation: None required.

Responsible Division: Planning Division

Completed by: Ryan Kuchenig

Date: 6/14/2018

Building	Potentially Significant Impact	Less than Sig. With Mitigation	Less Than Significant	No Impact	Source Other Than Project Description and Plans
43. Hydrology and Water Quality - Place housing within a 100-year floodplain, as mapped on a federal Flood Hazard Boundary or Flood Insurance Rate Map or other flood hazard delineation map?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	FEMA Flood Insurance Rate Map Effective 5/18/09 www.sunnyvaleplanning.com , California Building Code, Title 16 (Building) of the Sunnyvale Municipal Code
44. Hydrology and Water Quality - Place within a 100-year flood hazard area structures which would impede or redirect flood flows?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	FEMA Flood Insurance Rate Map Effective 5/18/09 www.sunnyvaleplanning.com , California Building Code, Title 16 (Building) of the Sunnyvale Municipal Code
45. Hydrology and Water Quality - Expose people or structures to a significant risk of loss, injury or death involving flooding, including flooding as a result of the failure of a levee or dam?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	1995 ABAG Dam Inundation Map www.abag.ca.gov , California Building Code, Title 16 (Building) of the Sunnyvale Municipal Code
46. Geology and Soils -Result in substantial soil erosion or the loss of topsoil?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Sunnyvale Municipal Code 12.60, Storm Water Quality Best Sunnyvale Management Practices Guideline Manual
47. Geology and Soils -Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction or collapse?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Safety and Noise Chapter of the Sunnyvale General Plan, www.sunnyvaleplanning.com California Plumbing, Mechanical, and Electrical Codes and Title 16 (Building) of the Sunnyvale Municipal Code
48. Geology and Soils -Be located on expansive soil, as defined by the current building code, creating substantial risks to life or property?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	California Plumbing, Mechanical, and Electrical Codes and Title 16 (Building) of the Sunnyvale Municipal Code

Further Discussion if “Less Than Significant” with or without mitigation: None required.

General Discussion: The California Building Code contains a series of building code requirements to address safety issues regarding seismic shaking, flooding, and soil types. In addition, Title 16.62 of the Sunnyvale Municipal Code requires a series of measures for provisions to reduce flood-related hazards to buildings. These standards are suggested by the Federal Emergency Management Agency and required by code by the City of Sunnyvale. These standards must be met for a building permit to be issued.

Responsible Division: Planning Division

Completed by: Ryan Kuchenig

Date: 6/14/2018

Engineering	Potentially Significant Impact	Less than Sig. With Mitigation	Less Than Significant	No Impact	Source Other Than Project Description and Plans
49. Utilities and Service Systems: Exceed wastewater treatment requirements of the applicable Regional Water Quality Control Board?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Environmental Management Chapter 7 of the Sunnyvale General Plan www.sunnyvaleplanning.com
50. Utilities and Service Systems: Require or result in construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Environmental Management Chapter 7 of the Sunnyvale General Plan www.sunnyvaleplanning.com
51. Utilities and Service Systems: Require or result in the construction of new storm water drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Environmental Management Chapter 7 of the Sunnyvale General Plan www.sunnyvaleplanning.com
52. Utilities and Service Systems: Have sufficient water supplies available to serve the project from existing entitlements and resources, or are new or expanded entitlements needed?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Environmental Management Chapter 7 of the Sunnyvale General Plan www.sunnyvaleplanning.com
53. Utilities and Service Systems: Result in a determination by the wastewater treatment provider which services or may serve the project determined that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Environmental Management Chapter 7 of the Sunnyvale General Plan www.sunnyvaleplanning.com
54. Utilities and Service Systems: Be served by a landfill with sufficient permitted capacity to accommodate the project's solid waste disposal needs?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Environmental Management Chapter 7 of the Sunnyvale General Plan www.sunnyvaleplanning.com
55. Hydrology and Water Quality - Violate any water quality standards or waste discharge requirements?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Regional Water Quality Control Board (RWQCB) Region 2 Municipal Regional Permit http://www.waterboards.ca.gov/

Engineering	Potentially Significant Impact	Less than Sig. With Mitigation	Less Than Significant	No Impact	Source Other Than Project Description and Plans
56. Hydrology and Water Quality - Substantially degrade groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or a lowering of the local groundwater table level (e.g., the production rate of pre-existing nearby wells would drop to a level which would not support existing land uses or planned uses for which permits have been granted)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	RWQCB, Region 2 Municipal Regional Permit http://www.waterboards.ca.gov/ City of Sunnyvale Stormwater Quality Best Management Practices (BMP) Guidance Manual for New and Redevelopment Projects www.sunnyvaleplanning.com
57. Hydrology and Water Quality - Substantially degrade groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or a lowering of the local groundwater table level (e.g., the production rate of pre-existing nearby wells would drop to a level which would not support existing land uses or planned uses for which permits have been granted)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	RWQCB, Region 2 Municipal Regional Permit http://www.waterboards.ca.gov/ City of Sunnyvale Stormwater Quality Best Management Practices (BMP) Guidance Manual for New and Redevelopment Projects www.sunnyvaleplanning.com
58. Hydrology and Water Quality - Otherwise substantially degrade water quality?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Santa Clara Valley Water District Groundwater Protection Ordinance www.valleywater.org
59. Hydrology and Water Quality - Create or contribute runoff which would exceed the capacity of existing or planned storm water drainage systems in a manner which could create flooding or provide substantial additional sources of polluted runoff?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	RWQCB, Region 2 Municipal Regional Permit http://www.waterboards.ca.gov/ City of Sunnyvale Stormwater Quality Best Management Practices (BMP) Guidance Manual for New and Redevelopment Projects www.sunnyvaleplanning.com
60. Hydrology and Water Quality - Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	RWQCB, Region 2 Municipal Regional Permit http://www.waterboards.ca.gov/ City of Sunnyvale Stormwater Quality Best Management Practices (BMP) Guidance Manual for New and Redevelopment Projects www.sunnyvaleplanning.com
61. Utilities and Service Systems: Comply with federal, state, and local statutes and regulations related to solid waste?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Environmental Management Chapter 7 of the Sunnyvale General Plan www.sunnyvaleplanning.com

Initial Study
Project Name: Six Townhomes at 669-673 Old San Francisco Road
File #2018-7048
Page 18 of 25

Engineering	Potentially Significant Impact	Less than Sig. With Mitigation	Less Than Significant	No Impact	Source Other Than Project Description and Plans
62. Public Services Infrastructure? Would the project result in substantial adverse physical impacts associated with the provision of new or physically altered government facilities, need for new or physically altered government facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Environmental Management Chapter 7 of the Sunnyvale General Plan www.sunnyvaleplanning.com

Further Discussion if "Less Than Significant" with or without mitigation: None required.

Responsible Division: Planning Division

Completed by: Ryan Kuchenig

Date: 6/14/2018

Public Safety	Potentially Significant Impact	Less than Sig. With Mitigation	Less Than Significant	No Impact	Source Other Than Project Description and Plans
63. Public Services Police and Fire protection - Would the project result in substantial adverse physical impacts associated with the provision of new or physically altered government facilities, need for new or physically altered government facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Safety and Noise Chapter 6 of the Sunnyvale General Plan www.sunnyvaleplanning.com
64. Public Services Police and Fire protection - Would the project result in inadequate emergency access?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	California Building Code SMC Section 16.52 Fire Code

Further Discussion if "Less Than Significant" with or without mitigation: None required.

Responsible Division: Planning Division

Completed by: Ryan Kuchenig

Date: 6/14/2018

Public Safety – Hazardous Materials	Potentially Significant Impact	Less than Sig. With Mitigation	Less Than Significant	No Impact	Source Other Than Project Description and Plans
65. Hazards and Hazardous Materials - Create a significant hazard to the public or the environment through the routine transport, use or disposal of hazardous materials?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Safety and Noise Chapter 6 of the Sunnyvale General Plan, www.sunnyvaleplanning.com
66. Hazards and Hazardous Materials - Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the likely release of hazardous materials into the environment?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Safety and Noise Chapter 6 of the Sunnyvale General Plan, www.sunnyvaleplanning.com Title 20 of the City of Sunnyvale Municipal Code Phase I Environmental Site Assessment prepared by AEI Consultants dated 5/18/15 and Soil Sampling Investigation Report performed by Technicon Engineering Services, Inc., dated 10/16/15
67. Hazards and Hazardous Materials - Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Sunnyvale Zoning Map www.sunnyvaleplanning.com
68. Hazards and Hazardous Materials - Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result would it create a significant hazard to the public or the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	State of California Hazardous Waste and Substances Site List (Cortese List), Department of Toxic Substance Control
69. Hazards and Hazardous Materials - Impair implementation of, or physically interfere with an adopted emergency response plan or emergency evacuation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Safety and Noise Chapter 6 of the Sunnyvale General Plan www.sunnyvaleplanning.com

Further Discussion if “Less Than Significant” with or without mitigation:

66. Hazards and Hazardous Materials (Less than Significant with Mitigation) – A Phase I Environmental Site Assessment was prepared by AEI Consultants, dated May 18, 2015. A soil sampling investigation report was also prepared by Technicon Engineering Services on October 16, 2015 as well as a letter confirming the validity of the earlier report on December 5, 2017. These studies are available for review at the City of Sunnyvale’s Community Development Department, Monday through Friday between 8 a.m. and 5 p.m.

Based on a review of aerial photographs, the subject property was historically used for agricultural purposes. The Phase 1 Site Assessment concluded that there is potential that agricultural chemicals, such as pesticides, herbicides and fertilizers, were used on site, and that the subject property has been impacted by the use of such agricultural chemicals. The report from Technicon includes an investigation of potential environmentally

persistent pesticide residues present in the site soils.

According to the laboratory reports, only trace concentrations of DDE and DDT were detected in three of the four soil samples collected during this investigation. Although the arsenic concentrations in the soil were above the CHHSL value, the concentrations were within the range of typical ambient concentrations at agricultural sites of 0 to 12 mg/kg that has been accepted by the DTSC (Department of Toxic Substances Control). Based on the results of this investigation, it is Technicon's opinion that elevated levels of environmentally persistent agricultural chemicals are not present at the subject site.

The Phase I study noted that the existing building materials may contain asbestos or lead based paint. Prior to demolition of the building an asbestos and lead based paint survey would be conducted by a qualified licensed professional and disposed of appropriately.

MITIGATION - Hazardous Materials Lead Based Paint and Asbestos

WHAT: Hazardous building materials surveys shall be conducted by a qualified and licensed professional for all structures. All loose and peeling lead-based paint and asbestos-containing material shall be abated by certified contractor(s) in accordance with local, state, and federal requirements. All other hazardous materials shall be removed from buildings prior to demolition in accordance with California Department of Industrial Relations, Division of Occupational Safety and Health regulations. The completion of the abatement activities shall be documented by a qualified environmental professional(s) and submitted to the City for review with applications for issuance of construction and demolition permits.

WHEN: These mitigations shall be incorporated into conditions of approval for the Special Development Permit (SDP) prior to its final approval by the City Council. The conditions will become valid when the SDP is approved and prior to building permit issuance.

WHO: The project applicant or property owner shall be solely responsible for implementation and maintenance of these mitigation measures.

HOW: The conditions of approval will require these mitigation measures to be incorporated into the construction plans.

Responsible Division: Planning Division

Completed by: Ryan Kuchenig

Date: 6/14/2018

Community Services	Potentially Significant Impact	Less than Sig. With Mitigation	Less Than Significant	No Impact	Source Other Than Project Description and Plans
70. Public Services Parks? Would the project result in substantial adverse physical impacts associated with the provision of new or physically altered government facilities, need for new or physically altered government facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Land Use and Transportation Chapter of the Sunnyvale General Plan, Community Character Chapter of the Sunnyvale General Plan www.sunnyvaleplanning.com
71. Recreation - Would the project increase the use of existing neighborhood or regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Land Use and Transportation Chapter of the Sunnyvale General Plan, Community Character Chapter of the Sunnyvale General Plan www.sunnyvaleplanning.com
72. Recreation - Does the project include recreational facilities or require the construction or expansion of recreational facilities which might have an adverse physical effect on the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Land Use and Transportation Chapter of the Sunnyvale General Plan, Community Character Chapter of the Sunnyvale General Plan www.sunnyvaleplanning.com

Further Discussion if "Less Than Significant" with or without mitigation: None required.

Responsible Division: Planning Division

Completed by: Ryan Kuchenig

Date: 6/14/2018

City of Sunnyvale General Plan:

Sunnyvale General Plan Consolidated in (2011)
generalplan.lnSunnyvale.com

- Community Vision
- Land Use and Transportation
- Community Character
- Housing
- Safety and Noise
- Environmental Management
- Appendix A: Implementation Plans

City of Sunnyvale Climate Action Plan 2014

City of Sunnyvale Municipal Code:

- Title 8 Health and Sanitation
- Title 9 Public Peace, Safety or Welfare
- Title 10 Vehicles and Traffic
- Title 12 Water and Sewers
- Chapter 12.60 Storm Water Management
- Title 13 Streets and Sidewalks
- Title 16 Buildings and Construction
 - Chapter 16.52 Fire Code
 - Chapter 16.54 Building Standards for Buildings Exceeding Seventy –Five Feet in Height
- Title 18 Subdivisions
- Title 19 Zoning
 - Chapter 19.28 Downtown Specific Plan District
 - Chapter 19.29 Moffett Park Specific Plan District
 - Chapter 19.39 Green Building Regulations
 - Chapter 19.42 Operating Standards
 - Chapter 19.54 Wireless Telecommunication Facilities
 - Chapter 19.81 Streamside Development Review
 - Chapter 19.96 Heritage Preservation
- Title 20 Hazardous Materials

Specific Plans:

- Downtown Specific Plan
- El Camino Real Precise Plan
- Lockheed Site Master Use Permit
- Moffett Park Specific Plan
- 101 & Lawrence Site Specific Plan
- Southern Pacific Corridor Plan
- Lakeside Specific Plan
- Arques Campus Specific Plan

Environmental Impact Reports:

- Futures Study Environmental Impact Report
- Lockheed Site Master Use Permit Environmental Impact Report

- Tasman Corridor LRT Environmental Impact Study (supplemental)
- Kaiser Permanente Medical Center Replacement Center Environmental Impact Report (City of Santa Clara)
- Downtown Development Program Environmental Impact Report
- Caribbean-Moffett Park Environmental Impact Report
- Southern Pacific Corridor Plan Environmental Impact Report
- East Sunnyvale ITR General Plan Amendment EIR
- Palo Alto Medical Foundation Medical Clinic Project EIR
- Luminaire (Lawrence Station Road/Hwy 237 residential) EIR
- NASA Ames Development Plan Programmatic EIS
- Mary Avenue Overpass EIR
- Mathilda Avenue Bridge EIR

Maps:

- General Plan Map
- Zoning Map
- City of Sunnyvale Aerial Maps
- Flood Insurance Rate Maps (FEMA)
- Santa Clara County Assessor's Parcel
- Utility Maps
- Air Installations Compatible Use Zones (AICUZ) Study Map
- 2010 Noise Conditions Map

Legislation / Acts / Bills / Resource Agency Codes and Permits:

- Subdivision Map Act
- Municipal Regional Stormwater NPDES Permit
- Santa Clara County Valley Water District Groundwater Protection Ordinance
- Section 404 of Clean Water Act

Lists / Inventories:

- Sunnyvale Cultural Resources Inventory List
- Heritage Landmark Designation List
- Santa Clara County Heritage Resource Inventory
- Hazardous Waste & Substances Sites List (State of California)
- List of Known Contaminants in Sunnyvale
- USFWS / CA Dept. F&G Endangered and Threatened Animals of California
<http://www.dfg.ca.gov/biogeodata/cnddb/pdfs/TEAnimals.pdf>
- The Leaking Underground Petroleum Storage Tank List www.geotracker.waterboards.ca.gov

- The Federal EPA Superfund List
www.epa.gov/region9/cleanup/california.html
- The Hazardous Waste and Substance Site List
www.dtsc.ca.gov/SiteCleanup/Cortese_List.cfm

Guidelines and Best Management Practices

- Storm Water Quality Best Management Practices Guidelines Manual 2007
- Sunnyvale Citywide Design Guidelines
- Sunnyvale Industrial Guidelines
- Sunnyvale Single-Family Design Techniques
- Sunnyvale Eichler Guidelines
- Blueprint for a Clean Bay
- Santa Clara Valley Water District (SCVWD) Guidelines and Standards for Land Use Near Streams
- The United States Secretary of the Interior 's Guidelines for Rehabilitation
- Criteria of the National Register of Historic Places

Transportation:

- California Department of Transportation Highway Design Manual
- California Department of Transportation Traffic Manual
- California Department of Transportation Standard Plans & Standard Specifications
- Highway Capacity Manual
- Institute of Transportation Engineers - Trip Generation Manual & Trip Generation Handbook
- Institute of Transportation Engineers - Traffic Engineering Handbook
- Institute of Transportation Engineers - Manual of Traffic Engineering Studies
- Institute of Transportation Engineers - Transportation Planning Handbook
- Institute of Transportation Engineers - Manual of Traffic Signal Design
- Institute of Transportation Engineers - Transportation and Land Development
- U.S. Dept. of Transportation Federal Highway Administration Manual on Uniform Traffic Control Devices for Street and Highways & CA Supplements
- California Vehicle Code
- Santa Clara County Congestion Management Program and Technical Guidelines
- Santa Clara County Transportation Agency Short Range Transit Plan
- Santa Clara County Transportation Plan
- Traffic Volume Studies, City of Sunnyvale Public works Department of Traffic Engineering Division

- Statewide Integrated Traffic Records System
- Sunnyvale Zoning Ordinance – including Titles 10 & 13
- City of Sunnyvale General Plan – land Use and Transportation Element
- City of Sunnyvale Bicycle Plan
- City of Sunnyvale Neighborhood Traffic Calming Program
- Valley Transportation Authority Bicycle Technical Guidelines
- Valley Transportation Authority Community Design & Transportation – Manual of Best Practices for Integrating Transportation and Land Use
- Santa Clara County Sub-Regional Deficiency Plan
- City of Sunnyvale Deficiency Plan
- AASHTO: A Policy on Geometric Design of Highways and Streets

Public Works:

- Standard Specifications and Details of the Department of Public Works
- Storm Drain Master Plan
- Sanitary Sewer Master Plan
- Water Master Plan
- Solid Waste Management Plan of Santa Clara County
- Geotechnical Investigation Reports
- Engineering Division Project Files
- Subdivision and Parcel Map Files

Miscellaneous Agency Plans:

- ABAG Projections 2013
- Bay Area Clean Air Plan
- BAAQMD CEQA Guidelines 1999 Thresholds

Building Safety:

- California Building Code,
- California Energy Code
- California Plumbing Code,
- California Mechanical Code,
- California Electrical Code
- California Fire Code
- Title 16.52 Sunnyvale Municipal Code
- Title 16.53 Sunnyvale Municipal Code
- Title 16.54 Sunnyvale Municipal Code
- Title 19 California Code of Regulations
- National Fire Protection Association (NFPA) standards

OTHER :

Project Specific Information

- Project Description
- Sunnyvale Project Environmental Information Form
- Project Development Plans dated 11/10/16
- Project Construction Schedule
- Project Draft Storm Water Management Plan (in project plans)
- Project Phase I Environmental Site Assessment by AEI Consultants, dated May 18, 2015
- Project Soil Sampling Investigation Report by Technicon Engineering Services, Inc., dated October 16, 2015 and an update letter dated December 5, 2017
- Project Tree Inventory by Anderson's Tree Care Specialists, Inc., dated December 1, 2017
- Project California Historical Resource Information System (CHRIS) Letter, dated January 21, 2015
- Project Green Building Checklist

Summary

This checklist identifies the minimum criteria a project must demonstrate to use the City's CAP for purposes of streamlining the analysis of greenhouse gas emissions under CEQA. Minimum criteria outlined below includes: 1) consistency with CAP forecasts, and 2) incorporation of applicable Near-Term (prior to 2016) strategies and measures from the CAP as binding and enforceable components of the project.

Section 1: Consistency with CAP Forecasts

The CAP's achievement of the 15% reduction below 2008 target is based on growth assumptions in the City's General Plan and regional growth forecasts. For eligibility to streamline from the CAP for purposes of an environmental analysis, projects must demonstrate consistency with CAP forecast assumptions using the criteria listed below. As appropriate, these criteria should be cited as evidence in any subsequent environmental document.

1A. Does the project include large stationary emissions sources that would be regulated by the Air District?

☐ Yes☒ No

*If **no**, then the project may be eligible to claim consistency with growth assumptions that were used for CAP modeling. Skip to question **1C** to determine consistency with CAP forecasts.*

*If **yes**, the project may trigger additional changes to the physical environment that were not considered in the CAP and would otherwise be regulated by the Bay Area Air Quality Management District. Complete **1B**.*

1B. If this project is a stationary source emitter as outlined under 1A, does it also include any of the following emissions sources?

Residential uses	<input type="checkbox"/> Yes	<input type="checkbox"/> No
Commercial uses	<input type="checkbox"/> Yes	<input type="checkbox"/> No

*If **no**, the project does not include any emissions sources that were assumed in CAP growth forecasts. Therefore, the project may trigger additional changes to the physical environment that were not considered in the CAP. CAP measures may be used to mitigate GHG emissions, but project-level analysis of GHG emissions using the California Emissions Estimator Model (CALEEMod) or another method must be prepared by a qualified air quality consultant. If **yes**, the project may include emissions sources mitigated by the CAP. Therefore, any sources identified in 1B may be eligible to claim consistency with the CAP. All stationary sources regulated by the Bay Area Air Quality Management District shall be analyzed separately. Other sources that were analyzed in the CAP may still qualify for streamlining, should the project demonstrate consistency with the CAP as outlined in **1C** and following sections below.*

1C. Does the project trigger an amendment to or adoption of any of the following planning documents?

General Plan	<input type="checkbox"/> Yes	<input checked="" type="checkbox"/> No
Specific Plan	<input type="checkbox"/> Yes	<input checked="" type="checkbox"/> No
Precise Plan for El Camino Real	<input type="checkbox"/> Yes	<input checked="" type="checkbox"/> No

Please describe any amendments or adoption of new specific plans or special planning areas, as applicable:

*If **no**, then the project is eligible to claim consistency with growth assumptions that were used for CAP forecasts.*

*If **yes**, the project would trigger an amendment to or adoption of one or more of the documents list above, complete **1D** below.*

1D. If the project triggers an amendment to the General Plan, specific plans, and/or special planning areas, complete the following table:

	Existing & Proposed Project			Proposed Project's Net Effect on Citywide Forecasts		
	Existing or Allowed Under Existing Zoning (A)	Proposed Project (B)	Net Change from Existing Zoning (C=B-A)	2020 CAP Forecast (D)	Proposed Project's Net Effect on Citywide 2020 Forecast (E = D+C)	Would Net Effect of Project Exceed the Citywide 2020 CAP Forecast?
Population				145,020		
Jobs				89,750		
Households / Dwelling Units				59,660		

Please describe any assumptions used to calculate existing, allowed, or proposed conditions:

*If **no for all indicators above**, then the project may be eligible to claim consistency with CAP growth assumptions. The project's assumed residents, employees, and households would not create a net increase on community-wide growth assumed in the CAP. The CAP uses these community-wide growth indicators to forecast community-wide emissions from residential energy use, nonresidential energy use, water-related emissions, and waste. Because the CAP uses these comparable indicators to forecast non-transportation related emissions, and the project would not exceed the CAP's assumed 2020 residents, employees, and dwelling units, the project's non-transportation emissions are therefore consistent with CAP growth assumptions and captured within the CAP's emissions forecast. Complete **1E** below.*

*If **yes to one or more indicators above**, the proposed project's net effect on citywide 2020 forecasts would exceed the 2020 CAP forecast assumptions. Therefore, the project may trigger additional emissions not assumed in CAP growth forecasts. Any projects that exceed the 2020 forecasts may still rely on the CAP for identification of measures and standards for mitigation. However, since such projects exceed the assumptions of the CAP forecast, it is recommended that the project demonstrate anticipated project-level GHG emissions estimates using CALEEMod or another tool. (estimates prepared by consultant).*

1E. If the project is consistent with CAP growth forecasts as identified in 1D above, provide the following information.

Would the project have a potentially significant impact after mitigation on any of the following standards of significance identified in the State CEQA Guidelines, Appendix G?

a) Conflict with an applicable plan, program, or policy establishing measures of effectiveness for the performance of the circulation system, taking into account all modes of transportation including mass transit and non-motorized travel and relevant components of the circulation system, including but not limited to intersections, streets, highways and freeways, pedestrian and bicycle paths, and mass transit?	<input type="checkbox"/> Yes	<input checked="" type="checkbox"/> No
b) Conflict with an applicable congestion management program, including, but not limited to level of service standards and travel demand measures, or other standards established by the county congestion management agency for designated roads or highways?	<input type="checkbox"/> Yes	<input checked="" type="checkbox"/> No
c) Conflict with adopted policies, plans, or programs regarding public transit, bicycle, or pedestrian facilities, or otherwise decrease the performance or safety of such facilities?	<input type="checkbox"/> Yes	<input checked="" type="checkbox"/> No

*If **yes to one or more standards above**, the proposed project's net effect on citywide 2020 forecasts is inconsistent with plans, programs, or policies that informed the assumptions for the 2020 transportation forecast. Therefore, the project is inconsistent with transportation emissions forecasts and is not eligible to claim consistency with the CAP for purposes of GHG emissions and impacts on climate change.*

*If **no for all standards above**, then the project is consistent with the plans, programs, policies, or ordinances that informed the travel demand model for the 2020 transportation forecast of the CAP. Therefore, the project is consistent with CAP growth assumptions for transportation emissions in the CAP and is eligible to claim consistency with CAP transportation forecasts.*

Section 2: Consistency with CAP Measures

The CAP provides measures that achieve a 15% reduction below 2008 emissions levels by 2020. Each of the measures contains a bulleted list of action items/project standards that help projects achieve that goal. Projects that wish to demonstrate consistency with the CAP must demonstrate consistency with all applicable measures and action items/project standards from the CAP. Consistency with all applicable measures should be cited as evidence to support tiering from the CAP.

2A. Using the action items/project standards identified on the following pages, identify all measures and action items/project standards that are applicable to the project. Identify applicability and project compliance with each action item/project standard.

If a project demonstrates all applicable mandatory standards, the project is eligible to claim consistency with CAP measures and is eligible for CAP streamlining.

If a project does not integrate all applicable mandatory standards, the project is ineligible to claim consistency with CAP measures and is not eligible for CAP streamlining.

Additional voluntary measures may also be recommended. Projects inconsistent with growth forecasts should consider integrating all feasible voluntary and mandatory CAP measures.

Standards for Climate Action Plan Consistency/Private Development

(Includes Near-Term Action Items and Action Items Already Implemented by the City)

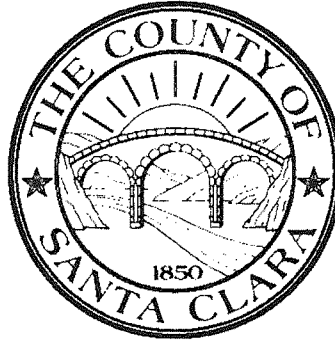
Applicable? (Yes or No)	Measure	Action Item/Project Standard	Describe whether standards are applicable and how the project demonstrates consistency with applicable standards
Yes	OS-2	Provide availability and access to outdoor space for recreation or social purposes, including access to public open spaces on privately owned property such as retail shopping centers	The project includes a private enclosed patio for each unit plus common area at the rear of the site. The project exceeds the minimum open space requirements. New sidewalks and street trees will be installed along Old San Francisco Road. Residents will be within a 6-minute walk from Braly Park. A commercial shopping center with restaurants and other retail uses is also within a short walking distance of the project site.
Yes	OS-3.1	Continue to implement the City's Tree Preservation requirements.	Five trees are proposed for removal, and three of them are "protected trees". The project will add approximately 23 trees to the site, many of which will be required to be planted as 24-inch or 36-inch box specimens.

Applicable? (Yes or No)	Measure	Action Item/Project Standard	Describe whether standards are applicable and how the project demonstrates consistency with applicable standards
Yes	EC-2.2	Continue to require energy-efficient siting of buildings. Buildings should be oriented and landscape material should be selected to provide maximum energy efficiency for the buildings	A GreenPoint Rated Checklist has been provided indicating that 80 points will be achieved as required for new multi-family developments. Many of the points relate to energy efficiency in building siting and construction, and water-efficiency in landscaping.
Yes	WC-2.3	Require new open space and street trees to be drought-tolerant	The project has been designed to comply with the Water-Efficient Landscaping requirements.
Yes	LW-2.1	Require multi-family homes to participate in the City's Multi-family Recycling Program	This is a code requirement.
Yes	LW-2.2	Select materials to be targeted for diversion methods, services or technologies based on the results of the Zero Waste Strategic Plan	The Zero Waste Strategic Plan is already being implemented.
No	CA-1.7	Actively promote the use of alternative modes of transportation as safe modes of travel. When applicable, promote viable programs sponsored by 511.org, the BAAQMD and other recognized agencies on the City's website and publications	The project site is located within proximity to two VTA bus lines (26 & 55).
No	CTO-1.1	Incorporate the provisions of AB 1358, the California Complete Streets Act of 2008, into roadway design, construction and maintenance activities	The existing public street in front of the property is not being modified.
No	CTO-1.2	Implement the street space allocation policy (RTC 8-085, April 28, 2009) in coordination with road reconstruction or resurfacing projects to provide road configurations that accommodate all travel modes.	The existing public street in front of the property is not being modified.
No	CTO-1.3	Require new development to provide cross-parcel access and linkages from the development entrance to the public sidewalk system, transit stops, nearby employment and shopping centers, schools, parks and other parcels for ease of pedestrian and cyclist access	The project site provides direct access from the front units to the public sidewalk. Adequate space on the internal driveway is provided for the remaining four units. The project will upgrade the existing sidewalk and which links to existing public sidewalk along Old San Francisco Road. These sidewalks directly link to nearby bus stops. An existing bike lane adjacent to the site will be maintained. Braly Park

Applicable? (Yes or No)	Measure	Action Item/Project Standard	Describe whether standards are applicable and how the project demonstrates consistency with applicable standards
			(to the south) and a commercial retail center (to the east) is located within short walking distance of the site (approximately 0.3 miles).
Yes	CTO-1.4	Improve pedestrian safety and comfort through design elements such as landscaped medians, pedestrian-level amenities, sidewalk improvements and compliance with ADA design standards, particularly for areas serving high volumes of traffic.	New sidewalks, street trees and street lights will be installed along the project frontage per current City standards.
No	CTO-1.5	Improve bicycle facilities and perceptions of comfort through pavement marking/coloring, physical separation, specialized signs and markings and other design elements.	The existing public street in front of the property is not being modified. The proposed garages exceed minimum size standards and will provide adequate space for bicycle storage.
Yes	CTO-1.6	Require sidewalks to be a minimum of 6 feet wide in order to allow side-by-side walking at identified locations that currently serve high pedestrian traffic volumes or locations planned to serve high volumes of pedestrian traffic.	The existing sidewalk will be upgraded to comply with current City standards.
Yes	CTO-2.1	Require public areas and new development to provide bicycle parking consistent with the VTA Bicycle Technical Guidelines, as amended.	The project complies with bicycle parking requirements by providing oversized garages for the parking of bicycles.
No	CTO-3.1	Continue sponsoring projects to provide transit rider amenities at bus stops and rail stations.	VTA bus stops (Routes #26 and #55) are located close to the project site on Old San Francisco Road and N. Fair Oaks Avenue.
No	CTO-4.1	Require existing and future major employers to utilize a variety of transportation demand management measures such as flexible work schedules, telecommuting, guaranteed rides home, low or no cost transit passes, parking "cash-out" incentives and other programs that provide employees with alternatives to single-occupant commutes.	The project is for residential use.

Applicable? (Yes or No)	Measure	Action Item/Project Standard	Describe whether standards are applicable and how the project demonstrates consistency with applicable standards
Yes	EP-2.3	Prevent buildings and additions from shading more than 10% of roofs of other structures.	A solar study was completed, demonstrating that existing adjacent roofs will not be shaded more than 10% by the project.
No	EP-2.3	Continue to allow and encourage solar facilities above paved parking areas.	A solar array is not proposed above the surface parking spaces. Solar facilities can generally be reviewed through a staff-level permit.
Yes	OR-1.3	In project review, encourage the replacement of high-maintenance landscapes (like grass turf) with native vegetation to reduce the need for gas-powered lawn and garden equipment.	The project has been designed to comply with the Water-Efficient Landscaping requirements.
Yes	OR-2.1	Idling times will be minimized either by shutting equipment off when not in use or reducing the maximum idling time to five minutes (as required by the California toxics control measure Title 13, Section 2485 of California Code of Regulations [CCR]) or less. Clear signage will be provided at all access points to remind construction workers of idling restrictions.	This is a standard condition of approval that will be implemented during construction.
Yes	OR-2.2	Construction equipment must be maintained per manufacturer's specifications	This is a standard condition of approval that will be implemented during construction.
Yes	OR-2.3	Planning and Building staff will work with project applicants from construction equipment by selecting one of the following measures, at a minimum, as appropriate to the construction project:	This is a standard condition of approval that will be implemented during construction.
		a. Substitute electrified or hybrid equipment for diesel and gasoline powered equipment where practical	
		b. Use alternatively fueled construction equipment on-site, where feasible, such as compressed natural gas (CNG), liquefied natural gas (LNG), propane or biodiesel.	

Applicable? (Yes or No)	Measure	Action Item/Project Standard	Describe whether standards are applicable and how the project demonstrates consistency with applicable standards
		c. Avoid the use of on-site generators by connecting to grid electricity or utilizing solar-powered equipment.	
		d. Limit heavy-duty equipment idling time to a period of three minutes or less, exceeding CARB regulation minimum requirements of five minutes.	



Regina Alcomendras
Santa Clara County
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(408) 299-5688
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Receipt: 18-120498

Product	Name	Extended
CEQA	ENVIRONMENTAL FILING	\$0.00
	# Pages	35
	Document #	ENV21611
	Document Info:	CITY OF SUNNYVALE
	Filing Type	F
Total		\$0.00
Change (Cash)		\$0.00

PLEASE KEEP FOR REFERENCE

**2018 ENVIRONMENTAL FILING FEE CASH RECEIPT**

DFW 753.5a (Rev. 10/31/17) Previously DFG 753.5a

RECEIPT NUMBER:

ENV21611

STATE CLEARINGHOUSE NUMBER (If applicable)

SEE INSTRUCTIONS ON REVERSE. TYPE OR PRINT CLEARLY.

LEAD AGENCY CITY OF SUNNYVALE	LEAD AGENCY EMAIL	DATE 06/18/2018
COUNTY/STATE AGENCY OF FILING SANTA CLARA	DOCUMENT NUMBER	
PROJECT TITLE 669 & 673 OLD SAN FRANCISCO ROAD - SIX THREE-STORY TOWNHOUSES		
PROJECT APPLICANT NAME INNOVATIVE CONCEPTS	PROJECT APPLICANT EMAIL	PHONE NUMBER (408) 985-1078
PROJECT APPLICANT ADDRESS 3550 STEVENS CREEK BLVD., STE 225	CITY SAN JOSE	STATE CA
		ZIP CODE 95117

PROJECT APPLICANT (Check appropriate box)

☐ Local Public Agency ☐ School District ☐ Other Special District ☐ State Agency ☒ Private Entity

CHECK APPLICABLE FEES:

☐ Environmental Impact Report (EIR) \$3,168.00 \$ _____
☐ Mitigated/Negative Declaration (MND)(ND) \$2,280.75 \$ _____
☐ Certified Regulatory Program document (CRP) \$1,077.00 \$ _____

☐ Exempt from fee☐ Notice of Exemption (attach)☐ CDFW No Effect Determination (attach)☐ Fee previously paid (attach copy of previously issued Environmental Filing Fee Cash Receipt (DFW 753.5a))

☐ Water Right Application or Petition Fee (State Water Resources Control Board only) \$850.00 \$ _____
☐ County documentary handling fee \$ _____
☐ Other \$ _____

PAYMENT METHOD:

☐ Cash ☐ Credit ☐ Check ☒ Other

TOTAL RECEIVED \$ \$0.00

SIGNATURE

AGENCY OF FILING PRINTED NAME AND TITLE

Nina Khamphilath, Deputy County Clerk-Recorder

August 7, 2018

Sunnyvale Planning Commission
456 W. Olive Avenue
Sunnyvale, CA 94086

Re: File #2018-7048 669-673 Old San Francisco Road (APN: 209-17-050 and 209-17-051)
Application for Special Development Permit and Vesting Tentative Map for construction of a six-unit residential development on this site

Dear Planning Commissioners:

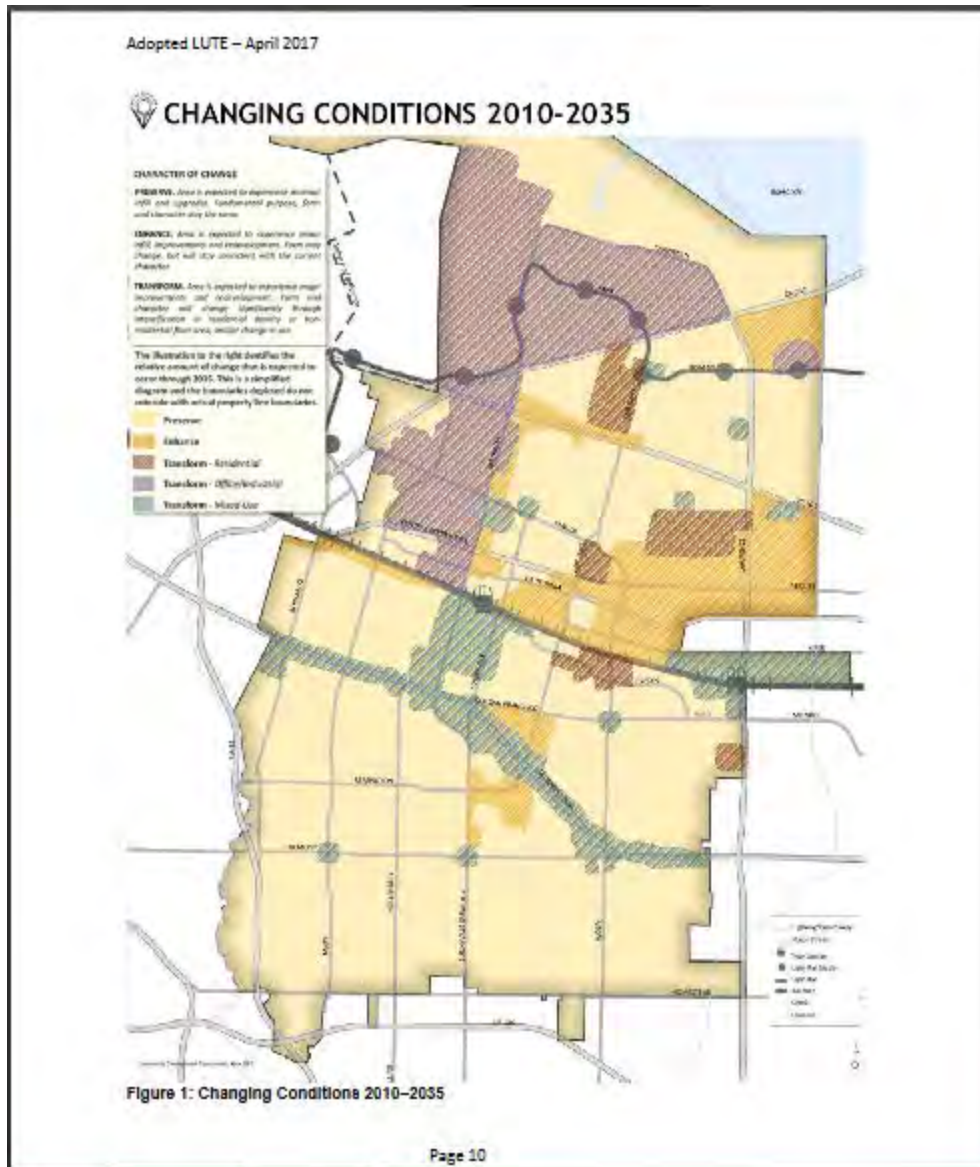
I am not against new housing developments, I just would like new development done properly and thoughtfully with consideration for affordability and input from current neighboring Sunnyvale residents who will be impacted the most by any new development. That being said, I would like to object to the proposed development at 669-673 Old San Francisco Road as it currently stands and ask that the special development permit and vesting tentative map not be granted.

The General Plan under Policy CC-1.3 is intended to ensure that new development is compatible with the character of special districts and residential neighborhoods. Goal CC-3 specifies well-designed sites and buildings, to ensure that buildings and related site improvements for private development are well designed and compatible with surrounding properties and districts. The current proposed design of this development is out of character with the surrounding neighborhood. The homes to each side are single-story or two-story with lots of open space on the street side. The proposed development is three stories high with a bulk and mass that is incompatible with this long-standing neighborhood, in particular because it is in the middle of the block and does not fit the character of the block. The proposed development overwhelms all the other homes on the block.

This is reiterated in the Sunnyvale Citywide Design Guidelines, amended April 8, 2014. The very first item is Site Design which states that "new development should adhere to the character of the existing neighborhood and be integrated into the surrounding development. New development should not dominate or interfere with the established character of its neighborhood. Site design of projects should be cohesive both functionally and visually." The established character of the neighborhood of this proposed development is single or two-story buildings with plenty of open space, not three stories crammed on minimal land.

The same is detailed under 1.A1 of the Guidelines which states that projects should be designed to be compatible with their surrounding development in intensity, setbacks, building forms, material, color, and landscaping unless there are specific planning goals to change the character of the area.

There are no plans to change the character of the area and according to the Land Use and Transportation Element (updated April 2017) [LUTE] of the Sunnyvale General Plan, under the Character of Change, as detailed on the Changing Conditions 2017-2035 Map, page 10, the side of Old San Francisco Road on which this development is slated to be built actually appears under the "preserve" section of Sunnyvale. The Character of Change for the "Preserve" areas is defined as an area expected to experience minimal infill and upgrades. Fundamental purpose, form and character stay the same. As it is designed today with its three stories, oversized massing, and general incompatibility with the rest of the neighborhood, the proposed development is in direct opposition to the General Plan and the Sunnyvale Citywide Design Guidelines. (Please see graphic on the next page.)



Policy LT-4.3 of the General Plan states the design review guidelines are to be enforced and zoning standards ensured so that the mass and scale of new structures are compatible with existing structures. LT-4.3c continues that design guidelines should be enforced to respect the character, scale, and context of the surrounding area. In no way does the design of the proposed development respect the character and scale of the surrounding properties. There are single-family homes and two-story condominiums on either side of the proposed development. In order to fit in with the character of the surrounding neighborhood, these proposed townhomes should be no more than two stories. This would also resolve the massing issue of so many units on such a small area.

New development, renovation, and redevelopment are required to be compatible and well-integrated with existing residential neighborhoods per Policy LT-5.3. LT5.3a clarifies further that the adopted Citywide Design Guidelines should be used in order to achieve compatible and complementary architecture and scale for new development. Given the three-story design of the proposed development versus the surrounding neighborhood, this was not adhered to at all.

The Citywide Design Guidelines also stress in 1.B3 that noise and odor generating functions should be located so they do not create a nuisance for the adjacent properties. With the guest parking spots currently slated so close to the property line, all the noise and pollution from those cars will directly impact the neighboring properties. A more thoughtful design would mitigate this – perhaps by designing smaller townhomes and allowing more space between these parking spaces and the adjacent properties.

According to the Guidelines, 1.C1 every project site should be designed for maximum utility of open space for ventilation, sunlight, recreation and views for both new and existing buildings. This is not the case with the proposed development. The design is much more massive than the surrounding buildings with so much massing on such a small site, there is no true open space, let alone room for ventilation, sunlight, recreation or view for either the proposed development or those already in existence. The home owners of the existing buildings bought where they did with a certain expectation that they would continue to enjoy their current views and access to both sunlight and ventilation. This should be considered in the design of the proposed development.

Furthermore, the Guidelines state C.1.4 that private usable open space areas for each unit and common usable open space for all units in attached single and multi-family residential developments should be provided. The design as it stands now for the proposed development has a very small common usable open space for all units. There are four parking spots, a driveway, and an 815 SF paved area considered “open space.” It seems disingenuous to provide such a small common area for a 6-unit multi-family housing development. These Guidelines are in place for a reason, they should be adhered to, in spirit and in deed, particularly since they were amended in April 2014 and can be considered current with contemporary needs. Even the Pebble Creek complex which was built in the early ‘80’s, well before the Sunnyvale Citywide Design Guidelines were even first adopted in 1992, has significant open space. In fact, trespassers consistently use this open space area to exercise their dogs or for other reasons and this will only get worse if there is no common open space of decent size in the proposed development directly next door.

In Guideline 1.C.5 it states that an average of 300 to 500 sq. feet of open space area per unit be provided for every residential project. According to the project Vicinity Map, the yards are either 285 SF or 298 SF with an average size of 289 sq. ft. This is certainly well under the average 300 sq. ft. that is detailed in the Guidelines. I was informed by the planner, that the balconies are approximately 30 square feet and are not considered part of the required usable open space for the project. With the 815 SF paved area added in, the average open space is 425 sq. ft. While the design may be “adhering” to the Guidelines regarding open space, it certainly is not abiding by the spirit in which the Guidelines were intended. Residents want open space, not just a shared paved area to go with a tiny yard, and they need it in order to enjoy quality of life.

In fact, Goal HE-2 refers to enhanced housing conditions and affordability – to maintain and enhance the conditions and affordability of existing housing in Sunnyvale and continues with Policy HE-2.2 which is slated to provide community outreach and comprehensive neighborhood improvements programs within Sunnyvale’s neighborhood enhancement areas to improve housing conditions and the overall quality of life.

The General Plan consistently refers to quality of life and looks to consider jobs, housing, transportation, and quality of life as inseparable when making planning decisions that affect any of these components. With quality of life one of the basic tenets of Sunnyvale’s General Plan, it is inconceivable that such a densely massed design without sufficient open space per unit as written in the Guidelines, would be deemed acceptable and in congruence with the Sunnyvale General Plan as well as the Citywide Design Guidelines.

The Building Design section of the Guidelines also details that the buildings should enhance the neighborhood and be harmonious in character, style, scale, color and materials with existing buildings in the neighborhood. Guideline 2.B5 specifically states that the upper stories of buildings three stories or taller need to be stepped back from public roads and adjacent lower scale developments, in order to reduce the bulk impact. Again, this project does not fit in with the rest of the neighborhood, not with

regard to the number of stories, scale, or color. It will stand out as an anomaly in the middle of the block in the neighborhood.

There should be respect for adjacent neighbors with any new development avoiding privacy, noise, light, and visual conflicts to the maximum, including mitigating large building volumes immediately adjacent to smaller homes on adjacent parcels, and sensitivity to blocking sun exposure and sky views of adjacent neighbors' windows and private outdoor spaces.

Sunnyvale's General Plan was intended to maintain or improve the quality of life of all its residents, not just a few. The current design of this planned development in no way enhances the lives of the surrounding Sunnyvale residents. If anything, the additional traffic, noise and pollution from more cars as well as proximity to other residences, will negatively impact everyone.

Policy HE-6.1 details that efforts must continue to balance the need for additional housing with other community values, including preserving the character of established neighborhoods, high quality design, and promoting a sense of identity in each neighborhood. This is a fundamental part of the Housing Chapter of the General Plan. However, from what I have seen of the plans for this development, it neither preserves the character of the current neighborhood, nor is it such a high-quality design that it adheres to the intent of the General Plan.

A quality design should not require a special development permit in order to fit into a designated space and detrimentally impact an entire neighborhood. The design should have been created in order to fit the designated space and established neighborhood. Since this was not done with either of the first two iterations of the planned design, I ask that the Planning Commission consider this proposed project for its size, scope, and lack of compatibility with the rest of the area of mostly single-story homes and smaller scale condominiums directly adjacent to this development, and reject the special development permit and vesting tentative map. This design is not sufficient for a sensitive use infill development, nor is there any greater community benefit being provided by the developer in exchange for his requested variances, in terms of the environment or public space.

The current design does not meet the General Plan or Design Guidelines set forth by the City of Sunnyvale. The design variances the developer is asking for are geared more toward his self-interest than providing affordable quality housing for the Sunnyvale community. The design is not appropriate for this space and ruins the scale and character of the entire neighborhood.

I would like to suggest that the number of units is decreased from 6 and/or the size of each unit is reduced to two bedrooms and limited to 2-stories. This would discourage renting and it would allow for more open space and address the massing on the .34 acre site, as well as the inconsistency of 3-story buildings in a neighborhood with single-level homes and two-story condominiums. The privacy of neighbors and quality of life for everyone should be kept in mind as well when a design is created and put forth for consideration.

Everyone knows we have a need for more affordable housing. However we need affordable housing which is compatible with the rest of the neighborhood and which encompasses a thoughtful design. As noted above, the General Plan states a need to balance housing needs with preserving the character of the current neighborhood while also accounting for the quality of life impact. Please encourage the developer to continue working with the surrounding community and the Senior Planner to provide a creative plan design that addresses the need for more affordable housing while also adhering to the intent of the General Plan and the Citywide Design Guidelines, so that Sunnyvale as a whole benefits and continues to be a city with a great quality of life for everyone who lives here.

Sincerely,
Britta Puschendorf

Cc: Ryan Kuchenig

Katherine Hall

From: Katherine Hall
Sent: Thursday, August 09, 2018 11:08 AM
To: Katherine Hall
Subject: FW: RE: File # 2018-7048 Location: 669-673 Old San Francisco Road (APN: 209-17-050 & 209-17-051)
Attachments: Concerns9.pdf

From: Erik Stauffer
Sent: Wednesday, August 08, 2018 7:22 AM
To: PlanningCommission AP <PlanningCommission@sunnyvale.ca.gov>; Ryan Kuchenig <RKuchenig@sunnyvale.ca.gov>
Cc: Erik Stauffer [REDACTED]
Subject: Re: RE: File # 2018-7048 Location: 669-673 Old San Francisco Road (APN: 209-17-050 & 209-17-051)

Hello Ryan and Planing Commission,

Attached are concerns that I have about the project at 669-673 Old San Francisco Road for the August 13th meeting. Please redact my mailing address and email address in any correspondence attached to the public comment section of any report.

thanks,
Erik

August 5, 2018

Planning Commission
456 W Olive Avenue
Sunnyvale, Ca 94088

Subject: File # 2018-7048 Location: 669-673 Old San Francisco Road (APN: 209-17-050 & 209-17-051)

Dear Planning Commission,

Thank you for your services to Sunnyvale. In regard to the proposed project at 669-673 Old San Francisco Road, several improvements have been made to the proposed project, but the project is still too large for the proposed space and out of character for the neighborhood. **Due to the nature of the remaining concerns, this building project should not be approved until the proposal is updated to be smaller and more in line with the character of the neighborhood, as provided by the City of Sunnyvale Citywide Design Guidelines and the General Plan Policy.**

Concerns:

1. The proposed front doors on each of the two front units of the structure face the street and are very undesirable, especially given how high they are above grade. **These doors should be moved to the side of the building to be more fitting with the character of the neighborhood.**
2. **The project should be at most 2 stories tall (including garages).** The proposed structure is not compatible with surrounding structure heights. All of the adjacent buildings are, at most, two stories tall. The proposed structure is three stories, which is much higher. (Other locations where three stories are adjacent to two stories are not working well). Since the buildings to the west are two story and the buildings to the east are one story, this project should be at most two stories as well. **This project as proposed is too bulky for the space available, doesn't match with the surrounding structures, changes the character of the neighborhood in a negative way, and overwhelms the neighbors. This is not transitional.**

This follows the recommendations of the City of Sunnyvale Citywide Design Guidelines and the General Plan Policy, as listed below:

City of Sunnyvale, Citywide Design Guidelines:

1.A1 *Design projects to be compatible with their surrounding development in intensity, setbacks, building forms, material, color, and landscaping unless there are specific planning goals to change the character of an area.*

The proposed structure is too large and too tall to fit in with the surrounding buildings.

1.A3 *Develop transition between projects with different uses and intensities to provide a cohesive visual and functional shift. Create transition by using appropriate setbacks, gradual building height, bulk, and landscaping.*

The proposed structure is too large and does not provide for a visual and functional shift. The building mass should be further reduced to allow for this by following the Sunnyvale design guidelines and policy below

2.B2 *Adjacent buildings should be compatible in height and scale*

2.B4 *Maintain similar horizontal and vertical proportions with the adjacent facades to maintain architectural unity.*

2.B5 *Step back upper stories of building three stories or taller from public roads and adjacent low scale development to reduce the bulk impact.*

2.C1 *Maintain diversity and individuality in style but be compatible with the character of the neighborhood.*

City of Sunnyvale, General Plan Policy:

LT -2.1 *Recognize that the City is composed of residential, industrial and commercial neighborhoods, each with its own individual character; and allow change consistent with reinforcing positive neighborhood values.*

LT – 4.1 *Protect the integrity of the City's neighborhoods; whether residential, industrial or commercial. (p. 3-13)*

LT-4.1a *Limit the intrusion of incompatible uses and inappropriate development into city neighborhoods. (p. 3-13)*

LT-4.1c *Use density to transition between land use and to buffer between sensitive uses and less compatible uses. (p. 3-13)*

LT-4.1d *Anticipate and avoid whenever practical the incompatibility that can arise between dissimilar uses. (p. 3-13)*

3.It is recommended to decrease the number of units from the proposed 6 and / or to reduce the size of each unit. This will allow the massing of the project to be further reduced and allow it to fit with the neighborhood.

4. At the community outreach meeting, the developer proposed that orange snow fence be setup to illustrate the height and location of the proposed structure. **This visual guide should be installed for review before permitting.**

5.The proposed structure places parking spaces near the rear, adjacent to existing structures. **Parking should be moved further away from the neighboring buildings.**

City of Sunnyvale, Citywide Design Guidelines:

1.B3. *Locate noise and odor generating functions so that they do not create a nuisance for the adjacent properties.*

6.To protect the environment, how will the demolition material be recycled? Which company will do the recycling? Are they certified?

7.The proposed development is located in a flood area. How will the risks of flooding be mitigated? Recessing the structure below grade will allow the parking area to flood with water. Increasing the height of the structure is also not recommended since this will make the structure much taller than surrounding buildings. Fire danger due to utilities in the garage, such as electric car chargers, risk the safety of existing surrounding structures and residents. There is also a safety concern because electric cars with large batteries parked in this recessed flood-prone area represent a fire and electrocution danger.

8.Parking is a concern in that the proposed development will add to the parking demand on Old San Francisco Road. This should be remedied by reducing the number of units and increasing on site parking.

9.The proposed rear parking area is recessed with a retaining wall and close to the rear property boundary. This recessed parking structure is closer to the property boundary than the rear setbacks of the property allow. The parking should be reconfigured so that a retaining wall for the parking area satisfies the rear setback. If a retaining wall is necessary, then there isn't enough room for these parking spaces.

10. The recessed parking area in the rear of the property presents a safety concern, as the only accessible way out of the area is up the driveway.

11.Several tall beautiful trees exist on the western property boundary (as shown below), but are not shown in the site plans. These trees appear to overlap with the planned building, and there is a concern for the preservation of these trees. **It would not be right for these trees to be adversely impacted by the new structure.**



E. Stauffer
Sunnyvale, Ca

cc: Ryan Kuchnenig

August 7, 2018

Via Email:
planningcommission@sunnyvale.ca.gov

Sunnyvale Planning Commission
456 W. Olive Avenue
Sunnyvale, CA 94086

Planning Commission Meeting: Monday, August 13th, 7:00 pm

Re: **File # 2018-7048** 669 - 673 Old San Francisco Road (APNs: 209-17-050 & 051)
Application for Special Development Permit and Vesting Tentative Map to construct a six-unit residential development on the site

Dear Planning Commissioners,

I am writing to express my opposition to the application for development at 669-673 Old San Francisco Road, and in particular the special development permit. I am a neighbor of the development who will be directly and negatively impacted by the applicant's special request for exemptions from applicable city requirements. At the time of this writing, I am unable to review the project data table, except for Attachment A, which was provided to me by Ryan Kuchenig, Senior Planner, as part of the initial submittal of the applicant. A staff report will not be completed until Friday August 10, 2018. The Planning Commission hearing is August 13, 2018.

As currently designed, the proposed development continues to maximize unit size and amenities at the sacrifice of thoughtful design that meets Sunnyvale standards. The inclusion of so many bedrooms and parking spots in each unit results in little room remaining on the .34 acre-site for setbacks, yards, open space, or the four guest parking spaces required under the municipal code. In an effort to cram all of these elements onto the two lots, one building is moved to within the required front setback, and four guest parking spaces are squeezed within the rear setback along the property line shared with my neighbors (Attachment A).

My email (Attachment B) to Ryan as of July 31, 2018 enumerates my additional concerns after meeting with Ryan and the City Arborist to discuss the plans on May 22, 2018, and further analyzing Citywide Design Guidelines (updated 2013, as amended). All comments regarding the proposed development were filtered through Ryan due to the fact that the owner, G. Nejat, repeatedly showed the community disregard for, and a lack of understanding of, their concerns at both the community meeting on March 29, 2018 as well as the study session on May 14, 2018. Because of Ryan's involvement, several of my requests have been addressed with the notable exception of:

(1) **Parking.** Uncovered guest parking remains too close to the living room and bedroom areas of four condominiums (particularly 578 Ironwood Terrace units 5, 6, 11 and 12) on the north side of the proposed development. The landscape buffer at that location is less than 4', contrary to the architectural rendering in Attachment A. Some guest parking would be less than 20' from those units. Additionally, ten of 12 units at 578 Ironwood Terrace would be subjected to the negative impact of exponentially increased daily exposure to carbon emissions, particulate matter and noise from a total of 16 vehicles coming and going from the site due to the current placement of the driveway and location of uncovered guest parking at the proposed development.

Sunnyvale Planning Commission
August 7, 2018
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As designed, parking behind the proposed development and adjacent to the condominium property line fence is too close to adjacent residential living areas and bedroom windows. The condominium complex adjacent to the proposed development was built in 1983. That development utilized high quality design which ensured no bedroom or living room areas were adjacent to parking, and avoided placing any uncovered parking area adjacent to any neighboring properties, including 669 and 673 Old San Francisco Road. All uncovered parking was situated in areas which would neither affect, nor be adjacent to, any adjacent R-0 homes.

Design improvements must be made to mitigate the negative impact of four uncovered parking spaces at the end of the driveway of the proposed development. The quality of life impact is a legal impact, affecting air quality, privacy and quiet enjoyment currently accessible to adjacent neighbors. The quality of life of adjacent neighbors should not be sacrificed due to the current cramped design of the proposed development.

(2) **Massing.** In accordance with the goals of the General Plan, particularly, **LUTE** (Land Use and Transportation Element of the General Plan) **Policy 55 (Executive Summary LT-5.3)** and **LUTE Policy 57 (Executive Summary LT-6.2)** (bold and italics added), decreased massing is needed on the third floor to be in compliance with **Citywide Design Guideline 2.B5** (discussed further below) to allow for compatibility within the established neighborhood on the north side of Old San Francisco Road, and to minimize shading and privacy impacts on the adjacent property at 578 Ironwood Terrace, in particular:

Policy 55

Require new development, renovation, and development to be compatible and well integrated with existing residential neighborhoods.

Action 1: Utilize adopted *City [sic] design guidelines* to achieve compatible and complementary architecture and scale for new development, renovation, and redevelopment.

Policy 57

Limit the intrusion of incompatible uses and inappropriate development in and near residential neighborhoods, but allow transition areas at the edges of neighborhoods.

Action 1: Where appropriate, use higher-density residential and higher-intensity uses as buffers between neighborhood commercial centers and transportation and rail corridors.

The proposed development does not follow the policies above. Further, with regard to Citywide Design Guidelines (updated 2013):

- The 18 du/acre proposed development is adjacent to a 14.22 du/acre condominium complex to the north and west, and a 3 du/acre parcel to the east.
- The 14.22 du/acre condominium complex is only .22 du/acres above the range for **R-2** Low-Medium Density Residential zoning (7-14 du/ac), and .78 du/acres below the range for **R-3** Medium Density Residential zoning (15-24 du/ac) (Sunnyvale General Plan, Land Use and Transportation, page 71).
- The proposed infill (sensitive use) R-3/PD development, at 18 du/ac, must incorporate decreased massing and bulk in its design in order to comply with **Citywide Design Guidelines 1.A3** and **2.B5**:

Sunnyvale Planning Commission
August 7, 2018
Page 3

1.A3. Develop transition between projects with different uses and intensities to provide a cohesive visual and functional shift. Create transition by using appropriate setbacks, gradual building height, bulk and landscaping.

2.B5. Step back upper stories of building [sic] three stories or taller from public roads and adjacent low scale development to reduce the bulk impact.

The north side of the third floor of the planned development does not step back sufficiently as required by **Citywide Design Guideline 2.B5**, as mandated to be adhered to by the General Plan. This is required to minimize the shading (4 of 12 units will no longer have sunlight on their decks or patios on and around winter solstice according to the shading/shadow analysis), and bulk impact on the adjacent low scale development. At 14.22 du/acre, 578 Ironwood Terrace is low scale (R-2) development.

The massing of the entire project must be further reduced by way of design to allow for transition between uses in compliance with **Citywide Design Guideline 1.A3**, or it will visually and literally be spot zoning between an R-2 scale condominium development (578 and 598 Ironwood Terrace at 14.22 du/acre) and an R-0 single family home development (717 Old San Francisco Road at 3 du/acre).

As designed, the proposed development's mass, scale and scope disregard Sunnyvale's General Plan to "preserve" the area in question. The .34 acre proposed development site is located on the General Plan map in an area designated as "PRESERVE" in the **LUTE** (p. 11, Figure 1). The definition of "PRESERVE" is as follows:

Area is expected to experience minimal infill and upgrades. Fundamental purpose, form and character stay the same.

If the proposed development moves forward without any corresponding decrease in mass, scale and scope as required by the General Plan, it will be in direct violation of the General Plan of Sunnyvale **LUTE** (Land Use and Transportation Element of the General Plan) **Policy 55**, and **LT-5.3** of the **Executive Summary**. The location of the proposed development is located in an area designated "preserve" in the General Plan of Sunnyvale. This means minimal change to, and preservation of, the area, not upheaval.

Sensitive uses include infill developments. The proposed infill development site is adjacent to condominiums built in 1983 and a single story home built in 1970. As designed, the proposed development has the greatest massing of any residence on the north side of Old San Francisco Road between Ironwood Terrace and Gail Avenue, rendering it incompatible in size, scale and scope with adjacent properties.

Rather than rewarding a developer with special treatment for the inadequate design of a sensitive use (infill) development, it is my hope that the Planning Commission carefully evaluates the requests for a special development permit and a tentative map, and encourages the developer to further work with the Senior Planner and community to submit a well-designed project that either provides community benefit, or does not seek variances and is compatible with adjacent residences along the streetscape of the north side of Old San Francisco Road. This can be accomplished by adhering to, utilizing and complying with established and readily available Citywide Design Guidelines, as mandated by the General Plan (Chapter 3, Policy 55, Action 1 and Executive Summary LT-5.3).

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Page 4

Special Development Permit

The applicant for this development has requested a special development permit because the development cannot meet the generally applicable rules for development in Sunnyvale. Essentially, the applicant is using the special development permit to request variances from those applicable requirements without providing any offsetting community benefits. The applicant claims it needs these variances, but in fact only desires them to accommodate an overly massive and incompatible project on the north side of Old San Francisco Road between Gail Avenue and Ironwood Terrace. A sensitive use design that would make better planning sense in alignment with the General Plan would further decrease the massing of the planned development, particularly on the third floor, or reduce the massing and create eight units with one or two bedrooms and a one car garage each. At the community meeting of March 29, 2018, the architect basically stated, "You're lucky we're designing this because the next developer might put in eight units." We would welcome eight units. That would allow one unit to be made available for affordable housing. Sunnyvale needs more affordable housing. This development avoided that requirement by reducing the number of units from eight to six.

Sunnyvale Municipal Code Section 19.26.020 states that the purpose of a planned development district is to "achieve superior community design, environmental preservation and public benefit." The proposed development will not achieve these goals. To the contrary, the overly crammed design will increase the likelihood of general conflict in the community, for example, as guests and residents attempt to park their vehicles in the four guest parking spaces at the end of the driveway. The noise and fumes from these conflicts will be located right outside the living areas of residents of 578 Ironwood Terrace, replacing what used to be the backyards of two single-family homes.

Here, the proposed development seeks a special development permit, but fails to contribute to the neighborhood in terms of public space, environmental protection or superior materials and design (not superior materials or design); at least one of which is required for a special development permit. There is no public space being provided, and no environmental protection is being offered. Unless this applicant commits to design changes that more respectfully consider the negative impacts on (1) all adjacent residences (including, but not limited to, substantially increased exposure to noise and particulate matter, and decreased privacy) and (2) the neighborhood (increased pollution, traffic and car trips, resulting in pedestrian safety concerns along Old San Francisco Road), or directly provides community benefits in tandem with the requested variances, a special development permit for this development should not be granted. Special development permits are provided to projects that improve neighborhoods through creative development, not to projects that prioritize profits over safe and thoughtful parking conditions, setbacks and neighborhood impacts.

Vesting Tentative Map

The negative impacts described above make it clear that (1) the design of the proposed subdivision is not consistent with the General Plan, (2) the site is not physically suitable for the proposed density of the development, and (3) the map fails to meet requirements imposed by Municipal Code (specifically, Sunnyvale Municipal Code Section 19.26.020).

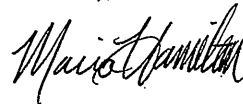
Sunnyvale Planning Commission
August 7, 2018
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Conclusion

It is important to balance property rights and development opportunities with quality of life impacts and concerns of existing residents. I respectfully request that the Planning Commission deny this application, and in particular deny the special development permit application. As a community, we care about our neighborhood as well as Sunnyvale design techniques and development guidelines that are in place to protect the public interest. If the special development permit is granted without any correlating community benefit, and deviations are allowed, good faith measures must be taken to protect adjacent neighbors, such as:

- Further decreasing the size of the units at the back (north) end of the lot, especially the third floors, to reduce shading and privacy impacts to neighboring residences.
- Decreasing the size of other units to more appropriately accommodate guest parking, open space and setback requirements.
- Relocating the guest parking outside of the rear setback.

Sincerely,

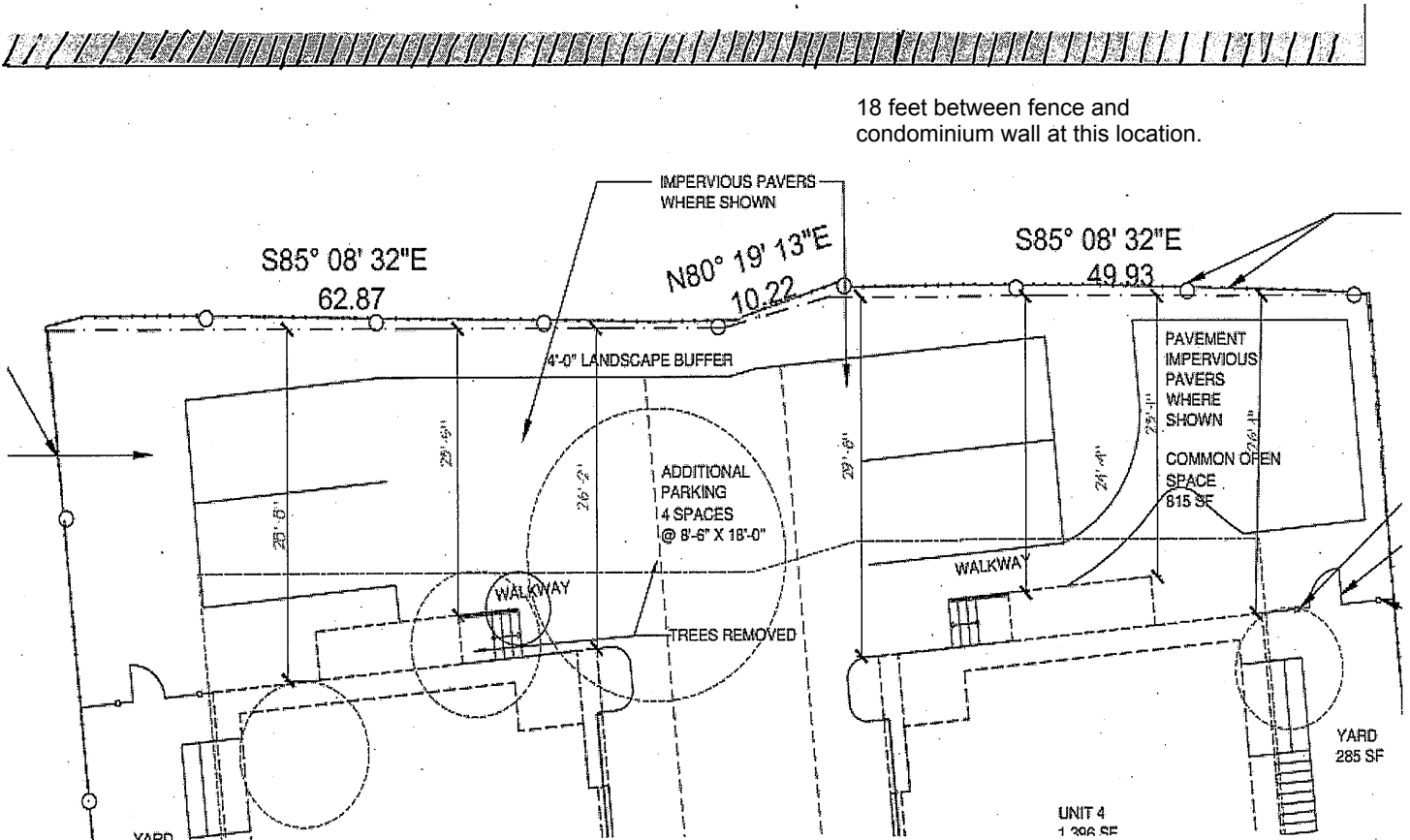


Maria Hamilton
Sunnyvale, CA

cc: R. Kuchenig

Attachment A

Condominiums



Living areas indicated by //////////////.

Attachment B

Hamilton, Maria

From: Hamilton, Maria
Sent: Tuesday, July 31, 2018 12:33 PM
To: 'Ryan Kuchenig'
Subject: 2018-7048 Residential Design Techniques
Attachments: Palo Alto Sample 2.jpg; Palo Alto Sample.jpg; Design Techniques Basic Design Principles; 8_13_2018 Handout Ryan Email

Ryan,

The owner of the homes at the proposed site development location has shown very little regard for adjacent properties. This leads me to question whether he was advised of residential design guidelines for the City of Sunnyvale by the Project Review Committee.

Design Techniques for single family homes in Sunnyvale were published in 2003. Yet Page 9 (attached to this email) refers to design principles under the Chapter "Community Expectations" which should "be respected in all residential projects." This is further discussed in the attached file "8_13_2018 Handout Ryan Email."

I handed out the bulk of the information in the "Handout" attachment at the Planning Commission Meeting of July 9, 2018, which enumerates my remaining concerns. I've revised it only slightly. I also attached some extraordinarily well-designed upscale residential architecture I've seen in Palo Alto. The first development is two units. The second is a single family home. Uncovered parking is in the front, detached garages are in the back at the end of a short driveway.

The colors and exterior design for both residences in Palo Alto are what I would like to see more of with this proposed project. These are examples of superior materials and design. The City should put a well-designed residential complex in this location, not a hastily assembled generic rendering that fits into the specifications of the two lots.

Thank you,

Maria

Palo Alto Sample



Palo Alto Sample 2



2.2 BASIC DESIGN PRINCIPLES

These design principles should be respected in all residential projects. They are the touchstones upon which all of the following design techniques are based, and, since design guidelines cannot anticipate every condition that might occur, they will be used in addressing conditions not specifically covered in the more detailed sections that follow.

1. REINFORCE PREVAILING NEIGHBORHOOD HOME ORIENTATION AND ENTRY PATTERNS

Maintain a sense of neighborhood by facing residences and home entries to primary public or private streets, providing convenient pedestrian access from the street, and including front windows, where common, to provide “eyes on the street” in order to enhance neighborhood safety.

2. RESPECT THE SCALE, BULK AND CHARACTER OF HOMES IN THE ADJACENT NEIGHBORHOOD

Buildings should be sympathetic to the predominant building forms and scale of their neighborhoods, including but not limited to, height, bulk, character, building form, roof form and orientation, window treatments, materials, and colors. Architectural styles, elements, and shapes need not necessarily be the same as those on adjacent and nearby homes, but improvements should avoid unnecessary visual conflicts.

3. DESIGN HOMES TO RESPECT THEIR IMMEDIATE NEIGHBORS

Every project should be respectful of adjacent homes and neighbors. New development should avoid privacy, noise, light and visual conflicts with adjacent uses to the maximum degree possible. Special care should be given to avoid tall blank walls and building volumes immediately adjacent to one story forms on adjacent parcels, and to the placement and treatment of windows and site landscaping to minimize views into neighboring homes’ windows and private outdoor spaces.

4. MINIMIZE THE VISUAL IMPACTS OF PARKING

Wherever possible, garages and their paved access drives should be subordinate to, rather than dominating, the entry and architecture of the house. In cases where garages are a major part of the street front in a neighborhood, existing patterns may be followed, but steps should be taken to soften the visual impact of the garage fronts. Visual elements might include landscape divider strips in the paving between garage entries, dividing double garage faces into individual doors, adding landscape trellises and lattices to soften garage fronts with landscaping and taking steps to provide special emphasis on the front entry.

5. RESPECT THE PREDOMINANT MATERIALS AND CHARACTER OF FRONT YARD LANDSCAPING

In neighborhoods where there is a discernible landscape character along street fronts, new home landscaping should take that into consideration. Where front landscape areas are primarily composed of living plant materials, that pattern should be repeated.

6. USE HIGH QUALITY MATERIALS and CRAFTSMANSHIP

Quality materials require less maintenance to remain attractive over time, and they convey a sense of pride in one’s home.

7. PRESERVE MATURE LANDSCAPING.

Wherever possible, mature trees should be protected during construction and integrated into new landscape plans.

EXCEPTIONS

Design guidelines cannot address every possible condition for every type of neighborhood or architectural style in the City. Although the principles set forth on this page and the guidelines contained in the following sections will be applicable for most cases, there may be unique characteristics of individual neighborhoods or specific sites. Where conflicts between the principles and guidelines in this document are in conflict with the specific characteristics of a neighborhood, reviews and approvals will be based on the most appropriate methods of fitting new construction into the context of existing neighborhoods.

Conditions of Approval excluded in staff report:

1. Exterior Design – Massing and Exterior Paint Color

“This LUTE is based on the following guiding principles, which include important concepts for land use and transportation in Sunnyvale:

- **Attractive Design.** Protect the design and feel of buildings and spaces to ensure an attractive community for residents and businesses.”

(Sunnyvale General Plan, Chapter 3, Land Use and Transportation, Page 13)

An improved design would decrease massing by at least one foot on the third floor, and add higher-end upscale colors like greens, grays and whites – the city needs to add more range other than beige and brown – to the exterior of this proposed development. Lighter, softer colors are needed, particularly to replace the brown color on the proposed development. Color, in addition to design, can also give the appearance of decreased bulk, especially along the streetscape.

Benefits of Decreased Massing:

- Increased affordability
- Better integration and compatibility with the neighborhood
- Adherence to Citywide Design Guidelines* 2.C1, 2.C2 and 2.C4

Solution: Decrease massing by at least 1’ around the entire perimeter of the third floor; replace the proposed brown exterior color with Benjamin Moore OC-151 White, 243 Baffin Island or 1498 Forest Floor.

2. Parking

Ignoring Citywide Design Guidelines* disregards the “quality of life” portion of the General Plan, as well as the Attractive Design portion of the LUTE. “The City Council considers these four components – jobs, housing, transportation, quality of life – as inseparable when seeking solutions” (Council Policy Manual, Policy 1.1.5, page 1). A design where uncovered parking is placed below multiple bedroom and living areas only feet from a fence that borders an uncovered parking area for four vehicles impacts adjacent residents’ quality of life audibly, visually and health-wise. I have observed that design implementation in Sunnyvale often places more emphasis on the streetscape aesthetic than the quality of life of adjacent residents, despite the Design Principles enumerated below.*

Four uncovered parking spaces only a few feet away from the living areas of neighboring residents will contribute to a decrease in air quality due to carbon emissions and particulate matter from vehicles, impacting the existing uses of the condominiums located at 578 Ironwood Terrace. It will also affect residents’ quality of life and adversely affect the quiet enjoyment of their own homes due to the noise of 16 cars coming and going from the proposed development only feet from the living areas of the majority of condominiums located at 578 Ironwood Terrace. A decrease in the amount of guest parking, relocation of guest parking, a design to allow for covered parking with a modified carport, or Installation of a trellis roof over uncovered parking that is less than 50% solid is needed.

Parking Lot Landscaping

3.C1. Adequately landscape all parking areas to reduce the effects of heat and glare from paving, and for visual relief.

Residential Parking

3.F2. A trellis roof qualifies as covered parking if it is more than 50 percent solid.

(Source: Citywide Design Guidelines*)

Solution: Install a trellis roof over uncovered parking that is less than 50% solid.

3. Landscaping

With only 1-foot below-grade level parking adjacent to ten of the 12 condominiums located at 578 Ironwood Terrace, additional small bushes need to be planted between the Crape Myrtle trees at the end of the driveway and along the guest parking spaces. These bushes will serve as a buffer in order to help absorb particulate matter and noise from the four vehicles that will be parked in the proposed uncovered guest parking area next to the fence adjacent to 578 Ironwood Terrace if the development moves forward.

Solution: If not already planned, mandate that additional small bushes be planted between the Crape Myrtle trees along the fence from one end of the guest parking area to the other.

4. Smoking Area

Common areas at the end of the driveway and adjacent to condominiums located at 578 Ironwood Terrace (units 2–6 and 8–12) may encourage a smoking area. Smoking areas on multi-family properties, as of September 23, 2016, violate Sunnyvale’s Ordinance No. 3072-16, even if they are designated smoking areas. The ordinance will be violated if any neighboring residence is affected by any smoking area. No area exists on the property where a person smoking would be farther than 20’ from the proposed development or the adjacent condominiums located at 598 Ironwood Terrace or 578 Ironwood Terrace. Therefore, any smoking on the premises will violate Sunnyvale Ordinance No. 3072-16.

Solution: Mandate that CC&Rs include a provision that smoking is prohibited anywhere on the premises in accordance with Sunnyvale City Ordinance 3072-16.

5. Construction Hours

It is requested that due to the residential location of the proposed development and its proximity to other residential properties, including a day care facility (Little Sheep Daycare at 721 Old San Francisco Road), construction hours be limited to begin at 8am Monday–Friday and at 9am Saturday. The quality of life in Sunnyvale can be improved by shortened construction hours. Palo Alto, San Carlos, Campbell, and San Francisco both regularly mandate construction hours begin at 8am M-F and 9am on Saturdays. Redwood City’s noise ordinance (section 24.30) prohibits construction noise between the hours of 8:00 PM to 7:00 AM weekdays, or at any time on Saturdays, Sundays, or holidays.

Solution: Add to Conditions of Approval, Construction Hours:

8am – 6pm Monday—Friday

9am – 5pm Saturday

6. Summary

The following Design Principles have not been adhered to (massing, design and compatibility) within the immediate neighborhood and streetscape of the proposed project. Although Sunnyvale Single Family Home Design Techniques - Community Expectations refers to single family homes, on page 9, it is specifically stated that “These design principles should be respected in all residential projects. They are the touchstones upon which [the City’s] design techniques are based, and, since design guidelines cannot anticipate every condition that might occur, they must be used in addressing conditions not specifically covered” within the design guideline documents.

- 2.2.1 REINFORCE PREVAILING NEIGHBORHOOD HOME ORIENTATION AND ENTRY PATTERNS
- 2.2.2 **RESPECT THE SCALE, BULK AND CHARACTER OF HOMES IN THE NEIGHBORHOOD**
- 2.2.3 DESIGN HOMES TO RESPECT THEIR IMMEDIATE NEIGHBORS
- 2.2.4 **MINIMIZE THE VISUAL IMPACTS OF PARKING**
- 2.2.5 RESPECT THE PREDOMINANT MATERIALS AND CHARACTER OF FRONT YARD LANDSCAPING
- 2.2.6 USE HIGH QUALITY MATERIALS AND CRAFTSMANSHIP
- 2.2.7 PRESERVE MATURE LANDSCAPING

***Citywide Design Guidelines (Updated 2013)**

1. Site Design

1.A1.

Design projects to be compatible with their surrounding development in intensity, setbacks, building forms, material, color, and landscaping unless there are specific planning goals to change the character of an area.

1.B3.

Locate noise and odor generating functions so that they do not create a nuisance for the adjacent properties.

1.C1.

Design every project site for maximum utility of open space for ventilation, sunlight, recreation and views for both new and existing buildings.

2. Building Design

Buildings should enhance the neighborhood and be harmonious in character, style, scale, color and materials with existing buildings in the neighborhood.

Scale and Character

2.B2.

Adjacent buildings should be compatible in height and scale.

2.B5.

Step back upper stories of building three stories or taller from public roads and adjacent low scale development to reduce the bulk impact.

2.B6.

Maintain the dominant existing scale of an area. Second story additions in a predominantly one story residential neighborhood should appear as one story.

2.B7.

Placement of windows and openings on second story additions should not create a direct line of sight into the living space or the back yard of adjacent properties to maintain privacy.

Architecture and Design

2.C1.

Maintain diversity and individuality in style but be compatible with the character of the neighborhood.

2.C2.

In areas where no prevailing architectural style exists, maintain the general neighborhood character by the use of similar scale, forms, and materials providing that it enhances the neighborhood.

2.C4.

"Corporate architecture" and generic designs are not recommended. Design each project specifically with respect to its own surrounding environment.

2.C7.

Utilize landscaping around the perimeter of new buildings to enhance buildings, not to cover an unacceptable design.

2.D4.

Vary roof levels and forms on a large building to create diversity and to decrease the apparent scale of the building.

2.E6.

Coordinate exterior colors of adjacent structures on the same or adjacent sites.

Ref: File#: Project 2018-7048

6 August 2018

TO: Sunnyvale Planning Commission

We continue to be concerned about the project reference 2018-7048. We drive by and/or walk by this property many times a day; we live around the corner on Gail Avenue. Our main concerns are listed below.

1. **The proposed development has the most massing of any residential development on the north side of Old San Francisco Road between Gail Avenue and Ironwood Terrace.**

We want to protect the integrity of our neighborhood. This project has a minimal setback and much higher massing compared to all adjacent properties. It changes the character of the neighborhood. It is recommended to decrease the number of units from the proposed 6 and/or reduce the size of each unit to allow the massing of the project to be more compatible with the neighborhood. See photo below.



Please note it does not meet with our understanding of Sunnyvale's Land Use and Transportation (LT) goals:

- a) LT-4.3: *Enforce design review guidelines and zoning standards that ensure the mass and scale of new structures are compatible with adjacent structures...*
- The massing on the proposed 6 unit property far exceeds the massing of the Ironwood Terrace development to the west and the single family homes to the east

Ref: File#: Project 2018-7048

- b) LT-5.3: *Require new development, renovation, and redevelopment to be compatible and well integrated with existing residential neighborhoods.*
 - There are no buildings between Gail Ave and Ironwood Terrace that compare in size and architecture with the proposed development; therefore no integrated compatibility!
- c) LT-6.1: *Improve and preserve the character and cohesiveness of existing residential neighborhoods.*
 - The Ironwood Terrace development has a large setback and many mature trees – one hardly notices the buildings from the street.
 - The single family dwellings to the east are all one level with driveways for parking and good setbacks.
 - The proposed development doesn't come close to being of similar character to these buildings and would definitely not be cohesive with these buildings.

Please note it does not meet with our understanding of the Sunnyvale's goals in the Citywide Design Guidelines:

- a) 1.A1. *Design projects to be compatible with their surrounding development in intensity, setbacks, building forms, material, color, and landscaping unless there are specific planning goals to change the character of an area.*
 - We know of no specific planning goal to change the character of this area, and the proposed development clearly would not be compatible with the surrounding development.
- b) 1.A3. *Develop transition between projects with different uses and intensities to provide a cohesive visual and functional shift. Create transition by using appropriate setbacks, gradual building height, bulk, and landscaping.*
 - Considering the large setback and many mature trees of the Ironwood Terrace development and the single level single family homes to the east, there would be no cohesive visual and functional shift from east to west through the proposed development property.
- c) 2 (Building Design): *Buildings should enhance the neighborhood and be harmonious in character, style, scale, color and materials with existing buildings in the neighborhood.*
 - The proposed development is totally different than the existing neighborhood.
- d) 2.B5. *Step back upper stories of building three stories or taller from public roads and adjacent low scale development to reduce the bulk impact.*

Ref: File#: Project 2018-7048

- There is no setback of the upper stories of the proposed development to reduce the bulk impact. In fact, the style makes the development look huge.

2. On-street parking and traffic have become dangerous and unsightly.

- a. With the increase in traffic to avoid the extremely busy El Camino Real, Old San Francisco Road has become a very busy street. Adding a driveway with many additional cars entering and exiting, and with limited visibility due to street parked cars, this would further increase traffic safety problems.
- b. With the influx of multiple families living in over-crowded single family housing, the street parking has become unsightly and congested and dangerous. Further, many families use their garages for storage or additional living area rather than car parking. Considering the proposed development and the limited proposed parking available, we expect a major impact to the street parking problem and a push of parked cars to adjacent streets to further add to safety concerns. A design improvement to have open parking in place of the proposed garages would help reduce the impact of additional parked cars.

Thank you for considering our concerns.

Gene and Debbie Hoyle
582 Gail Ave
Sunnyvale, CA

cc: Ryan Kuchenig

Katherine Hall

From: Katherine Hall
Sent: Tuesday, August 07, 2018 12:42 PM
To: Katherine Hall
Subject: FW: Project 2018-7048 Updated Concerns for August 13 Planning Commission Meeting

From: Laverne Martin
Sent: Tuesday, August 07, 2018 11:34 AM
To: Debbie & Gene Hoyle; Erik Stauffer; Tina Goulart
Cc: PlanningCommission AP <PlanningCommission@sunnyvale.ca.gov>; Ryan Kuchenig <RKuchenig@sunnyvale.ca.gov>
Subject: Re: Project 2018-7048 Updated Concerns for August 13 Planning Commission Meeting

Dear Mr. Rkuchenig,
I am writing on behalf of myself and my neighbors... I live at 574 Gail my neighbors being Mr and Mrs. Gene Hoyle and Erik and Katie Jo Stafford. They are very opposed to this new development as am I along with our other neighbors who are NOT happy about this new development...
Mr. Hoyle and Mr. Stafford have submitted an e-mail to you so I will not repeat what they have said except to say that I support everything they have said.
The proposals they have submitted mirror my own feelings about the development being considered on Old San Francisco...
I have lived here 47 years and have seen many changes. This would not be an improvement but a eye sore for all to see....

Respectively LaVerne Martin

On Mon, Aug 6, 2018 at 7:25 PM, Debbie & Gene Hoyle wrote:

Please see attached updated concern letter regarding subject project. Thank you for your consideration.

Sincerely,

Gene and Debbie Hoyle

Ryan Kuchenig

From: Carolyn James
Sent: Thursday, July 05, 2018 1:54 AM
To: Ryan Kuchenig; PlanningCommission AP
Subject: Fwd: Project 2018-7048

Sent from my iPhone

Begin forwarded message:

From: Carolyn James <
Date: June 29, 2018 at 2:12:21 PM GMT+2
To: rkuchenig@sunnyvale.ca.gov
Subject: Project 2018-7048

Hi Ryan,

I am a Sunnyvale resident who lives near this proposed development and I am very against it. I am concerned with the number of units and bedrooms the developer wants to put in. This development is going to extremely increase parking issues in my neighborhood, which are already a problem. This development is simply not including enough parking spaces in their plan for the number of potential tenants. Instead people will be parking on the street and so slowing down and making u turns to look for parking spots. Old San Francisco has recently already turned into a mini highway, this is going to be a safety concern if people are disrupting traffic patterns to look for parking on this busy street.

In addition, a three story building is too high for our neighborhood. No other property on this side of the street is three stories and I do not want my neighborhood turned into a high rise row. It's already multi story across the street. Please do not let us become a street of high rises. Doing this would go against the city plan of preserving neighborhoods.

I attended all of the meetings in regards to this proposed development last year to share my same concerns. The developer has not addressed the three story or parking concerns so I still do not think this should be approved. I unfortunately cannot attend the meeting on July 9th as I will be out of the country, but I do hope my comments are included in the packet for the board to consider.

Thank you,
Carolyn James

Sent from my iPhone

July 2, 2018

Planning Commission
456 W Olive Avenue
Sunnyvale, Ca 94088

Subject: File # 2018-7048 Location: 669-673 Old San Francisco Road (APN: 209-17-050 & 209-17-051)

Dear Planning Commission,

Thank you for your services to Sunnyvale. In regard to the proposed project at 669-673 Old San Francisco Road, several improvements have been made to the proposed project, but the project is still too large for the proposed space and out of character for the neighborhood. **Due nature of the remaining concerns, this building project should not be approved until the proposal is updated to be smaller and more in line with the character of the neighborhood.**

Concerns:

1. The proposed two doors on each of the front units of the structure face the street and are very undesirable, given how high they are above grade. **These doors should be moved to the side of the building to be more fitting with the character of the neighborhood.**

2. **The project should be at most 2 stories tall.** The proposed structure is not compatible with surrounding structure heights. All of the adjacent buildings are, at most, two stories tall. The proposed structure is three stories, which is much higher. (Other locations where three stories are adjacent to two stories are not working well). Since the buildings to the west are two story and the buildings to the east are one story, this project should be at most two stories as well.

This follows the recommendations of the general plan policy listed below:

General Plan Policy:

LT -2.1 Recognize that the City is composed of residential, industrial and commercial neighborhoods, each with its own individual character; and allow change consistent with reinforcing positive neighborhood values. □

LT – 4.1 Protect the integrity of the City's neighborhoods; whether residential, industrial or commercial. (p. 3-13)

LT-4.1a Limit the intrusion of incompatible uses and inappropriate development into city neighborhoods. (p. 3-13)

LT-4.1c Use density to transition between land use and to buffer between sensitive uses and less compatible uses. (p. 3-13)

LT-4.1d Anticipate and avoid whenever practical the incompatibility that can arise between dissimilar uses. (p. 3-13)

3. **It is recommended to decrease the number of units from the proposed 6 and / or to reduce the size of each unit.** This will allow the massing of the project to be further reduced and allow it to fit with the neighborhood.

4.To protect the environment, how will the demolition material be recycled? Which company will do the recycling? Are they certified?

5.The proposed development is located in a flood area. How will the risks of flooding be mitigated? Recessing the structure below grade will allow the parking area to flood with water. Increasing the height of the structure is also not recommended since this will make the structure much taller than surrounding buildings. Fire danger due to utilities in the garage, such as electric car chargers, risk the safety of existing surrounding structures and residents. There is also a safety concern because electric cars with large batteries parked in this recessed flood-prone area represent a fire and electrocution danger.

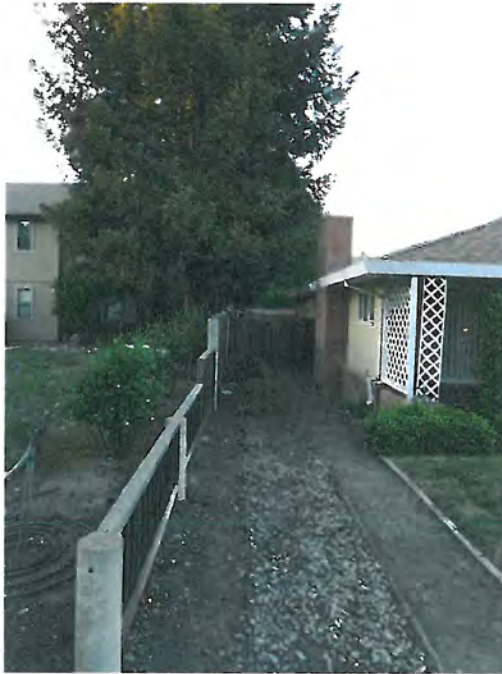
6.Parking is a concern in that the proposed development will add to the parking demand on Old San Francisco Road. This should be remedied by reducing the number of units and increasing on site parking.

7.The plan for the garbage storage and collection is still not clear. Garbage cans should not be placed on Old San Francisco road for collection and should not be visible from the street during the rest of the week.

8.The proposed rear parking area is recessed with a retaining wall and close to the rear property boundary. This recessed parking structure is closer to the property boundary than the rear setbacks of the property allow. The parking should be reconfigured so that a retaining wall for the parking structure satisfies the rear setback. If a retaining wall is necessary, then there isn't enough room for these parking spaces.

9. The recessed parking structure in the rear of the property presents a safety concern, as the only accessible way out of the area is up the driveway.

10.Several tall beautiful trees exist on the western property boundary (as shown below), but are not shown in the site plans. These trees appear to overlap with the planned building, and there is a concern for the preservation of these trees.



E. Stauffer
Sunnyvale, Ca

cc: Ryan Kuchnenig

Ref: File#: Project 2018-7048

2 July 2018

TO: Sunnyvale Planning Commission/Ryan Kuchenig

We continue to be concerned about the project reference 2018-7048. As previously stated, our main concerns are as follow:

We understand the rezoning for this project has been approved. However, the new development should be consistent with, and architecturally compatible with the neighborhood. We want to preserve our single-family dwelling neighborhood! This higher-density model is not the flavor our neighborhood! We have lived here over 45 years and are witnessing changes that are not what we bought into. We are concerned this sets a precedent going forward.

The massing of the project is NOT COMPATIBLE with the neighborhood. This development would have the highest massing of any residential development on the north side of Old San Francisco Road between Gail Avenue and Ironwood Terrace if approved. The nicest development that would model an acceptable development massing is the Pebble Creek development at 508-598 Old San Francisco Road. **See photo below.**



We are concerned with the additional traffic and encroachment on single family, once quiet neighborhoods. With more and more development we have seen an incredible traffic increase on Old San Francisco Road (not to mention elsewhere) and the street parking has become unsightly. It has become a dangerous situation.

Ref: File#: Project 2018-7048

To summarize our concerns:

1. **The proposed development has the most massing of any residential development on the north side of Old San Francisco Road between Gail Avenue and Ironwood Terrace.**
 - a. We want to protect the integrity of our neighborhood. That is the goal in Sunnyvale's General Plan Policies. Why isn't this happening? The General Plan says to maintain lower density residential development areas where feasible (LT-3.3). It also states to promote and preserve single-family detached housing where appropriate and in existing single-family neighborhoods (LT-3.3b). Additionally, it states to locate lower-density housing in proximity to existing lower density housing (LT-3.4b).
2. **On-street parking and traffic has become a dangerous and unsightly situation**
 - a. With the increase in traffic to avoid the extremely busy El Camino Real, Old San Francisco Road has become a very busy street. Adding driveways with many additional cars entering and exiting would further increase traffic safety problems.
 - b. With the influx of multiple families living in over-crowded single family housing, the street parking has become unsightly and congested. Further, many families use their garages for storage or additional living area, rather than car parking. It is anticipated that this situation would result in this development and the limited proposed parking contributing to impact the street parking problem and further add to safety concerns.
 - c. This has affected the quality of our neighborhood in a way we believe was not meant to be in the Goal LT-4 section of the Sunnyvale General Plan.

Thank you for considering our concerns.

Gene & Debbie Hoyle
Old San Francisco Road
Sunnyvale, CA

cc: Maria
LaVerne Martin
Joe & Tina Goulart
Neil & Victoria Jain
Erik & Katie Jo Stauffer

July 3, 2018

Sunnyvale Planning Commission
456 W. Olive Avenue
Sunnyvale, CA 94086

Re: File #2018-7048 669-673 Old San Francisco Road (APN: 209-17-050 and 209-17-051)
Application for Special Development Permit and Vesting Tentative Map for construction of a six-unit residential development on this site

Dear Planning Commissioners:

As someone who was raised in Sunnyvale, attended local schools, and now is also a home owner for almost twenty years, the development that has been happening within Sunnyvale's borders is concerning. Aside from the number of hotels going in all over the city, single-family homes are being torn down to put in multi-unit residential developments. Many of these multi-unit developments, while potentially addressing some of the housing need, though usually not affordable for the general population, are certainly not in keeping with Sunnyvale's General Plan, at least according to my understanding.

The development, in particular, to which I am expressing my opposition, is the six-unit development at 669-673 Old San Francisco Road, which is before the Planning Commission on July 9, 2018, with an application for a special development permit and tentative map. As a resident of a neighboring property, I feel our properties will be directly and adversely impacted by this development as the design currently stands. The massing of the planned development is too much and the design is not compatible with the rest of the neighborhood, particularly on that side of Old San Francisco Road.

In reading the Sunnyvale General Plan, the chapter on Community Character, Policy CC-1.3 specifically refers to ensuring that new development is compatible with the character of special districts and residential neighborhoods. Given the three-story design and that six units are being squeezed onto a .34-acre site, which is in direct contrast to the spaciousness of the design of the Pebble Creek complex or the other single-story homes along that side of Old San Francisco Road, it seems this development is not complying with the General Plan requirement that new development be compatible with the rest of the residential neighborhood.

Goal CC-2 of the General Plan calls for an attractive street environment, complimenting private and public properties and being comfortable for residents and visitors. Since the other residences surrounding this planned development at 669-673 Old San Francisco Road are considerably more spacious with regard to the land surrounding the actual abodes, allowing this level of density on a much smaller piece of land seems counterintuitive to Goal CC-2. More people vying for less space means less comfort for everyone.

In fact with 3 or 4 bedrooms in each of the planned six townhomes, there would potentially be at least 3 to 4 people per unit, and each unit has 2 garages, plus the 4 guest parking spots, meaning at a minimum an additional 16 cars would be in and out of this planned development on a daily basis. Attempting to exit Ironwood Terrace onto Old San Francisco Road currently is challenging enough. Another 16+ cars attempting to do the same thing on a daily basis may make it well-nigh impossible. This on top of trying to get down either Fair Oaks Avenue or Wolfe Road during peak transit hours in order to get home, invoke an even greater traffic nightmare.

Goal CC-3 designates well-designed sites and building in order to ensure that buildings and related site improvements for private development are well designed and compatible with surrounding properties and districts. Site design, compatibility with the built environment, integration with the roadway, and building design are all an integral part of this goal. How exactly does putting multi-residential housing of this density on such a small site comply with this goal? Particularly when other residential housing on this street is not as dense or as massive? With the current design everything is being crammed onto a .34-

acre site leaving barely any space for required setbacks, the necessary guest parking crammed into the back of the lots, let alone any community space or yards.

Policy HE 2.2 of the General Plan is to provide community outreach and comprehensive neighborhood improvement programs within Sunnyvale's neighborhood enhancement areas to improve housing conditions and the overall quality of life. It seems to me, Sunnyvale's General Plan was initiated to improve the quality of life of all its residents, not simply a select few, such as the developer. The current design of this planned development in no way enhances the lives of the surrounding Sunnyvale residents. If anything, the additional traffic, noise and pollution from more cars as well as proximity to other residences, will negatively impact everyone. In fact, the limited parking available for a multi-family housing development will surely cause issues as people vie for limited parking spaces. The other outcome is that more cars will be parked on the street, or worse, try to park in the Pebble Creek complex impacting residents there even further.

Policy HE-6.1 details that efforts must continue to balance the need for additional housing with other community values, including preserving the character of established neighborhoods, high quality design, and promoting a sense of identity in each neighborhood. This seems to be a fundamental part of the Housing Chapter of the General Plan. However, from what I have seen of the plans for this development, it neither preserves the character of the current neighborhood, nor does it seem to be such a high-quality design that it adheres to the intent of the General Plan.

In addition, Policy HE-6.2 requires that neighborhood vitality be promoted by providing adequate community facilities, infrastructure, landscaping and open space, parking, and public health and safety within new and existing neighborhoods. So far, the plans looks to have the minimum required parking, no real open space, and the landscaping could use some work as well. The guest parking is so close to the lot line, the cars here will have an adverse impact on the Pebble Creek community, both from an emissions standpoint, as well as a noise-level perspective, impacting public health and safety in our community.

As part of the Land Use and Transportation Element (updated April 2017) [LUTE] of the Sunnyvale General Plan, under the Character of Change, as detailed on the Changing Conditions 2017-2035 Map, page 11, the side of Old San Francisco Road on which this development is slated to be built actually appears under the "preserve" section of Sunnyvale. The Character of Change for the "Preserve" areas is defined as an area expected to experience minimal infill and upgrades. Fundamental purpose, form and character stay the same. This part of the General Plan is in direct opposition to the planned development as it is designed today with its three stories, oversized massing, and general incompatibility with the rest of the neighborhood.

Two of the guiding principles for the LUTE as part of the Plan Structure, are "attractive design – protect the design and feel of buildings and spaces to ensure an attractive community for residents and businesses" and "neighborhood preservation – ensure that all residential areas and business districts retain the desired character and are enhanced through urban design and compatible mixes of activities (page 13). Neither one of these Plan Structure principles appears to be adhered to, nor does this planned development appear to be in keeping with the General Plan. This development is a complete anomaly within this neighborhood.

Policy 34 of the LUTE speaks to supporting neighborhood traffic calming and parking policies that protect internal residential areas from city-wide and regional traffic, consistent with engineering criteria, operating parameters, and resident preferences. With the current level of additional cars and the minimal number of guest parking spaces, how does the current design comply with the General Plan? There would be no traffic calming, if anything traffic would be worse and resident preferences, let alone concerns, are certainly not addressed with this number of planned units and accompanying garage space.

LUTE Policy 40 on page 31 is geared toward providing safe access to city streets for all modes of transportation. Since it is already challenging to exit onto Old San Francisco Road from Ironwood Terrace, and often a problem to enter Ironwood Terrace due to the entrance/exit to the apartments almost

directly across the street from the Pebble Creek entrance/exit, how will adding another entrance/exit along almost the exact same pathway, enhance anyone's safe access to city streets?

Policy 51 of the LUTE seeks to enforce design review guidelines and zoning standards that ensure the mass and scale of new structures are compatible with adjacent structures with actions requiring that the City's zoning, building, and subdivision standards are reviewed to ensure they support and contribute to the urban design principles set forth in the General Plan policies (page 35). Additionally, Policy 51 is in place to safeguard that local guidelines are enforced, ensuring that buildings respect the character, scale and context of the surrounding area, while also using the development review and permitting processes to promote high-quality architecture and site design. Again, this does not seem to be the case with the current design of this planned development.

A quality design should not require a special development permit in order to fit into a designated space and thus impacting an entire neighborhood to its detriment. The design should have been created in order to suit the needs of the planned land. Since this was not done with either of the first two iterations of the planned design, I ask that the Planning Commission consider this planned project for its size, scope, and lack of compatibility with the rest of the neighborhood of mostly single-story homes and smaller scale condominiums directly adjacent to this development, and reject the special development permit. This design is not sufficient for a sensitive use infill development, nor is there any greater community benefit being provided by the developer in exchange for his requested variances, in terms of the environment or public space.

The current plan does not meet the general development guidelines set forth by the City of Sunnyvale. The developer only wants these design variances to be accepted as he is more interested in his personal profit than the well-being of Sunnyvale residents. It is of no concern to him since he does not live in the neighborhood, or even in Sunnyvale. What's good for the developer should also be good for Sunnyvale, and by extension, its residents.

No one is arguing the need for more housing. What we need is housing that is compatible with the rest of the neighborhood, with a thoughtful design. Some suggestions in this regard are smaller units, and perhaps fewer levels, along with prioritizing neighbors' privacy, as well as noise reduction and emission relief from cars, especially from guest parking. As noted above, the General Plan deems a need to balance housing needs with preserving the character of the current neighborhood while also accounting for the quality of life impact. Please encourage the developer to continue working with the surrounding community and the Senior Planner to provide a creative plan design that addresses the need for more housing while also adhering to the intent of the General Plan, so that Sunnyvale continues to be a city with a great quality of life and enjoyment for all.

Sincerely,

Britta Puschendorf

Cc: Ryan Kuchenig

Ryan Kuchenig

From: David Stephenson <>
Sent: Sunday, June 17, 2018 7:16 PM
To: Ryan Kuchenig
Subject: Public Hearing on 669 & 673 Old San Francisco Rd.

I am writing to challenge the issuing of a special development permit.

The region is designated for low density single family dwellings. The development of three-story six-unit townhouse development will negatively affect the surroundings in several ways.

- * it will adversely affect the value of the single family homes in the vicinity.
- * it will affect the degree of traffic in the area which is already approaching gridlock.
- * the increased population will place additional demands on the already strained infrastructure.

At some point we need to stop adding in people and let industry move to other locations. Better to stop the addition of housing and industry now, rather than when its too late. The ambiance and environment that people came here for is disappearing.

Sincerely,

David Stephenson

June 27, 2018

Sunnyvale Planning Commission
456 W. Olive Avenue
Sunnyvale, CA 94086

Regular Planning Commission Meeting: Monday, July 9th, 7:00 pm

Re: **File # 2018-7048** 669 - 673 Old San Francisco Road (APNs: 209-17-050 & 051)
Application for Special Development Permit and Vesting Tentative Map to construct a six-unit residential development on the site

Dear Planning Commissioners,

I am writing to express my opposition to the application for development at 669-673 Old San Francisco Road, and in particular the special development permit. I am a neighbor of the development who will be directly and negatively impacted by the applicant's special request for exemptions from applicable city requirements. At the time of this writing, I am unable to review the project data table, except for Attachment A, which was provided to me by Ryan Kuchenig, Senior Planner, as part of the initial submittal of the applicant. Per Ryan, a staff report will not be completed until Friday July 6, 2018. The Planning Commission hearing is July 9, 2018. In order to have my comments included in the packet distributed to the Planning Commission, the deadline is Tuesday July 3rd, with July 4th being a holiday.

As currently designed, the proposed development continues to maximize unit size and amenities at the sacrifice of thoughtful design that meets Sunnyvale standards. The inclusion of so many bedrooms and parking spots in each unit results in little room remaining on the .34 acre-site for setbacks, yards, or the four guest parking spaces required under the municipal code. In an effort to cram all of these elements onto the two lots, one building is moved to within the required front setback, and four guest parking spaces are squeezed within the rear setback along the property line shared with my neighbors (Attachment A).

My email with comments on the development to Ryan as of June 15, 2018 is attached as Attachment B. That email enumerates my additional concerns after meeting with Ryan and the City Arborist to discuss the plans on May 22, 2018. All comments regarding the development were filtered through Ryan due to the fact that the owner, G. Nejat, repeatedly showed the community disregard for and a lack of understanding of their concerns at both the community meeting on March 29, 2018 as well as the study session on May 14, 2018. Because of Ryan's involvement, most of my requests have been met with the exception of:

- (1) **Parking.** Parking remains too close to the living room and bedroom areas of four condominiums (particularly units 5, 6, 11 and 12) on the north side of the proposed development at 578 Ironwood Terrace. Guest parking at the rear of the proposed development would be less than 20' from residents there, subjecting them to increased daily exposure to carbon emissions, particulate matter and noise pollution from a total of 16 vehicles coming and going from the site.

Sunnyvale Planning Commission
June 27, 2018
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(2) **Massing.** In accordance with goals of the General Plan, particularly, **Goals LT-4.1c and LT-3.4b** (bold and italics added below), decreased massing is needed on the third floor to allow for compatibility within the established neighborhood, and to minimize shading and privacy impacts on the adjacent property at 578 Ironwood Terrace in particular.

GOAL LT - 4

QUALITY NEIGHBORHOODS AND DISTRICTS

Preserve and enhance the quality character of Sunnyvale's industrial, commercial and residential neighborhoods by promoting land use patterns and related transportation opportunities that are supportive of the neighborhood concept. (p. 3-13)

LT - 4.1 Protect the integrity of the City's neighborhoods; whether residential, industrial or commercial. (p. 3-13)

LT-4.1a Limit the intrusion of incompatible uses and inappropriate development into city neighborhoods. (p. 3-13)

LT-4.1c *Use density to transition between land use and to buffer between sensitive uses and less compatible uses.* (p. 3-13)

LT-4.1d Anticipate and avoid whenever practical the incompatibility that can arise between dissimilar uses. (p. 3-13)

and

LT-3.4b *Locate lower-density housing in proximity to existing lower density housing.* (p. 3-13)

Source: www.Generalplaninsunnyvale.com (consolidated 2011)
City of Sunnyvale general Plan – July 2011

Sensitive uses include infill developments. This proposed infill development is adjacent to condominiums built in 1983 and a 1-story single family home built in 1970. As designed, the proposed development has the greatest massing of any residence on the north side of Old San Francisco Road between Ironwood Terrace and Gail Avenue, rendering it incompatible in size, scale and scope with adjacent properties. Rather than rewarding a developer with special treatment for the inadequate design of a sensitive use (infill) development, it is my hope that the Planning Commission carefully evaluates the requests for a special development permit and tentative map, and encourages the developer to further work with the Senior Planner and community to submit a well-designed project that either provides community benefit, or does not seek variances and is compatible with adjacent residences on the north side of Old San Francisco Road.

Special Development Permit

The applicant for this development has requested a special development permit because the development cannot meet the generally applicable rules for development in Sunnyvale. Essentially, the applicant is using the special development permit to request variances from those applicable requirements without providing any offsetting community benefits. The applicant claims it needs these variances, but in fact only desires them to accommodate an overly massive and incompatible project on the north side of Old San Francisco Road between Gail Avenue and Ironwood Terrace. A sensitive use design that would make better planning sense would be to further decrease the massing of the planned development, particularly on the third floor.

Sunnyvale Planning Commission
June 27, 2018
Page 3

Sunnyvale Municipal Code Section 19.26.020 states that the purpose of a planned development district is to "achieve superior community design, environmental preservation and public benefit." The proposed development will not achieve these goals. To the contrary, the overly cramped design will increase the likelihood of general conflict, for example, as guests and residents attempt to park their vehicles in the four guest parking spaces at the end of the driveway. The noise and fumes from these conflicts will be located right outside the living areas of residents of 578 Ironwood Terrace, replacing what used to be the backyards of two single-family homes.

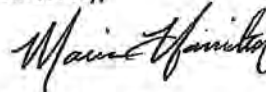
Here, the proposed development seeks a special development permit, but fails to contribute to the neighborhood in terms of public space, environmental protection or superior materials and design (not superior materials or design); at least one of which is required for a special development permit. Unless this applicant commits to design changes that more respectfully consider the negative community impacts (increased car trips resulting in increased particulate matter, noise pollution and traffic, for example) or directly provides community benefits in tandem with the requested variances, a special development permit for this development should not be granted. Special development permits are provided to projects that improve neighborhoods through creative development, not to projects that prioritize profits over safe and thoughtful parking conditions and setbacks.

Conclusion

It is important to balance property rights and development opportunities with quality of life impacts and concerns of existing residents. I respectfully request that the Planning Commission deny this application, and in particular deny the special development permit application. If the special development permit is granted without any correlating community benefit, and deviations are allowed, good faith measures must be taken to protect adjacent neighbors, such as:

- Further decreasing the size of the units at the back (north) end of the lot, especially the third floors, to reduce shading and privacy impacts to neighboring residences.
- Decreasing the size of other units to more appropriately accommodate guest parking and setback requirements.
- Including a carport or some variation of a carport or privacy screen to block the path of the noise and reduce visible impacts from guest parking spaces.
- Relocating the guest parking outside of the rear setback.

Sincerely,



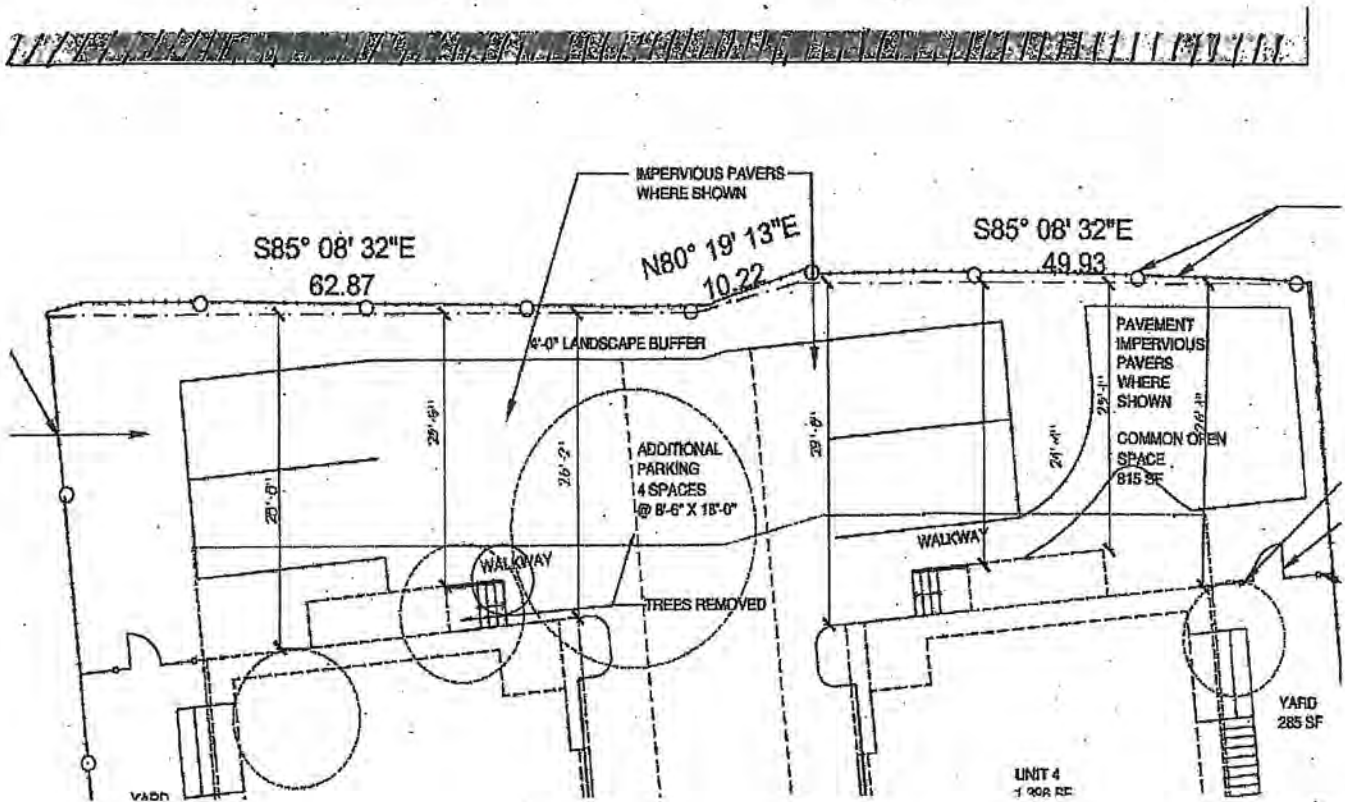
Maria Hamilton
Sunnyvale, CA

cc: R. Kuchenig
City Clerk

Attachment A

Attachment A

Condominiums



Living areas indicated by //////////////.

Attachment B

From: Hamilton, Maria
Sent: Friday, June 15, 2018 5:17 PM
To: 'Ryan Kuchenig'
Subject: 2018-7048 Conditions of Approval (Pending Issues)

Ryan,

When you are able, please email me a detail of the shade analysis based on the now revised decreased massing of the third (top) floor of the proposed development with regard to its impact on 578 Ironwood Terrace, to the north of the proposed development.

Pending issues since our meeting on May [22nd] include the following.

1. Parking

A design where uncovered parking is placed feet from a fence that borders property underneath multiple bedroom and living areas does not reflect high quality design, and impacts adjacent residents' quality of life both audibly and visually. There are multi-family dwellings with carports in Sunnyvale that cover two cars but have a support structure in the middle. In these cases, poles or beams support the center of the structure, not at the corners which produces a box-like effect. The actual implementation of design in Sunnyvale seems to put more emphasis on the streetscape aesthetic than the quality of life of the adjacent residents, despite the Design Principles (below). This also violates the General Plan. "The City Council considers these four components – jobs, housing, transportation, quality of life – as inseparable when seeking solutions" (Council Policy Manual, Policy 1.1.5, page 1).

Four uncovered parking spaces just feet away from the living areas of neighboring residents will contribute to a decrease in air quality due to carbon emissions and particulate matter from vehicles. It will also affect residents' quality of life and adversely affect the quiet enjoyment of their own homes due to the noise of 16 cars coming and going from the proposed development only feet from the living areas of the majority of condominiums located at 578 Ironwood Terrace. A decrease in the amount of guest parking, relocation of guest parking, or a special development permit to allow for covered parking with a carport or something similar is needed. Please refer to the design of 585 Old San Francisco Road for an example of a parking design which mitigated the problem of parking adjacent to a neighboring multi-family residence.

The following Design Principles have not been adhered to because of the massing, architectural design and compatibility within the immediate neighborhood and streetscape of the proposed project. These principles should be respected in all residential projects. They are the touchstones upon which the City's design techniques are based, and, since design guidelines cannot anticipate every condition that might occur, they must be used in addressing conditions not specifically covered within the design guideline documents.

- | | | |
|---------|---|-----------|
| • 2.2.1 | REINFORCE PREVAILING NEIGHBORHOOD HOME ORIENTATION AND ENTRY PATTERNS | No |
| • 2.2.2 | RESPECT THE SCALE, BULK AND CHARACTER OF HOMES IN THE NEIGHBORHOOD | No |
| • 2.2.3 | DESIGN HOMES TO RESPECT THEIR IMMEDIATE NEIGHBORS | No |
| • 2.2.4 | MINIMIZE THE VISUAL IMPACTS OF PARKING | No |
| • 2.2.5 | RESPECT THE PREDOMINANT MATERIALS AND CHARACTER OF FRONT YARD LANDSCAPING | No |
| • 2.2.6 | USE HIGH QUALITY MATERIALS AND CRAFTSMANSHIP | No |
| • 2.2.7 | PRESERVE MATURE LANDSCAPING | No |

2. Landscaping

With only a 1-foot below-grade level parking area adjacent to ten of the 12 condominiums located at 578 Ironwood Terrace, additional small bushes need to be planted at the end of the driveway between the Crape Myrtle trees. These bushes will serve as a buffer in order to help absorb particulate matter and noise from the four vehicles that will be parked in the uncovered guest parking area next to the fence adjacent to 578 Ironwood Terrace if the development moves forward.

3. Smoking Area

Common areas at the end of the driveway and adjacent to condominiums located at 578 Ironwood Terrace (#s 2–6 and #8–12) may encourage a smoking area. Smoking areas on multi-family properties, as of September 23, 2016, violate Sunnyvale's Ordinance No. 3072-16, even if they are designated smoking area. Why? The ordinance will be violated if any neighboring residence is affected by any smoking area. No area exists on the property where a person smoking would be farther than 20' from the proposed development or the adjacent condominiums located at 558 Ironwood Terrace or 578 Ironwood Terrace. Therefore, any smoking on the premises will violate Sunnyvale Ordinance No. 3072-16.

4. Construction Hours

It is requested that due to the residential location of the proposed development and its proximity to other residential properties, including a day care facility (Little Sheep Daycare at 721 Old San Francisco Road), that construction hours be limited to begin at 8am Monday–Friday and at 9am Saturday. The quality of life in Sunnyvale [can] be improved by shortened construction hours. Palo Alto, San Carlos, Campbell, and San Francisco both regularly mandate construction hours begin at 8am M-F and 9am on Saturdays. Redwood City's noise ordinance (section 24.30) prohibits construction noise between the hours of 8:00 PM to 7:00 AM weekdays, or at any time on Saturdays, Sundays, or holidays.

Thank you,

Maria Hamilton

PUBLIC HEARINGS/GENERAL BUSINESS

2. [18-0559](#) **Proposed Project:** Related applications on a .34-acre site:
 SPECIAL DEVELOPMENT PERMIT and VESTING
 TENTATIVE MAP to allow a three-story six-unit townhouse
 development.
Location: 669 & 673 Old San Francisco Rd. (APNs 209-17-050 &
209-17-051)
File #: 2018-7048
Zoning: R-3/PD
Applicant / Owner: Innovative Concepts / George Nejat
Environmental Review: Mitigated Negative Declaration
Project Planner: Ryan Kuchenig, (408) 730-7431,
rkuchenig@sunnyvale.ca.gov

Assistant Director Andrew Miner advised that the applicant has requested a continuance to the Planning Commission meeting on August 13, 2018 to make site plan corrections and respond to public comments.

Chair Rheume opened the Public Hearing.

Maria Hamilton, Sunnyvale resident, stated her opposition to the proposed project and discussed her concerns with the proposed project's impact on quality of life, neighborhood compatibility and parking.

Cecelia Morrison discussed her concerns with the height, density, parking, drainage and visibility impacts of the proposed project.

Chair Rheume closed the Public Hearing.

MOTION: Commissioner Howe moved and Commissioner Howard seconded the motion to continue this item to the Planning Commission meeting on August 13, 2018.

The motion carried by the following vote:

Yes: 5 - Commissioner Howard
Commissioner Howe
Commissioner Olevson
Commissioner Simons
Chair Rheaume

No: 0

Absent: 2 - Vice Chair Weiss
Commissioner Harrison

August 5, 2018

City of Sunnyvale
Planning Commissioners
Application No. 2018 – 7048
669 & 673 Old San Francisco Road

Dear Honorable Commissioners:

I appreciate the granting of the continuance for the above mentioned application and giving us the opportunity to review the letters from interested parties. The following items reflect our response to them and how they were addressed.

- 1: The revised design requests no deviations to the minimum required setbacks. All setbacks have been met or exceeded.
- 2: The revised design addresses neighbors concerns with respect to additional landscaping, window sill heights and massing of street facing entries. The landscape buffer at the rear of the property has been increased to a minimum of four feet and additional planting has been provided to address the concerns about car exhaust.
- 3: All the window sill heights at the rear of both buildings have been raised to a minimum of sixty inches above the floor levels. The front entries along Old San Francisco Road have been lowered to reduce the number of steps to allow for a more street level appearance.
- 4: Both building heights at less than the maximum allowed. We feel the proposed design, with respect to building height, is consistent with similar developments in the area. The building heights proposed at 29' are less than 30' maximum allowed. Directly across the street, the three story Renaissance Apartments are approximately 43 feet tall and west of us along Old San Francisco Road are other developments similar in pattern to ours that are relatively at the same heights.
- 5: The project design meets and exceeds the required off street parking requirements of ten parking spaces with a total of sixteen parking spaces being provided.
- 6: The revised design addresses the City Council's concerns with respect to overall building massing and use of quality building materials.

Reduced massing of the upper stories which results in a greatly architectural design.

The number of bedrooms have been reduced from six four bedroom units to two four bedroom units and four three bedroom units. The reduction in the number of bedrooms results in a reduction of the overall building area by approximately eight percent.

The introduction of wood horizontal siding to replace areas of stucco, colored vinyl windows and wood trim to replace white vinyl windows with foam molding, the use of metal gutters and downspouts and the use of larger scale stone provide for a higher quality of exterior materials.

One hundred percent of the pavers used in this revised design are pervious as opposed to the previous design where the amount of pervious pavers only meet the minimum amount required.

- 7: Flood, elevation certificate for both buildings have been submitted to City of Sunnyvale, and in both it is within (1) one foot above adjacent grade.
- 8: The proposed design comply with all instructions expressed by Commissioners at study session.
- 9: The proposed design is compatible and meets all the code requirements,
- 10: The proposed Townhomes will enhance the neighborhood aesthetics.
- 11: The revised Site Plan and Exterior Elevation drawings have been updated accordingly to address neighbor concerns with consistency and have been compared with the Civil Drawings for the same. The Landscape Plan and Solar Study drawings have also been updated to be consistent with the rest of the plan set.

In closing we feel the current design is compatible with similar developments within the same Zoning District and meets or exceeds all current zoning standards and requirements. Furthermore we feel that this project is consistent with the intent of the City of Sunnyvale's General Plan to allow for economic urban growth and development by providing the necessary housing for Sunnyvale's ever growing population.

I anxiously await and appreciate your consideration in the approval of this Project.

Sincerely,

George Nejat
Property Owner



City of Sunnyvale

Agenda Item

18-0648

Agenda Date: 8/13/2018

REPORT TO PLANNING COMMISSION

SUBJECT

CONTINUED FROM THE JULY 9, 2018 PUBLIC HEARING

Proposed Project: Related applications on a .34-acre site:

SPECIAL DEVELOPMENT PERMIT and **VESTING TENTATIVE MAP** to allow a three-story six-unit townhouse development.

Location: 669 & 673 Old San Francisco Rd. (APNs 209-17-050 & 209-17-051)

File #: 2018-7048

Zoning: R-3/PD

Applicant / Owner: Innovative Concepts / George Nejat

Environmental Review: Mitigated Negative Declaration

Project Planner: Ryan Kuchenig, (408) 730-7431, rkuchenig@sunnyvale.ca.gov

REPORT IN BRIEF

General Plan: Residential Medium Density

Existing Site Conditions: Two Single Family Homes

Surrounding Land Uses

North: Medium Density Residential - Condominiums (Pebble Creek Condos)

South: Medium Density Residential - Apartments (Renaissance Apartments across Old San Francisco Road)

East: Low Density Residential - Single Family Home

West: Medium Density Residential - Condominiums (Pebble Creek Condos)

Issues: Visual

Staff Recommendation: Recommend that the Planning Commission: Make the findings required by CEQA in Attachment 3, adopt the Mitigated Negative Declaration (Attachment 7); and approve the Special Development Permit for six residential townhome units and Vesting Tentative Map to subdivide two lots into six lots plus a common lot based on the findings in Attachment 3 and with the recommended conditions of approval in Attachment 4.

BACKGROUND

Description of Proposed Project

The project site is .34 acres in size and is currently developed with two single family homes. The application includes the redevelopment of the site with the construction of six townhomes through a Special Development Permit. In addition, a Tentative Map is proposed for the subdivision of the existing two lots into six individual townhouse lots and a common lot.

See Attachment 1 for a map of the vicinity and mailing area for notices and Attachment 2 for the Data Table of the project.

- **Special Development Permit**

18-0648Agenda Date: 8/13/2018

A Special Development Permit (SDP) is required for site and architectural review on project sites with the PD designation. A SDP also allows for consideration of deviations from specified development standards in exchange for superior design, environmental preservation or public benefit. The findings required to grant a SDP are discussed in Attachment 3.

- **Vesting Tentative Map**

The Tentative Map is required prior to a Final Map for the creation of six individual ownership lots and one common lot. The Tentative Map shows the location of the proposed lot lines, public and private streets and other improvements (see Attachment 5). A Vesting Tentative Map grants the developer the right to build the project for the life of the map and secures the approved project against future Sunnyvale Municipal Code (SMC) amendments that might otherwise affect the project. The Vesting Tentative Map is valid only in conjunction with the approved site plan and conditions of approval. The Tentative Map conditions of approval are listed in Attachment 4. The Final Map is approved by the Director of Public Works and must be in substantial conformance to the Vesting Tentative Map.

Previous Actions on the Site

A proposal to rezone the site from R-0 to R-3/PD and Special Development Permit/Tentative Map for six townhouses was considered by the Planning Commission and City Council in March and April of 2017. Ultimately, the City Council approved the rezone but denied the Special Development Permit/Tentative Map, based on concerns with the architectural design. Specifically, it was noted that the upper story massing appeared too large and that building materials needed to be improved. The project, as proposed, meets the R-3 standards.

A public hearing was held for the project on July 9, 2018. At the hearing, the applicant requested a continuance to respond to public comments that were received and make corrections to the plans. Two neighbors spoke at the hearing in opposition to the proposal noting concerns with the design and density of the project. A copy of the Minutes is provided in Attachment 9. Since the continuance, the applicant has revised site, civil, and landscaping plans to clarify the proposal and correct inconsistencies. The applicant has also submitted a letter to the Planning Commission (see Attachment 10), which summarizes modifications since earlier designs, and as a response to concerns from the public.

EXISTING POLICY

General Plan Goals and Policies: Key goals and policies from the Land Use and Transportation Chapter of the General Plan, Council Policy Manual and Citywide Design Guidelines which pertain to the proposed project are provided in Attachment 3.

ENVIRONMENTAL REVIEW

A Mitigated Negative Declaration has been prepared in compliance with California Environmental Quality Act (CEQA) provisions and City guidelines (see Attachment 7). An Initial Study has determined that the proposed project would not experience or create any significant environmental impacts with implementation of the recommended mitigation measures. Environmental issues that required mitigation include historical and cultural resources and hazardous materials. The Mitigation Measures have been incorporated as Conditions of Approval (see Attachment 4).

DISCUSSION

Present Site Conditions

18-0648Agenda Date: 8/13/2018

The approximately 14,977 sq. ft. (0.34 acre) project site is located on Old San Francisco Road and is developed with two one-story single family homes. The property is currently zoned R-3/PD (Medium Density/ Planed Developed). The project site is adjacent to two-story multi-family condominiums zoned R-3/PD to the west and north. East of the site are single family homes (R-0) that continue along Old San Francisco Road to Gail Avenue. The Renaissance Apartments (R-3) are located across Old San Francisco Road. VTA bus stops are located within (300-400 feet) of the project site. The Wolfe-Reed shopping center and Braly Park are located approximately 0.3 miles away from the site.

Special Development Permit

Use: The application is for the construction of six three-story multi-family residential townhouses. The existing two one-story single family homes will be demolished. A detached garage on the 673 Old San Francisco Road property and two sheds on the 669 Old San Francisco Road property will also be demolished.

Development Standards

The project complies with all applicable development standards in the SMC, such as setbacks, density, lot coverage, parking, landscaping, usable open space and solar access. The "Project Data Table" in Attachment 2 summarizes the project's compliance.

Site Layout and Architecture

The proposal would combine two lots and replace the two existing driveways positioned adjacent to a shared property line with one new centralized driveway off Old San Francisco Road. Two three-story three-unit buildings are proposed centrally on the site. Each building is slightly angled away from the front property line while parallel to the east and west property lines. The project plans are included in Attachment 5.

The proposed architectural design is contemporary in style. The previous proposal included a predominantly stucco material. This revised design includes a combination of stucco and horizontal wood siding material along the façade and stacked stone treatment within much of the first story. The stone wraps from the interior private street side elevations to the south side facing Old San Francisco Road. Wood material will also be utilized for balconies and window treatment including trim and faux shutters. A project rendering is included in Attachment 6.

Entrances are provided for the middle units at the garage level, which lead to a covered stairwell to the second floor. Covered exterior entrances are provided for the rear units, similar to the front units, but face the interior street, so that additional privacy is provided with respect to the neighboring property to the north. In response to privacy concerns raised by neighbors to the north (Pebble Creek Condos), the third story bedroom windows for each building were modified to a raised sill height.

To address concerns raised at the City Council hearing as part of the previous proposal, each of the end units (total of four) were reduced in size through the removal of one bedroom, resulting in the four end units now having three bedrooms while the two middle units retain four bedrooms. This modification reconfigures and reduces floor area within the second and third stories to improve massing and enable the overall design to appear less top-heavy. Overall, each of the units are smaller by approximately 100 to 300 square feet compared to the previous project. All six units retain two car garages. The project is in a flood zone that requires living space to be elevated; therefore, no

18-0648Agenda Date: 8/13/2018

living area is proposed for the first floor of the units.

The R-3 zoning district has a maximum height limit of 35 feet; however, since the project site is adjacent to a single family residential zoning district, Sunnyvale Municipal Code further restricts the height of townhouse uses to 30 feet (which is measured from top of the public curb). The height of the proposed buildings meets this standard at 29 feet when measured from the top of public curb. The site will be graded down slightly; therefore, the height of the home, when measured from the interior private street, is slightly taller (31 feet 3 inches), but meets the zoning requirement.

Setbacks

The previous proposal included a deviation to the front setback. This revised proposal meets all setback requirements. A list of the proposed setbacks is listed in the Project Data Table in Attachment 2.

Parking

Each unit provides two covered spaces within an enclosed garage. The garages exceed 450 sq. ft. in area to accommodate storage of solid waste and recycling carts as well as bicycle parking. The project provides the required guest parking with four spaces located towards the rear of the site. The location of the uncovered parking spaces meets City standards.

Landscaping and Tree Preservation

The project meets landscaping requirements by providing approximately 555 sq. ft. of landscaped area per unit where a minimum of 425 sq. ft. per unit is required. The proposed landscaping plan shows decorative pervious paving within the vehicular driveway entrance and through the private drive aisle. Decorative pavers are also planned to be utilized for the pedestrian walkways leading up to the front units as well as guest parking spaces and a common area at the rear corner of the site. In response to comments from a recent study session, the entire area has been modified to utilize pervious pavers.

There are two significant size trees on the site that are in poor condition and planned to be removed. Four other smaller fruit trees in poor condition would also be removed.

Several trees are located just beyond the property line to the west and north. Per the Conditions of Approval, tree protection measures are required to ensure these trees are not impacted during construction. Approximately 23 trees of varying size are to be added on site, plus four new street trees. To provide screening and improve privacy between the adjacent properties, nine of these trees would be located along the rear property line and eight within the yards of the individual units. These trees were selected based on input from residents of the neighboring development and the City Arborist.

The applicant discussed the condition of the six-foot wooden fence with the property management of the neighboring Pebble Creek Condos development, and it was agreed that a one-foot lattice would be added to the existing fence along the north and west property lines. Currently, the applicant is proposing to keep the six-foot wooden fence along the eastern boundary abutting the single-family home. Similar six-foot fences are proposed between the individual yards of the proposed units. In response to concerns raised by neighbors to the north, the proposed plan increases the landscape buffer at the rear from four feet behind the driveway and parking area to eight feet. The proposed species of these buffer trees (*Lagerstroemia Indica Natchez*) has also been coordinated with the adjacent complex.

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In response to recommendations made at a recent Planning Commission study session, enhancements have been made to improve the visual appearance of the homes along Old San Francisco Road. Landscaping has been added through a raised berm with shrubs to soften the south facing elevation. The number of steps to the entrance has also been reduced from three to one.

Usable Open Space

The project exceeds the minimum usable open space requirement with approximately 500 sq. ft. of area per unit proposed, where 400 sq. ft. per unit is required. Each unit includes a private rear patio. In addition to the private patio areas, common useable open space is provided at the northeast corner of the site, as currently designed with a decorative paved (pervious) surface.

Trash and Recycling Access

The project will utilize service similar to single family homes with curbside pickup along the public street. The previous proposal required trucks to enter the site and back up into the right-of-way. City staff recommended that the location for this service be modified so that trucks are no longer entering the private street, but rather would pick up along Old San Francisco. This service would be similar to single-family homes east the site. The bins will be brought to locations by homeowners to curbside locations as shown on the provide site plans.

Density

The project would create six lots for individual ownership and a common lot for the drive aisle and open space at the north end of the site. The unit count is within the R-3 standard of an average of 1,800 sq. ft. per dwelling unit. The project exceeds the minimum standard with 2,496 sq. ft. per dwelling unit. The common lot provides each property with access to a public street. The proposed subdivision meets the City's General Plan policy for building at least 75% of the allowable density for the R-3 Zoning District.

FISCAL IMPACT

The project is subject to Park In-lieu and Traffic Impact Fees, as noted in the Conditions of Approval. The project is also subject to payment of school impact fees to the Santa Clara Unified School District.

PUBLIC CONTACT

- Public Hearing Notice
- Published in the *Sun* newspaper
- Posted on the site
- 1,526 notices were mailed to property owners and residents within 1,000 ft. of the project site

Staff Report

- Posted on the City's Web site
- Provided at the Reference Section of the City's Public Library
- Made available at the City's One-Stop Permit Center

Agenda

- Posted on the City's official notice bulletin board

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- Posted on the City's Web site

As of the date of staff report preparation, staff has received 11 letters from seven members of the public noting concerns with the proposal (See Attachment 8). The comments note concerns with increased population, traffic, three-story design, and the impact to the value of nearby single family homes. Additional concerns note the location of parking for the new development, smoking by new residents and request for additional landscaping and reduced construction hours.

Outreach Meeting

An outreach meeting was held at Braly Park on March 29, 2018. Approximately 15-20 people, mostly neighbors of the project site, attended the meeting. Neighbors noted concerns related to the increased number of units proposed for the site, as well as privacy impacts and lack of parking. Increased landscaping was also recommended. As noted in the staff report, the applicant has since modified the project to reduce privacy impacts, including higher sill height windows, increased landscape buffer area, and added additional trees at the rear of the site.

ALTERNATIVES

1. Make the findings required by CEQA in Attachment 3, adopt the Negative Declaration; approve the Special Development Permit for six residential townhome units and Vesting Tentative Map to subdivide two lots into six lots plus a common lot based on the findings in Attachment 3 and with the recommended conditions of approval in Attachment 4.
2. Make the findings required by CEQA in Attachment 3, adopt the Negative Declaration; approve the Special Development Permit and Vesting Tentative Map with modified findings or conditions.
3. Make the findings required by CEQA in Attachment 3, adopt the Negative Declaration; deny the Special Development Permit and Vesting Tentative Map.
4. Do not adopt the Negative Declaration and direct staff as to where additional environmental analysis is required.

STAFF RECOMMENDATION

Alternative 1: Make the findings required by CEQA in Attachment 3, adopt the Negative Declaration; approve the Special Development Permit for six residential townhome units and Vesting Tentative Map to subdivide two lots into six lots plus a common lot based on the findings in Attachment 3 to the report and with the recommended conditions of approval in Attachment 4 to the report.

Prepared by: Ryan Kuchenig, Senior Planner

Reviewed by: Gerri Caruso, Principal Planner

Reviewed by: Andrew Miner, Assistant Director of Community Development

ATTACHMENTS

1. Vicinity and Noticing Radius Map
2. Project Data Table
3. Findings for Approval and General Plan Goals and Policies
4. Recommended Conditions of Approval

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5. Project Plans and Tentative Map
6. Project Rendering
7. Mitigated Negative Declaration
8. Letters from Interested Parties
9. Excerpt of Minutes from the Planning Commission Meeting on July 9, 2018
10. Letter from the Applicant to the Planning Commission

PUBLIC HEARINGS/GENERAL BUSINESS

2. [18-0648](#) CONTINUED FROM THE JULY 9, 2018 PUBLIC HEARING
- Proposed Project:** Related applications on a .34-acre site:
 SPECIAL DEVELOPMENT PERMIT and VESTING
 TENTATIVE MAP to allow a three-story six-unit townhouse
 development.
- Location:** **669 & 673 Old San Francisco Rd.** (APNs 209-17-050 &
209-17-051)
- File #:** 2018-7048
- Zoning:** R-3/PD
- Applicant / Owner:** Innovative Concepts / George Nejat
- Environmental Review:** Mitigated Negative Declaration
- Project Planner:** Ryan Kuchenig, (408) 730-7431,
rkuchenig@sunnyvale.ca.gov

Senior Planner Ryan Kuchenig presented the staff report.

Commissioner Howe asked staff about the exclusion of the City Council minutes in the staff report. Senior Planner Kuchenig advised that previous project proposals and associated minutes are generally excluded. Commissioner Howe asked staff about the changes in massing and square footage. Senior Planner Kuchenig provided an approximation of the change in square footage per unit. Commissioner Howe asked staff for the overall statistics to ensure that the proposed project meets the City Council's direction. Senior Planner Kuchenig stated that the City Council's direction was to enhance the proposed project's design and reduce the massing.

Vice Chair Simons confirmed with Senior Planner Kuchenig that staff would agree to the use of a darker color for the garage doors to maintain overall architectural continuity.

Commissioner Weiss confirmed with Senior Planner Kuchenig that a Homeowners Association is required for four or more units. Commissioner Weiss confirmed with Senior Planner Kuchenig that 400 square feet is the minimum required size for a two-car garage.

Commissioner Harrison asked staff if guest parking could be considered a nuisance, as suggested in a public comment letter. Senior Planner Kuchenig advised that guest parking is not specifically considered a nuisance. Assistant Director Andrew Miner provided examples of items that would be considered a nuisance as they generate noise beyond what is allowed by the noise ordinance.

Commissioner Harrison confirmed with Senior Planner Kuchenig that there are no setback requirements for uncovered parking adjacent to other properties.

Commissioner Harrison asked staff to clarify the wording for the consideration of deviations associated with a Special Development Permit (SDP). Assistant Director Miner commented that the applicant is not requesting any deviations and referred to the SDP Findings in Attachment 3. Senior Planner Kuchenig provided examples of typical SDP justifications to Commissioner Harrison.

Chair Howard opened the Public Hearing.

Jeff Guinta, representing Innovative Concepts, presented information about the proposed project.

Vice Chair Simons confirmed with Mr. Guinta that he is amenable to the use of a darker color for the garage doors.

Erik Stauffer discussed his concerns with the proposed project's height, massing, placement of the front doors, neighborhood compatibility, demolition materials, recycling process, drainage, parking and protection for the existing trees.

Maria Hamilton, Sunnyvale resident, discussed her concerns with the proposed project's size, scale, neighborhood compatibility, density and location.

Gene Hoyle, Sunnyvale resident, discussed his concerns with the proposed project's mass, scale, neighborhood compatibility and the transition between adjacent uses.

Mr. Guinta presented additional information about the proposed project.

Chair Howard closed the Public Hearing.

Commissioner Howe asked staff about previous actions on the site. Senior Planner Kuchenig provided information about the options that City Council had for the approved rezone. Senior Planner Kuchenig returned to Commissioner Howe's earlier question and advised that the proposed project has been reduced by 8% Floor Area Ratio (FAR). Commissioner Howe confirmed with Senior Planner Kuchenig that massing has been reduced, mainly through reconfiguration of the upper stories to create a more balanced building.

Commissioner Harrison clarified with Senior Planner Kuchenig that the current solar ordinance does not apply to the shading of a patio or deck.

Commissioner Harrison clarified the intention of the Planned Development (PD) overlay with Senior Planner Kuchenig. Senior Planner Kuchenig stated that the applicant eliminated the request for deviations because of the redesign. Assistant Director Miner advised that a PD designation allows for design flexibility and the opportunity to request deviations instead of variances.

Chair Howard confirmed the adjacent zoning and density buildouts with Senior Planner Kuchenig. Chair Howard confirmed with Senior Planner Kuchenig that the applicant chose to recess the building to make the height more compatible with the neighborhood. Assistant Director Miner provided information about the height variations between the proposed project and adjacent uses. Chair Howard confirmed with Assistant Director Miner that 30 feet is the building height maximum in an R-0 zone.

MOTION: Vice Chair Simons moved and Commissioner Howe seconded the motion for Alternative 2 – Make the findings required by CEQA in Attachment 3, adopt the Negative Declaration; approve the Special Development Permit and Vesting Tentative Map with modified findings or conditions –

Staff shall review the garage door color to ensure that it is not too light with respect to the rest of the building.

FRIENDLY AMENDMENT: Commissioner Howe offered a friendly amendment that staff will have final determination of the garage door color. Vice Chair Simons

accepted the friendly amendment.

Vice Chair Simons stated that he can make the findings and that the proposed project now meets the City Council's requests. Vice Chair Simons stated that the building height will be less than the 30-foot maximum and that the Conditions of Approval have protective measures for the neighborhood trees. Vice Chair Simons responded to a public comment and noted that the front doors should lead directly to the street to have a street presence. Vice Chair Simons stated that the final design has greatly improved and that many recommendations have been incorporated.

Commissioner Olevson stated that he will be supporting the motion and thanked members of the public for presenting written and oral comments. Commissioner Olevson commented that many of the policies cited by members of the public have been superseded by the City Council's action to rezone this site to R-3/PD. Commissioner Olevson noted that the applicant has met all the requirements and that the proposed project should be approved per City Council policies.

Commissioner Howe stated that he can make the findings as required by CEQA to adopt the Negative Declaration. Commissioner Howe commented that the City Council decided to rezone this site to R-3/PD and that a building height is allowed of up to 30 feet. Commissioner Howe noted that the massing has been improved by an 8% FAR reduction and that the proposed project should be approved as it meets City Council policy.

Chair Howard stated that he will be supporting the motion and can make the findings. Chair Howard commented that the City Council approved the rezone but requested project revisions. Chair Howard noted that the improved proposed project has addressed the concerns of the City Council. Chair Howard stated an opinion that the applicant has tried to harmonize the R-3 development with the adjacent residential zoning.

Commissioner Weiss stated that she can make the findings for the Special Development Permit and the applicable Goals and Policies of the General Plan. Commissioner Weiss commented that the applicant has made every effort to make the proposed project compatible with the existing residential neighborhood and has not requested any deviations. Commissioner Weiss noted that the usable open space per unit exceeds the 400-square foot requirement and that the proposed project will add to the housing stock and increase availability for housing ownership.

Commissioner Weiss noted her concern regarding the lack of affordable housing in the proposed project but stated that it is not required.

Commissioner Harrison stated that she will be supporting the motion and noted that the applicant has not requested any deviations. Commissioner Harrison commented that the SDP complies with the planned residential density for the area. Commissioner Harrison noted that the project has improved by eliminating the requested deviations, reducing the massing and increasing the diversity of materials.

The motion carried by the following vote:

Yes: 7 - Commissioner Weiss
Chair Howard
Commissioner Howe
Commissioner Olevson
Vice Chair Simons
Commissioner Rheaume
Commissioner Harrison

No: 0

Assistant Director Miner stated that this decision is final unless appealed to the City Council within 15 days or called up by the City Council within 15 days.

Maria Hamilton
578 Ironwood Terrace
Sunnyvale, CA 94086

August 24, 2018

Sunnyvale City Clerk
603 All America Way
Sunnyvale, CA 94086

Re: Appeal of Planning Commission Decision at
Planning Commission Meeting of Monday, August 13th, 7:00 pm
File # 2018-7048 669 - 673 Old San Francisco Road (APNs: 209-17-050 & 051)
Application for Special Development Permit and Vesting Tentative Map to construct a six-unit residential development on the site

Dear Sunnyvale City Clerk,

I am writing to appeal the decision of the Planning Commission of Monday August 13, 2018, approving the development located at 669-673 Old San Francisco Road, Sunnyvale, CA 94086 (File No. 2018-7048).

Facts and Basis for Appeal

The General Plan, Citywide Design Guidelines (updated 2013, as amended), and City Design Techniques (specifically, Section 2.2 Basic Design Principles that apply to "all residential projects") direct that a development is to be constructed in a manner that respects its immediate neighbors, and is compatible with the streetscape and neighborhood. Partly due to the direction of the City Council on April 25, 2017 with regard to the original proposed development, wherein the Special Development Permit and Vesting Tentative Map were denied (File No. 2015-8059), the newly proposed development 's (File No. 2018-7048) design was modified and massing decreased (by approximately 8%). The quality of the architecture and materials were greatly improved.

However, as proposed, the massing and scale remain inappropriate for the neighborhood, incompatible with the streetscape, and need to be further reduced according to Sunnyvale's General Plan, Citywide Design Guidelines and City Design Techniques. The massing decreased by 8%, however, the FAR (floor to area ratio) of this development increased by a net 7.75% (from 75% to 81%) from the previous design (File No. 2015-8059 rejected by City Council on April 25, 2017), offsetting any decrease in the massing.

At the Monday August 13, 2018 Planning Commission hearing, the standards of applicable Citywide Design Guidelines with respect to this development as mandated by the General Plan (Policy 55, Action 1 and LT 5.3 of the Executive Summary) and enumerated in detail in my two comment letters (Report to Planning Commission, Attachment 8, Page 37 of 44 and Attachment 8, Page 10 of 44), as well as the comment letters of others (particularly Report to Planning Commission, Attachment 8 Page 2 of 44, which clearly shows the area in question designated as "Preserve" in the LUTE section of the General Plan (Chapter 3, page 10, Figure 1) were not applied. At least one planning commissioner essentially stated, and I am paraphrasing, "A building in an area zoned R3/PD can be 30' tall. This building is less than 30' tall, therefore, it meets the standard." The only modification to the plan was by Commissioner Simons, who directed that the garage doors be painted a darker color.

The hearing of August 13, 2018 was delayed for over a month (from July 9, 2018) partly because there were inconsistencies in the materials (site plan corrections) circulated to the Planning Commission: Specifically, outdated studies and renderings that were based on the old design. Another reason for the delay that was given was that the owner of the two parcels needed additional time to respond to letters from interested parties.

Inconsistencies remained in the attachments for the proposed development, which I brought to the Senior Planner's (Ryan Kuchenig) attention in an email dated August 7, 2018, specifically regarding the shading analysis. I inquired if the shading analysis/solar study had been updated to show an updated shading impact with respect to the revised architectural renderings. I was told it was current. Mr. Kuchenig replied via email "Those drawings should reflect the revisions to the design." They may have reflected

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revisions to the design but there is no proof that a current solar study was included in the Report to Planning Commission of August 13, 2018, as Attachment 5, pages 18 and 19. The fact that the date of the solar study was blurred on both pages 18 and 19 could suggest that the shading analysis and solar study were not only outdated, but possibly intentionally misleading, altered documents. The same is true of a previous report, particularly the Report to Planning Commission of July 9, 2018, Attachment 5, page 13, with respect to the southwestern-most building's entry area, which is within the setback. The distance to that building from the property line is blurred.

Furthermore, in the previous development proposal for the same site (File No. 2015-8059) which was subsequently denied by City Council on April 25, 2017, the date on the solar study was not blurred or unreadable. The materials circulated to the commissioners and public and posted online for the hearing of August 13, 2018 were altered (blurred and unreadable). Specifically, the date of the solar study on pages 18 and 19 of Attachment 5, in addition to the distance to the lot line of the westernmost building that is within the front setback on page 5 of Attachment 5. That building encroaches on the front setback. This illustration is perhaps deliberately obscured (the distance from the building entrance to the lot line is basically indiscernible, but it is less than the 20' setback) in Attachment 5, Page 5 of the materials circulated at the August 13, 2018 Planning Commission hearing.

As currently designed, the proposed development continues to maximize unit size and amenities at the sacrifice of thoughtful design that meets Sunnyvale standards as enumerated in the Municipal Code, General Plan, Citywide Design Guidelines and City Design Principles. The inclusion of so many bedrooms and parking spots (16, instead of the required 10) in each unit results in little room remaining on the .34 acre-site for setbacks, yards, open space, or the four guest parking spaces required under the municipal code. In an effort to cram all of these elements onto the two lots, one building is moved to within the required front setback, and four guest parking spaces are squeezed within the rear setback along the property line shared with my neighbors (Attachment A hereto).

Parking. Uncovered guest parking remains too close to the living room and bedroom areas of four condominiums (578 Ironwood Terrace units 5, 6, 11 and 12) on the north side of the proposed development. At that location, the landscape buffer is less than 4', contrary to the architectural rendering in Attachment A. Some guest parking would be less than 20' from those units. Additionally, ten of 12 units at 578 Ironwood Terrace would be subjected to the negative impact of exponentially increased daily exposure to carbon emissions, particulate matter and noise from a total of 16 vehicles coming and going from the site due to the proposed location of the driveway and uncovered guest parking. This will impair the existing uses being made by current and future occupants of those residences, affecting their quality of life and quiet enjoyment of the premises.

The Pebble Creek condominium complex adjacent to the proposed development was built in 1983. That development utilized high quality design which ensured no bedroom or living room areas were adjacent to parking, and avoided placing any uncovered parking area adjacent to any neighboring properties, including 669 and 673 Old San Francisco Road. All uncovered parking was situated in areas which would neither affect, nor be adjacent to, any adjacent R-0 homes.

Design improvements must be made to mitigate the negative impact of four uncovered parking spaces at the end of the driveway of the proposed development. The quality of life impact is a legal impact, affecting air quality, privacy and quiet enjoyment currently accessible to adjacent neighbors. The quality of life of adjacent neighbors and the existing uses of their respective residences should not be compromised nor sacrificed due to the current crammed design of the proposed development.

Massing. In accordance with the goals of the General Plan, particularly, (bold and italics added) **LUTE** (Land Use and Transportation Element of the General Plan) **Policy 55 (Executive Summary LT-5.3)** and **LUTE Policy 57 (Executive Summary LT-6.2)**, decreased massing is needed, especially on the third floor, to be in compliance with **Citywide Design Guideline 2.B5** below to allow for compatibility within the established neighborhood on the north side of Old San Francisco Road. A design that follows these policies and guidelines will minimize the shading, privacy, bulk, and general nuisance impacts on the adjacent property at 578 Ironwood Terrace, in particular:

Policy 55

Require new development, renovation, and development to be compatible and well integrated with existing residential neighborhoods.

Action 1: Utilize adopted City [sic] *design guidelines* to achieve compatible and complementary architecture and scale for new development, renovation, and redevelopment.

Policy 57

Limit the intrusion of incompatible uses and inappropriate development in and near residential neighborhoods, but allow transition areas at the edges of neighborhoods.

Action 1: Where appropriate, use higher-density residential and higher-intensity uses as buffers between neighborhood commercial centers and transportation and rail corridors.

As detailed in this letter, the proposed development does not follow the policies above. Further, with regard to **Citywide Design Guidelines 1.A3** and **2.B5**:

- The 18 du/acre proposed development is adjacent to a 14.22 du/acre condominium complex to the north and west, and a 6 du/acre single family home R-0 parcel to the east.
- The 14.22 du/acre condominium complex is built to Low-Medium Density, not built to Medium Density as incorrectly stated on the Report to the Planning Commission Summary of August 13, 2018: "North: Medium Density Residential - Condominiums (Pebble Creek Condos)." It is only .22 du/acres above the range for R-2 Low-Medium Density Residential zoning (7-14 du/ac), and .78 du/acres below the range for R-3 Medium Density Residential zoning (15-24 du/ac) (Sunnyvale General Plan, Land Use and Transportation, pages 81-82).
- The proposed infill (sensitive use) R-3/PD development, at 18 du/ac, must incorporate decreased massing and bulk in its design in order to comply with **Citywide Design Guidelines 1.A3** and **2.B5**:
 - 1.A3.** Develop transition between projects with different uses and intensities to provide a cohesive visual and functional shift. Create transition by using appropriate setbacks, gradual building height, bulk and landscaping.
 - 2.B5.** Step back upper stories of building [sic] three stories or taller from public roads and adjacent low scale development to reduce the bulk impact.

The north and east sides of the third floors of the planned development do not step back sufficiently as required by **Citywide Design Guideline 2.B5**, and mandated to be adhered to by General Plan Policy 55, Action 1, above. This guideline is necessary to minimize the bulk impact on adjacent low-scale development. At 14.22 du/acre, 578 Ironwood Terrace is a low-medium scale (R-2) development. Four of 12 units there will no longer have sunlight on their decks or patios on and around winter solstice according to the undated shading/shadow analysis. It is recognized that solar access is not a protected amenity or right, but it will have a quality of life impact on the residents of adjacent lower scale developments.

The size, FAR (floor to area ratio), and massing of the entire project at **18 du/acre** must be further reduced by way of design to allow for transition between uses and to be in compliance with **Citywide Design Guideline 1.A3** above, or it will visually be spot zoning between an R-2 scale condominium development (578 and 598 Ironwood Terrace at **14.22 du/acre**) and an R-0 zoned single family home development (717 Old San Francisco Road at approximately **6 du/acre**).

If the proposed development moves forward without any corresponding decrease in mass, scale and scope as required by the General Plan, it will be in direct violation of the General Plan of Sunnyvale LUTE (Land Use and Transportation Element of the General Plan) Policy 55, and LT-5.3 of the Executive Summary. The location of the proposed development is in an area designated "preserve" in the General Plan of Sunnyvale. This means minimal change to, and preservation of, the area, not upheaval.

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As designed, the proposed development's mass, scale and scope disregard Sunnyvale's General Plan to "preserve" the area in question. The .34 acre proposed development site is located on the General Plan map in an area designated as "PRESERVE" in the LUTE (p. 11, Figure 1). The definition of "PRESERVE" is as follows:

Area is expected to experience minimal infill and upgrades. Fundamental purpose, form and character stay the same.

Sensitive uses include infill developments. The proposed infill development site is adjacent to condominiums built in 1983 and a single story home built in 1970. As designed, the proposed development has the greatest massing of any residence on the north side of Old San Francisco Road between Ironwood Terrace and Gail Avenue, rendering it incompatible in size, scale and scope with adjacent properties.

A sensitive use design that would make better planning sense and align with the General Plan without interfering with the existing uses being made of adjacent properties would further decrease the massing, size, scale and scope of the planned development, particularly on the third floor, and/or reduce the current massing to create eight units with one or two bedrooms and a one car garage each. Our community and neighborhood would welcome eight units; allowing for one unit to be an affordable housing unit. Sunnyvale needs more affordable housing. This development avoided that requirement by reducing the originally planned number of units from eight to six.

Special Development Permit and Tentative Map

The applicant needs to (1) commit to design changes that more respectfully consider the negative impacts on, and the existing uses being made, of (a) all adjacent residences (including, but not limited to, substantially increased exposure to noise and particulate matter, and decreased privacy), and (b) the neighborhood (increased pollution, traffic and car trips, resulting in pedestrian safety concerns along Old San Francisco Road), or (2) directly provide community benefits in order to obtain a special development permit. Special development permits are provided to projects that improve neighborhoods through creative development, not to projects that prioritize profits over safe and thoughtful parking conditions, setbacks and neighborhood impacts.

With regard to the Vesting Tentative Map requirements, the negative impacts described above make it clear that (1) the design of the proposed subdivision is not consistent with the General Plan, (2) the site is not physically suitable for the proposed density of the development, and (3) the map fails to meet requirements imposed by Municipal Code (specifically, Sunnyvale Municipal Code Section 19.26.020).

Sunnyvale Municipal Code Section 19.26.020 states that the purpose of a planned development district is to "achieve superior community design, environmental preservation and public benefit." The proposed development will not achieve these goals. To the contrary, the overly crammed design will increase the likelihood of general conflict in the community, for example, as guests and residents attempt to park their vehicles in the four guest parking spaces at the end of the driveway. The noise and fumes from these conflicts will be located right outside the living areas of residents of 578 Ironwood Terrace, replacing what used to be the backyards of two single-family homes.

Summary of Facts

- FAR increased from previous project plans from 75% to 81% (net increase of 7.75%).
- Massing decreased 8% (this will be offset by the increased FAR).
- Pages on the Report to Planning Commission for the hearing of August 13, 2018, Attachment 5, Project Plans and Tentative Map, pages 5, 18 and 19 were altered – dates and numbers were blurred regarding setbacks and the date of the solar analysis.
- The southwesternmost setback in the front of the proposed development is not adhered to—the building is partly inside of the setback, even though not starred on the project data table.

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- The planning commission hearing scheduled for July 9, 2018 had to be postponed to August 13, 2018 because of inconsistencies in the architectural renderings (I had pointed these out to Mr. Kuchenig, Senior Planner – the old design was used in some attachments).
- Shortly before the August 13, 2018 hearing, I was looking at the shading analysis. I thought, if they decreased the massing on the third floor by 8%, why is the shading analysis/solar study the same? I asked Mr. Kuchenig if it had been updated and his reply on August 7, 2018 was: "Those drawings should reflect the revisions to the design." I could not tell the date of the solar study/shading analysis because it was blurred out/erased.

Conclusion

It is important to balance property rights and development opportunities with quality of life impact concerns of existing residents. As a community, we care about our neighborhood as well as the Sunnyvale design techniques and development guidelines that are in place to protect the public interest. One of our goals as a community representing the public interest is to be sure that developers are held to the standards of Sunnyvale's Municipal Code regarding planned development districts (cited above), its General Plan, Citywide Design Guidelines, and City Design Techniques (Section 2.2 Basic Design Principles, Attachment B hereto). The carefully articulated directives of those codes, goals, policies, guidelines, and principles, if followed, will allow for a development that the community can support.

Action Sought

Deny the special development permit and tentative map for the reasons outlined in this letter. If the special development permit and tentative map approvals are retained, it is requested that good faith measures be taken to protect adjacent residents and the neighborhood from the negative impacts enumerated in this letter. Utilizing Citywide Design Guidelines and Design Techniques (which apply to all residential neighborhoods) will more appropriately align the proposed development with the goals and policies of the General Plan, and allow for more compatibility among the proposed development, the neighborhood and streetscape, and the intent of planned development districts. This can be accomplished by:

- Decreasing the size and massing of all units to more appropriately accommodate guest parking, open space and setback requirements;
- Decreasing the size and massing of the units at the back (north) end of the lot, especially the third floors, to reduce shading, privacy, bulk and general nuisance impacts to neighboring residences; and
- Relocating the guest parking outside of the rear setback.

Sincerely,



Maria Hamilton
 Sunnyvale, CA

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August 24, 2018
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**Supporters of the "Action Sought" on Page 5:
Sunnyvale, CA**

Susan M. Bowley, Ph.D.
Barry Cooper
Carolyn Cooper
Alice Delgado
Hannah Ewalt
Diego Gonzalez
Eugene Hoyle
Debbie Hoyle
Victoria Jain
Neil Jain
Michael Jeong
Cindy Kushner
Zachary L.
Carolyn Larsen
Larry Larsen
Tian Lian
Anzhelika Milstein
Cece Morrison
Dong Park
Britta Puschendorf
Angel Ramirez
Flora Rivera
Arushi Sabharwal
Becky Shan
Erik Stauffer
Katie Stauffer
Carolyn T.
Ashley Wolf

Living areas indicated by //////////////.

Attachment B

SUNNYVALE SINGLE FAMILY HOME DESIGN TECHNIQUES COMMUNITY EXPECTATIONS

2.2 BASIC DESIGN PRINCIPLES

These design principles should be respected in all residential projects. They are the touchstones upon which all of the following design techniques are based, and, since design guidelines cannot anticipate every condition that might occur, they will be used in addressing conditions not specifically covered in the more detailed sections that follow.

1. REINFORCE PREVAILING NEIGHBORHOOD HOME ORIENTATION AND ENTRY PATTERNS

Maintain a sense of neighborhood by facing residences and home entries to primary public or private streets, providing convenient pedestrian access from the street, and including front windows, where common, to provide "eyes on the street" in order to enhance neighborhood safety.

2. RESPECT THE SCALE, BULK AND CHARACTER OF HOMES IN THE ADJACENT NEIGHBORHOOD

Buildings should be sympathetic to the predominant building forms and scale of their neighborhoods, including but not limited to, height, bulk, character, building form, roof form and orientation, window treatments, materials, and colors. Architectural styles, elements, and shapes need not necessarily be the same as those on adjacent and nearby homes, but improvements should avoid unnecessary visual conflicts.

3. DESIGN HOMES TO RESPECT THEIR IMMEDIATE NEIGHBORS

Every project should be respectful of adjacent homes and neighbors. New development should avoid privacy, noise, light and visual conflicts with adjacent uses to the maximum degree possible. Special care should be given to avoid tall blank walls and building volumes immediately adjacent to one story forms on adjacent parcels, and to the placement and treatment of windows and site landscaping to minimize views into neighboring homes' windows and private outdoor spaces.

4. MINIMIZE THE VISUAL IMPACTS OF PARKING

Wherever possible, garages and their paved access drives should be subordinate to, rather than dominating, the entry and architecture of the house. In cases where garages are a major part of the street front in a neighborhood, existing patterns may be followed, but steps should be taken to soften the visual impact of the garage fronts. Visual elements might include landscape divider strips in the paving between garage entries, dividing double garage faces into individual doors, adding landscape trellises and lattices to soften garage fronts with landscaping and taking steps to provide special emphasis on the front entry.

5. RESPECT THE PREDOMINANT MATERIALS AND CHARACTER OF FRONT YARD LANDSCAPING

In neighborhoods where there is a discernible landscape character along street fronts, new home landscaping should take that into consideration. Where front landscape areas are primarily composed of living plant materials, that pattern should be repeated.

6. USE HIGH QUALITY MATERIALS and CRAFTSMANSHIP

Quality materials require less maintenance to remain attractive over time, and they convey a sense of pride in one's home.

7. PRESERVE MATURE LANDSCAPING.

Wherever possible, mature trees should be protected during construction and integrated into new landscape plans.

EXCEPTIONS

Design guidelines cannot address every possible condition for every type of neighborhood or architectural style in the City. Although the principles set forth on this page and the guidelines contained in the following sections will be applicable for most cases, there may be unique characteristics of individual neighborhoods or specific sites. Where conflicts between the principles and guidelines in this document are in conflict with the specific characteristics of a neighborhood, reviews and approvals will be based on the most appropriate methods of fitting new construction into the context of existing neighborhoods.

September 19, 2018

To: Honorable City Council,
Subject: Response to Neighbor Appeal to our project at 669 Old San Francisco Road.
From:
George Nejat – Property owner and project applicant

We respectfully request that the appeal to our project be considered by the full quorum. Our project, that includes construction of (6) Townhomes in a R-3/PD zoning district, redevelops the site which is currently developed with two single family homes.

Some of the Council members will be familiar with our project that was appealed, considered and denied by the Council in 2017. We heard the Council's concerns and have modified the project for increased neighborhood compatibility and improved building architecture. This improved project was approved by the Planning Commission by a 7-0 vote in August 2018.

Modifications to our project include:

1. Moved the buildings further back to meet front yard setback requirement;
2. Increased third story setbacks by reducing floor area;
3. Modified front entry areas to units facing Old San Francisco Road; for a more street level appearances.
4. Increased window sill heights to (60 inches) at 2nd and 3rd story level that face neighboring property on the north;
5. Increased width and area of landscaping for improved landscaping plantings and increased neighbor privacy.

The appellant has commented on several aspects of our project that begs a response from us:

a. FAR Data

Response: As per Sunnyvale's development standard, there is no FAR (Floor to area ratio) standard for the R-3 Zoning district; but to the Appellant's comment regarding FAR, when using the correct square footage of the buildings, the project is now at 81% FAR

Response Letter to Neighbor Appeal
Page 2 of 4

(12,190 s.f./ 14,977 s.f.) and it is lower than our previous project's FAR of 91%. The appellant has incorrectly calculated our previous FAR at 75%.

b. Massing

Response: Massing reduction of our current, approved project is 10.5% from the original design which is greater than the 8% reduction noted by the Appellant.

- The massing reduction for 3rd (top) Floor is 26.5%,
- Massing reduction for total area excluding Garages is 16%
- Massing reduction for total area including Garages is 10.5%

c. Inconsistencies

The appellant notes that our project plans were altered as they appeared blurred and unreadable.

Response: The project plans were perfectly legible and no part of it was blurred or unreadable and nothing was altered. Our plans were acceptable by Planning Division staff and deemed fit for public hearing with the Planning Commission. The Planning Commission was able to review our project and associated plans and approved our project. We are happy to share our plans in electronic format or the full-sized plans with the appellant and/or with the City Council.

There have not been any inconsistencies in our project plan;

- The average front setbacks exceed (20 ft.); it is shown on setback Table on sheet A0
- For Building 1, the average front setback is (20.7 ft.) or (20' 8")
- For Building 2, the average front setback is (23.5 ft) or (23' 6")

The plans for August 13, 2018 Plan Commission Public hearing were electronically sent to the project planner at the City on August 3, 2018
Response Letter to Neighbor Appeal
Page 3 of 4

by Sheila Quinta of Innovative Concepts. The appellant is more than welcome to view our email dated 8/3/18 or the full size drawings by contacting the project planner at City Hall.

d. Lack of Solar Study

Response: As required by Sunnyvale Code requirement, we have submitted Solar Plans to the Planning Division. The plans (Sheet A8) shows that the proposal would shade 7.3% of the neighboring roof where a maximum shading of 10% is allowed. The previous design denied by the City Council on April 2017 had a 10% shading.

e. Maximizing Unit size and amenities

Response: Our project includes six units that range from 1,449 s.f. to 1,570 s.f. We believe we have drastically reduced 1,350 square feet (1,350/5,098) or 26.48% from 3rd Floor (Upper story – top). Our project provides modest house size and we are not maximizing unit size and amenities.

In conclusion, we have:

- Proposed a design that has no deviations from the Sunnyvale Municipal code.
- All setbacks have been met or exceeded.
- This project meets all (R-3) Zoning development standards (setbacks, parking, building height, lot coverage, usable open space, etc.)
- Substantially reduced the massing and improved the quality of architecture and materials; even the appellant in her appeal letter commented the quality of the architecture and materials were

greatly improved.

Response Letter to Neighbor Appeal
Page 4 of 4

- Applied all of City Council and Plan Commission's recommendations and comments;
- The project provides housing for (6) families with a net increase of (4) units.
- The Planning Commission approved this project unanimously (7-0) on August 13, 2018 public hearing based on merit of this Project.

We realize that the project changes existing conditions and understand that sometimes change is not easy especially when it occurs close to your house and so we have gone above and beyond to address all the valid concerns from the neighbors that were feasible, including reducing building size and square footage. We have done everything that is feasible to make our project acceptable to the neighbors; but we cannot afford to keep the property as is.

The changes that we made based on City Council direction has resulted in a project that is improved in all areas especially neighborhood compatibility. We hope the City Council finds that the project meets City standards, is compatible with the neighborhood and approves the project that meets Sunnyvale standards and expectations.

Thanks for your consideration.
Truly yours,

George Nejat

October 21, 2018

456 W Olive Avenue
Sunnyvale, CA 94088

Subject: File No. 2018-7048 669-673 Old San Francisco Road (APN: 209-17-050 & 209-17-051)

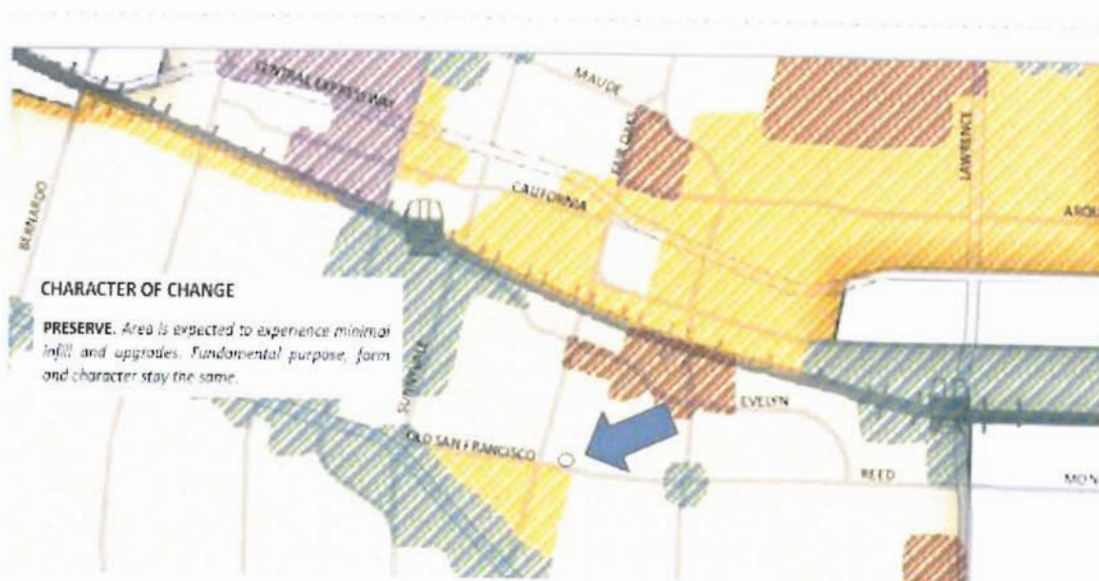
I'm concerned for the character of our neighborhood. Developing townhouses on this particular parcel is out of character with the surrounding residences. This negatively affects the look, feel, and community of the neighborhood. The parcels in question are bordered by R-0 single family homes and condominiums built to R-2 standards with 14 units per acre. An R-3 development is out of place and is effectively spot zoning.

The proposed structure is much bigger, taller (in relation to adjacent buildings on Old San Francisco road), and denser than the surroundings. This does not preserve the character of the neighborhood, and in fact, significantly degrades it.

Traffic and parking in the area are already stressed. Many cars cut the corner at Old San Francisco and Wolf road by going through the neighborhood there (using Blue Sage and Gail), and at high rates of speed. Parking along neighborhood streets is commonly full. The proposed project adds to the crowding, traffic, and parking stress in the area. This negatively impacts the quality of life for the existing residents.

These parcels are marked as preserve on the general plan. I'd like to request consideration of the existing residents and that the area be preserved in accordance with the General Plan.

LUTE – Page 11, Figure 1 (Updated April 2017)



The size of the proposed development at 669-673 Old San Francisco Road is too large for the location. This is inappropriate development. We are opposed to the large scale, scope and massing of the proposed development at 669-673 Old San Francisco Road.

I/We respectfully request City Council grant the appeal, and not approve the:

(1) Special Development Permit and (2) Tentative Map with regard to this project.

Name(s): E. Stauffer
 K. Stauffer

Address: Sunnyvale, Ca

Ryan Kuchenig

From: Council AnswerPoint
Sent: Tuesday, October 23, 2018 11:04 AM
To: Jennifer Nunez
Cc: Kent Steffens; Teri Silva; Trudi Ryan; Deborah Gorman; Ryan Kuchenig; CityClerk AP; Andrew Miner
Subject: POLICY--FW: File No. 2018-7048

Councilmembers:

Forwarding to you from Council AnswerPoint.

Jennifer Nuñez
Executive Assistant- Mayor & Council
Office of the City Manager
City of Sunnyvale
Phone: 408-730-7913

From: Debbie & Gene Hoyle [mailto:;]
Sent: Tuesday, October 23, 2018 10:02 AM
To: Council AnswerPoint <council@sunnyvale.ca.gov>
Subject: File No. 2018-7048

Sunnyvale City Council:

We continue to be **opposed to the subject development** (see our 2 July 2018) letter in your package.

The proposed project is **visually unlike any structures adjacent to it**, and is unsightly. It is **not consistent with the city goals** of new developments melding into the look of the existing neighborhood, and hints at a city goal of further development at any price regardless of the wishes of the adjacent community.

The enclosed garages will likely be used for storage or more living space, and will **further complicate the vehicle parking problems** on Old San Francisco and likely **push street parking into adjacent streets and private properties**.

There is already not enough parking for apartments and housing in the area, therefore our **neighborhood is now filled with street parking**. This development adds to the problem.

Additionally, even though city studies have said there is no traffic impact or safety issues with this project, of course there is. It **adds to the already increased amount of traffic and also the safety issues** of cars cutting through from Wolfe through Gail Avenue to Old San Francisco. This was a small, quiet neighborhood; it is no longer. Please don't add to these problems by allowing 3 and 4 bedrooms per unit.

We are so disappointed that this project was approved at last meeting and request you listen to the neighbors and to our wishes vs. the developer at this appeal meeting on 10/30/18. We would like to see less massing in this project. We thank you for reconsidering.

File No. 2018-7048

The size of the proposed development at 669-673 Old San Francisco Road is too large for the location. This is inappropriate development. We are opposed to the large scale, scope and massing of the proposed development at 669-673 Old San Francisco Road.

I/We respectfully request City Council grant the appeal, and not approve the:

(1) Special Development Permit and (2) Tentative Map with regard to this project.

Name(s): Eugene Hoyle
Debbie Hoyle

Address:
Sunnyvale

File No. 2018-7048

Location: 669-673 Old San Francisco Road, Sunnyvale, CA 94086

This is a new application. It is not a modification of an application, because the original application for a (1) special development permit and (2) vesting tentative map was denied by City Council on April 25, 2017 (File No. 2018-8059).

Original plans from 2015 show the owner wanted to develop a total of eight units, each four stories tall. The developer was told by the Planning Review Committee that he would have to have a below-market rate unit if he had eight units. He reduced the number to seven. The owner was also told that City code prohibited him from building four stories tall because of the single family home adjacent to the development on Old San Francisco Road is zoned R-0. City Code prohibits any building over 30' tall next to homes zoned R-0.

After a study session hosted by the Planning Commission at City Hall, and input from neighbors in April of 2016, the number of units was reduced to six. From April 2016 until April of 2017, I worked with the senior planner to incorporate concerns of adjacent neighbors and the neighborhood. The owner and developer never did reach out to the community separately, except through City mandated community meetings, and an email sent to an adjacent neighbor at the rear of the property regarding a bordering fence, which was not a call to be made by an individual property owner, but the Board of the Pebble Creek Condominiums.

A Planning Commission meeting that incorporated all of concerned neighbors' input via public comment and emails was held in March of 2017, and the plan passed by a vote of 5-2. However, a former mayor of Sunnyvale actually voted against the proposed development and inferred it was spot zoning – John Howe. The vice chair of the Planning Commission voted against it as well – Carol Weiss. Both are still on the Planning Commission.

The project was set for a City Council hearing on April 25, 2017. Councilmember Nancy Smith was absent. The proposed rezoning to R-3/PD passed, but the (1) special development permit and (2) vesting tentative map failed. City Council gave direction to the applicant to decrease the massing and improve the architectural details, such as quality of materials. In 2018, the property owner submitted a new application (File No. 2018-7048).

For reasons enumerated in the public comments of the Planning Commission hearing of August 13, 2018, as well as my appeal letter dated August 24, 2018, neither the special development permit nor the vesting tentative map should be approved unless and until the applicant adheres to Sunnyvale Municipal Code, the Sunnyvale General Plan, Citywide Design Guidelines and City Design Techniques. Had the developer adhered to the mandates in those codes, goals, policies and guidelines, which represent the public interest, we would not have had to file an appeal.

Council Policy Manual
Policy 1.1.5

“In recognition of the jobs/housing imbalance and related problems, the Sunnyvale City Council

- **Commits itself to encourage not only jobs and housing for as many of our citizens as possible but also to maintain and improve our quality of life. The City Council considers these four components – jobs, housing, transportation, quality of life – as inseparable when seeking solutions.”**

The City's primary focus on jobs and housing as opposed to transportation and quality of life needs to shift. The Policy Manual states all four of these issues must be considered: Focusing only on jobs and housing puts a heavy toll on the City's transportation system and ignores quality of life issues.

The proposed development at 669-673 Old San Francisco Road is incompatible in size, scale and scope with adjacent buildings and the neighborhood on the north side of Old San Francisco Road. This is essentially spot zoning, which is being pursued by the owner and developer, and needs to be stopped in Sunnyvale. The proposed development's initial privacy-encroaching footprint at 90%+ FAR was too large, too tall, too wide, and incompatible with surrounding residences. The first iteration of the project (2015-8059) was denied by City Council on April 25, 2017. Additionally, according to the senior planner on the project, Ryan Kuchenig, in an email to me dated October 9, 2018, he stated the applicant miscalculated the total square footage of the development, omitting one of the units in the calculation submitted to the Planning Commission and City Council: "The previous project plans had incorrectly tabulated the total of each of the units on the plans. The floor area of each of individual units was correctly noted on the plans; however, the listed 'total' tabulation of floor area was not."

Remaining front setback deviations further illustrate the proposed development's incompatibility with the neighborhood. If the proposed development were compatible with the general plan, no deviation would be necessary. Demolishing two moderate single family homes for the proposed development at the site will compromise the quality of life for surrounding neighbors and the community by contributing to:

- increased traffic on Old San Francisco Road;
- amplified pedestrian and bicyclist safety issues due to the effect of up to 16 vehicles entering and exiting one driveway multiple times daily;
- permanent noise pollution;
- permanent decrease in air quality due to an increase in particulate matter; and
- loss of privacy due to multiple overlooking windows on adjacent properties.

In exchange for additional living space for perhaps 20 people, the proposed development guarantees the erosion of quality of life, directly contributing to an increase in the negative effects of noise, pollution, and traffic elements in the neighborhood in general and on the north side of Old San Francisco Road in particular. The proposed development may turn into a corporate off-campus dormitory-type dwelling, with one or two adult individuals occupying each room. This could translate into an additional 30 or more cars in the neighborhood. The adjacent R-0 single family home neighborhood is already adversely impacted by traffic attempting to cut through Old San Francisco Road as a shortcut to avoid the light at the Old San Francisco Road and Wolfe Road intersection. Traffic may increase exponentially after the completion and full occupancy of two new Apple campuses nearby: (1) Cupertino at Wolfe and Highway 280; and (2) Wolfe Road and Central Expressway. The increased congestion on city streets will no doubt degrade current residents' quality of life.

The net value to the City will be to increase by 3.3 times the current housing capacity of the location from six to 20 bedrooms, and increased revenue from property taxes. The proposed development will impose additional traffic, pedestrian and bicyclist safety concerns for those who live and work near the proposed development due to multiple vehicles entering and exiting the site from one driveway, and possibly up to 40 additional individuals living on the premises. Regrettably, the City deemed that no traffic or pedestrian safety study was needed.

It is impractical and unrealistic for Sunnyvale to maintain a goal to provide enough housing to allow anyone who works in Sunnyvale to live in Sunnyvale. City administrators need to accept the fact that Sunnyvale cannot have loads of jobs and loads of housing without significantly degrading the quality of life for existing residents by overwhelming its already insufficient infrastructure.

Council Policy Manual **Policy 1.1.5**

Policy Statement "Defines the jobs-housing imbalance not only as a problem of too little housing but also as one of rapid industrial development serviced by an inadequate transportation network"

"... the City should be part of the solution, not part of the problem" (City Council Manual Policy Statement under Policy 1.1.5, Jobs/Housing Imbalance).

Approval of this proposed development will contribute to the problem of unmitigated growth in the city. The city of Sunnyvale needs to look at necessary infrastructure improvements to support increased development. This not only includes taking into account resource allocation, such as water consumption versus availability, but public transportation as well. Population growth brought about by new multi-family residential unit developments is outpacing the current public transportation infrastructure capacity in the area.

A high priority must be placed on the development of an adequate transportation network to be able to support the demands of development plans in Sunnyvale, as well as the construction of affordable housing. Luxury housing developments which avoid the mandate to build affordable housing units by building fewer than eight units, such as at this site on Old San Francisco Road, only drive up the cost of housing in Sunnyvale. The solution is not to build more housing fueled by residential developers attempting to cash in on the increased corporate development in the area, particularly the new Apple campuses in Cupertino and Sunnyvale. Sunnyvale will always be a hub for tech companies. Sunnyvale needs to abide by the guidelines of its own General Plan, which conceptually look 20 years in the future. The frenetic pace of residential development cannot be sustained by Sunnyvale's aging infrastructure and inadequate public transportation network.

The reality is that without Google and other corporate buses, rideshare and carpool apps such as Scoop, Lyft and Uber, our public transportation system would be saturated, leading to gridlock on surface streets. Perhaps a free city shuttle as proposed by Vice Mayor Larry Klein will help ease the already saturated transportation infrastructure. Free transfers on the VTA over a two hour period are a step in the right direction. The City needs to continue to work with Caltrain and the VTA to improve public transportation accessibility. With all the congestion to come, perhaps Apple could contribute funds toward future public transportation projects, in addition to transportation mitigation fees.

The proposed development adjacent to the Pebble Creek HOA required a zone change to an outdated general plan designation from 1979, which remains incompatible with the neighborhood on the north side of Old San Francisco Road. Property rights are valid, however, bulldozing the two homes at this site brings very little benefit to the neighborhood, but certainly a large monetary benefit to two people. If the public interest is defined as two people, then Sunnyvale needs to redefine "public interest." Growth needs to be inclusively managed as stated by the Council Policy Manual, which requires that City Council "considers these four components – jobs, housing, transportation, quality of life – as inseparable when seeking solutions." (**Council Policy Manual, Policy 1.1.5, page 1**)

The residents opposed to this development are not "anti-development." We would simply like to see a development that adheres to (1) Sunnyvale Municipal Code 19.26.020 with respect to Planned Developments (**Attachment A**), and (2) guidelines with respect to development enumerated in the Sunnyvale General Plan, Citywide Design Guidelines and City Design Techniques.

For reasons enumerated in this comment letter, the public comments of the Planning Commission hearing of August 13, 2018, as well as my appeal letter dated August 24, 2018, neither the special development permit nor the vesting tentative map should be approved unless and until the applicant adheres to the codes, policies, design guidelines and design techniques adopted by the city of Sunnyvale.

Submitted by:

Maria Hamilton
October 23, 2018
Sunnyvale, CA

Attachment A

Sunnyvale Municipal Code

Title 19. ZONING

Article 3. ZONING DISTRICTS, USES AND RELATED DEVELOPMENT REGULATIONS

Chapter 19.26. COMBINING DISTRICTS

19.26.020. Planned development (PD) combining district created—Purpose.

(a) There is hereby created a combining district to be known as planned development (PD) combining district which may be combined with any of the zoning districts designated in Chapter 19.16.

(b) The purpose of the PD combining district is to provide modifications, additions and limitations to other zoning districts to meet special conditions and situations concerning properties within such zoning districts that cannot otherwise be handled satisfactorily. This district is also intended to provide opportunities for creative development approaches and standards that will achieve superior community design, environmental preservation and public benefit....

October 24, 2018

The Honorable Glenn Hendricks, Mayor of Sunnyvale
And Members of the Sunnyvale City Council
P.O. Box 3707
Sunnyvale, CA 94088

Re: File #2018-7048 669-673 Old San Francisco Road (APN: 209-17-050 and 209-17-051)
Request to deny application for Special Development Permit and Vesting Tentative Map for
construction of a six-unit residential development on this site

Dear Mayor Hendricks and Members of the City Council:

While I appreciate the effort being made to increase Sunnyvale's housing stock, I am appalled at the speed with which new development is happening, only outpaced by the density of the new development, and disallowing any environmental preservation. Many of our long-time institutions, such as Barry's Shoe Repair and C.J. Olson's Cherries are disappearing as the price we pay for progress. I grew up in Sunnyvale and am a long-term homeowner as well, and we are losing the look and feel of Sunnyvale, and all the reasons many of us chose to put down roots in this city.

The Sunnyvale General Plan was specifically written to aid in the thoughtful and controlled development of Sunnyvale and is in fact, policy, and not merely a suggestion. Directly on the Sunnyvale City website it states: *"A General Plan is the local government's long-term blueprint for the community's vision of future growth. It includes goals, policies and programs that convey a long-term vision for the Sunnyvale community and guides local decision-making to advance that vision. The General Plan is the basis for determining acceptable land uses and related park, road and other infrastructure needs. The Sunnyvale General Plan contains the seven elements mandated by state law and was adopted as a consolidated document July 26, 2011."* (<https://sunnyvale.ca.gov/government/codes/plan.htm>)

Additionally, if the Sunnyvale General Plan does not suffice in terms of detail, the duly elected representatives of Sunnyvale residents approved a Citywide Design Guideline, amended April 8, 2014.

However, the General Plan is not being adhered to in the development currently taking place all over Sunnyvale, with no apparent thought to massing, traffic congestion, infrastructure, environmental concerns, or impact on current residents. In particular, I am opposed to the six-unit development at 669-673 Old San Francisco Road, which is before the Sunnyvale City Council on October 30, 2018, as part of the appeal process. As a resident of a neighboring property, I feel our properties will be directly and adversely impacted by this development as the design currently stands. The massing of the planned development is too much and the design is not compatible with the rest of the neighborhood, especially on that side of Old San Francisco Road. In particular, the massing is incongruent with the General Plan or the Citywide Design Guidelines.

Policy LT-4.3 of the General Plan states **the design review guidelines are to be enforced and zoning standards ensured so that the mass and scale of new structures are compatible with existing structures**. LT-4.3c continues that design guidelines should be enforced to respect the character, scale, and context of the surrounding area. In no way does the design of this proposed development respect the character and scale of the surrounding properties. There are two-story condominiums and single-family homes on either side of the proposed development. In order to fit in with the character of the surrounding neighborhood, these proposed townhomes should be no more than two stories. This would also resolve the massing issue of so many units on such a small site.

New development, renovation, and redevelopment are required to be compatible and well-integrated with existing residential neighborhoods per Policy LT-5.3. LT5.3a clarifies further that the adopted Citywide Design Guidelines should be used in order to achieve compatible and complementary architecture and scale for new development. Given the three-story design of the proposed development versus the surrounding neighborhood, this was not adhered to at all.

Two of the guiding principles for the Land Use and Transportation Element (updated April 2017) [LUTE] as part of the Plan Structure, are “attractive design – protect the design and feel of buildings and spaces to ensure an attractive community for residents and businesses” and “neighborhood preservation – ensure that all residential areas and business districts retain the desired character and are enhanced through urban design and compatible mixes of activities” (page 13). Neither one of these Plan Structure principles is being adhered to, nor is this proposed development in keeping with the General Plan. This development is a complete anomaly within this neighborhood.

Specifically the Sunnyvale Citywide Design Guidelines address the scale and character of building design. They state in Guideline 2B2 that adjacent buildings should be compatible in height and scale and in Guideline 2B4 that similar horizontal and vertical proportions should be maintained with the adjacent facades in order to maintain architectural unity.

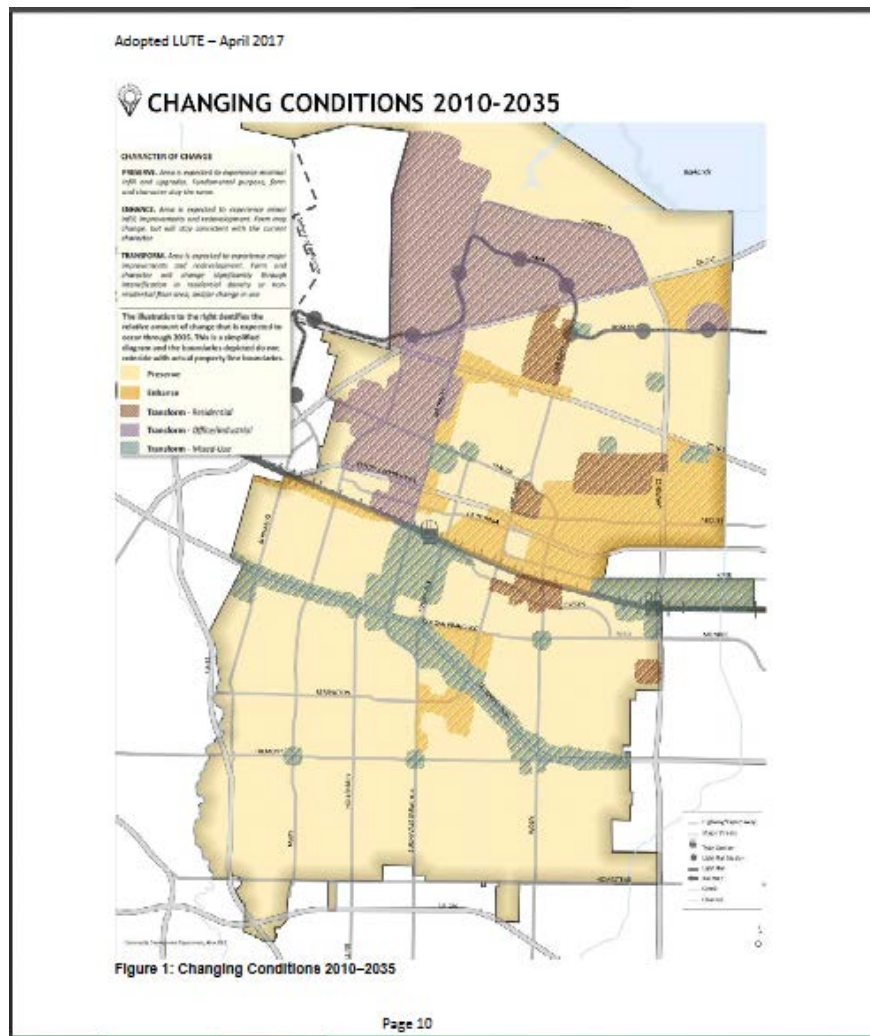
Additionally, the Building Design section of the Guidelines specifically provide that the buildings should be harmonious in character, style, scale, color and materials with existing buildings in the neighborhood and enhance the neighborhood. Guideline 2.B5 clearly states that the upper stories of buildings three stories or taller need to be stepped back from public roads and adjacent lower scale developments, in order to reduce the bulk impact. This proposed development will stand out as a variance in the middle of the block in this well-established neighborhood. This proposed project does not fit in with the rest of the area, not with regard to the number of stories, scale, or color – the massing is outsized for this location.

The Sunnyvale Citywide Design Guidelines, amended April 8, 2014 reiterate this. The first item laid out under Site Design is that “new development should adhere to the character of the existing neighborhood and be integrated into the surrounding development. New development should not dominate or interfere with the established character of its neighborhood. Site design of projects should be cohesive both functionally and visually.” As it stands today, the established character of the neighborhood of this proposed development is single or two-story buildings with plenty of open space, not three stories crowded on minimal land.

Policy HE-6.1 details that efforts must continue to balance the need for additional housing with other community values, including preserving the character of established neighborhoods, high quality design, and promoting a sense of identity in each neighborhood. This is a fundamental part of the Housing Chapter of the General Plan. However, the plans for this development neither preserve the character of the current neighborhood, nor is it such a high-quality design that it adheres to the intent of the General Plan.

A quality design should not require a special development permit in order to fit into a designated space and detrimentally impact an entire neighborhood. The design should have been created in order to fit the designated space and established neighborhood. Since this was not done with either of the first two iterations of the planned design, I ask that the City Council consider this proposed project for its size, scope, and lack of compatibility with the rest of the area of mostly single-story homes and smaller scale condominiums directly adjacent to this development, and grant the appeal and reject the special development permit and vesting tentative map. This design is not sufficient for a sensitive use infill development, nor is there any greater community benefit being provided by the developer in exchange for his requested variances, in terms of the environment or public space.

According to the Land Use and Transportation Element (updated April 2017) [LUTE] of the Sunnyvale General Plan, under the Character of Change, as detailed on the Changing Conditions 2017-2035 Map on the next page (LUTE - page 10), there are no plans to change the character of the area. The side of Old San Francisco Road on which this proposed .34 acre development is located is laid out in the General Plan map under the “preserve” section of Sunnyvale. The Character of Change for the “Preserve” areas is defined as an area expected to experience minimal infill and upgrades. Fundamental purpose, form and character stay the same. As it is designed today with its three stories, oversized massing, and general incompatibility with the rest of the neighborhood, the proposed development is not aligned with the General Plan and the Sunnyvale Citywide Design Guidelines.



If as a City we cannot adhere to the Plan and Guidelines we have approved, then there really is no point to having them in place, nor is everyone being treated equitably. The Plan and Guidelines should be in place for everyone to use as a required part of the design and planning process.

The Sunnyvale General Plan, in the chapter on Community Character, Policy CC-1.3 specifically refers to ensuring that new development is compatible with the character of special districts and residential neighborhoods. The proposed three-story design with 6 units squeezed onto a .34-acre site is in direct contrast to the spaciousness of the design of the Pebble Creek complex or the other single-story homes along that side of Old San Francisco Road. The homes to each side are single-story or two-story with lots of open space on the street side. The proposed development is three stories high with a bulk and mass that is incompatible with this long-standing neighborhood, in particular because it is in the middle of the block and does not fit the character of the surrounding block. The proposed development overwhelms all the other residences on the block. This proposed development is not complying with the General Plan requirement that new development be compatible with the rest of the residential neighborhood and is out of character with the neighborhood.

Goal CC-3 designates well-designed sites and building in order to ensure that buildings and related site improvements for private development are well designed and compatible with surrounding properties and districts. Site design, compatibility with the built environment, integration with the roadway, and building design are all an integral part of this goal. Multi-residential housing of this bulk on such a small site does not comply with this goal, particularly when other residential housing on this street is not as dense or as massive. With the current design everything is being crammed onto a .34-acre site leaving barely any space for required setbacks, the necessary guest parking is jam-packed into the back of the lot, let alone leaving room for any community space or yards. The proposed design does not fit into the neighborhood.

The same is detailed under 1.A1 of the Guidelines which states that projects should be designed to be compatible with their surrounding development in intensity, setbacks, building forms, material, color, and landscaping unless there are specific planning goals to change the character of the area.

Goal HE-2 refers to enhanced housing conditions and affordability – to maintain and enhance the conditions and affordability of existing housing in Sunnyvale. Policy HE-2.2 continues this and aims to provide community outreach and comprehensive neighborhood improvements programs within Sunnyvale's neighborhood enhancement areas to improve housing conditions and the overall quality of life.

The General Plan consistently refers to quality of life and looks to consider jobs, housing, transportation, and quality of life as inseparable when making planning decisions that affect any of these components. With quality of life one of the basic tenets of Sunnyvale's General Plan, it is inconceivable that such a densely massed design without sufficient open space per unit as written in the Guidelines, would be deemed acceptable and in congruence with the Sunnyvale General Plan as well as the Citywide Design Guidelines. Sunnyvale's General Plan was intended to maintain or improve the quality of life of all its residents, not just a few. The current design of this planned development in no way enhances the lives of the surrounding Sunnyvale residents who should be respected as adjacent neighbors. If anything, the additional traffic, noise and pollution from more cars as well as proximity to other residences, will negatively impact everyone.

The current design does not meet the General Plan or Design Guidelines set forth by the City of Sunnyvale. The design variances the developer is asking for are geared more toward his self-interest than providing affordable quality housing for the Sunnyvale community. The design is not appropriate for this space and ruins the scale and character of the entire neighborhood. In addition, it is my understanding that there were some discrepancies in the numbers and data provided by the applicant. I fail to understand how this was missed during the extensive review process. This also speaks to the developer's self-interest. Moreover, please note that I did not receive notice of the appeal meeting, nor did numerous nearby residents, until Tuesday, October 23rd, the day before the deadline for written comments to be included in the packets to be compiled and distributed to council members, and only four business days before the appeal hearing. If interested parties do not receive timely notice of the appeal meeting, it is not conceivable that a fair and transparent governmental process can take place.

I would like to suggest that the number of units is decreased from 6 and/or the size of each unit is reduced to two bedrooms and limited to 2-stories. This would discourage renting and it would allow for more open space and address the massing on the .34 acre site, as well as the inconsistency of 3-story buildings in a neighborhood with single-level homes and two-story condominiums. The privacy of neighbors and quality of life for everyone should be kept in mind as well when a design is created and put forth for consideration. The current trend to ignore, disrespect, or even violate Sunnyvale's General Plan and Design Guidelines also needs to stop. These documents were approved by previous City Councils and are the fiduciary duty of the current City Council as elected representatives of Sunnyvale residents to follow and adhere to without fail. Time would be saved and conflict avoided if the rules were followed by everyone who wants to develop in Sunnyvale. If developers do not care to follow the Citywide Design Guidelines or assert they are too complicated, perhaps their wish to develop in Sunnyvale should be politely declined.

We all know we have a need for more affordable housing. The proposed development must be compatible with the rest of the neighborhood and encompass a thoughtful design. As noted above, the General Plan requires a balance between housing needs while preserving the character of the current neighborhood and taking into account the quality of life impact. Please grant the appeal and reject the special development permit and vesting tentative map, while encouraging the developer to continue working with the surrounding community and the Senior Planner to design a plan that addresses the need for more affordable housing while also adhering to the intent of the Sunnyvale General Plan and the Citywide Design Guidelines. Then Sunnyvale as a whole benefits and continues to be a city with a great quality of life for everyone who lives here.

Sincerely,

Britta Puschendorf

Cc: Ryan Kuchenig



City of Sunnyvale

Agenda Item

18-0719

Agenda Date: 10/30/2018

REPORT TO COUNCIL

SUBJECT

Introduce an Ordinance Amending Chapter 5.36 (Taxicabs) of the Sunnyvale Municipal Code to Comply with State Law (AB 1069 and AB 939) and to Make Minor Operational Revisions

BACKGROUND

Sunnyvale Municipal Code (SMC) Chapter 5.36 (Taxicabs) regulates taxicab franchises to promote driver and passenger safety and risk reduction. On February 9, 2016, Council approved revisions to SMC Chapter 5.36 as recommended by staff (RTC No. 15-1104). The recommended revisions were the result of a study issue and consultant report that addressed the impact of ride-sharing businesses on the taxicab industry. At that time, staff acknowledged that the transportation industry would continue to evolve, and advised that staff would continue to monitor regulatory changes to this industry, both locally and throughout the state.

In October 2017, the State of California enacted into law Assembly Bill 1069 (AB 1069, Low), a bill that redirected regulatory authority for taxicabs from individual cities and towns to counties and regional authorities, establishing a statewide regulatory scheme for driver eligibility and vehicle safety by amending Section 53075.5 of the Government Code, adding sections 53075.51, 53075.52 and 53075.53 of the Government Code, and amending Section 1808.1 of the Vehicle Code. In September 2018, the State of California enacted Assembly Bill 939 (AB 939, Low) to additionally clarify portions of AB 1069. These laws take effect on January 1, 2019.

Previously, state law required every city or county to adopt a resolution or ordinance regarding taxicab transportation service. Taxicab companies and drivers were subject to the local laws of each city in which they operated, requiring multiple licenses and fees for each jurisdiction where they provided taxicab services. This model created barriers for market entry and made it difficult for taxicab companies to compete with other for-hire modes of transportation like Uber and Lyft.

AB 1069 creates a regional regulatory scheme throughout the state. In the absence of a regional authority, these new laws will allow a taxicab company to obtain a permit in the city or jurisdiction in which it is substantially located. (Currently, Santa Clara County does not have an established regional taxicab permitting authority.) This change in state law will obligate a taxicab company to obtain a permit in only one jurisdiction and will prohibit cities from requiring an additional local taxicab permit if a taxicab is operating with a lawfully issued permit from a jurisdiction within the same county. The law will also allow for the assessment of a minimum fine of \$5,000 for unpermitted taxicab operations.

AB 1069 and AB 939 will take effect on January 1, 2019, and as a result, the City must revise its existing ordinance related to taxicab regulation to align with the new state law. Staff has proposed minor updates to SMC Chapter 5.36 to comply with AB 1069 and AB 939 and incorporate

modifications to the SMC due to advances in technology and changes in the marketplace. A summary of the proposed changes is included in Attachment 2, Impact of AB 1069/AB 939.

EXISTING POLICY

General Plan

Chapter 6: Safety and Noise (SN):

Goal SN-3 Safe and Secure City: Ensure a safe and secure environment for people and property in the community by providing effective Public Safety response and prevention and education services.

Chapter 3: Land Use and Transportation (LT):

Goal LT-3 An Effective Multimodal Transportation System: Offer the community a variety of transportation modes for local travel that are also integrated with the regional transportation system and land use pattern.

ENVIRONMENTAL REVIEW

The action being considered does not require review under the California Environmental Quality Act (CEQA) because it can be seen with certainty that there is no possibility that it may have an impact on the environment. (CEQA Guidelines Section 15061(b)(3))

DISCUSSION

The attached proposed revisions to Chapter 5.36 implement modifications due to recent changes in state law and makes minor operational revisions. Key changes include:

- Adding definitions and modifications to allow a driver to obtain a permit in jurisdictions in which they are “substantially located,” as defined by state law.
- Modernizing communication requirements to allow a driver to use updated technology in the place of a taximeter, as well as telephone applications and the Internet to advertise rates and fares.
- Removing the requirement to submit rates to the City and post a schedule of fares in the taxicab to allow companies to remain competitive during periods of high demand; however, companies will be required to disclose rates to passengers prior to acceptance of the ride.
- Removing the requirement to display a flag indicating that a taxicab is available.
- Adding additional safeguards to protect the health and safety of the public, including updated insurance provisions, prohibitions on refusal of service for discriminatory purposes, enrolling in a DMV pull-notice system, and provisions for revocation or suspension of a permit based on felony and certain misdemeanor violations.

FISCAL IMPACT

Revenue and expenses related to taxicab activities are accounted for in the General Fund. Revenue from taxicab permitting operations in FY 2016/17 and FY 2017/18 totaled \$15,268.50 and \$20,787.00, respectively.

Currently, only two taxicab companies operate in Sunnyvale, and staff has confirmed that Orange Cab intends to obtain a permit in Sunnyvale under the new regulations. It is anticipated that the City's revenue derived from taxicab franchise agreements, taxi driver permits, and vehicle inspection stickers will decline. Therefore, there is an estimated net negative fiscal impact on the City; however,

it is small enough that it can be absorbed within the General Fund.

The current City Fee Schedule contains fees associated with taxicab transportation services that are calculated on a cost-recovery basis. It is not anticipated that the proposed revisions to SMC Chapter 5.36 will affect the level of activity required to issue a franchise agreement, taxicab license, or driver permit, so no change to the Fee Schedule is recommended at this time.

PUBLIC CONTACT

Public contact was made by posting the Council agenda on the City's official-notice bulletin board outside City Hall, at the Sunnyvale Senior Center, Community Center and Department of Public Safety; and by making the agenda and report available at the Sunnyvale Public Library, the Office of the City Clerk and on the City's website.

ALTERNATIVES

1. Introduce an Ordinance Amending Chapter 5.36 (Taxicabs) of the Sunnyvale Municipal Code to comply with state law (AB 1069 and AB 939) and to make minor operational revisions and find that the action is exempt under the California Environmental Quality Act (CEQA) pursuant to CEQA Guidelines Section 15061(b)(3).
2. Introduce the Ordinance Amending Chapter 5.36 (Taxicabs) of the Sunnyvale Municipal Code to comply with state law (AB 1069 and AB 939) and to make minor operational revisions with modifications and find that the action is exempt under the California Environmental Quality Act (CEQA) pursuant to CEQA Guidelines Section 15061(b)(3).
3. Do not introduce the Ordinance and provide alternative direction to staff.

STAFF RECOMMENDATION

Alternative 1: Introduce an Ordinance Amending Chapter 5.36 (Taxicabs) of the Sunnyvale Municipal Code to comply with state law (AB1069 and AB 939) and to make minor operational revisions and find that the action is exempt under the California Environmental Quality Act (CEQA) pursuant to CEQA Guidelines Section 15061(b)(3).

Prepared by: Elaine Ketell, Management Analyst
Reviewed by: Phan S. Ngo, Director of Public Safety
Reviewed by: Teri Silva, Assistant City Manager
Approved by: Kent Steffens, City Manager

ATTACHMENTS

1. Ordinance
2. Impact of AB 1069/AB 939 (Low)

ORDINANCE NO. _____

**AN ORDINANCE OF THE CITY COUNCIL OF THE CITY
OF SUNNYVALE TO AMEND VARIOUS SECTIONS OF
CHAPTER 5.36 (TAXICABS) OF TITLE 5 (BUSINESS
LICENSES AND REGULATIONS) OF THE SUNNYVALE
MUNICIPAL CODE TO COMPLY WITH NEW
PROVISIONS IN CALIFORNIA STATE LAW**

WHEREAS, the City of Sunnyvale desires to amend certain sections of the Sunnyvale Municipal Code Chapter 5.36 (Taxicabs) to comply with new provisions in California state law.

NOW, THEREFORE, THE CITY COUNCIL OF THE CITY OF SUNNYVALE DOES ORDAIN AS FOLLOWS:

SECTION 1. CHAPTER 5.36 AMENDED. Chapter 5.36 (Taxicabs) of Title 5 (Business Licenses and Regulations) of the Sunnyvale Municipal Code is hereby amended to read as follows:

Chapter 5.36.

TAXICABS

- | | |
|-----------------|---|
| 5.36.010 | Definitions. |
| 5.36.020 | Administrative authority. |
| 5.36.030 | Franchise- Compliance required. |
| 5.36.040 | Application and franchise fees. |
| 5.36.050 | Franchise-Application. |
| 5.36.060 | Public hearing. |
| 5.36.070 | Execution of franchise agreement. |
| 5.36.090 | Transfer of franchise. |
| 5.36.100 | Maintenance of records. |
| 5.36.110 | Franchise- denial. |
| 5.36.120 | Franchise-term. |
| 5.36.130 | Franchise suspension. |
| 5.36.140 | Franchise revocation. |
| 5.36.150 | Owner's responsibilities for maintenance and compliance with laws. |
| 5.36.160 | Direct route request. |
| 5.36.170 | Receipt. |
| 5.36.180 | Refusal of service. |
| 5.36.190 | Additional passenger |
| 5.36.200 | Seating capacity. |
| 5.36.210 | Posting of permits. |
| 5.36.220 | Local contact requirements. |

5.36.230	Twenty-four hour service.
5.36.240	Adequate personnel.
5.36.250	Insignia.
5.36.260	Taxicab identification.
5.36.270	Inspection permitted <u>required</u> .
5.36.280	Fare schedule.
5.36.290	Posting schedule <u>Fare disclosure</u> .
5.36.300	Insurance required.
5.36.310	Cancellation of insurance.
5.36.320	Taximeter required.
5.36.326	Daily trip manifest.
5.36.330	Display flag <u>Reserved</u> .
5.36.350	Driver's permit required.
5.36.350	Application for driver's permit.
5.36.355	Requirements.
5.36.360	Investigation of applicant.
5.36.365	Testing of taxicab drivers for controlled substances and alcohol.
5.35.370	Issuance of driver's permit.
5.35.380	Revocation or suspension of driver's permit.
5.35.390	Notice of intent to deny or revoke permit-Hearing.
5.35.400	Nonliability.
5.35.405	Investigation of complaints of unauthorized taxicab operation.
5.35.410	Prosecution of violations.

Section 5.36.010. Definitions.

For the purposes of this chapter, the following words and phrases shall have the meanings respectively ascribed to them by this section.

(1) "Company" means a passenger-for-hire business either based in the city of Sunnyvale or elsewhere whose business activity includes picking up any passengers in the city of Sunnyvale by on-demand requests, a radio dispatched service, or referral which results in charging the passenger by metered time and/or distance to a destination identified by the hiring passenger for an exclusive ride. Such taxi services are a business that is required to be licensed under this chapter. Taxi services which have picked up passengers outside of the city of Sunnyvale and delivered the passengers to a location within the city are excluded from the licensing requirements under this chapter. "Exclusive ride" as it is referred to in this section does not preclude a passenger from directing the taxicab driver to pick up additional passengers en-route to a destination and incurring the additional metered time and/or distance that would be required.

(2) "Division of Measurement Standards" means the state agency charged with and responsible for ensuring the accuracy of commercial weighing and measuring devices.

(~~4~~3) - (~~2~~ 4) [Renumbered; text unchanged]

(5) "Prearranged Trip" means a trip using an online enabled application.

dispatch, or Internet Web site. A company may provide prearranged trips anywhere within the county in which it has obtained a permit.

(6) “Sole Proprietor” means an independent, self-employed driver whose business is substantially located in the city of Sunnyvale and who enters into a franchise agreement with the city without employing additional drivers.

(7) “Substantially Located” means either the jurisdiction where a company maintains its primary business address, or the jurisdiction where the largest share of prearranged and non-prearranged trips originates, as defined in Government Code section 53075.5. “Substantially located” shall also mean, for companies establishing a new operation after January 1, 2019, the jurisdiction where a company has its primary business address for the first twelve (12) months of operation. A company or driver may be substantially located in more than one jurisdiction.

(38) [Renumbered; text unchanged]

(4-9) “Taximeter” means a ~~mechanical and/or electronic~~ device attached to or utilized by a taxicab, by means of which device the authorized charge for hire of such vehicle is mechanically calculated on the basis of distance traveled, or for waiting time, or a combination of both, which charges shall be indicated upon ~~such mechanical device~~ by means of figures in dollars and cents

5.36.020 Administrative authority.

[Text unchanged]

5.36.030 Franchise—Compliance required.

(a) It shall be unlawful ~~for any company or driver who is substantially located in the city~~ to operate ~~any taxicab~~ in the city unless the owner thereof applies for, and obtains a franchise to do so, which franchise shall be nonexclusive and shall be applied for, granted, and in effect, all in compliance with the provisions of this chapter.

(b) Beginning January 1, 2018, all companies and drivers shall collect data to determine in what jurisdictions each company and driver are substantially located. Beginning January 1, 2019, data for prearranged and non-prearranged trips collected in the previous twelve (12) months shall be provided upon date of renewal to the city for any company and driver that is substantially located in the city.

(c) Any company or driver who changes from being substantially located in another jurisdiction to being substantially located in the city of Sunnyvale shall provide the city with six months’ notice prior making that change.

(d) Any company or driver who establishes a new taxi operation after January 1, 2019 shall be considered substantially located in Sunnyvale if the company maintains its primary business address in Sunnyvale. After the first year of operation, the company shall submit and the city shall review all data collected in the previous twelve (12) months to determine where the taxicab company is substantially located.

5.36.040. Application and franchise fees.

[Text Unchanged]

5.36.050 Franchise—Application.

(a) The application for such owner's franchise shall be verified under oath by the applicant, and shall set forth:

(1) [Text unchanged];

(2) The residence and business address ~~and the citizenship of the applicant~~, including all members of any firm or partnership, or all officers and directors of any corporation applying;

(3) – (6) [Text unchanged];

~~(7)~~ The number of vehicles proposed to be operated under the franchise, and a complete description of the same including the distinguishing color or colors thereof, the model and year, the manufacturer's name, the license number and the vehicle registration number; the applicant must be the registered owner of at least five taxicabs to be included in the franchise at the time of filing of the application and must maintain at least five taxicabs while operating in the city of Sunnyvale, unless the applicant is a sole proprietor as defined in Section 5.36.010(6);

~~(8-9)~~ [Text unchanged]

~~(10) — A description of the proposed uniform to be worn by the drivers of applicant's taxicabs.~~

(b) In addition to the items of information required in subsection (a) of this section, the applicant shall, together with the franchise application, furnish the following:

(1) – (3) [Text unchanged];

(4) The city may require evidence of every material statement in the application of the ability of applicant to meet the provisions of this chapter, together with such further information as the city council, or such official of the city of Sunnyvale to whom the application may be referred, may require.

(5) [Text unchanged].

5.36.060. Public hearing.

[Text unchanged]

5.36.070. Notice of hearing.

[Text unchanged]

5.36.090. Transfer of franchise.

[Text unchanged]

5.36.100. Maintenance of records.

[Text unchanged]

5.36.110 Franchise—Denial.

The city council may deny a franchise to any applicant if it appears to its satisfaction that the applicant has been convicted of a felony or violation of any narcotic law or of any penal law involving moral turpitude; that the applicant's

proposed color scheme or other insignia will tend to confuse the identification of the vehicles proposed to be operated by such applicant with those of another owner operating in the city; that the applicant has failed to provide the necessary information required in Section 5.36.050 or made a material misstatement or misrepresentation in the application; that the applicant has been in violation of any of the terms of this chapter, or of any other laws or regulations relating to the conduct of a taxicab business; that the applicant has had a taxicab license revoked or suspended in the city of Sunnyvale or any other jurisdiction within five years prior to the date of the application; that the applicant has previously applied for a taxicab franchise in the city of Sunnyvale and been denied within two years prior to the date of the current application; or that any other reasonable cause exists which, within the council's sound discretion, would render the proposed operations undesirable to the city of Sunnyvale, pose a public health and safety risk, or would otherwise be inadequate.

5.36.120. Franchise—Term.

[Text unchanged]

5.36.130. Franchise suspension.

[Text unchanged]

5.36.140 Franchise revocation.

(a) The director of public safety, or designee, may revoke the franchise granted under this chapter, if any of the following determinations are made:

(1) – (3) [Text unchanged];

(4) If taxicabs are operated at a rate of fare other than that specified in the franchisee's rate schedule then in effect ~~and on file with the director of public safety~~;

(5) – (6) [Text unchanged].

(b) – (e) [Text unchanged].

5.36.150 Owner's responsibilities for maintenance and compliance with laws.

(a) It shall be the responsibility of the owner to assure that every taxicab operated under its franchise is in safe working order and meets the requirements of the Vehicle Code of the state of California. The interior and exterior of each taxicab shall be clean and well maintained at all times when in operation. ~~The interior of each taxicab shall be cleaned daily. The exterior of each taxicab shall be washed not less than once a week and painted once each year; provided, however, that the painting thereof may be less frequent so long as the appearance thereof, including the color scheme, owner's trademark, monogram or insignia or other markings conform to the generally prevailing standard in the trade in the county of Santa Clara.~~

(b) All accidents, regardless of the jurisdiction of occurrence, arising from or in connection with the operation of taxicabs which result in death or injury to any person, or in damage to any vehicle, or to any property in an amount

exceeding the sum of one hundred dollars, shall be reported to the public safety department within ten days from the time of occurrence.

(c) ~~It is also the responsibility of the owner to ensure that the driver of every taxicab operated under its franchise complies~~ All owners shall ensure all drivers comply with the provisions of this chapter.

(d) All owners shall comply with all relevant State law provisions regarding the operation of a taxicab, including but not limited to Section 53075.5(h) of the California Government Code.

5.36.160. Direct route request.

[Text Unchanged]

5.36.170. Receipt.

[Text Unchanged]

5.36.180 Refusal of service.

It is unlawful for any driver to refuse, when the vehicle is in service and not otherwise engaged, to transport any person who requests such service in a sober and orderly manner and for a lawful purpose. Taxicab companies shall not prejudice, disadvantage, or require different rates or provide different service to a person because of race, national origin, religion, color, ancestry, physical disability, medical condition, occupation, marital status, or any characteristic listed or defined in Section 11135 of the Government Code.

5.36.190. Additional passenger.

[Text Unchanged]

5.36.200. Seating capacity.

[Text Unchanged]

5.36.210. Posting of permits.

[Text Unchanged]

5.36.220 Local contact requirements.

(a) Each owner shall provide a ~~telephone number to its dispatcher, together with sufficient telephone lines to provide for prompt response to callers. Such number shall be~~ toll-free telephone number to the public for dispatch of taxicab services within the city of Sunnyvale. All requests for service inside the corporate limits of the city of Sunnyvale shall be answered as soon as practicable; and if the service cannot be rendered within a reasonable time the prospective passenger shall be informed how long it will be before the call can be answered and the reason for the delay.

(b) [Text unchanged].

(c) Notice of change of telephone number(s) shall be provided to ~~the city clerk and~~ the director of public safety at least seventy-two hours before said change is effected.

5.36.230. Twenty-four hour service.

[Text unchanged]

5.36.240. Adequate personnel.

[Text unchanged]

5.36.250. Insignia.

[Text unchanged]

5.36.260. Taxicab identification.

[Text unchanged]

5.36.270 Inspection ~~required~~permitted.

(a) Every vehicle operating under a franchise shall be presented by the owner or driver to the department of public safety for inspection once a year on or before the first day of March to ensure that the vehicle is maintained in a safe operating condition, and in compliance with the Vehicle Code. All vehicles added to the fleet of any franchise intended to be operated under the franchise shall be brought to the department of public safety for inspection prior to use in the fleet by the owner or driver.

(b) [Text unchanged].

5.36.280 Fare schedule.

Each franchisee shall adopt a uniform and reasonable schedule of charges for use of taxicabs operated under its franchise, based upon the distance traveled or waiting time, or both, as may be indicated by such taximeter, ~~which schedule of fares shall be filed with the director of public safety, or his or her designated representative, prior to being employed in the franchisee's operations~~

5.36.290 ~~Posting schedule~~Fare Disclosure.

~~All owners and operators shall keep posted in a conspicuous place, and in a permanent and secure manner in the passenger compartment of each taxicab, the schedule of fare rates chargeable for the use of the taxicab, and no owner or operator shall change the schedule of rates so posted unless a new schedule of rates has been filed with the director of public safety or his or her designee.~~ The company shall disclose fares, fees, or rates to the customer, and may satisfy this requirement by disclosing fares, fees, or rates on its Internet Web site, mobile telephone application, or telephone orders upon request by the customer.

The company shall notify the passenger of the applicable rate prior to the passenger accepting the ride for walkup rides and street hails. The rate may be provided on the exterior of the vehicle, within an application of a mobile telephone, device, or other Internet-connected device, or it may be clearly visible in either print or electronic form inside the taxicab.

5.36.300 Insurance required.

It is unlawful for an owner or driver to operate a taxicab unless there is in

full force and effect a commercial automobile insurance policy ~~of insurance in form as the city attorney deems proper~~, executed by an insurance carrier company authorized to conduct business in the state of California and with an A.M. Best rating of at least A:VII, whereby the owner and driver of each of the taxicabs operated under the franchise are insured against liability for damage to property and for injury to or death of any person as a result of the ownership, operation or other use thereof. The minimum liability limits upon each such vehicle shall not be less than one million dollars (\$1,000,000) combined single limit for bodily injury to or death of any persons and for damages to or destruction of property in any one accident. Such policy of insurance shall contain an endorsement providing that the policy shall not be canceled or materially modified until notice in writing has been given to the city, addressed to the director of public safety, city of Sunnyvale, California, at least thirty days immediately prior to the time such cancellation becomes effective. Further, such policy of insurance shall name the city, its officers, agents and employees as additional insured by separate endorsement. Any deviations from these requirements must be approved in writing by the city's risk manager. Additionally, a vehicle owner and driver shall indemnify, defend and hold harmless the city, its officers, agents and employees from claims arising from or alleged to arise from the negligence of the vehicle owner or driver.

5.36.310. Cancellation of insurance.

[Text unchanged]

5.36.320 Taximeter required.

It is unlawful for any owner or driver to operate any taxicab in the city unless and until such vehicle is equipped with a taximeter. A company may use any device or technology approved by the Division of Measurement Standards to calculate fares, including the use of Global Positioning System metering, provided that the device or technology complies with Section 12500.5 of the Business and Professions Code and all regulations established pursuant to Section 12107 of the Business and Professions Code. ~~and it~~ It shall be the duty of every owner operating a taxicab to maintain such taximeter in good serviceable condition so that it will at all times correctly indicate the correct charge for the distance traveled and waiting time. Every taximeter shall be equipped so as to register the cost of transportation of passengers in the city, and the taximeter shall be so placed in the taxicab that the reading dial showing the amount to be charged may be readily seen by the passengers in the taxicab. The owner shall cause such taximeter to be inspected annually by the Santa Clara County Sealer of Weights and Measures, and shall timely submit a report of such inspection to the director of public safety. The director of public safety is hereby authorized at his or her instance or upon complaint of any person, to investigate or cause any taximeter to be investigated, and upon discovery of any inaccuracy in such taximeter, to suspend the franchise for operation of the taxicab in which it was installed, until the director of public safety determines such taximeter ~~shall have~~ has been correctly adjusted.

5.36.325. Daily trip manifest.

[Text unchanged]

5.36.330. Reserved.Display flag.

~~It is unlawful for a taxicab driver while carrying passengers to display the flag or device attached to the taximeter in such position as to denote that the vehicle is for hire or to cause the taximeter to record when the vehicle is not actually employed or to fail to cause the device on the taximeter to be placed in a nonrecording position at the termination of each and every service.~~

5.36.340 Driver's permit required.

It is unlawful for any driver, owner, or sole proprietor ~~person~~ to operate or drive a taxicab that is substantially located in ~~which originates trips within~~ the city without having first obtained a driver's permit from the director of public safety. To secure such permission, a prospective taxicab driver shall file a written application with the director of public safety, which application shall be accompanied by a nonrefundable processing fee in an amount to be established by resolution of the city council. A permit card shall be issued by the director of public safety, which permit card shall not be transferable. Regardless of date of issuance, permits are issued for two calendar years and expire December 31st of the calendar year following issuance. The permittee shall post the permit card in the taxicab in view of the passengers therein, during all working hours. Applications for renewal of a driver's permit must be received by the director of public safety no later than thirty days before the date of expiration, or the driver will be assessed a late fee in an amount established by resolution of the city council.

5.36.350. Application for driver's permit.

[Text unchanged]

5.36.355. Requirements.

[Text unchanged]

5.36.360. Investigation of applicant.

[Text unchanged]

5.36.365. Testing of taxicab drivers for controlled substances and alcohol.

(a) – (b) [Text unchanged]

(c) No taxicab driver's permit shall be issued or renewed unless the applicant and the franchise holder for which the driver is authorized to operate a vehicle both certify that the driver has tested negatively for controlled substances under a mandatory controlled substance and alcohol testing certification program conforming to Part 40 Code of Federal Regulations and California Government Code Section 53075.5. Upon the request of a driver applying for a permit, the city shall provide the driver a list of the consortia certified pursuant to Part 382 (commencing with section 382.101) of Title 49 of Federal Regulations that offer

tests in or near the city.

5.36.370 Issuance of driver's permit.

(a) Upon approval of an application for a driver's permit and upon payment of the fee in an amount to be established by resolution, the director of public safety shall issue a permit to the applicant. Such permit shall bear the name and photograph of the applicant, date of expiration of the permit, and the name of the licensed franchise owner for which the driver is authorized to operate a vehicle. Such permit ~~shall be picked up by the driver or owner named therein and~~ shall be valid only so long as the driver continues in the employ of such owner, or for the two-year permit period, whichever is less.

(b) [Text unchanged]

5.36.380 Revocation or suspension of driver's permit.

Any person issued a driver's permit who subsequently is convicted of any felony or misdemeanor offense or who ceases to possess a valid state of California driver's license of the class required by state law for the operation of taxicabs shall immediately so inform his or her employer and the director of public safety.

The director of public safety may revoke or suspend any driver's permit for repeated violations of this chapter, for commission of any act or acts which would be grounds for a denial of a driver's permit, if the driver's state of California Department of Motor Vehicles record includes four or more moving violations within the preceding twelve-month period, any felony conviction or misdemeanor conviction of moral turpitude, or if the director of public safety determines that the driver is a danger to the public safety.

5.36.390. Notice of intent to deny or revoke permit—Hearing.

[Text unchanged]

5.36.400. Nonliability.

[Text unchanged]

5.36.405. Investigation of complaints of unauthorized taxicab operation.

[Text unchanged]

5.36.410. Prosecution of violations.

[Text unchanged]

SECTION 2. CEQA - EXEMPTION. The City Council finds, pursuant to Title 14 of the California Code of Regulations, Section 15061(b)(3), that this ordinance is exempt from the requirements of the California Environmental Quality Act (CEQA) in that it is not a Project which has the potential for causing a significant effect on the environment.

SECTION 3. CONSTITUTIONALITY; SEVERABILITY. If any section, subsection, sentence, clause or phrase of this ordinance is for any reason held to be invalid, such decision or decisions shall not affect the validity of the remaining portions of this ordinance. The City Council

hereby declares that it would have passed this ordinance, and each section, subsection, sentence, clause and phrase thereof irrespective of the fact that any one or more sections, subsections, sentences, clauses or phrases be declared invalid.

SECTION 4. EFFECTIVE DATE. This ordinance shall be in full force and effect no sooner than January 1, 2019.

SECTION 5. POSTING AND PUBLICATION. The City Clerk is directed to cause copies of this ordinance to be posted in three (3) prominent places in the City of Sunnyvale and to cause publication once in The Sun, the official publication of legal notices of the City of Sunnyvale, of a notice setting forth the date of adoption, the title of this ordinance, and a list of places where copies of this ordinance are posted, within fifteen (15) days after adoption of this ordinance.

Introduced at a regular meeting of the City Council held on _____, and adopted as an ordinance of the City of Sunnyvale at a regular meeting of the City Council held on _____, by the following vote:

AYES:

NOES:

ABSTAIN:

ABSENT:

RECUSAL:

ATTEST:

APPROVED:

City Clerk
Date of Attestation: _____

Mayor

(SEAL)

APPROVED AS TO FORM:

City Attorney

ATTACHMENT 2
Impact of AB 1069/AB 939 (Low)

Government Code	New Requirement/Language (AB 1069)	SMC Section	Response to New Law
53075.5(h)(9) 53075.52	Taxicab companies shall collect data beginning on January 1, 2018 to determine in what jurisdictions each company and driver are substantially located.	5.36.030(b)-(d)	Add language consistent with state law regarding determination of taxi company "substantial" location
53075.5(b)(1)(B-E)	Driver's permit is void upon termination of employment; owner must notify permitting authority; driver must return permit to permitting authority	5.36.370(a)	Amend to place responsibility of permit return on driver
53075.5(b)(2)(B)	A taxicab company may use any device approved by the Division of Measurement Standards to calculate fares, including GPS.	5.36.320	Expand the definition of "taximeter" to include electronic devices
53075.5(b)(2)(C) 53075.5(b)(2)(D)	The taxicab company shall disclose fares, fees or rates to the customer, using either website, telephone application, or telephone order, and shall notify the passenger of the fare prior to the passenger accepting the ride for walkups and street hails. May be on the exterior of the vehicle, within an application or mobile device, or clearly visible in print or electronic form inside the cab.	5.36.290	Update language updated to align with Government Code, add notification of passenger fare.
53075.5(b)(3)(A)	Mandatory controlled substance & alcohol testing certification	5.36.365(c)	Add language to ensure that a taxi driver is provided, on request, a list of consortia certified to offer mandated controlled substance testing
53075.5(d)(1)(A-E) 53075.5(g)	Taxi drivers issued photo permits, required to display in vehicle Taxicab companies shall not prejudice, disadvantage, or require different rates or provide difference service to a person because of race, national origin, religion, color, ancestry, physical disability, medical condition, occupation, marital status, or any characteristic listed or defined in Section 11135 of the Government Code	5.36.210, 5.36.320 5.36.180	No change Update language to align with language of Government Code.
53075.5(h)(1-9)	Taxicab companies must maintain reasonable financial responsibility to conduct taxicab transportation services in accordance with local ordinances; participate in a DMV pull-notice system to provide notice to the City of adverse driving records; participate in a safety education and training program; and a provide a disabled access education and training program for all drivers.	5.36.150	Add subsection (d), requiring companies to comply with this subsection of the Government Code in its entirety
	Taxicab companies must obtain a Bureau of Automotive Repair inspection annually for each vehicle	5.36.270(a)	Add this requirement to the existing DPS inspection requirement
	Taxicab companies must provide a business office address to the permitting authority	5.36.050(a)(3)	No Change
	Taxicab companies must provide for a fingerprint-based criminal history check & drug/alcohol testing program	5.36.355, 5.36.365	The City will continue to require owners and drivers to be fingerprinted
53075.5(k)	Defines "employment" to include self-employment as an independent driver; defines "substantially located"; defines "prearranged trip"	5.36.010	Add definitions to support these mandates



City of Sunnyvale

Agenda Item

18-0853

Agenda Date: 10/30/2018

REPORT TO COUNCIL

SUBJECT

Introduce an Ordinance to Amend Chapter 5.04 of the Sunnyvale Municipal Code - Business License Tax Ordinance to Reflect Changes to the Consumer Price Index Published by the United States Bureau of Labor Statistics

BACKGROUND

Chapter 5.04 of the Sunnyvale Municipal Code, also known as the Business License Tax Ordinance, was established in 1968. The ordinance requires all businesses that operate in Sunnyvale to obtain a license and pay a business license tax to conduct business in the City.

In November 2005, Sunnyvale voters approved Measure D, which increased the business license tax and tied future tax increases to the All Urban Consumers Consumer Price Index (CPI) for the San Jose/San Francisco/Oakland MSA (Metropolitan Statistical Area). In February 2018, the United States Bureau of Labor Statistics published new geographic area samples for the CPI to reflect the data from the 2010 Decennial Census on the distribution of urban populations. As a result of the 2018 geographic revisions, the San Jose/San Francisco/Oakland MSA has been revised and renamed the San Francisco/Oakland/Hayward CBSA (Core Based Statistical Area). City staff has confirmed with the United States Bureau of Labor Statistics that the San Francisco/Oakland/Hayward CBSA (Core Based Statistical Area) is the successor index for the San Jose/San Francisco/Oakland MSA (Metropolitan Statistical Area) and provides substantially equivalent statistical analysis of consumer price escalation in the greater Bay Area geographic region that includes the City of Sunnyvale.

Staff is requesting that the ordinance be amended to reflect the changes to the Consumer Price Index. The Municipal Code needs to be amended to reflect this successor index for future tax increases to be calculated.

EXISTING POLICY

Council Policy 7.1B.1.4 states that when considering a new tax or revenue source or an increase in an existing tax or revenue source, community/voter acceptance should be considered.

ENVIRONMENTAL REVIEW

The action being considered is exempt from the California Environmental Quality Act ("CEQA") pursuant to CEQA Guidelines section 15061 (b) (3) as the activity is covered by the general rule that CEQA applies only to projects which have the potential for causing a significant effect on the environment. Where it can be seen with certainty that there is no possibility that the activity in question may have a significant effect on the environment, the activity is not subject to CEQA.

DISCUSSION

The passage of Measure D placed into the ordinance that the business license tax should be adjusted in accordance with the San Jose/San Francisco/Oakland MSA CPI effective January 1, 2009. The creation of a successor CPI index necessitates amending the ordinance because it references the specific name of a CPI index, which no longer exists. City Council has the authority to update the Municipal Code as it relates to the CPI and as the amendment does not result in an escalation of the business license tax beyond what was originally authorized by the voters in 2005. Since the Bureau of Labor Statistics has identified the San Francisco/Oakland/Hayward CBSA as the successor to the previous CPI measure, an amendment to the ordinance can be approved by Council to specify the replacement CPI. Staff is proposing that the revisions account for potential future changes to the CPI should the Bureau of Labor Statistics make further adjustments to the geographic areas in the future and create future successor indexes.

This amendment is necessary to enable the tax rates to continue to be adjusted annually as mandated by the voters through Measure D. Without the amended ordinance, tax rates would remain unchanged from 2019 levels until the ordinance is amended or until voters elect changes to the business license tax. This would be inconsistent with the intent of the voters to use the Bureau of Labor Statistics CPI measure as a mechanism for inflationary increases to the business license tax.

FISCAL IMPACT

There is no fiscal impact if the updated ordinance is passed. The City would continue adjusting the business license tax annually in accordance with the updated CPI measure. Revenues from the Business License Tax go to the General Fund.

PUBLIC CONTACT

Public contact was made by posting the Council agenda on the City's official-notice bulletin board outside City Hall, at the Sunnyvale Senior Center, Community Center and Department of Public Safety; and by making the agenda and report available at the Sunnyvale Public Library, the Office of the City Clerk and on the City's website.

ALTERNATIVES

1. Introduce an ordinance to amend Section 5.04.110 of the Sunnyvale Municipal Code to reflect the changes to the Consumer Price Index adopted by the Bureau of Labor Statistics and find that the action is exempt from the California Environmental Quality Act ("CEQA") pursuant to CEQA Guidelines section 15061 (b) (3).
2. Introduce an ordinance to amend Section 5.04.110 of the Sunnyvale Municipal Code to reflect the changes to the Consumer Price Index adopted by the Bureau of Labor Statistics with modifications and find that the action is exempt from the California Environmental Quality Act ("CEQA") pursuant to CEQA Guidelines section 15061 (b) (3).
3. Do not introduce of an ordinance to amend Section 5.04.110 of the Sunnyvale Municipal Code.

STAFF RECOMMENDATION

Introduce an ordinance to amend Section 5.04.110 of the Sunnyvale Municipal Code to reflect the changes to the Consumer Price Index adopted by the Bureau of Labor Statistics and find that the action is exempt from the California Environmental Quality Act ("CEQA") pursuant to CEQA Guidelines section 15061 (b) (3).

Prepared by: Stephen Napier, Administrative Services Manager

Reviewed by: Timothy J. Kirby, Director of Finance

Reviewed by: Teri Silva, Assistant City Manager

Approved by: Kent Steffens, City Manager

ATTACHMENTS

1. Ordinance to Amend Section 5.04.110 of the Sunnyvale Municipal Code

ORDINANCE NO. _____

**AN ORDINANCE OF THE CITY COUNCIL OF THE CITY
OF SUNNYVALE TO AMEND SECTION 5.04.110 (LICENSE
TAX – IMPOSED) OF CHAPTER 5.04 (BUSINESS LICENSE
TAX) OF TITLE 5 (BUSINESS LICENSES AND
REGULATIONS) OF THE SUNNYVALE MUNICIPAL
CODE**

WHEREAS, Section 5.04.110 of the Sunnyvale Municipal Code was submitted to the voters of the City of Sunnyvale as Measure D at the General and Special Municipal Election held on November 8, 2005, and was passed by the voters with 51.21% of the vote; and

WHEREAS, Section 5.04.110(e) of the Sunnyvale Municipal Code provides that for the business license tax due January 1, 2009 and thereafter, the amount of the tax and cap shall be indexed annually based upon the San Jose/San Francisco/Oakland MSA (Metropolitan Statistical Area) Consumer Price Index; and

WHEREAS, in February 2018, the Bureau of Labor Statistics of the United States Department of Labor published new geographic area samples for the Consumer Price Index (CPI) to reflect the data from the 2010 Decennial Census on the distribution of the urban population; and

WHEREAS, as a result of the 2018 geographic revisions, the San Jose/San Francisco/Oakland MSA has been revised and renamed the San Francisco-Oakland-Hayward CBSA (Core Based Statistical Area); and

WHEREAS, the City Council may lawfully update Section 5.04.110(e) of the Sunnyvale Municipal Code without voter approval as long as such amendment will not result in an escalation of the business license tax obligation beyond what was authorized by the voters in the 2005 election; and

WHEREAS, the City Council finds that the San Francisco-Oakland-Hayward CBSA is the successor to the San Jose/San Francisco/Oakland MSA and provides a substantially equivalent statistical analysis of consumer price escalation in the greater Bay Area geographic region that includes the City of Sunnyvale.

NOW, THEREFORE, THE CITY COUNCIL OF THE CITY OF SUNNYVALE DOES ORDAIN AS FOLLOWS:

SECTION 1. SECTION 5.04.010 AMENDED. Section 5.04.010 of Chapter 5.04 (Business License Tax) of Title 5 (Business Licenses and Regulations) of the Sunnyvale Municipal Code is hereby amended to read as follows:

Section 5.04.110. License Tax - Imposed.

There is hereby imposed a yearly license tax upon every person transacting, engaging in, or carrying on any business within the city. The amount of the tax is calculated as follows:

(a)-(d) [Text unchanged]

(e) (For the tax due January 1, 2009 and thereafter) The amount of the tax and cap shall be indexed annually, based upon the ~~San Jose/San Francisco/Oakland MSA~~ San Francisco/Oakland/Hayward Consumer Price Index for the September to September period for the preceding year, or comparable successor index established by the Bureau of Labor Statistics of the United States Department of Labor. The indexed amounts shall be set forth each year in the city's master fee resolution.

SECTION 2. CEQA - EXEMPTION. The City Council finds, pursuant to Title 14 of the California Code of Regulations, Section 15061(b)(3), that this ordinance is exempt from the requirements of the California Environmental Quality Act (CEQA) in that it is not a Project which has the potential for causing a significant effect on the environment.

SECTION 3. CONSTITUTIONALITY; SEVERABILITY. If any section, subsection, sentence, clause or phrase of this ordinance is for any reason held to be invalid, such decision or decisions shall not affect the validity of the remaining portions of this ordinance. The City Council hereby declares that it would have passed this ordinance, and each section, subsection, sentence, clause and phrase thereof irrespective of the fact that any one or more sections, subsections, sentences, clauses or phrases be declared invalid.

SECTION 4. EFFECTIVE DATE. This ordinance shall be in full force and effect thirty (30) days from and after the date of its adoption.

SECTION 5. POSTING AND PUBLICATION. The City Clerk is directed to cause copies of this ordinance to be posted in three (3) prominent places in the City of Sunnyvale and to cause publication once in The Sun, the official publication of legal notices of the City of Sunnyvale, of a notice setting forth the date of adoption, the title of this ordinance, and a list of places where copies of this ordinance are posted, within fifteen (15) days after adoption of this ordinance.

Introduced at a regular meeting of the City Council held on _____, 2018, and adopted as an ordinance of the City of Sunnyvale at a regular meeting of the City Council held on _____, 2018, by the following vote:

AYES:
NOES:
ABSTAIN:
ABSENT:
RECUSAL:

ATTEST:

APPROVED:

City Clerk

Date of Attestation: _____

Mayor

(SEAL)

APPROVED AS TO FORM:

City Attorney



City of Sunnyvale

Agenda Item

18-0907

Agenda Date: 10/30/2018

Tentative Council Meeting Agenda Calendar



City of Sunnyvale

Tentative Council Meeting Agenda Calendar

Tuesday, November 13, 2018 - City Council

Study Session

- 18-0509** 5:30 P.M. SPECIAL COUNCIL MEETING (Study Session)
Joint Meeting of City Council with Board and Commission Chairs and Vice
Chairs to Review and Improve Overall Effectiveness of Commission Meetings

Presentation

- 18-0920** PRESENTATION - 2018 MISAC (Municipal Information Systems of
California) Awards
- 18-0865** PRESENTATION - Update on Caltrain Electrification Project (Caltrain)

Public Hearings/General Business

- 18-0550** Consider Terms of Disposition and Development Agreement (DDA) between
the City of Sunnyvale and Related Companies of California, LLC, providing
for financial assistance in the amount of up to \$12.5 Million in Housing
Mitigation and Low and Moderate Income Housing Asset Funds (LMIHAF) to
stablish an affordable housing development on Iowa Avenue between
Mathilda Avenue and Charles Street (Block 15)
- 18-0948** Proposed Project: General Plan Amendment Initiation request to study
changing the General Plan designation for a portion of the site from Public
Facility (P-F) to Medium Density Residential.
Location: 1050 West Remington Drive (APN: 202-26-007)
File #: 2018-7569
Zoning: P-F
Applicant / Owner: Catalyst Development Partners LLC (applicant) / Church of
Christ of Sunnyvale (owner)
Environmental Review: The project is exempt from the California
Environmental Quality Act (CEQA) pursuant to CEQA Guidelines Section
15378(a).
Project Planner: Aastha Vashist, (408) 730-7458, avashist@sunnyvale.ca.gov
- 18-0940** Proposed Project: General Plan Amendment Initiation to consider an
amendment to the Peery Park Specific Plan (PPSP) to increase the
office/industrial development capacity and allow housing on the 10 parcels in
the PPSP-Industrial Edge (IE) zoning district totaling 29.3 acres.
Locations: 400-840 W. California Avenue (165-26-009, 010, 011, 012, 013,
014, 016, 018, 019, 020)

File #: 2018-7576

Applicant / Owner: Skidmore, Owings, & Merrill LLP (applicant) / Steelwave LLC (owner)

Environmental Review: The project is exempt from the California Environmental Quality Act (CEQA) pursuant to CEQA Guidelines Section 15378 (a).

Project Planner: Amber Blizinski, 408-730-2723,
ablizinski@sunnyvale.ca.gov

18-0798 Public Hearing and Adopt a Resolution Ordering the Vacation of a Portion of Carl Road Westerly of Borregas Avenue

Tuesday, November 27, 2018 - City Council

Study Session

18-0847 6 P.M. SPECIAL COUNCIL MEETING (Study Session)
Encouraging Heat Pump Water and Space Heating (Study Issue ESD 18-01)

Public Hearings/General Business

18-0795 Authorize the City Manager or his designee to Execute a License Agreement Between the City of Sunnyvale and Jay Paul for 84 Parking Stalls at the Sunnyvale Golf Course

18-0894 Biennial Review of Priority Needs for Human Services and Recommendation to City Council

Tuesday, December 4, 2018 - City Council

Closed Session

18-0357 5 P.M. SPECIAL COUNCIL MEETING (Closed Session)
Closed Session held pursuant to California Government Code Section 54957: PUBLIC EMPLOYEE PERFORMANCE EVALUATION
Title: City Attorney

18-0358 6 P.M. SPECIAL COUNCIL MEETING (Closed Session)
Closed Session held pursuant to California Government Code Section 54957: PUBLIC EMPLOYEE PERFORMANCE EVALUATION
Title: City Manager
Closed Session held pursuant to California Government Code Section 54957.6: CONFERENCE WITH LABOR NEGOTIATORS
Agency designated representatives: City Council Compensation Subcommittee
Unrepresented Employee: City Manager

Public Hearings/General Business

18-0725 Consideration of Useable Open Space in Required Front Yards: Introduce an

Ordinance to Amend Section 19.37.100 (Usable Open Space Design Requirements) of Chapter 19.37 (Landscaping, Irrigation and Useable Open Space) of the Sunnyvale Municipal Code, and Find that the Action is Exempt from CEQA Pursuant to CEQA Guidelines Section 15061(b)(3). (Study Issue)

18-0860

Proposed Project:

PEERY PARK PLAN REVIEW PERMIT to construct two 162,000-square foot, four-story corporate/research and development (R&D) office buildings and a 6-level parking structure on a 10.4-acre site (Phase 2 of the W. Maude Ave Campus). An existing approximately 58,188 s.f. building is to be retained. A total of approximately 382,188 s.f. and 85% FAR is requested.

Location: 810-870 W Maude Ave & 470 Potrero Ave (APNs: 165-30-001; 165-30-011; 165-30-012)

File #: 2017-7066

Zoning: PPSP/IE & PPSP/MIC

Applicant / Owner: Simeon Commercial Properties (applicant) / 870 Maude LLC (owner)

Environmental Review: The project is exempt from additional CEQA review per CEQA Guidelines section 15168(c)(2) and (4) and Public Resources Code Section 21094 (c). The project is within the scope of the Peery Park Specific Plan Program EIR as no new environmental impacts are anticipated and no new mitigation measures are required.

Project Planner: Ryan Kuchenig, (408) 730-7431,
rkuchenig@sunnyvale.ca.gov

Tuesday, December 18, 2018 - City Council

Closed Session

18-0355

5 P.M. SPECIAL COUNCIL MEETING (Closed Session)

Closed Session held pursuant to California Government Code Section 54957: PUBLIC EMPLOYEE PERFORMANCE EVALUATION

Title: City Attorney

18-0356

Closed Session held pursuant to California Government Code Section

54957: PUBLIC EMPLOYEE PERFORMANCE EVALUATION

Title: City Manager

Study Session

18-0279

6:45 P.M. SPECIAL COUNCIL MEETING (Study Session)

Discussion of Upcoming Selection of Mayor for 2019-2020 and Vice Mayor for 2019

Public Hearings/General Business

18-0906

Certification of the November 6, 2018 Election Results for Ballot Measure K (Transient Occupancy Tax) and Ballot Measure L (Charter Amendment to amend Section 604 relating to Council Vacancies)

18-0910 Solid Waste & Recycling Collection Franchisee Performance Review
Findings; Process for Selection of Post-2021 Franchisee

Tuesday, January 8, 2019 - City Council

Special Order of the Day

19-0001 Certification of November 6, 2018 Election Results - Council Seats 1, 2 and 3

19-0002 SPECIAL ORDER OF THE DAY - Recognition of Outgoing
Councilmember(s)

19-0003 SPECIAL ORDER OF THE DAY - Ceremonial Oath of Office for
Councilmembers - Elect

Public Hearings/General Business

19-0004 Selection of Mayor for a Two-Year Term Effective January 8, 2019

19-0005 Selection of Vice Mayor for a One-Year Term Effective January 8, 2019

19-0006 Adopt a Resolution Approving the City Council Regular Meeting Calendar for
2019 through February 2020

19-0010 Determine the 2019 Seating Arrangements for City Council

Tuesday, January 15, 2019 - City Council

Study Session

18-0278 6 P.M. SPECIAL COUNCIL MEETING (Study Session)
Discussion of 2019 Council Intergovernmental Assignments

Public Hearings/General Business

19-0007 Annual Public Hearing - Discussion of Potential Council Study Issues and
Budget Issues for Calendar Year 2019

Tuesday, January 29, 2019 - City Council

Study Session

18-0740 5:30 P.M. SPECIAL COUNCIL MEETING (Study Session)
Discussion of Draft Climate Action Plan 2.0 (CAP 2.0)

Public Hearings/General Business

19-0009 Appoint Councilmembers to Intergovernmental Assignments; Ratify
Appointments of Councilmembers made by Outside Agencies; Take action to

Modify, Create, or Terminate Council Subcommittees

18-0859 Amend Sunnyvale Municipal Code Chapter 19.44 to add sign code provisions for theaters on the second floor.

Thursday, January 31, 2019 - City Council

Public Hearings/General Business

18-0035 8:30 A.M. SPECIAL COUNCIL MEETING
Strategic Session - Policy Prioritization Workshop
Location: Sunnyvale Community Center - Neighborhood Room, 550 East Remington Drive

Tuesday, February 5, 2019 - City Council

Study Session

18-0462 6 P.M. SPECIAL COUNCIL MEETING (Study Session)
Board and Commission Interviews (as needed)

Public Hearings/General Business

18-0187 2018 4th Quarterly Consideration of General Plan Amendment Initiation Requests

19-0008 Approve the Proposed 2019 Priority Advocacy Issues and Long-term Legislative Advocacy Positions (LAPs)

Tuesday, February 26, 2019 - City Council

Public Hearings/General Business

18-0463 Appoint Applicants to Boards and Commissions

Date to be Determined - City Council

Public Hearings/General Business

18-0289 Updates to the Murphy Station Heritage Landmark District Design Guidelines

18-0416 Eco-district Feasibility and Incentives (Study Issue ESD 13-05C)

18-0653 Approve the Bernardo Avenue Undercrossing Design Options and Selection of a Preferred Design Alternative



City of Sunnyvale

Agenda Item

18-0908

Agenda Date: 10/30/2018

Information/Action Items

Information/Action Items - Council Directions to Staff

Date	Directive/Action Required	Dept	Due Date	Completed
5/8/18	Staff to research any other available off road parking for the Bay Trails and provide an update by the end of the year.	DPW	12/18/18	
5/8/18	Provide information report to Council on the findings when the speed survey for Caribbean is complete.	DPW	Nov 2018	
5/8/18	Update Council Policy 7.1.1 – Fiscal – Long Range Goals and Financial Policies and bring back for Council adoption.	FIN	Nov 2018	
5/25/18	Provide more information on the connection between sales tax generation and land uses	OCM	Spring 2019	
5/25/18	Bring back the sales tax chart by City when the Council considers the El Camino Real Precise Plan update	CDD	Spring 2019	
7/17/18	Staff to work with the Chamber of Commerce in gathering information or making a recommendation on what options may be available to the hotels to encourage occupancy on the weekend.	OCM	Jan 2019	
8/28/18	Staff to provide quarterly updates to Council on ERP implementation progress	OCM	Dec 2018	
9/25/18	Include field use rules that will impact resident behavior with the installation of artificial turf when the Fair Oaks Park Renovation Project goes back to Council.	DPW	Dec 2019	

New Study/Budget Issues Sponsored by Council

Date Requested	Study Issue Topic	Requested By	Dept	Approved by City Manager
8/28/18	Study issue on the installment of a Eruv in Sunnyvale, requested by the South Peninsula Hebrew Day School.	Hendricks , Larsson	DPW	
9/25/18	Ascertain suitable location for the installation of youth cricket batting cages and potential funding sources.	Melton , Klein, Goldman	DPW	
9/25/18	Determine neighborhood interest in installing sidewalks in Raynor Park neighborhood and as appropriate investigate funding sources.	Melton , Klein, Goldman	DPW	



City of Sunnyvale

Agenda Item

18-0909

Agenda Date: 10/30/2018

Board/Commission Meeting Minutes



City of Sunnyvale

Meeting Minutes - Final Planning Commission

Monday, October 8, 2018

6:00 PM

Council Chambers and West Conference
Room, City Hall, 456 W. Olive Ave.,
Sunnyvale, CA 94086

Special Meeting - Study Session - 6:00 PM | Special Meeting - Public Hearing 7 PM

6 P.M. STUDY SESSION

Call to Order in the West Conference Room

Roll Call

Study Session

A. [18-0867](#)

Proposed Project:

SPECIAL DEVELOPMENT PERMIT to construct 58 single family homes and associated improvements on 6.1 acres of the 8.8-acre site (agricultural land known as the Corn Palace).

TENTATIVE MAP to subdivide the 8.8-acre site into 60 lots, including a 2-acre public park lot, a private street and 58 single family home lots.

Location: 1142 Dahlia Court (APN's: 213-12-001)

File #: 2017-7451

Zoning: R-1.5/PD

Applicant / Owner: Trumark Homes/ Gabriel Francia Trustee

Project Planner: Shetal Divatia, (408) 730-7628,
sdivatia@sunnyvale.ca.gov

B. [18-0866](#)

Proposed Project:

SPECIAL DEVELOPMENT PERMIT to allow the expansion and renovation of an existing 173-room hotel with the removal of 85 rooms, renovation of 88 rooms and construction of 270 new rooms that results in a 358-room hotel (net new of 185 rooms). The proposed project also includes 18,021 s.f. for meeting areas, 7,313 s.f. for food and beverage services and 8,241 s.f. for spa use, for a total gross floor area of 256,180 s.f. The project would be served by 296 parking spaces (203 spaces in a new 3-level

parking structure) and associated site improvements.

Location: 1100 N. Mathilda Avenue (APN's: 110-27-025)

File #: 2017-8044

Zoning: MPC

Applicant / Owner: DoveHill Capital Mgmt LLC/ Sof-X Sunnyvale
Owner

Project Planner: Shetal Divatia, (408) 730-7628,
sdivatia@sunnyvale.ca.gov

Public Comment on Study Session Agenda Items

Adjourn Study Session

7 P.M. PLANNING COMMISSION MEETING

CALL TO ORDER

Chair Howard called the meeting to order at 7:05 PM in the Council Chambers.

SALUTE TO THE FLAG

Chair Howard led the salute to the flag.

ROLL CALL

Present: 7 - Commissioner Carol Weiss
Chair Daniel Howard
Commissioner John Howe
Commissioner Ken Olevson
Vice Chair David Simons
Commissioner Ken Rheaume
Commissioner Sue Harrison

ORAL COMMUNICATIONS

CONSENT CALENDAR

Commissioner Weiss moved and Commissioner Harrison seconded the motion to approve the Consent Calendar. The motion carried by the following vote:

Yes: 5 - Commissioner Weiss
Chair Howard
Commissioner Olevson
Vice Chair Simons
Commissioner Harrison

No: 0

Abstained: 2 - Commissioner Howe
Commissioner Rheaume

1. A [18-0868](#) Approve Planning Commission Meeting Minutes of September 24, 2018

PUBLIC HEARINGS/GENERAL BUSINESS

3. [18-0869](#) **Proposed Project:**

DESIGN REVIEW - Allow a 505-sq. ft. second-story addition and a 151-sq. ft. first-story addition on an existing 2,062 sq. ft. single-story, single-family home resulting in 2,718 sq. ft. (2,174 sq. ft. living area, 431 sq. ft. garage, and 113 sq. ft. covered portion of patio) and 48% FAR.

USE PERMIT for a 7'5" fence along the reducible front yard property line (along Margaret Ct.);

VARIANCE to allow: 9'3" setback for patio columns when 10' is required and 4'11" setback for an AC unit in the reducible front yard when 9' is required.

Location: 902 W. Cardinal Drive (198-11-036)

File #: 2018-7675

Zoning: R-0 (Low Density Residential)

Applicant/Owner: M. Designs Architects, Alpheus Jessup/ Ray and Sinead Murphy

Environmental Review:

- Design Review and Variance for Air Conditioner: Categorically Exempt Class 1(e). Class 1 (e) (1) Categorical Exemptions, Section 15301(e) include additions to existing structures resulting in no more than a 50% increase in floor area.
- Use Permit (fence) Variance (patio): Categorical Exemption Class 3 relieves this project from California Environmental Quality Act (CEQA). Class 3 Categorical Exemptions includes construction of new accessory structures (patio) including fences (CEQA Section 15303 (e)).

Project Planner: Teresa Zarrin, (408) 730-7429,
tzarrin@sunnyvale.ca.gov

Chair Howard confirmed that there were no objections to hearing this item first to

accommodate the applicant's family.

Associate Planner Teresa Zarrin presented the staff report.

Commissioner Harrison confirmed with Associate Planner Zarrin that the requested variances are solely to accommodate the existing rear yard patio, fence and air conditioner and that the applicant is not expanding any of these elements.

Vice Chair Simons confirmed with Associate Planner Zarrin that City staff has recommended a hip roof on the second story. Vice Chair Simons commented on the need to provide architectural continuity within a neighborhood and still maintain differentiating architecture. Associate Planner Zarrin provided information about the unique standing-seam metal roof and consideration given to roof form with second story additions, especially given the single-story nature of the homes in this neighborhood.

Vice Chair Simons asked staff about a change to the garage door colors to deemphasize the garage and provide more balance to the house. Associate Planner Zarrin advised that staff can consider this change.

Commissioner Weiss commented on the potential increased sturdiness of a gable roof. Associate Planner Zarrin advised that both roof types are common and that it is unlikely that one type is more sturdy. Principal Planner Caruso advised that either roof would have to meet California Building Code.

Commissioner Weiss commented that the proposed project is not adjacent to the other two story homes and that it would not be necessary to have this roof blend with those homes. Associate Planner Zarrin advised that staff's intention was to minimize the massing and the proposed project's impact on the streetscape.

Chair Howard opened the Public Hearing.

Chip Jessup, representing M Designs Architects, presented information about the proposed project.

Ray Murphy, applicant, presented information about the proposed project.

Commissioner Harrison confirmed with Mr. Jessup that if gable ends were implemented on the second story the proposed project could still accommodate at

least two solar panels and a smaller skylight. Mr. Jessup provided additional information regarding the rationale for the gable ends.

Vice Chair Simons asked the applicant about modifying the garage so that the darker color is used for the trim. Mr. Jessup stated that this would be fine and provided information about garage alternatives.

Commissioner Rheume confirmed with Mr. Jessup that the darker garage color represents the fog glass. Commissioner Rheume confirmed with Mr. Jessup that they will replace the windows.

Chair Howard closed the Public Hearing.

MOTION: Commissioner Howe moved and Commissioner Rheume seconded the motion for Alternative 2 – Approve the Design Review, Use Permit and Variances with modified conditions –

The applicant can choose the roof type and their choice must meet Building Code requirements.

Commissioner Howe stated an opinion that this is a great project and commented on the impact of the letters submitted by the neighbors in support of their project. Commissioner Howe commented that the applicant has done a thorough job.

Commissioner Rheume stated that he can make the findings. Commissioner Rheume commented that the project has a nice design and will be a good investment in the City. Commissioner Rheume noted that the requested variances are due to existing conditions.

Vice Chair Simons stated that he will be supporting the motion and can make the findings. Vice Chair Simons noted that a glass garage door, potentially frameless, would be a good addition to the proposed architecture. Vice Chair Simons commented on the Silicon Valley modern architecture.

Commissioner Harrison noted her appreciation of the eight-foot plate heights for both stories in conjunction with the gable ends. Commissioner Harrison commented that this will allow for higher ceilings while keeping the plate heights aligned with the neighborhood. Commissioner Harrison stated that she will be supporting the motion.

Commissioner Olevson stated that he can make the findings for the Design Review and noted that the variances should be granted because they are for legal non-conforming items.

Commissioner Weiss stated that she will be supporting the motion and noted her appreciation that the applicant will have their choice of roof type. Commissioner Weiss commented that this design will make the house special and noted that there were no problems that she could identify with the fence, air conditioner unit or patio. Commissioner Weiss stated an opinion that there would be no privacy issues for the adjacent home due to the configuration of the extension.

The motion carried by the following vote:

Yes: 7 - Commissioner Weiss
Chair Howard
Commissioner Howe
Commissioner Olevson
Vice Chair Simons
Commissioner Rheume
Commissioner Harrison

No: 0

Principal Planner Caruso stated that this decision is final unless appealed to the City Council within 15 days or called up by the City Council within 15 days.

2. [18-0813](#) **Proposed Project: DESIGN REVIEW** to allow a Verizon wireless telecommunications facility on an existing utility pole located in the public right-of-way.
Location: In the public right-of-way on south side 1055 Dunford Way (APN 313-10-004) southwest of the intersection at Quail Avenue and Dunford Way and adjacent to the Raynor Park baseball diamond.
File #: 2018-7412
Zoning: PF (Public Facility)
Applicant / Owner: Verizon Wireless C/O The CBR Group / City of Sunnyvale
Environmental Review: Class 3 Categorical Exemption relieves this project from the California Environmental Quality Act (CEQA) provisions that includes installation of small new equipment and facilities in small structures (CEQA Guidelines Section 15303).
Project Planner: Cindy Hom, (408) 730-7411, chom@sunnyvale.ca.gov

Associate Planner Cindy Hom presented the staff report.

Vice Chair Simons confirmed with Associate Planner Hom that the applicant would be responsible for maintenance of the proposed landscaping. Vice Chair Simons noted his concerns and asked staff about the potential for the City to manage the landscaping. Principal Planner Gerri Caruso stated that the intent of the Condition of Approval (COA) is that the applicant would be responsible for landscaping maintenance and that staff can coordinate with the appropriate City department.

Chair Howard opened the Public Hearing.

Mark Peters, representing the CBR Group and Verizon Wireless (Verizon), presented images and information about the proposed project.

Daniel Ro, representing Hammett & Edison, presented information about the proposed project.

Vice Chair Simons asked the applicant if Verizon has an artwork program for their equipment cabinets. Mr. Peters noted his uncertainty about this possibility because the proposed equipment cabinet does not have solid panels.

Commissioner Weiss commented that per the COA all facilities and related equipment must be kept in good repair. Commissioner Weiss confirmed with Mr. Peters that the phone number listed on the caution sign would be the correct way to inform Verizon of vandalism or disrepair.

Commissioner Harrison asked the applicant how the antennae will be textured. Mr. Peters advised that the antennae will be painted to match the wood grain but not textured.

Commissioner Harrison confirmed with Mr. Peters that adjacent landscaping would not impact the equipment cabinet's passive cooling system. Commissioner Harrison asked the applicant about the landscaping installation and irrigation. Mr. Peters stated that Verizon is unable to utilize existing irrigation systems but that they would hire a contractor for the installation.

Commissioner Howe asked staff if the City has the authority to dictate that the applicant install and maintain landscaping. Senior Assistant City Attorney Rebecca

Moon stated an opinion that it would be reasonable to require Verizon to install the landscaping in coordination with the City but not to maintain it since Verizon does not have a landscaping program. Commissioner Howe confirmed with Principal Planner Caruso that the COA does not include approval by a City department. Commissioner Howe confirmed with Mr. Peters that they are amenable to coordinating with the appropriate City department.

Karen Eung, Sunnyvale resident, asked about the purpose of adding the antennae, the associated technology and the availability of use by other wireless carriers. Mr. Peters provided information about the proposed project and noted that this wireless telecommunications facility could be used by another carrier in the event of calling 911 when without network connectivity. Ms. Eung noted her concern regarding potential long term health hazards for her family.

Mr. Ro presented additional information about the proposed project.

Chair Howard commented that the Federal Communications Commission (FCC) has the final authority on the safety of telecommunications facilities.

Vice Chair Simons asked staff about modifying the landscaping COA. Principal Planner Caruso stated that it could be modified to include coordination with the appropriate City department, as feasible.

Commissioner Harrison asked staff about texturing the antennae. Associate Planner Teresa Zarrin stated that this has not been done before and may not provide additional visual mitigation.

Commissioner Harrison confirmed with Principal Planner Caruso that staff would work with the applicant and the appropriate City department to determine a landscaping solution, as feasible.

Chair Howard closed the Public Hearing.

MOTION: Commissioner Howe moved and Commissioner Simons seconded the motion for Alternative 2 – Approve the Design Review with modified conditions –

The appropriate City department will work with the applicant to ensure that sustainable landscaping is reasonably implemented, as feasible.

Commissioner Howe stated that he can make the findings. Commissioner Howe noted that the City does not have the final authority for telecommunications facilities and that the best course of available action is to install landscaping that can be maintained by the City.

FRIENDLY AMENDMENT: Vice Chair Simons offered a friendly amendment to specify that the antennae will be painted a deep earth color, as described by the applicant. Commissioner Howe accepted the friendly amendment.

Vice Chair Simons stated that it would be disappointing if the City did not maintain the landscaping that Verizon will install, since it would be an improvement to the proposed project. Vice Chair Simons highly recommended installation of the landscaping and noted that if an art program was available that would have been a good alternative since it does not require the same level of maintenance.

FRIENDLY AMENDMENT: Commissioner Harrison offered a friendly amendment to remove the “textured” antennae requirement as outlined in the COA. Commissioner Howe and Vice Chair Simons accepted the friendly amendment.

The motion carried by the following vote:

Yes: 7 - Commissioner Weiss
Chair Howard
Commissioner Howe
Commissioner Olevson
Vice Chair Simons
Commissioner Rheaume
Commissioner Harrison

No: 0

Principal Planner Caruso stated that this decision is final unless appealed to the City Council within 15 days or called up by the City Council within 15 days.

STANDING ITEM: CONSIDERATION OF POTENTIAL STUDY ISSUES

Commissioner Howe asked staff to provide a study issue form if such a document exists.

Commissioner Harrison asked staff about the review process for study issues

proposed by the Planning Commission. Administrative Aide Katherine Hall provided information about the upcoming study issue process. Commissioner Harrison commented that sometimes there is a misunderstanding with the intention of proposed study issues. Administrative Aide Hall advised that staff will try and provide rough drafts of the study issue papers to the Commissioners who proposed them.

NON-AGENDA ITEMS AND COMMENTS

-Commissioner Comments

Chair Howard noted that per their pediatrician flu shots are pertinent due to the high incidence of the flu.

-Staff Comments

Principal Planner Caruso provided information about the approval of the Civic Center Modernization Master Plan and certification of the Program Environmental Impact Report (EIR), as well as the granting of the appeal for the proposed project at 1441 Norman Drive by the City Council.

Commissioner Harrison noted that the Accessory Dwelling Unit (ADU) for 1441 Norman Drive was internal and commented on the discussion by the City Council of potentially adjusting the Floor Area Ratio (FAR) when an ADU is also being considered. Commissioner Harrison proposed a study issue to amend the FAR when an ADU is being considered, as an addition or new build, so that the FAR threshold for Planning Commission review is examined. Commissioner Harrison commented that other cities use a 10% differential to increase the FAR threshold from 45% to 55%.

Chair Howard noted that he may propose a study issue to examine modifying the residential zoning code to allow for more housing.

ADJOURNMENT

Chair Howard adjourned the meeting at 8:08 PM.